

Development Plan

Holdfast Bay Council

Consolidated – 26 November 2020

Please refer to the Holdfast Bay Council page at www.sa.gov.au/developmentplans to see any amendments not consolidated.



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Introduction

Section

Amendment Record Table

The following table is a record of authorised amendments and their consolidation dates for the Holdfast Bay Council Development Plan since its inception on 15 April 1999. Further information on authorised amendments prior to this date may be researched through the Planning and Land Use Services Division of the Attorney-General's Department or by viewing Gazette records.

| Consolidated | Amendment - [Gazette date] |
|-------------------|--|
| 15 April 1999 | City of Holdfast Bay - Consolidation and Public Notification PAR - [15 April 1999] |
| 26 August 1999 | Section 29(2) Amendment - [12 March 1998] Waste Disposal (Landfill) PAR (<i>Ministerial</i>) - [19 August 1999] |
| 10 February 2000 | Metropolitan Adelaide - Industrial Land and Development PAR (<i>Ministerial</i>) - [25 November 1999] Structure Plan and Policy PAR - [9 February 2000] Section 29(2) (a) Amendment - [10 February 2000] |
| Not consolidated | Telecommunications Facilities State-wide Policy Framework PAR Interim (<i>Ministerial</i>) - [31 August 2000] |
| 29 March 2001 | Metropolitan Adelaide Significant Tree Control PAR (<i>Ministerial</i>) - [21 December 2000] Local Heritage PAR - [29 March 2001] |
| 30 August 2001 | Telecommunications Facilities State-wide Policy Framework PAR (<i>Ministerial</i>) - [30 August 2001] |
| 19 December 2002 | Stormwater in Urban Areas (<i>Ministerial</i>) - [12 November 2002] |
| 25 September 2003 | Wind Farms PAR (<i>Ministerial</i>) - [24 July 2003] |
| 9 February 2006 | North Brighton Coastal PAR (<i>Ministerial</i>) (Interim) - [9 February 2006] |
| 2 March 2006 | Residential Policies and Miscellaneous Amendments PAR - [23 February 2006] |
| 30 March 2006 | Editorial Correction to Council Wide PDC 93 |
| 26 October 2006 | North Brighton Coastal PAR - [26 October 2006] |
| 10 January 2008 | Residential Parks and Caravan and Tourist Parks DPA (<i>Ministerial</i>) (Interim) - [13 December 2007] |
| 27 November 2008 | Buckle Street, Glenelg North and Surrounds DPA - [27 November 2009] |
| 11 December 2008 | Residential Parks and Caravan and Tourist Parks DPA (<i>Ministerial</i>) - [11 December 2008] |
| 26 April 2012 | Regulated Trees DPA (Interim) (<i>Ministerial</i>) - [17 November 2011] |
| 21 March 2013 | Regulated Trees DPA (Interim) (<i>Ministerial</i>) - [15 November 2012] Heritage and Character DPA (Interim) – [21 March 2013] Editorial correction to maps HoB/5 and HoB/15 |
| 13 February 2014 | Heritage and Character DPA – [13 February 2014] |
| 27 November 2014 | Better Development Plan (BDP) Conversion DPA – [27 November 2014] |
| 18 December 2014 | Brighton and Hove District Centre DPA – [18 December 2014] |
| 30 July 2015 | Minda Brighton Campus and General Section DPA (<i>Ministerial</i>) – [30 July 2015] |
| 26 November 2015 | Section 29(1) Amendment – [5 November 2015] Section 29(2)(b)(ii) Amendment – [26 November 2015] |
| 28 April 2016 | Existing Activity Centres Policy Review DPA (<i>Ministerial</i>) – [21 April 2016] |

| Consolidated | Amendment - [Gazette date] |
|---------------------|--|
| 2 June 2016 | Glenelg District Centre (Jetty Road) and Residential High Density Zone Review DPA (<i>Ministerial</i>) – [26 May 2016] |
| 26 November 2020 | Seacliff Park Residential and Centre DPA – [26 November 2020] |

Consolidated: The date of which an authorised amendment to a Development Plan was consolidated (incorporated into the published Development Plan) pursuant to section 31 of the *Development Act 1993*.

Gazetted: The date of which an authorised amendment was authorised through the publication of a notice in the Government Gazette pursuant to Part 3 of the *Development Act 1993*.

Introduction to the Development Plan

Welcome to the Development Plan for the Holdfast Bay Council.

This introduction has been prepared by the Attorney-General's Department as a guide to assist you in understanding this Development Plan.

For full details about your rights and responsibilities, you are advised to refer to the *Development Act 1993* and the associated *Development Regulations 2008* and/or consult your council.

A number of guides and additional information regarding South Australia's Planning and Development Assessment System are available via the website plan.sa.gov.au or by contacting the Planning and Land Use Services Division, Attorney-General's Department at Level 5, 50 Flinders Street, Adelaide, SA 5000.

Overview of the Planning System

South Australia has an integrated planning and development system, with three distinct but interrelated parts, these being:

- Legislation
- The Planning Strategy
- Development Plans.

The **legislative framework** establishing the planning and development system and setting out its statutory procedures is provided by the *Development Act 1993* and its associated *Development Regulations 2008*. The *Development Act* is the core legislation enacted by the South Australian Parliament to establish the planning and development system framework and many of the processes required to be followed within that framework (including processes for assessing development applications). The Regulations provide more details about the framework and are updated from time to time by the Governor (on the advice of the Minister for Planning).

The State Government's broad vision for sustainable land use and the built development of the state is outlined in the **Planning Strategy**. The relevant volumes of the Planning Strategy for this Development Plan is The 30-Year Plan for Greater Adelaide.

The Planning Strategy, which covers a full range of social, economic and environmental issues, informs and guides policies both across Government and in local area Development Plans. The Planning Strategy is required under section 22 of the *Development Act, 1993* and is updated by the State Government every few years. Local councils also prepare strategic plans which guide the same matters but at a local level. These strategic plans are not, however, development assessment tools, that is the role of Development Plans.

Development Plans are the key on-the-ground development assessment documents in South Australia. They contain the rules that set out what can be done on any piece of land across the state, and the detailed criteria against which development applications will be assessed. Development Plans cover distinct and separate geographic areas of the state. There is a separate Development Plan for each one of the 68 local council areas, plus a handful of other Development Plans covering areas not situated within local government boundaries. Development Plans outline what sort of developments and land use are and are not envisaged for particular zones (eg residential, commercial, industrial), and various objectives, principles and policies further controlling and affecting the design and other aspects of proposed developments.

What is Development?

'Development' is defined in Section 4 of the *Development Act 1993* as:

- a change in the use of land or buildings
- the creation of new allotments through land division (including Strata and Community Title division)
- building work (including construction, demolition, alteration and associated excavation/fill)

- cutting, damaging or felling of significant trees
- specific work in relation to State and Local heritage places
- prescribed mining operations
- other acts or activities in relation to land as declared by the Development Regulations.

No development can be undertaken without an appropriate **Development Approval** being obtained from the relevant authority after an application and assessment process.

How does the Development Plan relate to other legislation?

The Development Plan is a self-contained policy document prepared under and given statutory recognition pursuant to *the Development Act 1993*.

It is generally independent of other legislation but is one of many mechanisms that control or manage the way that land and buildings are used.

The *Development Act, 1993* and *Development Regulations, 2008* contain a number of provisions to ensure that development applications are referred to other government agencies when appropriate.

What doesn't a Development Plan do?

Development Plans are applicable only when new development is being designed or assessed. They do not affect existing development (see above for a description of what constitutes 'development').

Once a Development Approval is issued, the details contained within the application and any conditions attached to that approval are binding.

Development Plan policies guide the point in time assessment of a development application but do not generally seek to control the on-going management of land, which is the role of other legislation (eg the *Environment Protection Act 1993*, *Natural Resources Management Act 2004*, *Liquor Licensing Act 1997*).

When do you use the Development Plan?

The Development Plan should be used during a development application process. This may include:

- when undertaking or proposing to undertake 'development' (eg building a house or factory or converting an office into a shop)
- when assessing or determining a development proposal (eg by council staff, a Council or Regional Development Assessment Panel or the Development Assessment Commission)
- when you believe you could be affected by a proposed development and you are given an opportunity to comment on it as part of the assessment process.

How to read the Development Plan

Development Plans are comprised of several sections as described below.

All sections and **all** relevant provisions within each section of the Development Plan must be considered in relation to a development proposal or application.

Development Plans use three text font colours:

- (a) Black text is used to identify all standard policy that forms the basis of all council Development Plans.
- (b) Green text is used to identify additional council-specific policy or variables that have been included in the Development Plan to reflect local circumstances.
- (c) Blue text illustrates hyperlinks to maps, overlays and tables in the Development Plan. These hyperlinks are operational only when viewing electronic versions of the Development Plan.

Development Plan Structure Overview

| Advisory Section | Function |
|---|--|
| Table of Contents | Navigational aid to reference sections within the Development Plan by name and page number. |
| Amendment Record Table | Tabled information recording previously-authorised Development Plan amendments and their consolidation dates. |
| Introduction Overview of the Planning System What is Development? How does the Development Plan relate to other legislation? What doesn't a Development Plan do? When do you use the Development Plan? How to read the Development Plan? | A general overview of the context, purpose and way a Development Plan is set out (this section is advisory only and not used for development assessment purposes). |
| Strategic Setting State Strategic Setting <i>(30-Year Plan for Greater Adelaide/Regional Planning Strategy)</i> Council Strategic Setting <i>(Council Strategy)</i> | To be developed, but intended to reflect the relevant Planning Strategy (as it relates to the council area) and council's own local strategic investigations. |
| Council Preface Map | Map of the entire Development Plan boundary and its spatial relationship to other Development Plans' boundaries. |
| Assessment Section | Function |
| General Section Provisions Objectives Principles of Development Control | <p>These policies apply across the whole council area and relate to a range of social, environmental, and economic development issues such as:</p> <ul style="list-style-type: none"> ▪ site and design criteria ▪ access and vehicle parking requirements ▪ heritage and conservation measures ▪ environmental issues ▪ hazards ▪ infrastructure requirements ▪ land use specific requirements. <p>They establish the development standards that apply to all forms of development and provide a yardstick against which the suitability of development proposals is measured.</p> <p>Many policies include a number of subclauses. All subclauses should be met, unless otherwise stated.</p> |
| Overlay Section Provisions | These policies are spatially located through the application of specific overlay mapping where there is a envisaged land use outcome or application of appropriate design requirements determined by the locality of the proposed development. |

| Assessment Section | Function |
|-----------------------------------|---|
| Zone Section Provisions | <p>These policies give greater certainty and direction about where certain forms of developments should be located. Maps are referenced within zones that show where land uses are suitable to be located.</p> <p>Generally, envisaged forms of development within a zone are identified and encouraged through carefully worded policies.</p> |
| Desired Character Statements | These express a vision about how the zone should look and feel in the future. They may describe the valued elements of the neighbourhood or area to be retained and/or what level and nature of change is desired. |
| Objectives | These are the specific planning policies that determine what land uses are encouraged or discouraged in the zone. They often contain detailed provisions to further guide the scale and design of development. |
| Principles of Development Control | These also provide lists of complying and non-complying development and any public notification provisions that vary from those in the Development Regulations. |
| Policy Area | Policy areas apply to a portion of a zone and contain additional objectives, desired character statements and principles of development control for that portion. |
| Precincts | <p>Precincts are used to express policies for a small sub-area of a zone or a policy area.</p> <p>Precincts are used if additional site-specific principles of development control are needed to reflect particular circumstances associated with those sub-areas.</p> |
| Procedural Matters | <p>All zones have a procedural matters section that identifies and lists complying, non-complying and public notification categories for various forms of development.</p> <p>Policy areas and/or precincts, which are a sub-set of the zone, share this procedural matters section. Their respective lists can be modified to accommodate policy area and precinct variations.</p> |
| Table Section Provisions | <p>These tables provide detailed data for the assessment of certain elements of development, for example, numeric values for setbacks from road boundaries and car parking rates for certain types of development.</p> <p>Conditions for complying development are grouped into their respective tables.</p> |
| Mapping Section | |
| Structure Plan Maps | Structure Plan maps will commonly show the general arrangement and broad distribution of land uses; key spatial elements; and movement patterns throughout the council area and major urban areas. |
| Council Index Maps | <p>This is the first point of reference when determining the appropriate map(s) applying to a specific property.</p> <p>An enlargement index map may be included where needed, eg for large townships.</p> |

| Assessment Section | Function |
|---|--|
| Extent Map Series Location Maps | <p>Individual overlay and spatial-based maps (based on the Council Index Maps) originate from a single Location Map and ‘drill down’ through relevant extent maps affecting that location.</p> <p><i>Note: the entire council area will always be represented as the first map in the extent map series and will commence as Map 1.</i></p> |
| Overlay Maps | <p>Used to show issue areas or features that run across a number of zones, and are spatially defined to a cadastre, for example:</p> <ul style="list-style-type: none"> ▪ Transport ▪ Development Constraints ▪ Heritage ▪ Heritage ▪ Natural Resources ▪ Affordable Housing ▪ Noise and Air Emissions ▪ Strategic Transport Routes. <p><i>Note: issues that are not spatially defined to a cadastre can appear in this section; however they will be presented as illustrative maps only.</i></p> |
| Zone Maps | Used to determine which zone applies to which land. |
| Policy Area Maps | Used to depict the presence and location of any applicable policy area. |
| Precinct Maps | Used to depict the presence and location of any applicable precincts. |
| Bushfire Maps <i>(where applicable)</i> Bushfire Protection Area BPA Maps - Bushfire Risk | Bushfire Protection Area - BPA Maps are used to determine the potential bushfire risk (high, medium or general), associated with an allotment located within an area prone to bushfires. |
| Concept Plan Maps | <p>Concept Plans are used to depict graphically key features and conceptual layouts of how specific areas should be developed.</p> <p>Concept Plans appear at the end of the extent map series as a separate section. Concept Plans are consecutively numbered, commencing with number 1.</p> |

Further info

Contact the [City of Holdfast Bay](#).

Visit the Planning and Land Use Services website: www.plan.sa.gov.au.

Discuss your matter with your planning consultant.

Council Strategic Setting

The City of Holdfast Bay extends along 9 kilometres of premier seaside. It is located 11 kilometres south-west of the city of Adelaide and covers an area of 1340 hectares, with no part of the Council area more than 2.5 kilometres from the foreshore. Proclaimed in 1997, following the amalgamation of the former Glenelg and Brighton Councils, the area contains the suburbs of Glenelg, Glenelg North (part), Glenelg East, Glenelg South, Somerton Park, North Brighton, Hove, Brighton, South Brighton, Seacliff, Seacliff Park (part) and Kingston Park.

The Council area is rich in cultural history and Council respects the significance of the Tjilbruke Dreaming Trail, and in particular, the Tjilbruke Springs site at Kingston Park. Glenelg also played an important role in European settlement on the South Australian mainland, with Governor John Hindmarsh proclaiming the Province of South Australia in Glenelg on 28 December 1836.

Council values its significant heritage as South Australia's 'birthplace' and as an early focus for coastal tourism at Glenelg and Brighton and for operation of early train and tram services. In this context, the Development Plan includes a number of Historic Conservation Areas and designates many Local Heritage Places.

Holdfast Bay's seaside location has historically attracted a range of large institutions, including Townsend House, Minda Incorporated and Kate Cocks Memorial Babies Home.

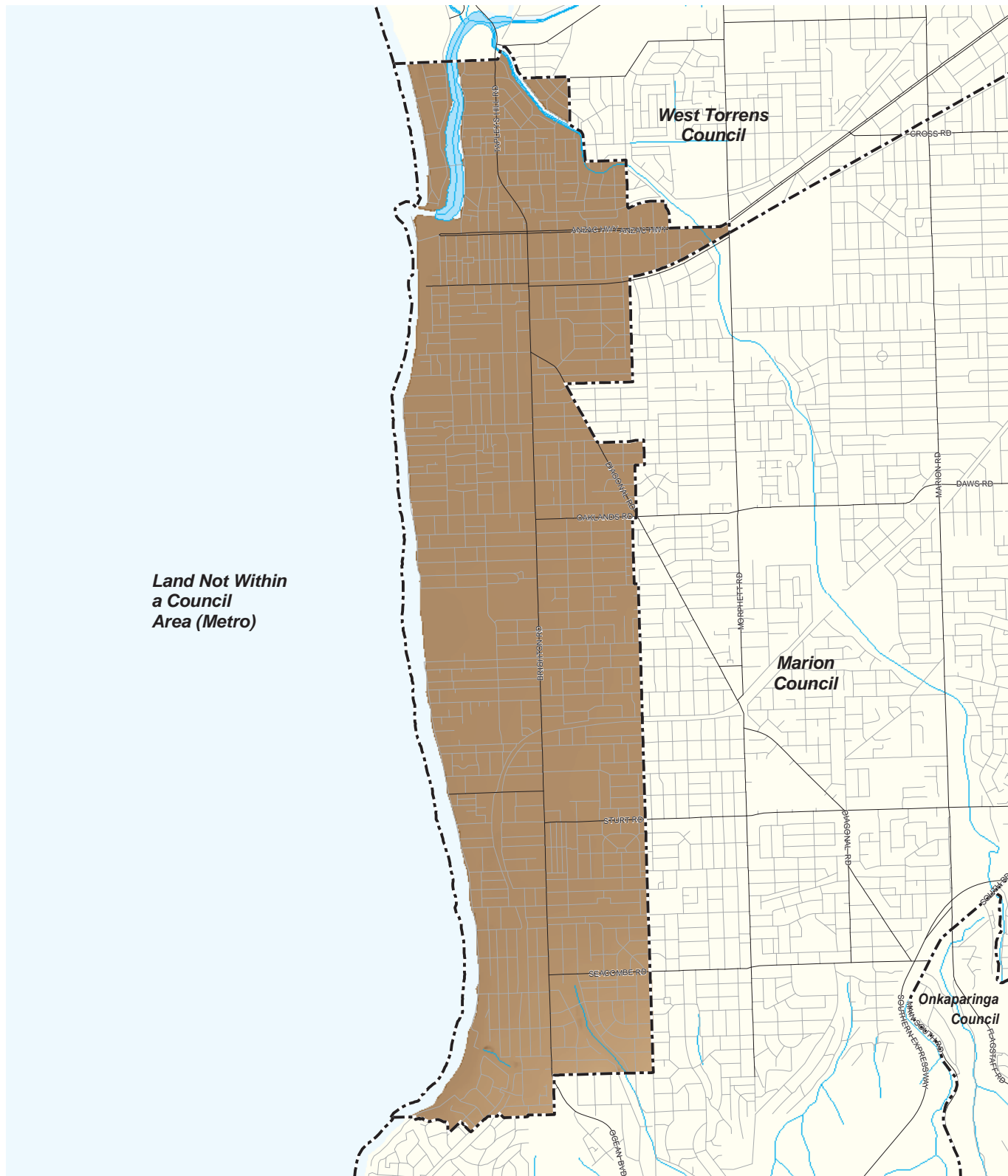
Glenelg acts as the City's premier tourist destination and primary retail precinct, being part of one of two district centre zones within the Council area. It will also be a focus for future transit oriented development along with areas surrounding the Glenelg tram, Anzac Highway and the Seaford train corridors. It is supported by the district centre at Brighton and Hove, within which further revitalisation of the small to medium scale retail and commercial facilities are envisaged to occur in the future. These two centres, with a small light industrial precinct in Somerton Park and a number of thriving smaller centres throughout the City, support the predominantly residential suburbs within (and around) the Council area.

The Council is committed to building a strong community, creating a sustainable environment, delivering economic prosperity, and enhancing city design and function along with investments in bike paths and delivery of the shared-use metropolitan Coast Park through the Council area. Council also recognises the changing needs of the City's ageing community and will progressively review and adapt its Development Plan policy to meet these changing needs. Development Plan zoning protects recreational and community spaces and the efficient delivery of services within these spaces is proposed through the development of strategically located community hubs.

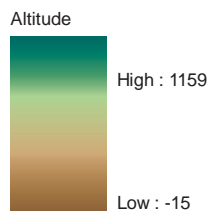
The Development Plan provides for employment opportunities through industrial, commercial and activity centre zones, with mixed-use development encouraged along Anzac Highway in Glenelg, providing opportunities for medium density and affordable housing development (in addition to other areas along Tapleys Hill Road, Brighton Road, Jetty Road Brighton, and close to the Hove and Brighton railway stations).

The City provides for a wide diversity of housing forms ranging from single storey low density dwellings through to low-rise medium density forms including row and group dwellings, to high density residential flat buildings up to 12 storeys in height. Relatively high land values within the Council area will, however, present challenges to delivery of affordable housing into the future.

At a broader level, the city is influenced and affected by Marion Regional Centre (and future transit oriented development), retail and commercial development at the Adelaide Airport site (and future bulky goods and specialist shopping) as well as its commercial industries, the Glenelg Wastewater Treatment Plant, the Linwood quarry at Marino, and links to the centre of Adelaide.



To identify the precise location of the Development Plan boundary refer to Council Index Map then select the relevant map number.



Council Preface Map

General Section

Advertisements

OBJECTIVES

- 1 Urban landscapes that are not disfigured by advertisements and/or advertising hoardings.
- 2 Advertisements and/or advertising hoardings that do not create a hazard.
- 3 Advertisements and/or advertising hoardings designed to enhance the appearance of the building and locality.
- 4 Outdoor advertising displays that are designed to provide clearly visible property and business identification without dominating the appearance of the site upon which it is located or the streetscape.
- 5 Outdoor advertising displays that are specifically designed to have an overall co-ordinated appearance with all other advertisements associated with the building or site.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 The location, siting, design, materials, size, and shape of advertisements and/or advertising hoardings should be:
 - (a) consistent with the predominant character of the urban or rural landscape
 - (b) in harmony with any buildings or sites of historic significance or heritage value in the area
 - (c) coordinated with and complement the architectural form and design of the building they are to be located on.
- 2 The number of advertisements and/or advertising hoardings associated with a development should be minimised to avoid:
 - (a) clutter
 - (b) disorder
 - (c) untidiness of buildings and their surrounds
 - (d) driver distraction.
- 3 Buildings occupied by a number of tenants should exhibit coordinated and complementary advertisements and/or advertising hoardings to identify the tenants and their type of business.
- 4 The content of advertisements should be limited to information relating to the legitimate use of the associated land.
- 5 Advertisements and/or advertising hoardings should:
 - (a) be completely contained within the boundaries of the subject allotment
 - (b) be sited to avoid damage to, or pruning or lopping of, on-site landscaping or street trees
 - (c) not obscure views to vistas or objects of high amenity value.

- 6 Advertisements and/or advertising hoardings should not be erected on:
 - (a) a public footpath or veranda post
 - (b) a road, median strip or traffic island
 - (c) a vehicle adapted and exhibited primarily as an advertisement
 - (d) residential land.
- 7 Advertisements and/or advertising hoardings attached to buildings should not be sited on the roof or higher than the walls of a building, the advertisement or advertising hoarding is appropriately designed to form an integrated and complementary extension of the existing building.
- 8 Advertisements and/or advertising hoardings erected on a veranda or that project from a building wall should:
 - (a) have a **minimum** clearance of **2.5 metres** over a footway to allow for safe and convenient pedestrian access
 - (b) where erected on the side of a veranda, not exceed the width of the veranda or project from the veranda
 - (c) where erected on the front of a veranda, not exceed the length of the veranda or project from the veranda
 - (d) where projecting from a wall, have the edge of the advertisement or advertising hoarding abutting the surface of the wall.
- 9 Advertisements should be designed to conceal their supporting advertising hoarding from view.
- 10 Advertisements should convey the owner/occupier and/or generic type of business, merchandise or services using simple, clear and concise language, symbols, print style and layout and a small number of colours.
- 11 Advertisements which perform a secondary role in identifying the business, goods or services should only be readable in the immediate vicinity of the site.

Safety

- 12 Advertisements and/or advertising hoardings should not create a hazard by:
 - (a) being so highly illuminated as to cause discomfort to an approaching driver, or to create difficulty in the driver's perception of the road or persons or objects on the road
 - (b) being liable to interpretation by drivers as an official traffic sign, or convey to drivers information that might be confused with instructions given by traffic signals or other control devices, or impair the conspicuous nature of traffic signs or signals
 - (c) distracting drivers from the primary driving task at a location especially where the demands on driver concentration are high
 - (d) obscuring a driver's view of other road or rail vehicles at/or approaching level crossings, or of pedestrians or of features of the road that are potentially hazardous (eg junctions, bends, changes in width, traffic control devices).
- 13 Any internally illuminated advertising signs and/or advertising hoardings which utilise LED, LCD or other similar technologies should be located a minimum of 80 metres from traffic signals, level crossings and other important traffic control devices.

Freestanding Advertisements

- 14 Freestanding advertisements and/or advertising hoardings should be:
- (a) limited to only one primary advertisement per site or complex, **except where a site has multiple road frontages, in which case, a maximum of one freestanding advertising hoarding per road frontage**
 - (b) of a scale and size in keeping with the desired character of the locality and compatible with the development on the site.
- 15 Freestanding advertisements and/or advertising hoardings for multiple-business tenancy complexes should:
- (a) incorporate the name or nature of each business or activity within the site or complex in a single advertisement
 - (b) be integrally designed and mounted below the more predominant main complex or site identity advertisement.
- 16 Portable, easel or A-frame advertisements should be displayed only where:
- (a) no other appropriate opportunity exists for an adequate coordinated and permanently erected advertisement and/or advertising hoarding
 - (b) they do not obstruct or compromise the safety of pedestrians or vehicle movement
 - (c) there is no unnecessary duplication or proliferation of advertising information
 - (d) there is no damage to, or removal of, any landscaping on the site.
- 17 Portable, easel or A-frame advertisements associated with a development should be displayed only during the hours the development is open for trading.

Flags, Bunting, Streamers and Suspended Objects (including Blimps and Balloons)

- 18 Advertisements and/or advertising hoardings incorporating any flags, bunting, streamers, or suspended objects should be placed or arranged to complement and accord with the scale of the associated development and not be:
- (a) displayed in residential areas
 - (b) located in areas where they are likely to detrimentally affect the amenity of the locality
 - (c) positioned higher than the building they are attached or related to (other than in the case of flags).

Advertising along Arterial Roads

- 19 Advertising and/or advertising hoardings should not be placed along arterial roads that have a speed limit of 80 km/h or more.

Animal Keeping

OBJECTIVES

- 1 Animals not kept at a density beyond the carrying capacity of the land or water.
- 2 Animal keeping development sited and designed to avoid adverse effects on surrounding development.
- 3 Intensive animal keeping protected from encroachment by incompatible development.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Animal keeping and associated activities should not create adverse impacts on the environment or the amenity of the locality.
- 2 Storage facilities for manure, used litter and other wastes should be designed and sited:
 - (a) to be vermin proof
 - (b) with an impervious base
 - (c) to ensure that all clean rainfall runoff is excluded from the storage area
 - (d) outside the 1-in-100 year average return interval flood event area.

Horse Keeping

- 3 Stables, horse shelters or associated yards should be sited:
 - (a) at least 50 metres from a watercourse
 - (b) on land with a slope no greater than 1-in-10.
- 4 A concrete drainage apron should be provided along the front of stables directing water from wash-down areas onto a suitably vegetated area that can absorb all the water, or into a constructed drainage pit.
- 5 Stables, horse shelters or associated yards should be sited at least 30 metres from any dwelling on the site and from the nearest allotment boundary to avoid adverse impacts from dust, erosion and odour.
- 6 All areas accessible to horses should be separated from septic tank drainage areas.

Dairies

- 7 Dairies and associated wastewater lagoons and liquid/solid waste storage and disposal areas should be located at a distance from nearby dwellings, public roads and outside the 1-in-100 year average return interval flood event area of any watercourse to avoid adverse impacts or nuisance by noise, smell or pollution on nearby sensitive receptors such as dwellings.
- 8 Dairies should include a lagoon for the storage or treatment of milking shed effluent which should be located:
 - (a) at least 20 metres from a public road
 - (b) at least 200 metres from any dwelling not located on the land

- (c) outside any 1-in-100 year average return interval flood event area of any watercourse.

Intensive Animal Keeping

- 9 Intensive animal keeping operations and their associated components, including holding yards, temporary feeding areas, movement lanes and similar, should not be located on land within any of the following areas:
 - (a) 800 metres of a public water supply reservoir
 - (b) the 1-in-100 year average return interval flood event area of any watercourse
 - (c) 200 metres of a major watercourse (third order or higher stream)
 - (d) 100 metres of any other watercourse, bore or well used for domestic or stock water supplies
 - (e) 2000 metres of a defined and zoned township, settlement or urban area (except for land based aquaculture)
 - (f) 500 metres of a dwelling (except for a dwelling directly associated with the intensive animal keeping facility).
- 10 Intensive animal keeping operations in uncovered situations should incorporate:
 - (a) a controlled drainage system which:
 - (i) diverts runoff from external areas, and
 - (ii) directs surface runoff into an effluent management system that has sufficient capacity to hold run off from the controlled drainage area
 - (b) pen floors which:
 - (i) ensure that effluent does not infiltrate and contaminate groundwater or soil, and
 - (ii) are graded to a consistent uniform slope of between 2 per cent and 6 per cent
 - (c) effluent drainage into an effluent lagoon(s) that has sufficient capacity to hold runoff from the controlled drainage area.
- 11 Intensive animal keeping facilities and associated wastewater lagoons and liquid/solid waste disposal areas should be sited, designed, constructed and managed to avoid adverse odour impacts on nearby sensitive land uses.

Kennels

- 12 The floor of kennels should be constructed of concrete or similar impervious material and be designed to allow for adequate drainage when kennels are cleaned.
- 13 Kennels and exercise yards should be designed and sited to minimise noise nuisance to neighbours through:
 - (a) orienting their openings away from sensitive land uses such as dwellings
 - (b) siting them as far as practicable from allotment boundaries.
- 14 Kennels should occur only where there is a permanently occupied dwelling on the land.

Land Based Aquaculture

- 15 Land-based aquaculture and associated components should not be located on land within 500 metres of a defined and zoned township, settlement or urban area.
- 16 Land-based aquaculture ponds should be sited and designed to:
 - (a) prevent surface flows from entering the ponds in a 1-in-100 year average return interval flood event
 - (b) prevent pond leakage that would pollute groundwater
 - (c) prevent the farmed species escaping and entering into any waters
 - (d) minimise the need for intake and discharge pipes to traverse sensitive environments.
- 17 Buildings associated with land-based aquaculture should provide enclosed storage areas to accommodate all equipment associated with aquaculture operations in a manner which is integrated with the use of the land.
- 18 Development should ensure that pipe inlet and outlets associated with land-based aquaculture are located to minimise the risk of disease transmission.

Marine Based Aquaculture

- 19 Marine aquaculture and other offshore development should be ecologically sustainable and be located, designed, constructed and managed to:
 - (a) minimise adverse impacts on marine habitats and ecosystems, and public access to beaches, public watercourses or the foreshore
 - (b) take into account the requirements of traditional indigenous and commercial fishing grounds
 - (c) ensure satisfactory removal and disposal of litter, disused material, debris, detritus and dead animals from the development
 - (d) prevent the build up of waste.
- 20 In marine waters, marine aquaculture (other than inter tidal aquaculture) and other offshore development should be located a minimum of 100 metres seaward of the high-water mark.
- 21 Marine aquaculture development should not significantly obstruct or adversely affect any of the following:
 - (a) areas of high public use
 - (b) areas established for recreational activities
 - (c) areas of outstanding visual, environmental, commercial or tourism value
 - (d) sites, including beaches, used for recreational activities such as swimming, fishing, skiing, sailing and other water sports.
- 22 Marine aquaculture should be sited, designed, constructed and managed to minimise interference and obstruction to the natural processes of the coastal and marine environment.
- 23 Marine aquaculture should be developed in areas where an adequate water current exists to disperse sediments and be sited a sufficient height above the sea floor to:
 - (a) prevent the fouling of waters, publicly owned wetlands or the nearby coastline

- (b) minimise seabed damage.
- 24 Racks, floats and other farm structures associated with marine aquaculture or other offshore development should where practicable be visually unobtrusive from the shoreline.
- 25 Marine aquaculture development should:
 - (a) use feed hoppers that are painted in subdued colours and suspended as low as possible above the water
 - (b) position structures to protrude the minimum distance practicable above water
 - (c) avoid the use of shelters and structures above cages and platforms unless necessary to exclude predators and protected species from interacting with the farming structures and/or stock inside the cages, or for safety reasons.
- 26 Marine aquaculture should be developed to maintain existing rights of way within or adjacent to a site.
- 27 Marine aquaculture access, launching and maintenance facilities should:
 - (a) where possible, use existing and established roads, tracks, ramps and paths to or from the sea
 - (b) be developed cooperatively and co-located.
- 28 Marine aquaculture and other offshore development should be located at least:
 - (a) 550 metres from a proclaimed shipwreck
 - (b) 1000 metres seaward from the boundary of any reserve under the *National Parks and Wildlife Act 1972*, unless a lesser distance is agreed with the Minister responsible for that *Act*.
- 29 Marine aquaculture development should be located so as not to obstruct nor interfere with navigation channels, access channels, frequently used natural launching sites, safe anchorage areas, known diving areas, commercial shipping lanes or activities associated with existing jetties and wharves.
- 30 Marine aquaculture development should contribute to navigational safety by being:
 - (a) suitably marked for navigational purposes
 - (b) sited to allow an adequate distance between farms for safe navigation
 - (c) located at least 250 metres from a commercial shipping lane
 - (d) comprised of structures that are secured and/or weighted to prevent drifting from the licensed site
 - (e) able to be rehabilitated when no longer operational.

Building near Airfields

OBJECTIVES

- 1 Development that ensures the long-term operational, safety, commercial and military aviation requirements of airfields (airports, airstrips and helicopter landing sites) continue to be met.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 The height and location of buildings and structures should not adversely affect the long-term operational, safety, commercial and military aviation requirements of airfields.
- 2 Buildings and structures that exceed the airport building heights as shown on the *Overlay Maps - Development Constraints* and that *penetrate the obstacle limitation surfaces (OLS)* should not be developed unless a safety analysis determines that the building/structure does not pose a hazard to aircraft operations.
- 3 Development in the vicinity of airfields should not create a risk to public safety, in particular through any of the following:
 - (a) lighting glare
 - (b) smoke, dust and exhaust emissions
 - (c) air turbulence
 - (d) storage of flammable liquids
 - (e) attraction of birds
 - (f) reflective surfaces (eg roofs of buildings, large windows)
 - (g) materials that affect aircraft navigational aids.
- 4 Outdoor lighting within 6 kilometres of an airport should be designed so that it does not pose a hazard to aircraft operations.
- 5 Development that is likely to increase the attraction of birds should not be located within 3 kilometres of an airport used by commercial aircraft. If located closer than 3 kilometres the facility should incorporate bird control measures to minimise the risk of bird strikes to aircraft.
- 6 Development within areas affected by aircraft noise should be consistent with *Australian Standard AS2021 - Acoustics - Aircraft Noise Intrusion - Building Siting and Construction*.

Centres and Retail Development

OBJECTIVES

- 1 Shopping, administrative, cultural, community, entertainment, educational, religious and recreational facilities located in integrated centres.
- 2 Centres that ensure rational, economic and convenient provision of goods and services and provide:
 - (a) a focus for community life
 - (b) safe, permeable, pleasant and accessible walking and cycling networks.
- 3 The provision of a safe pedestrian environment within centres which gives high priority to pedestrians, public and community transport.
- 4 Increased vitality and activity in centres through the introduction and integration of housing.
- 5 Centres developed in accordance with a hierarchy based on function, so that each type of centre provides a proportion of the total requirement of goods and services commensurate with its role.
- 6 Development of centres outside of Greater Adelaide in accordance with the following hierarchy:
 - (a) Regional Centre
 - (b) District Centre
 - (c) Town Centre (for smaller towns with a single centre zone)
 - (d) Local Centre (subsidiary centres for towns with a regional or district centre).
- 7 The central business district of the City of Adelaide providing the principal focus for the economic, social and political life of Greater Adelaide and the State.
- 8 Vibrant multi-purpose centres at Glenelg, Brighton and Seacliff.
- 9 Retention and re-enforcement of the Glenelg and Brighton District Centres as the principal foci of retail and civic administration.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development within centres should:
 - (a) integrate facilities within the centre
 - (b) allow for the multiple use of facilities and the sharing of utility spaces
 - (c) allow for the staging of development within the centre
 - (d) be integrated with public and community transport
 - (e) be of a scale and nature consistent with their location and role within the centre hierarchy.

- 2 Development within centres should be designed to be compatible with adjoining areas. This should be promoted through landscaping, screen walls, centre orientation, location of access ways, buffer strips and transitional use areas.
- 3 Development within centres should provide:
 - (a) public spaces such as malls, plazas and courtyards
 - (b) street furniture, including lighting, signs, litter bins, seats and bollards, that is sited and designed to complement the desired character
 - (c) unobtrusive facilities for the storage and removal of waste materials
 - (d) public facilities including toilets, infant changing facilities for parents, seating, litter bins, telephones and community information boards
 - (e) access for public and community transport and sheltered waiting areas for passengers
 - (f) lighting for pedestrian paths, buildings and associated areas
 - (g) a single landscaping theme
 - (h) safe and secure bicycle parking.
- 4 A single architectural theme should be established within centres through:
 - (a) constructing additions or other buildings in a style complementary to the existing shopping complex
 - (b) renovating the existing shopping complex to complement new additions and other buildings within the centre
 - (c) employing a signage theme.
- 5 The design of undercroft or semi-basement car parking areas should not detract from the visual quality and amenity of adjacent pedestrian paths, streets or public spaces.
- 6 Undercroft or semi-basement car parking areas should not project above natural or finished ground level by more than 1 metre.

Arterial Roads

- 7 Centres should develop on one side of an arterial road or in one quadrant of an arterial road intersection.
- 8 Centre development straddling an arterial road should:
 - (a) concentrate on one side of the arterial road or one quadrant of the arterial road intersection
 - (b) minimise the need for pedestrian and vehicular movement from one part of the centre to another across the arterial road.
- 9 Development should not generate pedestrian or vehicular traffic onto or across an arterial road in such a way as to substantially impair the movement of traffic on that road or to cause safety hazards.

Retail Development

- 10 A shop or group of shops with a gross leasable area of greater than 250 square metres should be located within a centre zone or other recognised activity centre.

- 11 A shop or group of shops with a gross leaseable area of less than 250 square metres should not be located on arterial roads unless within a centre zone.
- 12 A shop or group of shops located outside of zones that allow for retail development should:
 - (a) be of a size and type that will not hinder the development, function or viability of any centre zone
 - (b) not demonstrably lead to the physical deterioration of any designated centre
 - (c) be developed taking into consideration its effect on adjacent development.
- 13 Bulky goods outlets located within centres zones should:
 - (a) complement the overall provision of facilities
 - (b) be sited towards the periphery of those centres where the bulky goods outlet has a gross leaseable area 500 square metres or more.

Coastal Areas

OBJECTIVES

- 1 The protection and enhancement of the natural coastal environment, including environmentally important features of coastal areas such as mangroves, wetlands, sand dunes, cliff-tops, native vegetation, wildlife habitat shore and estuarine areas.
- 2 Protection of the physical and economic resources of the coast from inappropriate development.
- 3 Preservation of areas of high landscape and amenity value including stands of vegetation, shores, exposed cliffs, headlands, islands and hill tops, and areas which form an attractive background to urban and tourist areas.
- 4 Development that maintains and/or enhances public access to coastal areas with minimal impact on the environment and amenity.
- 5 Development only undertaken on land which is not subject to or that can be protected from coastal hazards including inundation by storm tides or combined storm tides and stormwater, coastal erosion or sand drift, and probable sea level rise.
- 6 Development that can accommodate anticipated changes in sea level due to natural subsidence and probable climate change during the first 100 years of the development.
- 7 Development which will not require, now or in the future, public expenditure on protection of the development or the environment.
- 8 Management of development in coastal areas to sustain or enhance the remaining natural coastal environment.
- 9 Low intensity recreational uses located where environmental impacts on the coast will be minimal.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should be compatible with the coastal environment in terms of built-form, appearance and landscaping including the use of walls and low pitched roofs of non-reflective texture and natural earth colours.

Environmental Protection

- 2 The coast should be protected from development that would adversely affect the marine and onshore coastal environment, whether by pollution, erosion, damage or depletion of physical or biological resources, interference with natural coastal processes or any other means.
- 3 Development should not be located in delicate or environmentally-sensitive coastal features such as sand dunes, cliff-tops, wetlands or substantially intact strata of native vegetation.
- 4 Development should not be undertaken where it will create or aggravate coastal erosion, or where it will require coast protection works which cause or aggravate coastal erosion.
- 5 Development should be designed so that solid/fluid wastes and stormwater runoff is disposed of in a manner that will not cause pollution or other detrimental impacts on the marine and on-shore environment of coastal areas.

- 6 Effluent disposal systems incorporating soakage trenches or similar should prevent effluent migration onto the inter-tidal zone and be sited at least 100 metres from whichever of the following requires the greater distance:
 - (a) the mean high-water mark at spring tide, adjusted for any subsidence for the first 50 years of development plus a sea level rise of 1 metre
 - (b) the nearest boundary of any erosion buffer determined in accordance with the relevant provisions in this Development Plan.
- 7 Unavoidable stormwater and effluent outfalls should be designed and located to minimise the impact on the environment and if discharging across a beach, do so at beach level and be comprised of suitably constructed pipes or channels.
- 8 Development that proposes to include or create confined coastal waters, as well as water subject to the ebb and flow of the tide should be designed to ensure the quality of such waters is maintained at an acceptable level.
- 9 Development should be designed and sited so that it does not prevent natural landform and ecological adjustment to changing climatic conditions and sea levels and should allow for the following:
 - (a) the unrestricted landward migration of coastal wetlands
 - (b) new areas to be colonised by mangroves, samphire and wetland species
 - (c) sand dune drift
 - (d) where appropriate, the removal of embankments that interfere with the abovementioned processes.

Maintenance of Public Access

- 10 Development should maintain or enhance public access to and along the foreshore.
- 11 Development should provide for a public thoroughfare between the development and any coastal reserve.
- 12 Other than small-scale infill development in a predominantly urban zone, development adjacent to the coast should not be undertaken unless it incorporates an existing or proposed public reserve, not including a road or erosion buffer, of at least 50 metres width between the development and the landward toe of the frontal dune or the top edge of an escarpment.
- 13 If an existing reserve is less than 50 metres wide, the development should incorporate an appropriate width of reserve to achieve a total 50 metres wide reserve.
- 14 Except where otherwise specified in a particular zone or policy area, buildings on land abutting coastal reserves should be set back either a distance of 8 metres from any boundary with the reserve or in line with adjacent development, whichever is the greater distance.
- 15 Development that abuts or includes a coastal reserve should be sited and designed to be compatible with the purpose, management and amenity of the reserve, as well as to prevent inappropriate access to the reserve.
- 16 Development, including marinas and aquaculture, should be located and designed to ensure convenient public access along the waterfront to beaches and coastal reserves is maintained, and where possible enhanced through the provision of one or more of the following:
 - (a) pedestrian pathways and recreation trails
 - (b) coastal reserves and lookouts

- (c) recreational use of the water and waterfront
 - (d) safe public boating facilities at selected locations
 - (e) vehicular access to points near beaches and points of interest
 - (f) car parking.
- 17 Where a development such as a marina creates new areas of waterfront, provision should be made for public access to, and recreational use of, the waterfront and the water.
- 18 Public access through sensitive coastal landforms, particularly sand dunes, wetlands and cliff faces, should be restricted to defined pedestrian paths constructed to minimise adverse environmental impact.
- 19 Access roads to the coast and lookouts should preferably be spur roads rather than through routes, other than tourist routes where they:
- (a) do not detract from the amenity or the environment
 - (b) are designed for slow moving traffic
 - (c) provide adequate car parking.

Hazard Risk Minimisation

- 20 Development and its site should be protected against the standard sea-flood risk level which is defined as the 1-in-100 year average return interval flood extreme sea level (tide, stormwater and associated wave effects combined), plus an allowance to accommodate land subsidence until the year 2100.
- 21 Development including associated roads and parking areas, other than minor structures unlikely to be adversely affected by flooding, should be protected from sea level rise by ensuring all of the following apply:
- (a) achieve (not less than those prescribed by the constraint) the 'Coastal Building and Site Levels (Australian Height Datum)' shown on the *Overlay Maps - Development Constraints*, including sill levels around underground car parking, cellars or the like
 - (b) there are practical measures available to protect the development against an additional sea level rise of 0.7 metres, plus an allowance to accommodate land subsidence until the year 2100 at the site.
- 22 Buildings to be sited over tidal water or which are not capable of being raised or protected by flood protection measures in future, should have a floor level of at least 1.25 metres above the standard sea-flood risk level.
- 23 Development that requires protection measures against coastal erosion, sea or stormwater flooding, sand drift or the management of other coastal processes at the time of development, or in the future, should only be undertaken if all of the following apply:
- (a) the measures themselves will not have an adverse effect on coastal ecology, processes, conservation, public access and amenity
 - (b) the measures do not nor will not require community resources, including land, to be committed
 - (c) the risk of failure of measures such as sand management, levee banks, flood gates, valves or stormwater pumping, is acceptable relative to the potential hazard resulting from their failure
 - (d) binding agreements are in place to cover future construction, operation, maintenance and management of the protection measures

- 24 Development should not compromise the structural integrity of any sea wall or levee bank adjacent to the foreshore, or compromise its capacity to protect against coastal flooding and erosion.

Erosion Buffers

- 25 Development should be set back a sufficient distance from the coast to provide an erosion buffer (in addition to a public reserve) which will allow for at least 100 years of coastal retreat for single buildings or small scale developments, or 200 years of coastal retreat for large scale developments (ie new townships) unless either of the following applies:
- (a) the development incorporates appropriate private coastal protection measures to protect the development and public reserve from the anticipated erosion.
 - (b) the council is committed to protecting the public reserve and development from the anticipated coastal erosion.
- 26 Where a coastal reserve exists or is to be provided it should be increased in width by the amount of any required erosion buffer. The width of an erosion buffer should be based on the following:
- (a) the susceptibility of the coast to erosion
 - (b) local coastal processes
 - (c) the effect of severe storm events
 - (d) the effect of a 0.3 metres sea level rise over the next 50 years on coastal processes and storms
 - (e) the availability of practical measures to protect the development from erosion caused by a further sea level rise of 0.7 metres per 50 years thereafter.
- 27 Development should not occur where essential services cannot be economically provided and maintained having regard to flood risk and sea level rise, or where emergency vehicle access would be prevented by a 1-in-100 year average return interval flood event, adjusted for 100 years of sea level rise.

Land Division

- 28 Land in coastal areas should only be divided if:
- (a) it or the subsequent development and use of the land will not adversely affect the management of the land, adjoining land or the coast
 - (b) sand dunes, wetlands and substantially intact strata of native vegetation are maintained or consolidated within single allotments.
- 29 Land division in coastal areas outside of designated urban or settlement zones should not increase either of the following:
- (a) the number of allotments abutting the coast or a reserve
 - (b) the number of allotments, including community title allotments and those that incorporate rights of way, with direct access to the coast or a reserve.
- 30 Land should not be divided for commercial, industrial or residential purposes unless a layout can be achieved whereby roads, parking areas and development sites on each allotment are at least 0.3 metres above the standard sea-flood risk level, unless the land is, or can be provided with appropriate coastal protection measures.

Protection of Economic Resources

- 31 Development should be sited, designed and managed so as not to conflict with or jeopardise the continuance of an existing aquaculture development.

Development in Appropriate Locations

- 32 Development along the coast should be in the form of infill in existing developed areas or concentrated into appropriately chosen nodes and not be in a scattered or linear form.
- 33 Development of a kind or scale (eg commercial or large-scale retail) that does not require a coastal location and would not significantly contribute to the community's enjoyment of the coast should not be located in coastal areas.

Community Facilities

OBJECTIVES

- 1 Location of community facilities including social, health, welfare, education and recreation facilities where they are conveniently accessible to the population they serve.
- 2 The proper provision of public and community facilities including the reservation of suitable land in advance of need.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Community facilities should be sited and developed to be accessible by pedestrians, cyclists and public and community transport.
- 2 Community facilities should be integrated in their design to promote efficient land use.
- 3 Design of community facilities should encourage flexible and adaptable use of open space and facilities for a range of uses over time.
- 4 Community facilities should be located in association with centre and retail development and/or educational establishments.

Crime Prevention

OBJECTIVES

- 1 A safe, secure, crime resistant environment where land uses are integrated and designed to facilitate community surveillance.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should be designed to maximise surveillance of public spaces through the incorporation of clear lines of sight, appropriate lighting and the use of visible permeable barriers wherever practicable.
- 2 Buildings should be designed to overlook public and communal streets and public open space to allow casual surveillance.
- 3 Development should provide a robust environment that is resistant to vandalism and graffiti.
- 4 Development should provide lighting in frequently used public spaces including those:
 - (a) along dedicated cyclist and pedestrian pathways, laneways and access routes
 - (b) around public facilities such as toilets, telephones, bus stops, seating, litter bins, automatic teller machines, taxi ranks and car parks.
- 5 Development, including car park facilities should incorporate signage and lighting that indicate the entrances and pathways to, from and within sites.
- 6 Landscaping should be used to assist in discouraging crime by:
 - (a) screen planting areas susceptible to vandalism
 - (b) planting trees or ground covers, rather than shrubs, alongside footpaths
 - (c) planting vegetation other than ground covers a minimum distance of two metres from footpaths to reduce concealment opportunities.
- 7 Site planning, buildings, fences, landscaping and other features should clearly differentiate public, communal and private areas.
- 8 Buildings should be designed to minimise and discourage access between roofs, balconies and windows of adjoining dwellings.
- 9 Public toilets should be located, sited and designed:
 - (a) to promote the visibility of people entering and exiting the facility (eg by avoiding recessed entrances and dense shrubbery that obstructs passive surveillance)
 - (b) near public and community transport links and pedestrian and cyclist networks to maximise visibility.
- 10 Development should avoid pedestrian entrapment spots and movement predictors (eg routes or paths that are predictable or unchangeable and offer no choice to pedestrians).

Design and Appearance

OBJECTIVES

- 1 Development of a high design standard and appearance that responds to and reinforces positive aspects of the local environment and built form.
- 2 Roads, open spaces, paths, buildings and land uses laid out and linked so that they are easy to understand and navigate.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Buildings should reflect the desired character of the locality while incorporating contemporary designs that have regard to the following:
 - (a) building height, mass and proportion
 - (b) external materials, patterns, colours and decorative elements
 - (c) roof form and pitch
 - (d) façade articulation and detailing
 - (e) verandas, eaves, parapets and window screens.
- 2 Where a building is sited on or close to a side boundary, the side boundary wall should be sited and limited in length and height to minimise:
 - (a) the visual impact of the building as viewed from adjoining properties
 - (b) overshadowing of adjoining properties and allow adequate sun light to neighbouring buildings.
- 3 The external walls and roofs of buildings should not incorporate highly reflective materials which will result in glare to neighbouring properties or drivers.
- 4 Structures located on the roofs of buildings to house plant and equipment should form an integral part of the building design in relation to external finishes, shaping and colours.
- 5 Building form should not unreasonably restrict existing views available from neighbouring properties and public spaces.
- 6 Balconies should:
 - (a) be integrated with the overall form and detail of the building and make a positive contribution to the external and external amenity of residential buildings
 - (b) be sited adjacent to the main living areas, such as the living room, dining room or kitchen to extend the dwelling's living space
 - (c) include balustrade detailing that enables line of sight to the street
 - (d) be recessed where wind would otherwise make the space unusable
 - (e) have a minimum dimension of 2 metres for upper level balconies or terraces.

- 7 Transportable buildings and buildings which are elevated on stumps, posts, piers, columns or the like **are not encouraged, but if developed they** should have their suspended footings enclosed around the perimeter of the building with brickwork or timber, and the use of verandas, pergolas and other suitable architectural detailing to give the appearance of a permanent structure.

Development Adjacent Heritage Places

- 8 The design of multi-storey buildings should not detract from the form and materials of adjacent State and local heritage places listed in [Table HoB/ 5 - State Heritage Places](#) or in [Table HoB/4- Local Heritage Places](#).
- 9 Development on land adjacent to a State or local heritage place, as listed in [Table HoB/5 - State Heritage Places](#) or in [Table Hob/4 - Local Heritage Places](#), should be sited and designed to reinforce the historic character of the place and maintain its visual prominence.

Overshadowing

- 10 The design and location of buildings should enable direct winter sunlight into adjacent dwellings and private open space and minimise the overshadowing of:
- (a) windows of habitable rooms
 - (b) upper-level private balconies that provide the primary open space area for a dwelling
 - (c) solar collectors (such as solar hot water systems and photovoltaic cells).

Visual Privacy

- 11 Development should minimise direct overlooking of habitable rooms and private open spaces of dwellings through measures such as:
- (a) off-setting the location of balconies and windows of habitable rooms with those of other buildings so that views are oblique rather than direct
 - (b) building setbacks from boundaries (including building boundary to boundary where appropriate) that interrupt views or that provide a spatial separation between balconies or windows of habitable rooms
 - (c) screening devices (including fencing, obscure glazing, screens, external ventilation blinds, window hoods and shutters) that are integrated into the building design and have minimal negative effect on residents' or neighbours' amenity.
- 12 Permanently fixed external screening devices should be designed and coloured to complement the associated building's external materials and finishes.

Relationship to the Street and Public Realm

- 13 Buildings (other than ancillary buildings or group dwellings) should be designed so that their main façade faces the primary street frontage of the land on which they are situated.
- 14 Buildings, landscaping, paving and signage should have a coordinated appearance that maintains and enhances the visual attractiveness of the locality.
- 15 Buildings should be designed and sited to avoid creating extensive areas of uninterrupted walling facing areas exposed to public view.
- 16 Building design should emphasise pedestrian entry points to provide perceptible and direct access from public street frontages and vehicle parking areas.
- 17 **Outdoor lighting should not result in light spillage on adjacent land.**

- 18 The ground floor of mixed use buildings should comprise non-residential land uses.
- 19 In mixed use areas, development facing the street should be designed to activate the street frontage(s):
 - (a) including features that attract people to the locality such as frequent doors and display windows, retail shopfronts and/or outdoor eating or dining areas
 - (b) minimising the frontage for fire escapes, service doors, plant and equipment hatches
 - (c) avoiding undercroft or ground floor vehicle parking that is visible from the primary street frontage
 - (d) using colour, vertical and horizontal elements, roof overhangs and other design techniques to provide visual interest and reduced massing.
- 20 Where zero or minor setbacks are desirable, development should incorporate shelter over footpaths to enhance the quality of the pedestrian environment.

Outdoor Storage and Service Areas

- 21 Outdoor storage, loading and service areas should be:
 - (a) screened from public view by a combination of built form, solid fencing and/or landscaping
 - (b) conveniently located and designed to enable the manoeuvring of service and delivery vehicles
 - (c) sited away from sensitive land uses.

Building Setbacks from Road Boundaries

- 22 Except in areas where a new character is desired, the setback of **development** from public roads should be:
 - (a) screened from public view by a combination of built form, solid fencing and/or landscaping
 - (b) conveniently located and designed to enable the manoeuvring of service and delivery vehicles
 - (c) sited away from sensitive land uses.
- 23 All setbacks from the road frontage should be additional to the road widening setback established under the *Metropolitan Adelaide Road Widening Plan Act 1972*.
- 24 **Petrol filling stations should:**
 - (a) have a frontage to a public road of not less than 30 metres
 - (b) be located at least 60 metres from an intersection or junction affecting a controlled access road
 - (c) provide a kerb or barrier, or both, along the allotment boundary to prevent vehicles leaving or entering the site except by the vehicular crossings
 - (d) provide a hard paved surface to driveways, service and car parking areas
 - (e) provide driveway crossovers that are designed and sited in accordance with the following:
 - (i) no more than two crossovers per road frontage
 - (ii) a setback of at least 9 metres from any other vehicular access points on the same site
 - (iii) a maximum width along the kerb of no more than 9 metres
 - (iv) an angle between the centreline of the crossing and the road alignment of at least 60 degrees.

Energy Efficiency

OBJECTIVES

- 1 Development designed and sited to conserve energy.
- 2 Development that provides for on-site power generation including photovoltaic cells and wind power.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should provide for efficient solar access to buildings and open space all year around.
- 2 Buildings should be sited and designed:
 - (a) to ensure adequate natural light and winter sunlight is available to the main activity areas of adjacent buildings
 - (b) so that open spaces associated with the main activity areas face north for exposure to winter sun
 - (c) to promote energy conservation by maintaining adequate access to winter sunlight to the main ground level of living areas of existing dwellings on adjoining land.
- 3 Except for buildings that take advantage of coastal views, development should promote the efficient consumption of energy through the use of larger but appropriately shaded windows on the north and east building surfaces and smaller windows on the south and west building surfaces.

On-site Energy Generation

- 4 Development should facilitate the efficient use of photovoltaic cells and solar hot water systems by:
 - (a) taking into account overshadowing from neighbouring buildings
 - (b) designing roof orientation and pitches to maximise exposure to direct sunlight.
- 5 Public infrastructure and lighting, should be designed to generate and use renewable energy.

Hazards

OBJECTIVES

- 1 Maintenance of the natural environment and systems by limiting development in areas susceptible to natural hazard risk.
- 2 Development located away from areas that are vulnerable to, and cannot be adequately and effectively protected from the risk of natural hazards.
- 3 Critical community facilities such as hospitals, emergency control centres, major service infrastructure facilities, and emergency service facilities located where they are not exposed to natural hazard risks.
- 4 Development located and designed to minimise the risks to safety and property from flooding.
- 5 Development located to minimise the threat and impact of bushfires on life and property.
- 6 Expansion of existing non-rural uses directed away from areas of high bushfire risk.
- 7 The environmental values and ecological health of receiving waterways and marine environments protected from the release of acid water resulting from the disturbance of acid sulphate soils.
- 8 Protection of human health and the environment wherever site contamination has been identified or is suspected to have occurred.
- 9 Appropriate assessment and remediation of site contamination to ensure land is suitable for the proposed use and provides a safe and healthy living and working environment.
- 10 Minimisation of harm to life, property and the environment through appropriate location of development and appropriate storage, containment and handling of hazardous materials.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should be excluded from areas that are vulnerable to, and cannot be adequately and effectively protected from, the risk of hazards.
- 2 Development located on land subject to hazards as shown on the *Overlay Maps - Development Constraints* should not occur unless it is sited, designed and undertaken with appropriate precautions being taken against the relevant hazards.
- 3 There should not be any significant interference with natural processes in order to reduce the exposure of development to the risk of natural hazards.

Flooding

- 4 Development should not occur on land where the risk of flooding is likely to be harmful to safety or damage property.
- 5 Development should not be undertaken in areas liable to inundation by tidal, drainage or flood waters unless the development can achieve all of the following:
 - (a) it is developed with a public stormwater system capable of catering for a 1-in-100 year average return interval flood event
 - (b) buildings are designed and constructed to prevent the entry of floodwaters in a 1-in-100 year average return interval flood event.

- 6 Development, including earthworks associated with development, should not do any of the following:
 - (a) impede the flow of floodwaters through the land or other surrounding land
 - (b) increase the potential hazard risk to public safety of persons during a flood event
 - (c) aggravate the potential for erosion or siltation or lead to the destruction of vegetation during a flood
 - (d) cause any adverse effect on the floodway function
 - (e) increase the risk of flooding of other land
 - (f) obstruct a watercourse.
- 7 Where flood protection measures are provided (e.g. levees or pumping stations) they should be designed such that building sites are not lower than the estimated water level for flooding resulting from a 1-in-100 year average return interval flood.
- 8 Where flood protection measures are not provided for development situated on low-lying or flood affected land, development should be at least 0.25 metres above the minimum 1-in-100 year average return interval flood.

Salinity

- 9 Development should not increase the potential for, or result in an increase in, soil and water salinity.
- 10 Preservation, maintenance and restoration of locally indigenous plant species should be encouraged in areas affected by dry land salinity.
- 11 Irrigated horticulture and pasture should not increase groundwater-induced salinity.

Acid Sulfate Soils

- 12 Development and activities, including excavation and filling of land, that may lead to the disturbance of potential or actual acid sulfate soils should be avoided unless such disturbances are managed in a way that effectively avoids the potential for harm or damage to any of the following:
 - (a) the marine and estuarine environment
 - (b) natural water bodies and wetlands
 - (c) agricultural or aquaculture activities
 - (d) buildings, structures and infrastructure
 - (e) public health.
- 13 Development, including primary production, aquaculture activities and infrastructure, should not proceed unless it can be demonstrated that the risk of releasing acid water resulting from the disturbance of acid sulfate soils is minimal.

Site Contamination

- 14 Development, including land division, should not occur where site contamination has occurred unless the site has been assessed and remediated as necessary to ensure that it is suitable and safe for the proposed use.

Containment of Chemical and Hazardous Materials

- 15 Hazardous materials should be stored and contained in a manner that minimises the risk to public health and safety and the potential for water, land or air contamination.

- 16 Development that involves the storage and handling of hazardous materials should ensure that these are contained in designated areas that are secure, readily accessible to emergency vehicles, impervious, protected from rain and stormwater intrusion and other measures necessary to prevent:
- (a) discharge of polluted water from the site
 - (b) contamination of land
 - (c) airborne migration of pollutants
 - (d) potential interface impacts with sensitive land uses.

Landslip

- 17 Land identified as being at risk from landslip should not be developed.
- 18 Development, including associated cut and fill activities, should not lead to an increased danger from land surface instability or to the potential of landslip occurring on the site or on surrounding land.
- 19 Development on steep slopes should promote the retention and replanting of vegetation as a means of stabilising and reducing the possibility of surface movement or disturbance.
- 20 Development in areas susceptible to landslip should:
- (a) incorporate split level designs to minimise cutting into the slope
 - (b) ensure that cut and fill and heights of faces are minimised
 - (c) ensure cut and fill is supported with engineered retaining walls or are battered to appropriate grades
 - (d) control any erosion that will increase the gradient of the slope and decrease stability
 - (e) ensure the siting and operation of an effluent drainage field does not contribute to landslip
 - (f) provide drainage measures to ensure surface stability is not compromised
 - (g) ensure natural drainage lines are not obstructed.

Heritage Places

OBJECTIVES

- 1 The conservation of State and local heritage places.
- 2 The continued use, or adaptive re-use of State and local heritage places that supports the conservation of their cultural significance.
- 3 Conservation of the setting of State and local heritage places.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 A heritage place spatially located on *Overlay Maps – Heritage* and more specifically identified in [Table HoB/5 - State Heritage Places](#) or in [Table HoB/4- Local Heritage Places](#) should not be demolished, destroyed or removed, in total or in part, unless either of the following apply:
 - (a) that portion of the place to be demolished, destroyed or removed is excluded from the extent of the listing identified in the *Table(s)*
 - (b) the structural condition of the place represents an unacceptable risk to public or private safety and the place cannot reasonably be rehabilitated.
- 2 Development of a State or local heritage place should retain those elements contributing to its heritage value, which may include (but not be limited to):
 - (a) principal elevations
 - (b) important vistas and views to and from the place
 - (c) setting and setbacks
 - (d) building materials
 - (e) outbuildings and walls
 - (f) trees and other landscaping elements
 - (g) access conditions (driveway form/width/material)
 - (h) architectural treatments
 - (i) the use of the place.
- 3 Development of a State or local heritage place should be compatible with the heritage value of the place.
- 4 Original unpainted plaster, brickwork, stonework, or other masonry of existing State or local heritage places should be preserved, unpainted.
- 5 New buildings or building additions should not be placed or erected between the front street boundary and the façade of existing State or local heritage places.

- 6 Development that materially affects the context within which the heritage place is situated, including development on adjoining properties and on nearby properties where there would be an influence upon the character, integrity and setting, should be compatible with the heritage place. It is not necessary to replicate historic detailing, however design elements that should be compatible include, but are not limited to:
 - (a) scale, bulk and form
 - (b) width of frontage
 - (c) boundary setback patterns
 - (d) proportion and composition of design elements such as rooflines, window and door openings, fencing and landscaping
 - (e) colour and texture of external materials.
- 7 Multi-storey additions to a State or local heritage place should be compatible with the heritage value of the place through a range of design solutions such as:
 - (a) extending into the existing roof space or to the rear of the building
 - (b) retaining the elements that contribute to the building's heritage value
 - (c) distinguishing between the existing and new portion of the building
 - (d) stepping in parts of the building that are taller than the front facade.
- 8 The introduction of advertisements and signage to a State or local heritage place should:
 - (a) be placed on discrete elements of its architecture such as parapets and wall panels, below the canopy, or within fascias and infill end panels and windows
 - (b) not conceal or obstruct historical detailing of the heritage place
 - (c) not project beyond the silhouette or skyline of the heritage place
 - (d) not form a dominant element of the place.
- 9 The division of land adjacent to or containing a State or local heritage place should occur only where it will:
 - (a) create an allotment pattern that maintains or reinforces the integrity of the heritage place and the character of the surrounding area
 - (b) create an allotment or allotments of a size and dimension that can accommodate new development that will reinforce and complement the heritage place and the zone or policy area generally
 - (c) be of a size and dimension that will enable the siting and setback of new buildings from allotment boundaries so that they do not overshadow, dominate, encroach on or otherwise impact on the setting of the heritage place
 - (d) provide an area for landscaping of a size and dimension that complements the landscape setting of the heritage place and the landscape character of the locality
 - (e) enables the State or local heritage place to have a curtilage of a size sufficient to protect its setting.

- 10 Development of a State or Local Heritage Place, or development on land adjacent to a State or Local Heritage Place should conserve, maintain, enhance and reinforce the historic character of individual buildings and/or the existing streetscape character by exhibiting architectural and roof-form designs, street frontage widths, front and side boundary set-backs, materials, colours, fences and landscape settings which complement and give prominence to historic buildings or their detailing.
- 11 The development of local heritage places which are public open spaces should:
 - (a) conserve, respect, complement and reinstate where appropriate the particular built and landscape characteristics which establish the historic significance of the relevant place; and
 - (b) retain the open space nature of the relevant place.

Historic Conservation Area

Refer to the [Map Reference Tables](#) for a list of the *Overlay Maps - Heritage* that relate to this Historic Conservation Area.

OBJECTIVES

- 1 The conservation of areas of historical significance
- 2 Development that promotes, conserves and enhances the cultural significance and historic character of identified places and areas.
- 3 Development that complements the historic significance of the area.
- 4 The retention and conservation of places such as land, buildings, structures and landscape elements that contribute positively to the historic character of the area.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should not be undertaken unless it is consistent with the desired character for the Historic Conservation Area.
- 2 Places such as land, buildings, structures and landscape elements that contribute to the historic character of the Historic Conservation Area identified on the *Overlay Maps - Heritage* and more specifically identified in the respective [Table HoB/3 - Contributory Items](#), should be retained and conserved.
- 3 Development of a contributory item should:
 - (a) not compromise its value to the historic significance of the area
 - (b) retain its present integrity or restore its original design features
 - (c) maintain or enhance the prominence of the original street facade
 - (d) ensure additions are screened by, and/or located to the rear of the building
 - (e) ensure original unpainted plaster, brickwork, stonework, or other masonry is preserved, unpainted.
- 4 New buildings should not be placed or erected between the front street boundary and the facade of contributory items.
- 5 Buildings and structures should not be demolished in whole or in part, unless they are:
 - (a) structurally unsafe and/or unsound and cannot reasonably be rehabilitated
 - (b) inconsistent with the desired character for the Historic Conservation Area and associated with a proposed replacement development that supports the desired character for the Historic Conservation Area.
- 6 Development should take design cues from the existing historic built forms. In doing this, it is not necessary to replicate historic detailing; however design elements for consideration should be compatible with building and streetscape character and should include but not be limited to:
 - (a) scale, bulk and form

- (b) width of frontage
 - (c) boundary setback patterns
 - (d) proportion and composition of design elements such as roof lines, pitches, window and door openings, verandas, fencing and landscaping
 - (e) colour and texture of external materials
 - (f) visual interest.
- 7 Development should:
- (a) restore and rehabilitate existing places of historic value
 - (b) incorporate compact extensions to buildings of a scale, bulk and external appearance that are sympathetic and compatible with the historic character of these places
 - (c) conserve, enhance and reinforce the historic significance of buildings and streetscapes.
- 8 Additions to a contributory item should be screened by the original building and/or located to the rear of the original building.
- 9 Garages and carports should be designed to maintain the prominence of existing historic buildings in the Historic Conservation Area and to not dominate the appearance of the front elevation of the associated building.
- 10 New buildings should not be placed or erected between the front street boundary and the façade of contributory items.
- 11 New residential development should include landscaped front garden areas that complement the desired character.
- 12 The conversion of an existing dwelling into two or more dwellings may be undertaken provided that the building and front yard retain the original external appearance to the public road.
- 13 Second storey additions to single storey dwellings should achieve one or more of the following:
- (a) a sympathetic two storey addition that uses existing roof space or incorporates minor extensions to the roof space at the rear of the dwelling
 - (b) a second storey within the roof space, where the overall building height, scale and form is compatible with existing single-storey development in the policy area
 - (c) second storey windows having a total length less than 30 per cent of the total roof length along each elevation.
- 14 Front fences and gates on the primary and secondary street boundary extending back to the front dwelling and alignment should reflect and conserve the traditional period, style and form of the associated building.
- 15 Rear and side boundary fences located behind the front dwelling alignment should be no more than 1.8 metres in height.
- 16 Advertisements and/or advertising hoardings associated with places and areas of heritage significance in Historic Conservation Areas should:
- (a) be of a size, colour, shape and materials that enhances the character of the locality
 - (b) not dominate or detract from the prominence of any place and/or area of historic significance.

- 17 Development should respect the existing topography and the relationship of sites to street levels and to adjoining land and not involve substantial cut and/or fill or sites.
- 18 The division of land should occur only where it will maintain the traditional pattern and size of allotments.
- 19 New buildings and structures adjacent to a contributory item should be sited, designed and built in a manner that reinforces and enhances the historic character of the contributory item. New buildings should be designed to maintain the prominence of existing historic buildings.

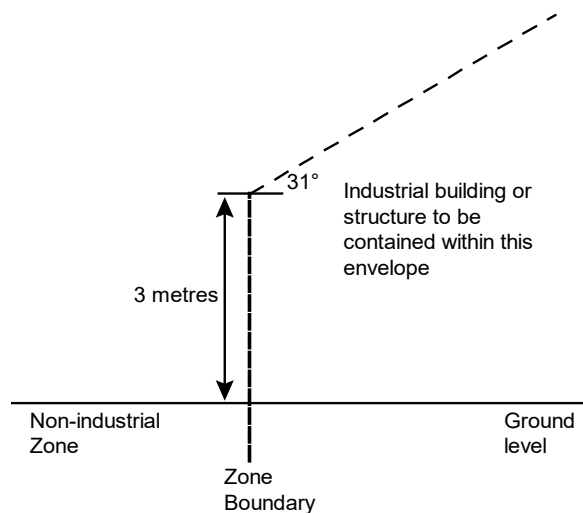
Industrial Development

OBJECTIVES

- 1 Industrial, warehouse, storage and transport distribution development on appropriately located land, integrated with transport networks and designed to minimise potential impact on these networks.
- 2 Industrially zoned allotments and uses protected from encroachment by adjoining uses that would reduce industrial development or expansion.
- 3 Industrial, warehouse, storage and commercial development occurring without adverse effects on the health and amenity of occupiers of land in adjoining zones.
- 4 Compatibility between industrial, warehouse, storage and commercial uses within industrial zones.
- 5 The improved amenity of industrial areas.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Offices and showrooms associated with industrial, warehouse, storage and transport development should be sited at the front of the building with direct and convenient pedestrian access from the main visitor parking area.
- 2 Any building or structure for an industrial, warehouse, storage or commercial development, located on, or abutting, the boundary of a non-industrial zone should be restricted to a height of 3 metres above ground level at the boundary and a plane projected at 31 degrees above the horizontal into the development site from that 3 metre height, as shown in the following diagram:



- 3 Industrial, warehouse, storage and commercial development should enable all vehicles to enter and exit the site in a forward direction.
- 4 Industrial, warehouse, storage and commercial development abutting an arterial road, a non-industrial zone boundary, or significant open space should be developed in a manner that avoids adverse visual impacts on the locality.
- 5 Industrial, warehouse, storage and commercial building facades facing a non-industrial zone, public road, or public open space should:
 - (a) use a variety of building finishes

- (b) not consist solely of metal cladding
 - (c) contain materials of low reflectivity
 - (d) incorporate design elements to add visual interest
 - (e) avoid large expanses of blank walls.
- 6 Industrial, **warehouse, storage and commercial** development should minimise significant adverse impact on adjoining uses due to hours of operation, traffic, noise, fumes, smell, dust, paint or other chemical over-spray, vibration, glare or light spill, electronic interference, ash or other harmful or nuisance-creating impacts.
- 7 Landscaping **for industrial, warehouse, storage and commercial activities** should be incorporated as an integral element of industrial development along non-industrial zone boundaries.
- 8 Fencing (including colour-coated wire mesh fencing) **for industrial, warehouse, storage and commercial development** adjacent to public roads should be set back in one of the following ways:
- (a) in line with the building facade
 - (b) behind the building line
 - (c) behind a landscaped area that softens its visual impact.
- 9 Marine aquaculture onshore storage, cooling and processing facilities should not impair the coastline and its visual amenity and should:
- (a) be sited, designed, landscaped and developed at a scale and using external materials that minimise any adverse visual impact on the coastal landscape
 - (b) be sited and designed with appropriate vehicular access arrangement
 - (c) include appropriate waste treatment and disposal.
- 10 **Industrial, warehouse, storage and commercial development, including a motor repair station or service trade premises should have a maximum site coverage, including outbuildings, of no more than 50 per cent of the site.**

Infrastructure

OBJECTIVES

- 1 Infrastructure provided in an economical and environmentally sensitive manner.
- 2 Infrastructure, including social infrastructure, provided in advance of need.
- 3 Suitable land for infrastructure identified and set aside in advance of need.
- 4 The visual impact of infrastructure facilities minimised.
- 5 The efficient and cost-effective use of existing infrastructure.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should not occur without the provision of adequate utilities and services, including:
 - (a) electricity supply
 - (b) water supply
 - (c) drainage and stormwater systems
 - (d) waste disposal
 - (e) effluent disposal systems
 - (f) formed all-weather public roads
 - (g) telecommunications services
 - (h) social infrastructure, community services and facilities
 - (i) gas services.
- 2 Development should only occur where it provides, or has access to, relevant easements for the supply of infrastructure.
- 3 Development should incorporate provision for the supply of infrastructure services to be located within common service trenches where practicable.
- 4 Development should not take place until adequate and coordinated drainage of the land is assured.
- 5 Development in urban areas should not occur without provision of an adequate reticulated domestic quality mains water supply and an appropriate waste treatment system.
- 6 In areas where no reticulated water supply is available, buildings whose usage is reliant on a water supply should be equipped with an adequate and reliable on-site water storage system.
- 7 Urban development should not be dependent on an indirect water supply.
- 8 Electricity infrastructure should be designed and located to minimise its visual and environmental impacts.

- 9 In urban areas, electricity supply (except transmission lines) serving new development should be installed underground, excluding lines having capacity greater than or equal to 33kV.
- 10 Utilities and services, including access roads and tracks, should be sited on areas already cleared of native vegetation. If this is not possible, their siting should cause minimal interference or disturbance to existing native vegetation and biodiversity.
- 11 Utility buildings and structures should be grouped with non-residential development where possible.
- 12 Development in proximity to infrastructure facilities should be sited and be of a scale to ensure adequate separation to protect people and property.

Interface between Land Uses

OBJECTIVES

- 1 Development located and designed to prevent adverse impact and conflict between land uses.
- 2 Protect community health and amenity and support the operation of all desired land uses.
- 3 **Protect desired land use forms from the encroachment of incompatible development.**

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should not detrimentally affect the amenity of the locality or cause unreasonable interference through any of the following:
 - (a) the emission of effluent, odour, smoke, fumes, dust or other airborne pollutants
 - (b) noise
 - (c) vibration
 - (d) electrical interference
 - (e) light spill
 - (f) glare
 - (g) hours of operation
 - (h) traffic impacts.
- 2 Development should be sited and designed to minimise negative impact on existing and potential future land uses considered appropriate in the locality.
- 3 Development adjacent to a **Residential Zone** should be designed to minimise overlooking and overshadowing of nearby residential properties.
- 4 Residential development adjacent to non-residential zones and land uses should be located, designed and/or sited to protect residents from potential adverse impacts from non-residential activities.
- 5 Sensitive uses likely to conflict with the continuation of lawfully existing developments and land uses considered appropriate for the zone should not be developed or should be designed to minimise negative impacts.
- 6 Non-residential development on land abutting a residential zone should be designed to minimise noise impacts to achieve adequate levels of compatibility between existing and proposed uses.

Noise Generating Activities

- 7 Development that emits noise (other than music noise) should include noise attenuation measures that achieve the relevant *Environment Protection (Noise) Policy* criteria when assessed at the nearest existing noise sensitive premises.
- 8 Development with the potential to emit significant noise (e.g. industry) should incorporate noise attenuation measures that prevent noise from causing unreasonable interference with the amenity of noise sensitive premises.

- 9 Outdoor areas (such as beer gardens or dining areas) associated with licensed premises should be designed or sited to minimise adverse noise impacts on adjacent existing or future noise sensitive development.
- 10 Development proposing music should include noise attenuation measures that achieve the following desired noise levels:

| Noise level assessment location | Desired noise level |
|--|--|
| Adjacent existing <i>noise sensitive development</i> property boundary | Less than 8 dB above the level of background noise ($L_{90,15min}$) in any octave band of the sound spectrum and Less than 5 dB(A) above the level of background noise ($LA_{90,15min}$) for the overall (sum of all octave bands) A-weighted level |
| Adjacent <i>land</i> property boundary | Less than 65dB(Lin) at 63Hz and 70dB(Lin) in all other octave bands of the sound spectrum or Less than 8 dB above the level of background noise ($L_{90,15min}$) in any octave band of the sound spectrum and 5 dB(A) overall (sum of all octave bands) A-weighted level |

Air Quality

- 11 Development with the potential to emit harmful or nuisance-generating air pollution should incorporate air pollution control measures to prevent harm to human health or unreasonable interference with the amenity of sensitive uses within the locality.
- 12 Chimneys or exhaust flues associated with commercial development (including cafes, restaurants and fast food outlets) should be designed to ensure they do not cause a nuisance or health concerns to nearby sensitive receivers by:
 - (a) incorporating appropriate treatment technology before exhaust emissions are released to the atmosphere
 - (b) ensuring that the location and design of chimneys or exhaust flues maximises dispersion and takes into account the location of nearby sensitive uses.

Land Division

OBJECTIVES

- 1 Land division that occurs in an orderly sequence allowing efficient provision of new infrastructure and facilities and making optimum use of existing under utilised infrastructure and facilities.
- 2 Land division that creates allotments appropriate for the intended use.
- 3 Land division layout that is optimal for energy efficient building orientation.
- 4 Land division that is integrated with site features, including landscape and environmental features, adjacent land uses, the existing transport network and the availability of infrastructure.
- 5 Land division restricted in rural areas to ensure the efficient use of rural land for primary production and avoidance of uneconomic infrastructure provision.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 When land is divided:
 - (a) stormwater should be capable of being drained safely and efficiently from each proposed allotment and disposed of from the land in an environmentally sensitive manner
 - (b) a sufficient water supply should be made available for each allotment
 - (c) provision should be made for the disposal of wastewater, sewage and other effluent from each allotment without risk to health
 - (d) proposed roads should be graded, or be capable of being graded to connect safely and conveniently with an existing road or thoroughfare.
- 2 Land should not be divided if any of the following apply:
 - (a) the size, shape, location, slope or nature of the land makes any of the allotments unsuitable for the intended use
 - (b) any allotment will not have a frontage to one of the following:
 - (i) an existing road
 - (ii) a proposed public road
 - (iii) access to a public road via an internal roadway in a plan of community division
 - (c) the intended use of the land is likely to require excessive cut and/or fill
 - (d) it is likely to lead to undue erosion of the subject land or land within the locality
 - (e) the wastewater treatment plant to which subsequent development will be connected does not have sufficient capacity to handle the additional wastewater volumes and pollutant loads generated by such development
 - (f) the area is unsewered and cannot accommodate an appropriate onsite wastewater disposal system within the allotment that complies with (or can comply with) the relevant public and environmental health legislation applying to the intended use(s)

- (g) any allotments will straddle more than one zone, policy area or precinct
 - (h) the allotments unreasonably restrict access to publicly owned land such as recreation areas.
- 3 Common property associated with Community Title subdivision should only be created for the purposes of at least one of the following:
- (a) access
 - (b) services
 - (c) landscaping
 - (d) outdoor living
 - (e) other common functions.
- 4 The average site areas for group dwellings and residential flat buildings, where specified in a particular zone or policy area, should exclude common areas such as access ways, driveways and landscaping.

Design and Layout

- 5 Land divisions should be designed to ensure that areas of native vegetation and wetlands:
- (a) are not fragmented or reduced in size
 - (b) do not need to be cleared as a consequence of subsequent development.
- 6 The design of a land division should incorporate:
- (a) roads, thoroughfares and open space that result in safe and convenient linkages with the surrounding environment, including public and community transport facilities, and which, where necessary, facilitate the satisfactory future division of land and the inter-communication with neighbouring localities
 - (b) new road and allotment access points providing appropriate separation distances from existing road junctions or level crossings
 - (c) safe and convenient access from each allotment to an existing or proposed road or thoroughfare
 - (d) areas to provide appropriate separation distances between potentially conflicting land uses and/or zones
 - (e) suitable land set aside for useable local open space
 - (f) public utility services within road reserves and where necessary within dedicated easements
 - (g) the preservation of significant natural, cultural or landscape features including State and local heritage places
 - (h) protection for existing vegetation and drainage lines
 - (i) where appropriate, the amalgamation of smaller allotments to ensure coordinated and efficient site development
 - (j) the preservation of significant trees.
- 7 Land division should result in allotments of a size suitable for their intended use.
- 8 Land division should facilitate optimum solar access for energy efficiency.

- 9 Allotments in the form of a hammerhead/battleaxe configuration should not be developed within the **Residential Character Zone**, except if they are located within **Seacliff Policy Area 12**.
- 10 Except where located within the **Residential Character Zone**, an allotment in the form of a hammer head or battleaxe configuration (including for group dwellings), should only be created where all of the following is achieved:
- (a) except where specified in a particular zone or policy area, a minimum of site area of at least 450 square metres per allotment (excluding the area of the 'handle' of such an allotment)
 - (b) sufficient area is provided for a vehicle to enter and exit the allotment in a forward direction
 - (c) the driveway or 'handle' portion of the allotment is located in a manner that is compatible with the prevailing pattern of development and would not result in multiple access points onto the road in order to minimise the impact of access points on the amenity of the streetscape
 - (d) the driveway or 'handle' portion of the allotment has a maximum deviation angle for driveway tapering of no more than 20 degrees
 - (e) the minimum width of the driveway or 'handle' portion of the allotment (including a landscaped strip on each side of the driveway, which is at least 0.5 metres wide or 1 metre wide where the driveway provides access to 8 or more dwellings) is not less than that shown in the following table:

| Dwellings or allotments to be served by the driveway (whichever is the greater) | At the front property boundary and for the first 6 metres | Width beyond the first 6 metres | Widening required for passing |
|---|--|---------------------------------|--|
| 1 | 4 metres | 3.5 metres | Not required |
| At least 2 and no more than 7 | 8 metres where the site is adjacent to an arterial road 6 metres in all other circumstances | 4 metres | The minimum width is increased to 6 metres for a length of 6 metres at a point 25 metres from the front property boundary and every 25 metres thereafter |
| 8 or more | 8 metres | 6 metres | Not required |

- 11 Allotments should have an orientation, size and configuration to encourage development that:
- (a) minimises the need for earthworks and retaining walls
 - (b) maintains natural drainage systems
 - (c) faces abutting streets and open spaces
 - (d) does not require the removal of native vegetation to facilitate that development
 - (e) will not overshadow, dominate, encroach on or otherwise detrimentally affect the setting of the surrounding locality.
- 12 The layout of a land division should keep flood-prone land free from development.
- 13 The arrangement of roads, allotments, reserves and open space should enable the provision of a stormwater management drainage system that:
- (a) contains and retains all watercourses, drainage lines and native vegetation
 - (b) enhances amenity

- (c) integrates with the open space system and surrounding area.

Roads and Access

- 14 Road reserves should be of a width and alignment that can:
 - (a) provide for safe and convenient movement and parking of projected volumes of vehicles and other users
 - (b) provide for footpaths, cycle lanes and shared-use paths for the safety and convenience of residents and visitors
 - (c) allow vehicles to enter or reverse from an allotment or site in a single movement allowing for a car parked on the opposite side of the street
 - (d) accommodate street tree planting, landscaping and street furniture
 - (e) accommodate the location, construction and maintenance of stormwater drainage and public utilities
 - (f) provide unobstructed, safe and efficient vehicular access to individual allotments and sites
 - (g) allow for the efficient movement of service and emergency vehicles.
- 15 The design of the land division should facilitate the most direct route to local facilities for pedestrians and cyclists and enable footpaths, cycle lanes and shared-use paths to be provided of a safe and suitable width and reasonable longitudinal gradient.
- 16 The layout of land divisions should result in roads designed and constructed to ensure:
 - (a) that traffic speeds and volumes are restricted where appropriate by limiting street length and/or the distance between bends and slow points
 - (b) there are adequate sight distances for motorists at intersections, junctions, pedestrian and cyclist crossings, and crossovers to allotments to ensure the safety of all road users and pedestrians
 - (c) that existing dedicated cycling and walking routes are not compromised.
- 17 The design of the land division should provide space sufficient for on-street visitor car parking for the number and size of allotments, taking account of:
 - (a) the size of proposed allotments and sites and opportunities for on-site parking
 - (b) the availability and frequency of public and community transport
 - (c) on-street parking demand likely to be generated by nearby uses.
- 18 The design of the land division should provide at least one readily accessible on-street car parking space adjacent to every two allotments created, except along an arterial road.
- 19 The layout of land divisions should incorporate street patterns designed to enhance the efficient movement of traffic and minimise trip lengths.

Landscaping, Fences and Walls

OBJECTIVES

- 1 The amenity of land and development enhanced with appropriate planting and other landscaping works, using locally indigenous plant species where possible.
- 2 Functional fences and walls that enhance the attractiveness of development.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should incorporate open space and landscaping and minimise hard paved surfaces in order to:
 - (a) complement built form and reduce the visual impact of larger buildings (eg taller and broader plantings against taller and bulkier building components)
 - (b) enhance the appearance of road frontages
 - (c) screen service yards, loading areas and outdoor storage areas
 - (d) minimise maintenance and watering requirements
 - (e) enhance and define outdoor spaces, including car parking areas
 - (f) maximise shade and shelter
 - (g) assist in climate control within and around buildings
 - (h) minimise heat absorption and reflection
 - (i) maintain privacy
 - (j) maximise stormwater re-use
 - (k) complement existing vegetation, including native vegetation
 - (l) contribute to the viability of ecosystems and species
 - (m) promote water and biodiversity conservation
 - (n) establish buffers to adjacent development and areas.
- 2 Landscaping should:
 - (a) include mature vegetation, the planting of locally indigenous species where appropriate and species tolerant of salt-laden winds near the coast
 - (b) be oriented towards the street frontage
 - (c) result in the appropriate clearance from powerlines and other infrastructure being maintained.
- 3 Landscaping should not:
 - (a) unreasonably restrict solar access to adjoining development

- (b) cause damage to buildings, paths and other landscaping from root invasion, soil disturbance or plant overcrowding
 - (c) introduce pest plants
 - (d) increase the risk of bushfire
 - (e) remove opportunities for passive surveillance
 - (f) increase leaf fall in watercourses
 - (g) increase the risk of weed invasion
 - (h) obscure driver sight lines
 - (i) create a hazard for train or tram drivers by obscuring sight lines at crossovers.
- 4 Existing substantial vegetation should be retained and incorporated within landscaping of new development where practicable.
- 5 Fences and walls, including retaining walls, should:
- (a) not result in damage to neighbouring trees
 - (b) be compatible with the associated development and with existing predominant, attractive fences and walls in the locality
 - (c) enable some visibility of buildings from and to the street to enhance safety and allow casual surveillance
 - (d) incorporate articulation or other detailing where there is a large expanse of wall facing the street
 - (e) assist in highlighting building entrances
 - (f) be sited and limited in height, to ensure adequate sight lines for motorists and pedestrians especially on corner sites
 - (g) in the case of side and rear boundaries, be of sufficient height to maintain privacy and/or security without adversely affecting the visual amenity or access to sunlight of adjoining land
 - (h) be constructed of non-flammable materials.
- 6 Retaining walls should be constructed as a stepped series of low walls, incorporate landscaping to soften the appearance of the retaining wall and use locally indigenous plant species where possible.
- 7 Front fencing should be open in form to allow cross ventilation and access to sunlight.

Marinas and Maritime Structures

OBJECTIVE

- 1 The provision, in appropriate locations, of marinas, pontoons, jetties, piers, wharves and boat moorings that cater for vessels and:
 - (a) maintain public access to the waterfront
 - (b) do not compromise public safety
 - (c) preserve the structural integrity of the marine infrastructure
 - (d) minimise adverse impacts on the natural environment.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Marina development should include one or more of the following:
 - (a) wet and dry berthing of boats
 - (b) launching and retrieval of recreational boats and associated trailer and car parking areas
 - (c) access ramps, landings, storage and other structures associated with a marina
 - (d) clubrooms for maritime organisations.
- 2 The design of marinas, berths, channels, fairways, gangways and floating structures should comply with:
 - (a) *Australian Standard AS 3962: Guidelines for Design of Marinas*
 - (b) *Australian Standard AS 4997: Guidelines for the Design of Maritime Structures.*
- 3 Development should not obstruct or impair:
 - (a) navigation and access channels
 - (b) maintenance activities of marine infrastructure including revetment walls
 - (c) the operation of wharves.
- 4 Safe public access should be provided or maintained to:
 - (a) the waterfront
 - (b) known diving areas
 - (c) jetties, wharves and associated activities.
- 5 Marinas should be designed to:
 - (a) facilitate water circulation and exchange
 - (b) maximise the penetration of sunlight into the water.

Medium and High Rise Development (3 or More Storeys)

OBJECTIVES

- 1 Medium and high rise development that provides housing choice and employment opportunities.
- 2 Residential development that provides a high standard of amenity and adaptability for a variety of accommodation and living needs.
- 3 Commercial, office and retail development that is designed to create a strong visual connection to the public realm and that contributes to the vitality of the locality.
- 4 Buildings designed and sited to be energy and water efficient.

PRINCIPLES OF DEVELOPMENT CONTROL

Design and Appearance

- 1 Buildings should:
 - (a) achieve a human scale at ground level through the use of elements such as canopies, verandas or building projections
 - (b) provide shelter over the footpath where minimal setbacks are desirable
 - (c) ensure walls on the boundary that are visible from public land include visually interesting treatments to break up large blank facades.
- 2 The ground floor level of buildings (including the foyer areas of residential buildings) should be designed to enable surveillance from public land to the inside of the building at night.
- 3 Entrances to multi-storey buildings should:
 - (a) be oriented towards the street
 - (b) be clearly identifiable
 - (c) provide shelter, a sense of personal address and transitional space around the entry
 - (d) provide separate access for residential and non-residential land uses.

Visual Privacy

- 4 The visual privacy of ground floor dwellings within multi-storey buildings should be protected through the use of design features such as the elevation of ground floors above street level, setbacks from street and the location of verandas, windows porticos or the like.

Building Separation and Outlook

- 5 Residential buildings (or the residential floors of mixed use buildings) should:
 - (a) have adequate separation between habitable room windows and balconies from other buildings to provide visual and acoustic privacy for dwelling occupants and allow the infiltration of daylight into interior and outdoor spaces
 - (b) ensure living rooms have, at a minimum, a satisfactory short range visual outlook to public or communal space.

Dwelling Configuration

- 6 Buildings comprising more than 20 dwellings should provide a variety of dwelling sizes and a range in the number of bedrooms per dwelling.
- 7 Dwellings with 3 or more bedrooms located on the ground floor of medium and high rise buildings should, where possible, have the windows of habitable rooms overlooking internal courtyard space or other public space.

Adaptability

- 8 Multi-storey buildings should include a variety of internal designs that will facilitate adaptive reuse.

Environmental

- 9 Multi-storey buildings should:
 - (a) minimise detrimental micro-climatic and solar access impacts on adjacent land or buildings, including effects of patterns of wind, temperature, daylight, sunlight, glare and shadow
 - (b) incorporate roof designs that enable the provision of rain water tanks (where they are not provided elsewhere), photovoltaic cells and other features that enhance sustainability.
- 10 Green roofs (which can be a substitute for private or communal open space provided they can be accessed by occupants of the building) are encouraged on all new residential, commercial or mixed use buildings.
- 11 Development of 5 or more storeys, or 21 metres or more in building height (excluding the rooftop location of mechanical plant and equipment), should be designed to minimise the risk of wind tunnelling effects on adjacent streets by adopting one or more of the following:
 - (a) a podium at the base of a tall tower and aligned with the street to deflect wind away from the street
 - (b) substantial verandas around a building to deflect downward travelling wind flows over pedestrian areas
 - (c) the placement of buildings and use of setbacks to deflect the wind at ground level.

Site Facilities and Storage

- 12 Dwellings should provide a covered storage area of not less than 8 cubic metres in one or more of the following areas:
 - (a) in the dwelling (but not including a habitable room)
 - (b) in a garage, carport or outbuilding
 - (c) within an on-site communal facility.
- 13 Development should provide a dedicated area for the on-site collection and sorting of recyclable materials and refuse.
- 14 Development with a gross floor area of 2000 square metres or more should provide for the communal storage and management of waste.

Metropolitan Open Space System

OBJECTIVES

- 1 A Metropolitan Open Space System (MOSS), in and around metropolitan Adelaide, that:
 - (a) is comprised of public and private land
 - (b) is clearly defined and linked
 - (c) has an open or natural character
 - (d) provides a visual and scenic contrast to the built urban environment
 - (e) separates different parts of the metropolitan area.
- 2 Conservation and restoration of existing and modified habitats.
- 3 Conservation of sites of scientific or heritage interest.
- 4 The provision of corridors for the movement of wildlife.
- 5 A range of recreation and leisure areas including a network of recreation trails integrating MOSS and adjoining land uses.
- 6 A range of active/**passive** recreation and sporting facilities of regional or State significance, including facilities that can be used for national and international events.
- 7 Stormwater management in association with recreation, aquifer recharge and water quality management.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should preserve and enhance the natural and open character and amenity of land located within the MOSS.
- 2 Development within the MOSS should contribute to the rehabilitation and restoration of aquatic and terrestrial ecosystems and water catchments.
- 3 Landscaping within MOSS locations should:
 - (a) incorporate remnant vegetation
 - (b) use locally indigenous plant species wherever possible
 - (c) provide shade and windbreaks along cyclist and pedestrian routes and around picnic and barbecue areas, seating and car parking areas
 - (d) maximise opportunities for passive surveillance along the park.

- 4 Development on public land within the MOSS should incorporate:
- (a) easily identified access points
 - (b) pedestrian and cycle linkages and horse trails within open spaces and between other open space networks
 - (c) facilities to provide and support a range of recreation and leisure activities.

Natural Resources

OBJECTIVES

- 1 Retention, protection and restoration of the natural resources and environment.
- 2 Protection of the quality and quantity of South Australia's surface waters, including inland, *marine and estuarine* and underground waters.
- 3 The ecologically sustainable use of natural resources including water resources, including *marine waters*, ground water, surface water and watercourses.
- 4 Natural hydrological systems and environmental flows reinstated, and maintained and enhanced.
- 5 Development consistent with the principles of water sensitive design.
- 6 Development sited and designed to:
 - (a) protect natural ecological systems
 - (b) achieve the sustainable use of water
 - (c) protect water quality, including receiving waters
 - (d) reduce runoff and peak flows and prevent the risk of downstream flooding
 - (e) minimise demand on reticulated water supplies
 - (f) maximise the harvest and use of stormwater
 - (g) protect stormwater from pollution sources.
- 7 Storage and use of stormwater which avoids adverse impact on public health and safety.
- 8 Native flora, fauna and ecosystems protected, retained, conserved and restored.
- 9 Restoration, expansion and linking of existing native vegetation to facilitate habitat corridors for ease of movement of fauna.
- 10 Minimal disturbance and modification of the natural landform.
- 11 Protection of the physical, chemical and biological quality of soil resources.
- 12 Protection of areas prone to erosion or other land degradation processes from inappropriate development.
- 13 Protection of the scenic qualities of natural and rural landscapes.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should be undertaken with minimum impact on the natural environment, including air and water quality, land, soil, biodiversity, and scenically attractive areas.
- 2 Development should ensure that South Australia's natural assets, such as biodiversity, water and soil, are protected and enhanced.

- 3 Development should not significantly obstruct or adversely affect sensitive ecological areas such as creeks, wetlands, estuaries and significant seagrass and mangrove communities.
- 4 Development should be appropriate to land capability and the protection and conservation of water resources and biodiversity.

Water Sensitive Design

- 5 Development should be designed to maximise conservation, minimise consumption and encourage re-use of water resources.
- 6 Development should not take place if it results in unsustainable use of surface or underground water resources.
- 7 Development should be sited and designed to:
 - (a) capture and re-use stormwater, where practical
 - (b) minimise surface water runoff
 - (c) prevent soil erosion and water pollution
 - (d) protect and enhance natural water flows
 - (e) protect water quality by providing adequate separation distances from watercourses and other water bodies
 - (f) not contribute to an increase in salinity levels
 - (g) avoid the water logging of soil or the release of toxic elements
 - (h) maintain natural hydrological systems and not adversely affect:
 - (i) the quantity and quality of groundwater
 - (ii) the depth and directional flow of groundwater
 - (iii) the quality and function of natural springs.
- 8 Water discharged from a development site should:
 - (a) be of a physical, chemical and biological condition equivalent to or better than its pre-developed state
 - (b) not exceed the rate of discharge from the site as it existed in pre-development conditions.
- 9 Development should include stormwater management systems to protect it from damage during a minimum of a 1-in-100 year average return interval flood.
- 10 Development should have adequate provision to control any stormwater over-flow runoff from the site and should be sited and designed to improve the quality of stormwater and minimise pollutant transfer to receiving waters.
- 11 Development should include stormwater management systems to mitigate peak flows and manage the rate and duration of stormwater discharges from the site to ensure the carrying capacities of downstream systems are not overloaded.
- 12 Development should include stormwater management systems to minimise the discharge of sediment, suspended solids, organic matter, nutrients, bacteria, litter and other contaminants to the stormwater system.

- 13 Stormwater management systems should preserve natural drainage systems, including the associated environmental flows.
- 14 Stormwater management systems should:
 - (a) maximise the potential for stormwater harvesting and re-use, either on-site or as close as practicable to the source
 - (b) utilise, but not be limited to, one or more of the following harvesting methods:
 - (i) the collection of roof water in tanks
 - (ii) the discharge to open space, landscaping or garden areas, including strips adjacent to car parks
 - (iii) the incorporation of detention and retention facilities
 - (iv) aquifer recharge.
- 15 Where it is not practicable to detain or dispose of stormwater on site, only clean stormwater runoff should enter the public stormwater drainage system.
- 16 Artificial wetland systems, including detention and retention basins, should be sited and designed to:
 - (a) ensure public health and safety is protected
 - (b) minimise potential public health risks arising from the breeding of mosquitoes.

Water Catchment Areas

- 17 Development should ensure watercourses and their beds, banks, wetlands and floodplains are not damaged or modified and are retained in their natural state, except where modification is required for essential access or maintenance purposes.
- 18 No development should occur where its proximity to a swamp or wetland will damage or interfere with the hydrology or water regime of the swamp or wetland.
- 19 A wetland or low-lying area providing habitat for native flora and fauna should not be drained, except temporarily for essential management purposes to enhance environmental values.
- 20 Along watercourses, areas of remnant native vegetation, or areas prone to erosion, that are capable of natural regeneration should be fenced off to limit stock access.
- 21 Development such as cropping, intensive animal keeping, residential, tourism, industry and horticulture, that increases the amount of surface run-off should include a strip of land at least 20 metres wide measured from the top of existing banks on each side of a watercourse that is:
 - (a) fenced to exclude livestock
 - (b) kept free of development, including structures, formal roadways or access ways for machinery or any other activity causing soil compaction or significant modification of the natural surface of the land
 - (c) revegetated with locally indigenous vegetation comprising trees, shrubs and other groundcover plants to filter run-off so as to reduce the impacts on native aquatic ecosystems and to minimise soil loss eroding into the watercourse.

- 22 Development resulting in the depositing of an object or solid material in a watercourse or floodplain or the removal of bank and bed material should not:
- (a) adversely affect the migration of aquatic biota
 - (b) adversely affect the natural flow regime
 - (c) cause or contribute to water pollution
 - (d) result in watercourse or bank erosion
 - (e) adversely affect native vegetation upstream or downstream that is growing in or adjacent to a watercourse.
- 23 The location and construction of dams, water tanks and diversion drains should:
- (a) occur off watercourse
 - (b) not take place in ecologically sensitive areas or on erosion-prone sites
 - (c) provide for low flow by-pass mechanisms to allow for migration of aquatic biota
 - (d) not negatively affect downstream users
 - (e) minimise in-stream or riparian vegetation loss
 - (f) incorporate features to improve water quality (eg wetlands and floodplain ecological communities)
 - (g) protect ecosystems dependent on water resources.
- 24 Irrigated horticulture and pasture should not increase groundwater-induced salinity.
- 25 Development should comply with the current *Environment Protection (Water Quality) Policy*.

Biodiversity and Native Vegetation

- 26 Development should retain existing areas of native vegetation and where possible contribute to revegetation using locally indigenous plant species.
- 27 Development should be designed and sited to minimise the loss and disturbance of native flora and fauna, including marine animals and plants, and their breeding grounds and habitats.
- 28 Native vegetation should be conserved and its conservation value and function not compromised by development if the native vegetation does any of the following:
- (a) provides an important habitat for wildlife or shade and shelter for livestock
 - (b) has a high plant species diversity or includes rare, vulnerable or endangered plant species or plant associations and communities
 - (c) provides an important seed bank for locally indigenous vegetation
 - (d) has high amenity value and/or significantly contributes to the landscape quality of an area, including the screening of buildings and unsightly views
 - (e) has high value as a remnant of vegetation associations characteristic of a district or region prior to extensive clearance for agriculture
 - (f) is growing in, or is characteristically associated with a wetland environment.

- 29 Native vegetation should not be cleared if such clearing is likely to lead to, cause or exacerbate any of the following:
- (a) erosion or sediment within water catchments
 - (b) decreased soil stability
 - (c) soil or land slip
 - (d) deterioration in the quality of water in a watercourse or surface water runoff
 - (e) a local or regional salinity problem
 - (f) the occurrence or intensity of local or regional flooding.
- 30 Development that proposes the clearance of native vegetation should address or consider the implications that removing the native vegetation will have on the following:
- (a) provision for linkages and wildlife corridors between significant areas of native vegetation
 - (b) erosion along watercourses and the filtering of suspended solids and nutrients from run-off
 - (c) the amenity of the locality
 - (d) bushfire safety
 - (e) the net loss of native vegetation and other biodiversity.
- 31 Where native vegetation is to be removed, it should be replaced in a suitable location on the site with locally indigenous vegetation to ensure that there is not a net loss of native vegetation and biodiversity.
- 32 Development should be located and occur in a manner which:
- (a) does not increase the potential for, or result in, the spread of pest plants, or the spread of any non-indigenous plants into areas of native vegetation or a conservation zone
 - (b) avoids the degradation of remnant native vegetation by any other means including as a result of spray drift, compaction of soil, modification of surface water flows, pollution to groundwater or surface water or change to groundwater levels
 - (c) incorporates a separation distance and/or buffer area to protect wildlife habitats and other features of nature conservation significance.
- 33 Development should promote the long-term conservation of vegetation by:
- (a) avoiding substantial structures, excavations, and filling of land in close proximity to the trunk of trees and beneath their canopies
 - (b) minimising impervious surfaces beneath the canopies of trees
 - (c) taking other effective and reasonable precautions to protect both vegetation and the integrity of structures and essential services.
- 34 Horticulture involving the growing of olives should be located at least:
- (a) 500 metres from:
 - (i) a national park
 - (ii) a conservation park

- (iii) a wilderness protection area
 - (iv) the edge of a substantially intact stratum of native vegetation greater than 5 hectares in area
 - (b) 50 metres from the edge of stands of native vegetation 5 hectares or less in area.
- 35 Horticulture involving the growing of olives should have at least one locally indigenous tree that will grow to a height of at least 7 metres sited at least every 100 metres around the perimeter of the orchard.

Soil Conservation

- 36 Development should not have an adverse impact on the natural, physical, chemical or biological quality and characteristics of soil resources.
- 37 Development should be designed and sited to prevent erosion.
- 38 Development should take place in a manner that will minimise alteration to the existing landform.
- 39 Development should minimise the loss of soil from a site through soil erosion or siltation during the construction phase of any development and following the commencement of an activity.

Open Space and Recreation

OBJECTIVES

- 1 The creation of a network of linked parks, reserves, recreational trails and recreation areas at State, regional, district, neighbourhood and local levels.
- 2 Pleasant, functional and accessible open spaces providing a range of physical environments.
- 3 A wide range of settings for active and passive recreational opportunities.
- 4 The provision of open space in the following hierarchy:
 - State
 - Regional
 - District
 - Neighbourhood
 - Local.
- 5 Major open spaces developed as the focal point for active and passive recreation, notably the Glenelg Oval and environs, Kingston Park, the Patawalonga, Sturt River, and the Colley, Wigley, John Miller, Angus Neill and Kauri Parade reserves.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Urban development should include public open space and recreation areas.
- 2 Public open space and recreation areas should be of a size, dimension and location that:
 - (a) facilitate a range of formal and informal recreation activities
 - (b) provide for the movement of pedestrians and cyclists
 - (c) incorporate existing vegetation and natural features, watercourses, wildlife habitat and other sites of natural or cultural value
 - (d) link habitats, wildlife corridors, public open spaces and existing recreation facilities
 - (e) enable effective stormwater management
 - (f) provides for the planting and retention of large trees and vegetation.
- 3 Open space should be designed to incorporate:
 - (a) pedestrian, cycle linkages to other open spaces, centres, schools and public transport nodes
 - (b) park furniture, shaded areas and resting places to enhance pedestrian comfort
 - (c) safe crossing points where pedestrian routes intersect the road network
 - (d) easily identified access points
 - (e) frontage to abutting public roads to optimise pedestrian access and visibility
 - (f) re-use of stormwater for irrigation purposes

- (g) recreational trails where appropriate.
- 4 Where practical, access points to regional parks should be located close to public transport.
- 5 District level parks should be at least 3 hectares in size, and provided within 2 kilometres of all households that they serve.
- 6 Neighbourhood parks should be at least 0.5 hectares and generally closer to 1 hectare in size, and provided within 500 metres of households that they serve.
- 7 Local parks should be:
 - (a) a minimum of 0.2 hectares in size
 - (b) centrally located within a residential area, close to schools, shops and generally within 300 metres of households that they serve.
- 8 No more than 20 per cent of land allocated as public open space should:
 - (a) have a slope in excess of 1-in-4
 - (b) comprise creeks or other drainage areas.
- 9 Signage should be provided at entrances to and within public open space to provide clear orientation to major points of interest such as the location of public toilets, telephones, safe routes and park activities.
- 10 Buildings in open space, including structures and associated car parking areas, should be designed, located and of a scale that is unobtrusive and does not detract from the desired open space character.
- 11 Development in open space should:
 - (a) be clustered where practical to ensure that the majority of the site remains open
 - (b) where practical, be developed for multi-purpose use
 - (c) be constructed to minimise the extent of hard paved areas.
- 12 Open spaces and recreation areas should be located and designed to maximise safety and security by:
 - (a) ensuring that within urban areas, their edges are overlooked by housing, commercial or other development that can provide effective informal surveillance
 - (b) ensuring fenced parks and playgrounds have more than one entrance or exit when fenced
 - (c) locating play equipment where it can be informally observed by nearby residents and users during times of use
 - (d) clearly defining the perimeters of play areas
 - (e) providing lighting around facilities such as toilets, telephones, seating, litter bins, bike storage and car parks
 - (f) focusing pedestrian and bicycle movement after dark along clearly defined, adequately lit routes with observable entries and exits.
- 13 Landscaping associated with open space and recreation areas should:
 - (a) not compromise the drainage function of any drainage channel

- (b) provide shade and windbreaks along cyclist and pedestrian routes, around picnic and barbecue areas and seating, and in car parking areas
 - (c) maximise opportunities for informal surveillance throughout the park
 - (d) enhance the visual amenity of the area and complement existing buildings
 - (e) be designed and selected to minimise maintenance costs
 - (f) provide habitat for local fauna.
- 14 Development of recreational activities in areas not zoned for that purpose should be compatible with surrounding activities.
- 15 Recreation facilities development should be sited and designed to minimise negative impacts on the amenity of the locality.

Orderly and Sustainable Development

OBJECTIVES

- 1 Orderly and economical development that creates a safe, convenient and pleasant environment in which to live.
- 2 Development occurring in an orderly sequence and in a compact form to enable the efficient provision of public services and facilities.
- 3 Development that does not jeopardise the continuance of adjoining authorised land uses.
- 4 Development that does not prejudice the achievement of the provisions of the Development Plan.
- 5 Development abutting adjoining Council areas having regard to the policies of that Council's Development Plan.
- 6 Gateways at the entry to the city on Tapleys Hill Road, Anzac Highway, Oaklands/Diagonal Road and Brighton Road, Seacliff.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should not prejudice the development of a zone for its intended purpose.
- 2 The economic base of the region should be expanded in a sustainable manner.
- 3 Urban development should form a compact extension to an existing built-up area.
- 4 Ribbon development should not occur along the coast, water frontages or arterial roads shown in Overlay Maps -Transport.
- 5 Development should be located and staged to achieve the economical provision of public services and infrastructure, and to maximise the use of existing services and infrastructure.
- 6 Where development is expected to impact upon the existing infrastructure network (including the transport network), development should demonstrate how the undue effect will be addressed.
- 7 Vacant or underutilised land should be developed in an efficient and co-ordinated manner to not prejudice the orderly development of adjacent land.
- 8 Development in localities having an inappropriate or unsatisfactory layout, or unhealthy or obsolete development should improve or rectify those conditions.
- 9 Development should be undertaken in accordance with the following Structure Plan Map and Concept Plan Maps:
 - (a) [Structure Plan Map HoB/1 - Holdfast Bay](#)
 - (b) [Structure Plan Map HoB/2 - Brighton and Hove District Centre](#)
 - (c) [Concept Plan Map HoB/1 - Jetty Road and Moseley Square](#)
 - (d) [Concept Plan Map HoB/2 - Car Parking Areas](#)

- (e) [Concept Plan Map HoB/3 - Foreshore and Patawalonga](#)
- (f) [Concept Plan Map HoB/4 - Buckle Street](#)
- (g) [Concept Plan Map HoB/5 - Extent - Glenelg Foreshore and Patawalonga Zone](#)
- (h) [Concept Plan Map HoB/6 - Minda Incorporated Brighton Campus](#)
- (i) [Concept Plan Map HoB/7 - Seacliff Park](#)

Regulated Trees

OBJECTIVES

- 1 The conservation of regulated trees that provide important aesthetic and/or environmental benefit.
- 2 Development in balance with preserving regulated trees that demonstrate one or more of the following attributes:
 - (a) significantly contributes to the character or visual amenity of the locality
 - (b) indigenous to the locality
 - (c) a rare or endangered species
 - (d) an important habitat for native fauna.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should have minimum adverse effects on regulated trees.
- 2 A regulated tree should not be removed or damaged other than where it can be demonstrated that one or more of the following apply:
 - (a) the tree is diseased and its life expectancy is short
 - (b) the tree represents an unacceptable risk to public or private safety
 - (c) the tree is causing damage to a substantial building or structure of value
 - (d) development that is reasonable and expected would not otherwise be possible
 - (e) the work is required for the removal of dead wood, treatment of disease, or is in the general interests of the health of the tree.
- 3 Tree damaging activity other than removal should seek to maintain the health, aesthetic appearance and structural integrity of the tree.

Renewable Energy Facilities

OBJECTIVES

- 1 The development of renewable energy facilities, such as wind and biomass energy facilities, in appropriate locations.
- 2 Location, siting, design and operation of renewable energy facilities to avoid or minimise adverse impacts and maximise positive impacts on the environment, the local community and the State.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Renewable energy facilities, including wind farms and ancillary developments, should be located in areas that maximise efficient generation and supply of electricity.
- 2 Wind farms and ancillary development such as substations, maintenance sheds, access roads and connecting power-lines, should be sited, designed and operated in a manner that:
 - (a) avoids or minimises negative impacts on the character, landscape quality, visual significance or amenity of the area
 - (b) uses elements of the landscape and appropriate materials and finishes to minimise visual impact
 - (c) avoids or minimises the potential for adverse impact on areas of native vegetation, conservation, environmental, geological, tourism or built or natural heritage significance
 - (d) does not impact on the safety of water or air transport and the operation of ports, airfields and designated landing strips
 - (e) avoids or minimises nuisance or hazard to nearby property owners and/or occupiers, road users and wildlife by not:
 - (i) causing shadowing, flickering, reflection or blade glint impacts
 - (ii) creating excessive noise
 - (iii) interfering with television and radio signals
 - (iv) modifying vegetation, soils and habitats
 - (v) striking birds or bats.

Residential Development

OBJECTIVES

- 1 Safe, convenient, pleasant and healthy-living environments that meet the full range of needs and preferences of the community.
- 2 A diverse range of dwelling types and sizes available to cater for changing demographics, particularly smaller household sizes and supported accommodation.
- 3 Medium and high-density residential development in areas close to activity centres, public and community transport and public open spaces.
- 4 The revitalisation of residential areas to support the viability of community services and infrastructure.
- 5 Affordable housing, student housing and housing for aged persons provided in appropriate locations.
- 6 Increased affordable housing opportunities through land division and the conversion of buildings to a residential use.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Residential allotments and sites should maximise solar orientation and have the area and dimensions to accommodate:
 - (a) the siting and construction of a dwelling and associated ancillary outbuildings
 - (b) the provision of landscaping and private open space
 - (c) convenient and safe vehicle, pedestrian and cycling access and parking
 - (d) water sensitive design systems that enable the storage, treatment and reuse of stormwater.
- 2 Buildings on **hammerhead**/battleaxe allotments or the like should be single storey and be designed to maintain the privacy of adjoining properties.
- 3 Residential allotments should be of varying sizes to encourage housing diversity.

Design and Appearance

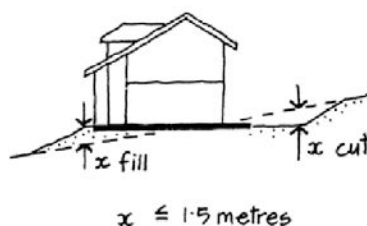
- 4 Dwellings and accommodation at ground floor level should contribute to the character of the locality and create active, safe streets by incorporating one or more of the following:
 - (a) front landscaping or terraces that contribute to the spatial and visual structure of the street while maintaining adequate privacy for occupants
 - (b) individual entries for ground floor accommodation
 - (c) opportunities to overlook adjacent public space.
- 5 Residential development should be designed to ensure living rooms have an external outlook.
- 6 Entries to dwellings should be clearly visible from the streets that they front to enable visitors to identify a specific dwelling easily.

- 7 The roof form and design of a semi-detached dwelling in a locality where the predominant dwelling type is detached dwellings, should achieve the appearance of a detached dwelling as illustrated by the figure below:



Design and roof form of semi-detached dwellings to provide streetscape variety

- 8 The vertical distance between any lower floor of a building and the natural ground level should not exceed 1.5 metres at any point to minimise the depth of excavation and/or height of filling of land, as illustrated by the figure below:



- 9 Development on sloping sites should be designed to reduce the bulk and scale of the proposed development and where possible include a split level design that follows the fall of the land.

Overshadowing

- 10 The design and location of buildings should ensure that direct winter sunlight is available to adjacent dwellings, with particular consideration given to:
- (a) windows of habitable rooms (all rooms excluding bathrooms, laundries and hallways), particularly living areas
 - (b) ground-level private open space

- (c) upper-level private balconies that provide the primary open space area for any dwelling
 - (d) access to solar energy.
- 11 Development should ensure that north-facing windows to habitable rooms (**all rooms excluding bathrooms, laundries and hallways**) of existing dwelling(s) on the same allotment, and on adjacent allotments, receive at least 3 hours of direct sunlight over a portion of their surface between 9 am and 5 pm on the 21 June.
- 12 Development should ensure that ground-level open space of existing buildings receives direct sunlight for a minimum of two hours between 9 am and 3 pm on 21 June to at least the smaller of the following:
- (a) half of the existing ground-level open space
 - (b) 35 square metres of the existing ground-level open space (with at least one of the area's dimensions measuring 2.5 metres).

Development should not increase the overshadowed area by more than 20 per cent in cases where overshadowing already exceeds these requirements.

Garages, Carports and Outbuildings

- 13 Garages, carports and outbuildings should have a roof form and pitch, building materials and detailing that complement the associated dwelling.
- 14 **Garages and carports facing the street should not dominate the streetscape and should be designed in accordance with the following (except where facing a laneway in the **Suburban Neighbourhood Zone**):**
- (a) have a maximum total width of garage or carport openings of 6 metres or 50 per cent of the dwelling frontage width, whichever is the lesser
 - (b) be located at least 0.5 metres behind the main face of the associated dwelling
 - (c) where it is in the form of an enclosed double carport or garage, be setback at least 8 metres from the primary road frontage and incorporate one of the following:
 - (i) two individual doors with a distance of not less than 300 millimetres between them
 - (ii) double tilt-up doors with moulded door panels having a maximum width of no more than 5 metres
 - (d) be constructed of materials that integrate with those of the associated dwelling, or pre-coloured treated metal.
- 15 **Garages, carports and domestic outbuildings should be set back from the secondary road frontage in line with or greater than the setback of the associated dwelling.**
- 16 Residential outbuildings, including garages and sheds, should not be constructed unless in association with an existing dwelling.

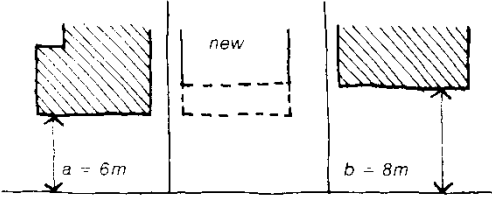
- 17 Garages, carports and outbuildings should be designed within the following parameters (except within the **Suburban Neighbourhood Zone**):

| Parameter | Total floor area (maximum) | Wall height above natural ground level | Wall length | Maximum height | Setback from any existing structure on the site located on the same boundary |
|--|---|--|---|----------------|--|
| Within 3 metres of a side or rear boundary | Sites 600 square metres or more: 60 square metres Sites 400-600 square metres: 40 square metres Sites less than 400 square metres: 30 square metres | 3 metres | 9 metres | 4.5 metres | n/a |
| On a side or rear boundary | Sites 600 square metres or more: 60 square metres Sites 400-600 square metres: 40 square metres Sites less than 400 square metres: 30 square metres | 3 metres | 8 metres, provided the total length of all existing and proposed boundary walls does not exceed 30 per cent of the total common boundary length | 4.5 metres | 6 metres |

- 18 An outbuilding should not reduce the area of useable private open space by less than 80 per cent of that required by the relevant zone, policy area or precinct.

Street and Boundary Setbacks

- 19 Except where specified in a particular zone, policy area or precinct or **Residential High Density Zone**, the main face of a building should be set back from the primary road frontage in accordance with the following table:

| Setback difference between buildings on adjacent allotments with frontage to the same primary street | Setback of new building |
|--|---|
| Up to 2 metres | The same setback as one of the adjacent buildings, as illustrated below:  <i>When $b - a < 2$, setback of new dwelling = a or b</i> |
| Greater than 2 metres | At least the average setback of the adjacent buildings. |

- 20 Dwelling setbacks from side and rear boundaries should be progressively increased as the height of the building increases to:

- (a) minimise the visual impact of buildings from adjoining properties
- (b) minimise the overshadowing of adjoining properties.

- 21 Residential development (other than where located on a boundary) should be setback from side and rear boundaries in accordance with the following parameters (except within the **Suburban Neighbourhood Zone**):

| Parameter | Value |
|--|--|
| Side walls with a height up to (and including) 3 metres at any point above the natural ground level | 1 metre |
| Side walls with a height exceeding 3 metres and up to (and including) 6 metres at any point above the natural ground | 1.5 metres plus an additional 500 millimetres for every metre in height above 4 metres |
| Side walls greater than 6 metres at any point above the natural ground level | 2.5 metres plus the increase in wall height above 6 metres |
| Rear boundary setback for single storey buildings with a wall height 3 metres or less above natural ground level | 4 metres |
| Rear boundary setback for a building of two or more storeys with a wall height more than 3 metres above natural ground level | 6 metres |

- 22 Dwellings and or residential flat buildings on an allotment in the form of a hammerhead/battleaxe configuration should:

- (a) be setback a minimum of 4 metres from the side boundary to the two storey component of any part of the building
- (b) contain sufficient area on the allotment for a vehicle to turn around to enable it to egress the allotment in a forward direction
- (c) enhance the streetscape character by:
 - (i) ensuring that no fencing is located between the main face of the dwelling and the front property boundary
 - (ii) providing additional landscaping and/or deviation of the driveway where a driveway length exceeds 20 metres
- (d) minimise impact on the amenity of neighbouring properties by locating the driveway:
 - (i) on the side of the property that is adjacent to an existing neighbouring garage, carport or driveway
 - (ii) a minimum of 2 metres from a bedroom window of an existing dwelling on neighbouring land where separated by a fence measuring a minimum of 1.8 metres above the driveway pavement.

- 23 Side boundary walls in residential areas should be limited in length and height to:

- (a) minimise their visual impact on adjoining properties
- (b) minimise the overshadowing of adjoining properties.

- 24 Walls associated with a dwelling located on a side boundary should be designed in accordance with the following parameters (except within the **Suburban Neighbourhood Zone**):
- (a) a height not exceeding 3 metres above natural ground level
 - (b) a length not exceeding 8 metres
 - (c) the wall, when its length is added to the length of any other relevant walls or structures located on that boundary:
 - (i) will not result in all such relevant walls and structures exceeding a length equal to 45 per cent of the length of the boundary
 - (ii) will not be within 3 metres of any other relevant wall or structure located along the boundary, except where the side wall is located immediately abutting the wall of an existing or simultaneously constructed building on the adjoining site and is constructed to the same or to a lesser length and height.
- 25 Walls with a height of up to (and including) 3 metres above natural ground level (excluding veranda, porch and balcony structures) should be setback 2 metres from the secondary street frontage (except within the **Suburban Neighbourhood Zone**).
- 26 Walls with a height of more than 3 metres above natural ground level (excluding veranda, porch and balcony structures) should be setback 4 metres from the secondary street frontage (except within the **Suburban Neighbourhood Zone**).
- 27 Carports and garages should be set back from road and building frontages so as to:
- (a) contribute to the desired character of the area
 - (b) not adversely impact on the safety of road users
 - (c) provide safe entry and exit
 - (d) not dominate the appearance of dwellings from the street.

Site Coverage

- 28 Site coverage (the proportion of a site covered by ground floor level buildings and structures including dwelling, garage, carport, verandas and outbuildings but excluding unroofed pergolas and unroofed balconies) should not exceed the following values (except within the **Suburban Neighbourhood Zone**):

| Parameter | Value |
|---|-------------|
| Site with an area less than or equal to 300 square metres | 60 per cent |
| Site with an area greater than 300 square metres | 50 per cent |

- 29 Site coverage should ensure sufficient space is provided for:
- (a) pedestrian and vehicle access and vehicle parking
 - (b) domestic storage
 - (c) outdoor clothes drying
 - (d) rainwater tanks
 - (e) private open space and landscaping (f) convenient storage of household waste and recycling receptacles.

- 30 Site coverage determinations for group dwellings and residential flat building sites should not include common areas such as access ways, driveways and landscaping.

Private Open Space

- 31 Private open space (land available for exclusive use by residents of each dwelling) should be provided for each dwelling (including a dwelling within a residential flat building) and should be sited and designed:
- (a) to be accessed directly from the habitable rooms of the dwelling
 - (b) to be generally at ground level (other than for residential flat buildings) and to the side or rear of a dwelling and screened for privacy
 - (c) to take advantage of, but not adversely affect, natural features of the site
 - (d) to minimise overlooking from adjacent buildings
 - (e) to achieve separation from bedroom windows on adjoining sites
 - (f) to have a northerly aspect to provide for comfortable year-round use
 - (g) to not be significantly shaded during winter by the associated dwelling or adjacent development
 - (h) to be shaded in summer
 - (i) to minimise noise and air quality impacts that may arise from traffic, industry or other business activities within the locality
 - (j) to have sufficient area and shape to be functional, taking into consideration the location of the dwelling, and the dimension and gradient of the site.
- 32 Dwellings and residential flat buildings at ground level should include private open space that conforms to the requirements identified in the following table (except within the **Suburban Neighbourhood Zone**):

| Site area of dwelling | Minimum area of private open space | Provisions |
|------------------------------|------------------------------------|---|
| 250 square metres or greater | 20 per cent of site area | <p>Balconies, roof patios, decks and the like, can comprise part of this area provided the area of each is 10 square metres or greater.</p> <p>One part of the space should be directly accessible from a kitchen, lounge room, dining room or living room (excluding a bedroom) and have an area equal to or greater than 10 per cent of the site area with a minimum dimension of 5 metres and a maximum gradient of 1-in-10.</p> |
| Less than 250 square metres | 35 square metres | <p>Balconies, roof patios and the like can comprise part of this area provided the area of each is 8 square metres or greater.</p> <p>One part of the space is directly accessible from a kitchen, lounge room, dining room or living room (excluding a bedroom) and has an area of 16 square metres with a minimum dimension of 4 metres and a maximum gradient of 1-in-10.</p> |

- 33 Private open space should not include driveways, front yards (except where it is a group dwelling that has no frontage to a public road and the private open space is screened from adjacent dwellings), effluent drainage areas, rubbish bin storage, sites for rainwater tanks and other utility areas and common areas such as parking areas and communal open space.
- 34 Private open space at ground level should be designed to provide a consolidated area of deep soil (an area of natural ground which excludes areas where there is a structure underneath, pools and non-permeable paved areas) to:
- (a) assist with ease of drainage
 - (b) allow for effective deep planting
 - (c) reduce urban heat loading and improve micro-climatic conditions around sites and buildings.
- 35 Except where otherwise specified, dwellings located above ground level should provide private open space in accordance with the following table:

| Dwelling type | Minimum area of private open space |
|---|------------------------------------|
| Studio (where there is no separate bedroom) | No minimum requirement |
| One bedroom dwelling | 8 square metres |
| Two bedroom dwelling | 11 square metres |
| Three + bedroom dwelling | 15 square metres |

- 36 Private open space located above ground level should have a minimum dimension of 2 metres and be directly accessible from a habitable room.
- 37 Private open space may be substituted for the equivalent area of communal open space where:
- (a) at least 50 per cent of the communal open space is visually screened from public areas of the development
 - (b) ground floor communal space is overlooked by habitable rooms to facilitate passive surveillance
 - (c) it contains landscaping and facilities that are functional, attractive and encourage recreational use.

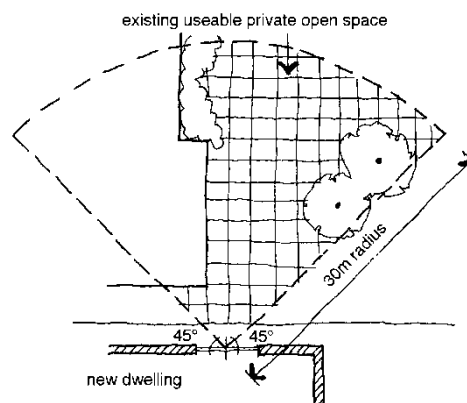
Communal Open Space

- 38 Communal open space should be shared by more than one dwelling, not be publicly accessible and exclude:
- (a) private open space
 - (b) public rights of way
 - (c) private streets
 - (d) parking areas and driveways
 - (e) service and storage areas
 - (f) narrow or inaccessible strips of land.

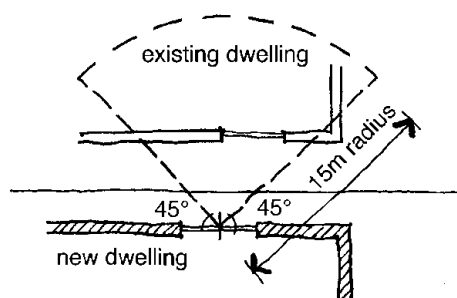
- 39 Communal open space should only be located on elevated gardens or roof tops where the area and overall design is useful for the recreation and amenity needs of residents and where it is designed to:
- (a) address acoustic, safety, security and wind effects
 - (b) minimise overlooking into habitable room windows or onto the useable private open space of other dwellings
 - (c) facilitate landscaping and food production
 - (d) be integrated into the overall facade and composition of buildings.

Visual Privacy

- 40 Except for buildings of 3 or more storeys in the Minda Incorporated Brighton Campus, upper level windows, balconies, terraces and decks that overlook habitable room windows or private open space of dwellings should maximise visual privacy through the use of measures such as sill heights of not less than 1.7 metres or permanent screens having a height of 1.7 metres above finished floor level.
- 41 Where development is greater than single storey (excluding the Minda Incorporated Brighton Campus):
- (a) any upper storey window that directly overlooks the private open space of an adjoining residential property that is within 30 metres from the vertical centre line of the overlooking window and beyond a 45 degree angle from the plane of the wall containing the overlooking window (as illustrated by the figure below) should be glazed in fixed obscure glass or have window sills a minimum of 1.7 metres above the upper floor level:



- (b) any upper storey window that directly overlooks habitable rooms (all rooms excluding bathrooms, laundries and hallways) of residential buildings that are within 15 metres from the vertical centre line of the overlooking window and beyond a 45 degree angle from the plane of the wall containing the overlooking window (as illustrated by the figure below) should be glazed in fixed obscure glass or have window sills a minimum of 1.7 metres above the upper floor level:



- (c) any upper storey balcony should be located and/or designed to avoid directly overlooking the private open space of adjoining residential properties and into habitable rooms (all rooms excluding bathrooms, laundries and hallways) of other dwellings.

Noise

- 42 Noise generated by fixed noise sources such as air conditioning units and pool pumps should be located, designed and attenuated to avoid causing potential noise nuisance to adjoining landowners and occupiers.
- 43 External noise and artificial light intrusion into bedrooms should be minimised by separating or shielding these rooms from:
- (a) active communal recreation areas, parking areas and vehicle access ways
 - (b) service equipment areas and fixed noise sources on the same or adjacent sites.
- 44 Residential development close to high noise sources (eg major roads, railway lines, tram lines, industry, and airports) should be designed to locate bedrooms, living rooms and private open spaces away from those noise sources, or protect these areas with appropriate noise attenuation measures.
- 45 Residential development on sites abutting established collector or higher order roads, **or on a road indicated within the following table**, should include front fences and walls that will supplement the noise control provided by the building facade:

| Collector roads | Higher order roads |
|---|-------------------------------------|
| Adelphi Terrace, Glenelg North | Augusta Street, Glenelg |
| Cedar Avenue, Brighton | Augusta Street, Glenelg |
| Commercial Road, Brighton | Bowker Street, North Brighton |
| Cudmore Street, Somerton Park | Cliff Street, Glenelg East |
| Dyson Street, Glenelg East | Dunrobin Road, North Brighton |
| Edwards Street, Brighton/South Brighton | Gordon Street, Glenelg |
| Elizabeth Street, Glenelg | King George Avenue, Somerton Park |
| Grove Street, Glenelg East | Lapthorne Street, Glenelg East |
| Harris Street, Glenelg East | Moseley Street, Glenelg |
| Hight Avenue, Brighton | Patawalonga Frontage, Glenelg North |
| Jetty Road, Brighton | Partridge Street, Glenelg |
| King George Avenue, Hove | Pier Street, Glenelg |
| Miller Street, Glenelg East | Scholefield Road, Kingston Park |
| Moore Street, Glenelg East | Sherlock Road, Kingston Park |
| Nile Street, Glenelg | The Crescent, Brighton |
| Old Tapleys Hill Road, Glenelg North | The Broadway, Glenelg South |
| Penzance Street, Glenelg | Wheatland Street, Seacliff |
| Rugless Terrace, Glenelg East | Whyte Street, Somerton Park |
| Scarborough Street, Somerton Park | |
| Sixth Avenue, Glenelg East | |
| Waterloo Street, Glenelg | |
| Wattle Avenue, Hove | |
| Wenlock Street, Brighton | |

- 46 The number of dwellings sharing a common internal pedestrian entry within a residential flat building should be minimised to limit noise generation in internal access ways.

Site Facilities and Storage

- 47 Site facilities for group dwellings, residential parks and residential flat buildings and should include:
- (a) mail box facilities sited close to the major pedestrian entrance to the site
 - (b) bicycle parking for residents and visitors
 - (c) household waste and recyclable material storage areas away from dwellings
 - (d) external clothes drying areas, which are readily accessible to each dwelling and complement the development and streetscape character for dwellings which do not incorporate ground level private open space
 - (e) a storage area of not less than 8 square metres for each dwelling.

Affordable Housing

- 48 Affordable housing should be well integrated and complementary in design and appearance to other dwellings within the development.

Dependent Accommodation

- 49 Dependent accommodation (ie accommodation where the living unit is connected to the same services of the main dwelling) should be developed on the same allotment as the existing dwelling only where:
- (a) the site is of adequate size and configuration and the minimum total site is 600 square metres
 - (b) the accommodation has a small floor area relative to the associated main dwelling with a floor area not exceeding 60 square metres
 - (c) adequate outdoor space of a minimum of 100 square metres is provided for the use of all occupants
 - (d) adequate on-site car parking is provided by one additional car parking space being provided on the site which can be used exclusively by the occupants of the dependent accommodation
 - (e) the building is designed to, and comprises colours and materials that will, complement the original dwelling.

Swimming Pools and Outdoor Spas

- 50 Swimming pools, outdoor spas and associated ancillary equipment and structures should be sited so as to protect the privacy and amenity of adjoining residential land.

Significant Trees

OBJECTIVES

- 1 The conservation of significant trees, in Metropolitan Adelaide, that provide important aesthetic and environmental benefit.
- 2 The conservation of significant trees in balance with achieving appropriate development.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should preserve the following attributes where a significant tree demonstrates at least one of the following attributes:
 - (a) makes an important contribution to the character or amenity of the local area; or
 - (b) is indigenous to the local area and its species is listed under the *National Parks and Wildlife Act 1972* as a rare or endangered native species
 - (c) represents an important habitat for native fauna
 - (d) is part of a wildlife corridor or a remnant area of native vegetation
 - (e) is important to the maintenance of biodiversity in the local environment
 - (f) forms a notable visual element to the landscape of the local area.
- 2 Development should be undertaken so that it has a minimum adverse effect on the health of a significant tree.
- 3 Significant trees should be preserved, and tree-damaging activity should not be undertaken, unless:
 - (a) in the case of tree removal, where at least one of the following apply:
 - (i) the tree is diseased and its life expectancy is short
 - (ii) the tree represents an unacceptable risk to public or private safety
 - (iii) the tree is within 20 metres of a residential, tourist accommodation or habitable building and is a bushfire hazard within a Bushfire Prone Area
 - (b) the tree is shown to be causing or threatening to cause substantial damage to a substantial building or structure of value
 - (c) all other reasonable remedial treatments and measures have been determined to be ineffective
 - (d) it is demonstrated that all reasonable alternative development options and design solutions have been considered to prevent substantial tree-damaging activity occurring.
 - (e) in any other case, any of the following circumstances apply:
 - (i) the work is required for the removal of dead wood, treatment of disease, or is in the general interests of the health of the tree
 - (ii) the work is required due to unacceptable risk to public or private safety

- (iii) the tree is within 20 metres of a residential, tourist accommodation or habitable building and is a bushfire hazard within a Bushfire Prone Area
 - (iv) the tree is shown to be causing or threatening to cause damage to a substantial building or structure of value
 - (v) the aesthetic appearance and structural integrity of the tree is maintained
 - (vi) it is demonstrated that all reasonable alternative development options and design solutions have been considered to prevent substantial tree-damaging activity occurring.
- 4 Development involving ground work activities such as excavation, filling, and sealing of surrounding surfaces (whether such work takes place on the site of a significant tree or otherwise) should only be undertaken where the aesthetic appearance, health and integrity of a significant tree, including its root system, will not be adversely affected.
- 5 Land should not be divided or developed where the division or development would be likely to result in a substantial tree-damaging activity occurring to a significant tree.

Siting and Visibility

OBJECTIVES

- 1 Protection of scenically attractive areas, particularly natural, rural and coastal landscapes.
- 2 Development that recognises significant views and vistas of the coast and the Southern Mount Lofty Ranges.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should be sited and designed to minimise its visual impact on:
 - (a) the natural, rural or heritage character of the area
 - (b) areas of high visual or scenic value, particularly rural and coastal areas
 - (c) views from the coast, near-shore waters, public reserves, tourist routes and walking trails
 - (d) the amenity of public beaches.
- 2 Buildings should be sited in unobtrusive locations and, in particular, should:
 - (a) be grouped together
 - (b) where possible be located in such a way as to be screened by existing vegetation when viewed from public roads as shown on *Overlay Maps - Transport*.
- 3 Buildings outside of urban areas and in undulating landscapes should be sited in unobtrusive locations and in particular should be:
 - (a) sited below the ridgeline
 - (b) sited within valleys or behind spurs
 - (c) sited in such a way as to not be visible against the skyline when viewed from public roads as shown on *Overlay Maps - Transport*
 - (d) set well back from public roads, particularly when the allotment is on the high side of the road as shown on *Overlay Map - Transport*.
- 4 Buildings and structures should be designed to minimise their visual impact in the landscape, in particular:
 - (a) the profile of buildings should be low and the rooflines should complement the natural form of the land
 - (b) the mass of buildings should be minimised by variations in wall and roof lines and by floor plans which complement the contours of the land
 - (c) large eaves, verandas and pergolas should be incorporated into designs so as to create shadowed areas that reduce the bulky appearance of buildings.
- 5 The nature of external surface materials of buildings should not detract from the visual character and amenity of the landscape.

- 6 The number of buildings and structures on land outside of urban areas should be limited to that necessary for the efficient management of the land.
- 7 Driveways and access tracks should be designed and surfaced to blend sympathetically with the landscape and to minimise interference with natural vegetation and landforms.
- 8 Development should be screened through the establishment of landscaping using locally indigenous plant species:
 - (a) around buildings and earthworks to provide a visual screen as well as shade in summer, and protection from prevailing winds
 - (b) along allotment boundaries to provide permanent screening of buildings and structures when viewed from adjoining properties and public roads
 - (c) along the verges of new roads and access tracks to provide screening and minimise erosion.

Sloping Land

OBJECTIVES

- 1 Development on sloping land designed to minimise environmental and visual impacts and protect soil stability and water quality.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development and associated driveways and access tracks should be sited and designed to integrate with the natural topography of the land and minimise the need for earthworks.
- 2 Development and associated driveways and access tracks, including related earthworks, should be sited, designed and undertaken in a manner that:
 - (a) minimises their visual impact
 - (b) reduces the bulk of the buildings and structures
 - (c) minimises the extent of cut and/or fill
 - (d) minimises the need for, and the height of, retaining walls
 - (e) does not cause or contribute to instability of any embankment or cutting
 - (f) avoids the silting of watercourses
 - (g) protects development and its surrounds from erosion caused by water run-off.
- 3 Driveways and access tracks across sloping land should be accessible and have a safe, all-weather trafficable surface.
- 4 Development sites should not be at risk of landslip.
- 5 Development on steep land should include site drainage systems to minimise erosion and avoid adverse impacts on slope stability.
- 6 Steep sloping sites in unsewered areas should not be developed unless the physical characteristics of the allotments enable the proper siting and operation of an effluent drainage field suitable for the development intended.

Supported Accommodation, Housing for Aged Persons and People with Disabilities

OBJECTIVES

- 1 Provision of well designed supported accommodation for community groups with special needs in appropriate locations.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Supported accommodation (including nursing homes, hostels, retirement homes, retirement villages, residential care facilities and special accommodation houses) and housing for aged persons and people with disabilities should be:
 - (a) located within walking distance of essential facilities such as convenience shops, health and community services and public and community transport
 - (b) located where on-site movement of residents is not unduly restricted by the slope of the land
 - (c) sited and designed to promote interaction with other sections of the community, without compromising privacy
 - (d) of a scale and appearance that reflects the residential style and character of the locality
 - (e) provided with public and private open space and landscaping.
- 2 Supported accommodation and housing for aged persons and people with disabilities should be designed to provide safe, secure, attractive, convenient and comfortable living conditions for residents that include:
 - (a) internal communal areas and private spaces
 - (b) useable recreation areas for residents and visitors, including visiting children
 - (c) spaces to accommodate social needs and activities, including social gatherings, internet use, gardening, keeping pets, preparing meals and doing personal laundry
 - (d) storage areas for items such as boats, trailers ,caravans and specialised equipment
 - (e) mail boxes and waste disposal areas within easy walking distance of all units.
- 3 Access roads within supported accommodation and development incorporating housing for aged persons and people with disabilities should:
 - (a) not have steep gradients
 - (b) provide convenient access for emergency vehicles, visitors and residents
 - (c) provide space for manoeuvring cars and community buses
 - (d) include kerb ramps at pedestrian crossing points
 - (e) have level-surface passenger loading areas.

- 4 Car parking associated with supported accommodation and housing for aged persons and people with disabilities should:
 - (a) be conveniently located on site within easy walking distance of resident units
 - (b) be adequate for residents, service providers and visitors
 - (c) include covered and secure parking for residents' vehicles
 - (d) have slip-resistant surfaces with gradients not steeper than 1-in-40
 - (e) allow ease of vehicle manoeuvrability
 - (f) be designed to allow the full opening of all vehicle doors
 - (g) minimise the impact of car parking on adjacent residences owing to visual intrusion and noise
 - (h) be appropriately lit to enable safe and easy movement to and from vehicles.
- 5 Supported accommodation should include:
 - (a) ground-level access or lifted access to all units
 - (b) an interesting and attractive outlook from units and communal areas for all residents including those in wheelchairs
 - (c) adequate living space allowing for the use of wheelchairs with an attendant
 - (d) storage for items such as small electric powered vehicles and other personal items, including facilities for recharging small electric powered vehicles.
- 6 Car parking associated with supported accommodation should:
 - (a) have adequate identifiable provisions for staff
 - (b) include private parking spaces for independent living units
 - (c) include separate and appropriately marked places for people with disabilities and spaces for small electrically powered vehicles.
- 7 Residential development specifically for the aged and people with disabilities on sites larger than 1800 square metres may include higher density development at the centre of the site so long as low density development is situated on the perimeter of the site which is compatible with the scale and character of residential development in the locality.

Telecommunications Facilities

OBJECTIVES

- 1 Telecommunications facilities provided to deliver communication services to the community.
- 2 Telecommunications facilities sited and designed to minimise visual impact on the amenity of the local environment.
- 3 Telecommunication facilities located in industrial, centre and commercial zones.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Telecommunications facilities should:
 - (a) be located in a coordinated manner to deliver communication services efficiently
 - (b) use materials and finishes that minimise visual impact
 - (c) have antennae located as close as practical to the support structure
 - (d) be located primarily in industrial, commercial, business, office, centre and rural zones
 - (e) where technically feasible, be co-located with other telecommunications facilities
 - (f) incorporate landscaping to screen the development, particularly equipment shelters and huts
 - (g) be designed and sited to minimise the visual impact on the character and amenity of the local environment, in particular visually prominent areas, main focal points and significant vistas.
- 2 Telecommunications facilities in areas of high visitation and community use should use innovative design techniques (eg sculpture and other artworks) where possible and where the resulting design would positively contribute to the character of the area.
- 3 Telecommunications facilities should be located in residential zones only if sited and designed to minimise visual impact by:
 - (a) using existing buildings and vegetation for screening
 - (b) incorporating the facility within an existing structure that may serve another purpose
 - (c) taking into account the size, scale, context and characteristics of existing structures, landforms and vegetation so as to complement the local environment.
- 4 Telecommunications facilities should not have a direct or significant effect on the amenity, character and settings of Historic Conservation Areas, local heritage places, State heritage places or State Heritage Areas.

Tourism Development

OBJECTIVES

- 1 Environmentally sustainable and innovative tourism development.
- 2 Tourism development that assists in the conservation, interpretation and public appreciation of significant natural and cultural features including State or local heritage places.
- 3 Tourism development that sustains or enhances the local character, visual amenity and appeal of the area.
- 4 Tourism development that protects areas of exceptional natural value, allows for appropriate levels of visitation, and demonstrates an environmental analysis and design response which enhances environmental values.
- 5 Tourism development that contributes to local communities by adding vitality to neighbouring townships, regions and settlements.
- 6 Increased opportunities for visitors to stay overnight.
- 7 Ensure new development, together with associated bushfire management minimise the threat and impact of bushfires on life and property while protecting the environment.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Tourism development should have a functional or locational link with its natural, cultural or historical setting.
- 2 Tourism development and any associated activities should not damage or degrade any significant natural and cultural features.
- 3 Tourism development should ensure that its scale, form and location will not overwhelm, over commercialise or detract from the intrinsic natural values of the land on which it is sited or the character of its locality.
- 4 Tourism development should, where appropriate, add to the range of services and accommodation types available in an area.
- 5 Any upgrading of infrastructure to serve tourism development should be consistent with the landscape and the intrinsic natural values of the land and the basis of its appeal.
- 6 Major tourism developments should generally be located within designated areas and existing townships, towns or cities.

Tourism Development in Association with Dwelling(s)

- 7 Tourist facilities developed on the site of a dwelling should not detrimentally affect residential amenity.
- 8 Car parking for tourist accommodation associated with a dwelling should be provided at the rate of one space for each guest room or suite of rooms, and ensure that:
 - (a) parking areas are attractively developed and landscaped, or screen fenced, and do not dominate the street frontage

- (b) the bedrooms of residential neighbours are suitably shielded from noise and headlight glare associated with guest vehicle movements
- (c) a domestic character is retained through the scale and appearance of landscaping and paving materials that provide a suitable all-weather surface.

Residential Parks and Caravan and Tourist Parks

- 9 Residential parks which are principally designed for residents should be located in areas with access to employment, shops, schools, public transport and community and recreation facilities.
- 10 Residential parks and Caravan and Tourist parks should be designed to:
 - (a) minimise potential conflicts between long-term residents and short-term tourists
 - (b) protect the privacy and amenity of occupants through landscaping and fencing
 - (c) minimise traffic speeds and provide a safe environment for pedestrians
 - (d) include centrally located recreation areas
 - (e) include extensive landscaping that enhances the appearance of the locality, with a landscape buffer around the perimeter of the site
 - (f) ensure that adequate amenity blocks (showers, toilets, laundry and kitchen facilities) and service facilities (eg public telephones, kiosks and restaurants) are provided to serve the population to be accommodated by the facility.
- 11 Visitor car parking should be provided at the rate of:
 - (a) one space per 10 sites to be used for accommodation for parks with less than 100 sites
 - (b) one space per 15 sites to be used for accommodation for parks with greater than 100 sites.
- 12 On-site visitor parking in Caravan and Tourist parks should:
 - (a) be designed and located to be accessible to visitors at all times
 - (b) not dominate the internal site layout
 - (c) be clearly defined as visitor spaces and not specifically associated with any particular accommodation site.
- 13 Long-term occupation of Caravan and Tourist parks should not lead to the displacement of existing tourist accommodation, particularly in important tourist destinations, such as in coastal or riverside locations.
- 14 A minimum of 12.5 per cent of a park should comprise communal open space, landscaped areas and recreation areas.
- 15 Landscaping should comprise locally indigenous species that are appropriate to the development and the subject land, and facilitate amenity and environmental sustainability.

Transportation and Access

OBJECTIVES

- 1 A comprehensive, integrated, affordable and efficient air, rail, sea, road, cycle and pedestrian transport system that will:
 - (a) provide equitable access to a range of public, community and private transport services for all people
 - (b) ensure a high level of safety
 - (c) effectively support the economic development of the State
 - (d) have minimal negative environmental and social impacts
 - (e) maintain options for the introduction of suitable new transport technologies.
- 2 Development that:
 - (a) provides safe and efficient movement for all transport modes
 - (b) ensures access for vehicles including emergency services, public infrastructure maintenance and commercial vehicles
 - (c) provides off-street parking
 - (d) is appropriately located so that it supports and makes best use of existing transport facilities and networks
 - (e) provides convenient and safe access to public transport stops.
- 3 A road hierarchy that promotes safe and efficient transportation in an integrated manner throughout the State.
- 4 Provision of safe, pleasant, accessible, integrated and permeable pedestrian and cycling networks that are connected to the public transport network.
- 5 Safe and convenient freight and people movement throughout the State.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 Land uses arranged to support the efficient provision of sustainable transport networks and encourage their use.

Movement Systems

- 2 Development should be integrated with existing transport networks, particularly major rail, road and public transport corridors as shown on *Location Maps and Overlay Maps - Transport*, and designed to minimise its potential impact on the functional performance of the transport network.
- 3 Transport corridors should be sited and designed so as to not unreasonably interfere with the health and amenity of adjacent sensitive land uses.

- 4 Roads should be sited and designed to blend with the landscape and be in sympathy with the terrain.
- 5 Land uses that generate large numbers of visitors such as shopping centres, places of employment, schools, hospitals and medium to high density residential uses should be located so that they can be serviced by the public transport network and encourage walking and cycling.
- 6 Development generating high levels of traffic, such as schools, shopping centres and other retail areas, and entertainment and sporting facilities should incorporate passenger pick-up and set-down areas. The design of such areas should minimise interference to existing traffic and give priority to pedestrians, cyclists and public and community transport users.
- 7 The location and design of public and community transport set-down and pick-up points should maximise safety and minimise the isolation and vulnerability of users.
- 8 Development should provide safe and convenient access for all anticipated modes of transport.
- 9 Development at intersections, pedestrian and cycle crossings, and crossovers to allotments should maintain or enhance sightlines for motorists, cyclists and pedestrians to ensure safety for all road users and pedestrians.
- 10 Driveway crossovers affecting pedestrian footpaths should maintain the level and surface colour of the footpath.
- 11 Driveway crossovers should be separated and the number minimised to optimise the provision of on-street visitor parking (where on-street parking is appropriate).
- 12 Development should be designed to discourage commercial and industrial vehicle movements through residential streets and adjacent other sensitive land uses.
- 13 Industrial/commercial vehicle movements should be separated from passenger vehicle car parking areas.
- 14 Development should provide for the on-site loading, unloading and turning of all traffic likely to be generated.

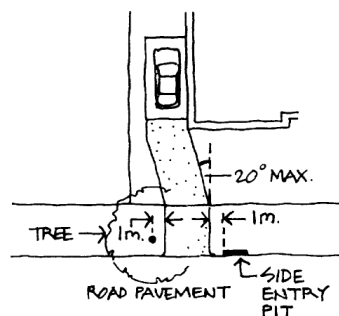
Cycling and Walking

- 15 Development should ensure that a permeable street and path network is established that encourages walking and cycling through the provision of safe, convenient and attractive routes with connections to adjoining streets, paths, open spaces, schools, pedestrian crossing points on arterial roads, public and community transport stops and activity centres.
- 16 Development should provide access, and accommodate multiple route options, for pedestrians and cyclists by enhancing and integrating with:
 - (a) open space networks, recreational trails, parks, reserves, and sport and recreation areas
 - (b) Adelaide's Metropolitan Open Space System
 - (c) Adelaide's principal cycling network (Bikedirect), which includes arterial roads, local roads and off-road paths as depicted in *Overlay Maps - Transport*.
- 17 Cycling and pedestrian networks should be designed to be permeable and facilitate direct and efficient passage to neighbouring networks and facilities.
- 18 New developments should give priority to and not compromise existing designated bicycle routes.
- 19 Where development coincides with, intersects or divides a proposed bicycle route or corridor, development should incorporate through-access for cyclists.

- 20 Development should encourage and facilitate cycling as a mode of transport by incorporating end-of-journey facilities including:
- (a) showers, changing facilities and secure lockers
 - (b) signage indicating the location of bicycle facilities
 - (c) secure bicycle parking facilities.
- 21 On-site secure bicycle parking facilities should be:
- (a) located in a prominent place
 - (b) located at ground floor level
 - (c) located undercover
 - (d) located where surveillance is possible
 - (e) well lit and well signed
 - (f) close to well used entrances
 - (g) accessible by cycling along a safe, well lit route.
- 22 Pedestrian and cycling facilities and networks should be designed and provided in accordance with relevant provisions of the *Australian Standards and Austroads Guides*.

Car Parking and Access

- 23 Driveway crossovers should be:
- (a) single width and appropriately separated, to preserve and enhance street character, and facilitate opportunities for landscaping, fencing and street tree planting
 - (b) minimised in number so as to optimise the provision of on-street visitor parking
 - (c) placed to avoid relocation of street trees, utility and infrastructure inspection points, poles and equipment
 - (d) a maximum of 30 per cent of the frontage of the site (except in **Institution Policy Area 4, South West Policy Area 7** and **Seacliff Policy Area 12**).
 - (e) located a minimum of 1 metre from property boundaries, existing street trees, stormwater side entry pits and above ground utility and infrastructure equipment and poles. Where trees or infrastructure require deviation in the crossover, the maximum deviation between the garage / carport and the crossover at the boundary is 20 degrees, as illustrated in the following figure:



Crossovers should have a 1m clearance to street trees, stobie poles and stormwater side entry pits

- 24 Paving and driveway surfaces should not constitute more than 50 percent of the area between the front property boundary and the forward most building alignment of a dwelling.
- 25 Driveways on arterial roads that serve more than one dwelling should be designed to cater for the simultaneous two-way movements of the largest vehicles expected to enter and exit the site.
- 26 On-site parking and manoeuvring areas servicing development abutting arterial roads should be designed to enable all vehicles to enter and exit the site in a forward direction.
- 27 Except where located within the **Residential Character Zone**, a dwelling should only be developed on an allotment in the form of a hammer head or battleaxe configuration (including for group dwellings), where all of the following is achieved:
- (a) sufficient area is provided for a vehicle to enter and exit the allotment in a forward direction
 - (b) the driveway or 'handle' portion of the allotment is located in a manner that is compatible with the prevailing pattern of development and would not result in multiple access points onto the road in order to minimise the impact of access points on the amenity of the streetscape
 - (c) the driveway or 'handle' portion of the allotment has a maximum deviation angle for driveway tapering of no more than 20 degrees
 - (d) the minimum width of the driveway or 'handle' portion of the allotment (including a landscaped strip on each side of the driveway, which is at least 0.5 metres wide or 1 metre wide where the driveway provides access to 8 or more dwellings) is not less than that shown in the following table:

| Dwellings or allotments to be served by the driveway (whichever is the greater) | At the front property boundary and for the first 6 metres | Width beyond the first 6 metres | Widening required for passing |
|---|--|---------------------------------|--|
| 1 | 4 metres | 3.5 metres | Not required |
| At least 2 and no more than 7 | 8 metres where the site is adjacent to an arterial road 6 metres in all other circumstances | 4 metres | The minimum width is increased to 6 metres for a length of 6 metres at a point 25 metres from the front property boundary and every 25 metres thereafter |
| 8 or more | 8 metres | 6 metres | Not required |

- 28 Development should have direct access from an all-weather public road.
- 29 Development should be provided with safe and convenient access which:
- (a) avoids unreasonable interference with the flow of traffic on adjoining roads
 - (b) provides appropriate separation distances from existing roads or level crossings
 - (c) accommodates the type and volume of traffic likely to be generated by the development or land use and minimises induced traffic through over-provision
 - (d) is sited and designed to minimise any adverse impacts on the occupants of and visitors to neighbouring properties.
- 30 Development should not restrict access to publicly owned land such as recreation areas.

- 31 The number of vehicle access points onto arterial roads shown on *Overlay Maps – Transport* should be minimised and, where possible, access points should be:
- (a) limited to local roads (including rear lane access)
 - (b) shared between developments.
- 32 The number of access points for cyclists and pedestrians onto all adjoining roads should be maximised.
- 33 Development with access from roads identified below or where existing or projected traffic volumes exceeding 6000 vehicles per day should be sited to avoid the need for vehicles to reverse onto or from the road:
- (a) Augusta Street, Glenelg
 - (b) Bowker Street, North Brighton
 - (c) Cliff Street, Glenelg East
 - (d) Dunrobin Road, North Brighton
 - (e) Edwards Street, Brighton /South Brighton
 - (f) Gordon Street, Glenelg
 - (g) King George Avenue, Somerton Park
 - (h) Lapthorne Street, Glenelg East
 - (i) Moseley Street, Glenelg
 - (j) Patawalonga Frontage, Glenelg North
 - (k) Partridge Street, Glenelg
 - (l) Pier Street, Glenelg
 - (m) Scholefield Road, Kingston Park
 - (n) The Crescent, Brighton
 - (o) The Broadway, Glenelg South
 - (p) Wheatland Street, Seacliff
 - (q) Whyte Street, Somerton Park.
- 34 Development with access from arterial roads or roads as shown on *Overlay Maps – Transport* should be sited to avoid the need for vehicles to reverse onto or from the road.
- 35 Structures such as canopies and balconies that encroach onto the footpath of an arterial road should not cause visual or physical obstruction to:
- (a) signalised intersections
 - (b) heavy vehicles
 - (c) street lighting
 - (d) overhead electricity lines

- (e) street trees
 - (f) bus stops.
- 36 Driveways, access tracks and parking areas should be designed and constructed to:
- (a) follow the natural contours of the land
 - (b) minimise excavation and/or fill
 - (c) minimise the potential for erosion from surface runoff
 - (d) avoid the removal of existing vegetation
 - (e) be consistent with *Australian Standard AS: 2890 - Parking facilities*.

Access for People with Disabilities

- 37 Development should be sited and designed to provide convenient access for people with a disability.
- 38 Where appropriate and practical, development should provide for safe and convenient access to the coast and beaches for disabled persons.
- 39 Parking for people with a disability should be located conveniently to major entrances and ramps and adequately signposted or identified as being for people with a disability only.

Vehicle Parking

- 40 Development should provide off-street vehicle parking and specifically marked accessible car parking places to meet anticipated demand in accordance with [Table HoB/1 – Off Street Vehicle Parking Requirements](#) (with resultant numerical figure rounded to the nearest whole number) unless all the following conditions are met:
- (a) the site is located within the **Glenelg Policy Area 2**
 - (b) an agreement is reached between the Council and the applicant for a reduced number of parking spaces
 - (c) a financial contribution is paid into the Council Car Parking Fund specified by the Council, in accordance with the gazetted rate per car park.
- 41 Development should be consistent with *Australian Standard AS: 2890 - Parking facilities*.
- 42 Vehicle parking areas should be sited and designed in a manner that will:
- (a) facilitate safe and convenient pedestrian linkages to the development and areas of significant activity or interest in the vicinity of the development
 - (b) include safe pedestrian and bicycle linkages that complement the overall pedestrian and cycling network
 - (c) not inhibit safe and convenient traffic circulation
 - (d) result in minimal conflict between customer and service vehicles
 - (e) avoid the necessity to use public roads when moving from one part of a parking area to another
 - (f) minimise the number of vehicle access points onto public roads
 - (g) avoid the need for vehicles to reverse onto public roads

- (h) where practical, provide the opportunity for shared use of car parking and integration of car parking areas with adjoining development to reduce the total extent of vehicle parking areas and the requirement for access points
 - (i) not dominate the character and appearance of a site when viewed from public roads and spaces
 - (j) provide landscaping that will shade and enhance the appearance of the vehicle parking areas
 - (k) include infrastructure such as underground cabling and connections to power infrastructure that will enable the recharging of electric vehicles.
- 43 Where vehicle parking areas are not obviously visible or navigated, signs indicating the location and availability of vehicle parking spaces associated with businesses should be displayed at locations readily visible to users.
- 44 Vehicle parking areas that are likely to be used during non-daylight hours should provide floodlit entry and exit points and site lighting directed and shaded in a manner that will not cause nuisance to adjacent properties or users of the parking area.
- 45 Vehicle parking areas should be sealed or paved to minimise dust and mud nuisance.
- 46 To assist with stormwater detention and reduce heat loads in summer, outdoor vehicle parking areas should include soft (living) landscaping.
- 47 Vehicle parking areas should be line-marked to delineate parking bays, movement aisles and direction of traffic flow.
- 48 On-site visitor parking spaces for group and multiple dwellings and residential flat buildings should be sited and designed to:
- (a) not dominate internal site layout
 - (b) be clearly defined as visitor spaces not specifically associated with any particular dwelling
 - (c) be accessible to visitors at all times.

Vehicle Parking for Residential Development

- 49 On-site vehicle parking should be provided having regard to:
- (a) the number, nature and size of proposed dwellings
 - (b) proximity to centre facilities, public and community transport within walking distance of the dwellings
 - (c) the anticipated mobility and transport requirements of the likely occupants, particularly groups such as aged persons.
 - (d) availability of on-street car parking.
 - (e) any loss of on-street parking arising from the development (e.g. an increase in number of driveway crossovers).
- 50 Vehicle parking areas servicing more than one dwelling should be of a size and location to:
- (a) serve users, including pedestrians, cyclists and motorists, efficiently, conveniently and safely
 - (b) provide adequate space for vehicles, including emergency service vehicles, to manoeuvre between the street and the parking area

- (c) reinforce or contribute to attractive streetscapes.
- 51 The provision of ground level vehicle parking areas, including garages and carports (other than where located along a rear lane access way), should:
 - (a) not face the primary street frontage
 - (b) be located to the rear of buildings with access from a shared internal laneway
 - (c) ensure vehicle park entries are recessed at least 0.5 metres behind the main face of the building.

Undercroft and Below Ground Garaging and Parking of Vehicles

- 52 Undercroft and below ground garaging of vehicles should only occur where envisaged in the relevant zone or policy area or precinct and ensure:
 - (a) the overall height and bulk of the undercroft structure does not adversely impact on streetscape character of the locality or the amenity of adjacent properties
 - (b) vehicles can safely enter and exit from the site without compromising pedestrian or cyclist safety or causing conflict with other vehicles
 - (c) the site slopes up from the street
 - (d) driveway gradients provide for safe and functional entry and exit
 - (e) driveways and adjacent walls, fencing and landscaping are designed to provide adequate sightlines from vehicles to pedestrians using the adjacent footpath
 - (f) openings to undercroft areas are integrated with the main building so as to minimise visual impact
 - (g) landscaping, mounding and/or fencing is incorporated to improve its presentation to the street and to adjacent properties
 - (h) the overall streetscape character of the locality is not adversely impaired (e.g. visual impact, building bulk, front setbacks relative to adjacent development)
 - (i) the height of the car park ceiling does not exceed 1 metre above the finished ground level.
- 53 Buildings with four storeys or more above natural surface level should include provision for undercroft parking.
- 54 Semi-basement or undercroft car parking should be suitably integrated with building form.
- 55 In the case of undercroft and below ground car parks where cars are visible from public areas, adequate screening and landscaping should be provided.

Waste

OBJECTIVES

- 1 Development that, in order of priority, avoids the production of waste, minimises the production of waste, reuses waste, recycles waste for reuse, treats waste and disposes of waste in an environmentally sound manner.
- 2 Development that includes the treatment and management of solid and liquid waste to prevent undesired impacts on the environment including, soil, plant and animal biodiversity, human health and the amenity of the locality.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should be sited and designed to prevent or minimise the generation of waste (including wastewater) by applying the following waste management hierarchy in the order of priority as shown below:
 - (a) avoiding the production of waste
 - (b) minimising waste production
 - (c) reusing waste
 - (d) recycling waste
 - (e) recovering part of the waste for re-use
 - (f) treating waste to reduce the potentially degrading impacts
 - (g) disposing of waste in an environmentally sound manner.
- 2 The storage, treatment and disposal of waste materials from any development should be achieved without risk to health or impairment of the environment.
- 3 Development should avoid as far as practical, the discharge or deposit of waste (including wastewater) onto land or into any waters (including processes such as seepage, infiltration or carriage by wind, rain, sea spray, stormwater or by the rising of the water table).
- 4 Untreated waste should not be discharged to the environment, and in particular to any water body.
- 5 Development should include appropriately sized area to facilitate the storage of receptacles that will enable the efficient recycling of waste.
- 6 Development that involves the production and/or collection of waste and/or recyclable material should include designated collection and storage area(s) that are:
 - (a) screened and separated from adjoining areas
 - (b) located to avoid impacting on adjoining sensitive environments or land uses
 - (c) designed to ensure that wastes do not contaminate stormwater or enter the stormwater collection system
 - (d) located on an impervious sealed area graded to a collection point in order to minimise the movement of any solids or contamination of water

- (e) protected from wind and stormwater and sealed to prevent leakage and minimise the emission of odours
- (f) stored in such a manner that ensures that all waste is contained within the boundaries of the site until disposed of in an appropriate manner.

Wastewater

- 7 The disposal of wastewater to land should only occur where methods of wastewater reduction and reuse are unable to remove the need for its disposal, and where its application to the land is environmentally sustainable.
- 8 Wastewater lagoons should not be sited in any of the following areas:
 - (a) within land subject to a 1-in-100 year average return interval flood event
 - (b) within 50 metres of the top of the bank of a watercourse
 - (c) within 500 metres of the coastal high water mark
 - (d) where the base of the lagoon would be below any seasonal water table.
- 9 Artificial wetland systems for the storage of treated wastewater, such as wastewater lagoons, should be:
 - (a) sufficiently separated from adjoining sensitive uses to minimise potential adverse odour impacts
 - (b) sited and designed to minimise potential public health risks arising from the breeding of mosquitoes.

Waste Treatment Systems

- 10 Development that produces any sewage or effluent should be connected to a waste treatment system that complies with (or can comply with) the relevant public and environmental health legislation applying to that type of system.
- 11 The methods for, and siting of, effluent and waste storage, treatment and disposal systems should minimise the potential for environmental harm and adverse impacts on:
 - (a) the quality of surface and groundwater resources
 - (b) public health
 - (c) the amenity of a locality
 - (d) sensitive land uses.
- 12 Waste treatment should only occur where the capacity of the treatment facility is sufficient to accommodate likely maximum daily demands including a contingency for unexpected high flows and breakdowns.
- 13 Any on-site wastewater treatment system/ re-use system or effluent drainage field should be located within the allotment of the development that it will service.
- 14 A dedicated on-site effluent disposal area should not include any areas to be used for, or could be reasonably foreseen to be used for, private outdoor open space, driveways, car parking or outbuildings.
- 15 The spreading or discharging of treated liquid or solid waste onto the ground should only occur where the disposal area consists of soil and vegetation that has the capacity to store and use the waste without contaminating soil or surface or ground water resources or damaging crops.

- 16 Stock slaughter works, poultry processors, saleyards, piggeries, cattle feedlots, milking sheds, milk processing works, fish processing works, wineries, distilleries, tanneries and fellmongeries, composting works, waste or recycling depots and concrete batching works should have a wastewater management system that is designed so as not to discharge wastes generated by the premises:
- (a) into any waters
 - (b) onto land in a place where it is reasonably likely to enter any waters by processes such as:
 - (i) seepage
 - (ii) infiltration
 - (iii) carriage by wind, rain, sea spray, or stormwater
 - (iv) the rising of the watertable.

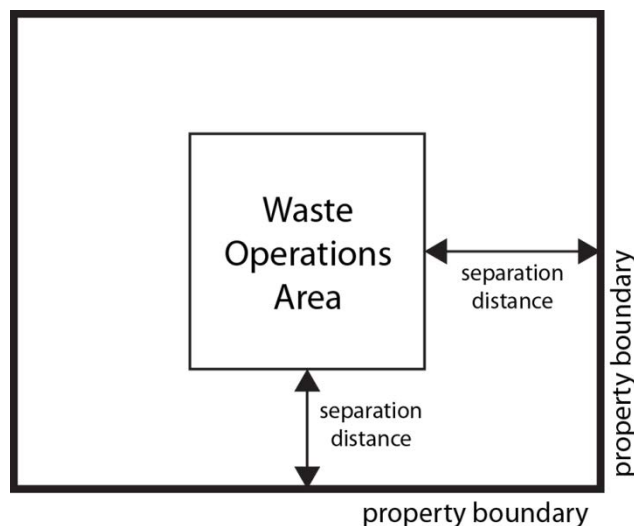
Waste Management Facilities

OBJECTIVES

- 1 The orderly and economic development of waste management facilities in appropriate locations.
- 2 Minimisation of human and environmental health impacts from the location and operation of waste management facilities.
- 3 Protection of waste management facilities from incompatible development.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Waste management facilities should be located and designed to minimise adverse impacts on both the site and surrounding areas from the generation of surface water and groundwater pollution, traffic, noise, odours, dust, vermin, weeds, litter, gas and visual impact.
- 2 Waste management facilities in the form of land fill and organic processing facilities should not be located in existing or future township, living, residential, centre, office, business, institutional or environmental protection, conservation, landscape, water protection and open space areas.
- 3 Waste management facilities should not be located where access to the facility requires, or is likely to involve, the use of non-arterial roads in adjacent residential areas.
- 4 Waste management facilities should:
 - (a) be appropriately separated from sensitive land uses and environmentally-sensitive areas
 - (b) incorporate the separation distance between the waste operations area (including all closed, operating and future cells) and sensitive uses within the development site as illustrated in the figure below:



- (c) not incorporate other land uses and activities within the separation distance unless they are compatible with both a waste management facility and any adjacent land uses.
- 5 Separation and/or noise attenuation should be used to ensure noise generation associated with the waste management operation does not unreasonably interfere with the amenity of sensitive land uses.

- 6 Sufficient area should be provided within the waste operations area for the:
 - (a) maximum expected volume of material on the site at any one time
 - (b) containment of potential groundwater and surface water contaminants
 - (c) diversion of clean stormwater away from the waste and potentially-contaminated areas.
- 7 Processing facilities and operational areas should be screened from public view.
- 8 Waste management sites should be accessed by appropriately constructed and maintained roads.
- 9 Traffic circulation movements within any waste management site should:
 - (a) be of a dimension and constructed to support all vehicles transporting waste
 - (b) enable all vehicles to enter and exit the site in a forward direction.
- 10 Suitable access for emergency vehicles should be provided to and within waste management sites.
- 11 Chain wire mesh or pre-coated painted metal fencing to a minimum height of 2 metres should be erected on the perimeter of a waste management facility site to prevent access other than at entry points.
- 12 Plant, equipment or activities that could cause a potential hazard to the public should be enclosed by a security fence.
- 13 Litter control measures that minimise the incidence of wind blown litter should be provided.
- 14 The waste operations area of a landfill or organic waste processing facility should be sited at least:
 - (a) 3 kilometres from an airfield used by commercial aircraft to minimise the risk of bird strikes to aircraft
 - (b) 500 metres from:
 - (i) the boundaries of the allotment
 - (ii) the nearest dwelling, shop, office, public institution or other building designed primarily for human occupation in the case of an organic waste processing facility for the composting of waste
 - (c) 250 metres from a public open space reserve, forest reserve, national park, conservation zone or policy area
 - (d) 100 metres from:
 - (i) the nearest surface water (whether permanent or intermittent)
 - (ii) a 1-in-100 year average return interval flood event area.
- 15 The waste operations area of a landfill should not be located on land:
 - (a) that is subject to land slipping
 - (b) with ground slopes greater than 10 per cent, except where the site incorporates a disused quarry.
- 16 The waste operations area of an organic waste processing facility should not be located on land:
 - (a) that is subject to land slipping

- (b) with ground slopes greater than 6 per cent
 - (c) where the interface of the engineered landfill liner and natural soils would be within any of the following:
 - (i) 15 metres of unconfined aquifers bearing groundwater with less than 3000 milligrams per litre total dissolved salts
 - (ii) 5 metres of groundwater with a water quality of 3000 to 12 000 milligrams per litre total dissolved salts
 - (iii) 2 metres of groundwater with a water quality of greater than 12 000 milligrams per litre total dissolved salts.
- 17 Where required, a leachate barrier should be provided between the operational areas and underlying soil and groundwater.
- 18 Landfill activities that have a total storage capacity exceeding 230 000 cubic metres should make sustainable use of landfill gas emissions. For smaller landfill activities, if the sustainable use of the landfill gas emissions is not practical or feasible, flaring should be used to avoid gases being vented directly to the air.

Overlay Section

Affordable Housing Overlay

Refer to the [Map Reference Tables](#) for a list of maps that relate to this overlay.

The following policies apply to the 'designated area' marked on the relevant *Overlay Map(s) - Affordable Housing*.

INTERPRETATION

Where the Objectives and/or Principles of Development Control that apply in relation to this Overlay are in conflict with the relevant General Objectives and/or Principles of Development Control in the Development Plan, the Overlay will prevail.

OBJECTIVES

- 1 Affordable housing that is integrated into residential and mixed use development.
- 2 Development that comprises a range of affordable dwelling types that cater for a variety of household structures.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development comprising 20 or more dwellings should include a minimum of 15 per cent affordable housing unless the development is to occur in stages and it can be demonstrated that any shortfall in affordable housing from any stage of development will be accommodated in another stage or stages.

Noise and Air Emissions Overlay

Refer to the [Map Reference Tables](#) for a list of maps that relate to this overlay.

The following policies apply to the 'designated area' marked on the relevant *Overlay Map(s) –Noise and Air Emissions*.

INTERPRETATION

Where the Objectives and/or Principles of Development Control that apply in relation to this overlay are in conflict with the relevant General Objectives and/or Principles of Development Control in the Development Plan, the overlay will prevail.

OBJECTIVES

- 1 Protect community health and amenity from adverse impacts of noise and air emissions.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Noise and air quality sensitive development located adjacent to high noise and/or air pollution sources should:
 - (a) shield sensitive uses and areas through one or more of the following measures:
 - (i) placing buildings containing less sensitive uses between the emission source and sensitive land uses and areas
 - (ii) within individual buildings, place rooms more sensitive to air quality and noise impacts (e.g. bedrooms) further away from the emission source
 - (iii) erecting noise attenuation barriers provided the requirements for safety, urban design and access can be met
 - (b) use building design elements such as varying building heights, widths, articulation, setbacks and shapes to increase wind turbulence and the dispersion of air pollutants provided wind impacts on pedestrian amenity are acceptable
 - (c) locate ground level private open space, communal open space and outdoor play areas within educational establishments (including childcare centres) away from the emission source.

Strategic Transport Routes Overlay

Refer to the [Map Reference Tables](#) for a list of maps that relate to this overlay.

The following policies apply to the 'designated area' marked on the relevant *Overlay Map(s) - Strategic Transport Routes*.

INTERPRETATION

Where the Objectives and/or Principles of Development Control that apply in relation to this overlay are in conflict with the relevant General Objectives and/or Principles of Development Control in the Development Plan, the overlay will prevail.

OBJECTIVES

- 1 Development that recognises the importance of strategic transport routes and does not impede traffic flow or create hazardous conditions for pedestrians, cyclists or drivers of vehicles, including emergency services vehicles.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development adjacent to a strategic transport route should:
 - (a) avoid the provision of parking on the main carriageway
 - (b) be accessible via service roads, where possible, that provide:
 - (i) parking off the main carriageway
 - (ii) a buffer from the main carriageway for pedestrian and cycle activity
 - (c) not impede the potential for overhead cabling and associated infrastructure to be established in an existing or proposed tram corridor.
- 2 Vehicular site access should not be provided on to a strategic transport route where an alternative access is available.
- 3 Development adjacent kerbside bus stops should be set back to provide sufficient space for indented bus bays with associated hard stand area, shelter and a **minimum** 1.2 metre wide continuous accessible path behind the bus shelter.

Zone Section

Caravan and Tourist Park Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

OBJECTIVES

- 1 A zone primarily for short-term tourist accommodation and associated facilities.
- 2 A zone accommodating a range of short-term tourist accommodation predominantly in the form of caravan and camping sites, cabins, serviced apartments and transportable dwellings surrounded by open landscaped areas.
- 3 Development that is designed to enhance the natural features of the local environment, including visual amenity, landforms, fauna and flora.
- 4 Development that contributes to the desired character of the zone.

DESIRED CHARACTER

The zone incorporates the area occupied by a caravan park in Kingston Park and surrounding natural features and open space. The zone primarily accommodates a range of tourist accommodation uses, including camping sites, caravans and cabins. Dwellings and other long-term accommodation will not displace existing short-term tourist accommodation in this high demand location, and are not envisaged within the zone.

To minimise the visual impact of the caravan park on the landscape as viewed from scenic vantage points, public lookouts and tourist routes, development will be restricted to one to two storeys in height and built to blend in with the natural environment. Vegetation and landscaping will be important in integrating the caravan park into the landscape and provide screening from surrounding land uses, assisting to reduce the visual impact of built form and activities within the zone, enhancing the natural beauty of the area, while also reducing noise impacts and providing privacy for caravan park users.

Development of the caravan park and adjacent open spaces will be sensitive and sympathetic to the unique environmental, cultural and historic landscape of the Kingston Park precinct, in particular the Tjilbruke Spring and historic Kingston House, a State Heritage Place. It will also recognise its siting in close proximity to the coast (and the implications of sea-level rise) and the adjacent **Coastal Open Space Zone**.

Circulation and movement within the caravan park will be pedestrian friendly and promote low speed vehicle movement that integrates with the local traffic networks and future extension of the shared use Metropolitan Coast Park. Pedestrian pathways and cycle routes will link with the Coast Park located immediately to the west of the zone.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the zone:
 - amenity block, including shower, toilet, laundry and kitchen facilities
 - cabin
 - camping ground
 - caravan park
 - caravan permanently fixed to land
 - recreation area including tennis court, basketball court, playground

- shop less than 500 square metres in gross floor area and in association with and ancillary to tourist accommodation.
- swimming pool/spa
- tourist park.

2 Development listed as non-complying is generally inappropriate.

Form and Character

- 3 Development should not be undertaken unless it is consistent with the desired character for the zone.
- 4 Permanent buildings should be limited to tourist accommodation, a dwelling (manager's / caretaker's house), shop (in association with and ancillary to a caravan and tourist park), community or recreational facility, toilets / amenities and interpretive centre.
- 5 Recreation facilities should be provided of a scale that is suitable to maintain the open natural character of the area and ancillary to the primary role and function of the park.
- 6 The total number of tourist accommodation sites in the park should be at least 60 per cent of the total number of sites available.
- 7 Every caravan, cabin and dwelling site should be greater than 81 square metres in area.
- 8 Buildings should not contain more than two storeys above undercroft car parking areas.
- 9 Landscaping should form an integral part of the design and be used to define spaces, reinforce internal networks, screen utility areas and enhance the visual amenity of the area.

Car Parking and Access

- 10 Every caravan, cabin or dwelling site should have parking for at least one vehicle, either located on the site or grouped within the park.
- 11 Internal road surfaces should be surfaced to prevent dust becoming a nuisance.

Street and Boundary Setbacks

- 12 Every dwelling, annex, caravan fixed to land, recreational facility or amenities building should be set back a minimum of:
- (a) 1 metre from an internal road (with all internal road surfaces being treated to prevent the generation of nuisance dust)
 - (b) 6 metres from a public road
 - (c) 2 metres from the boundary of the caravan park or camping ground.

Natural Hazards

- 13 In areas prone to flooding, bushfire or other natural hazards, buildings and structures (including annexes attached to caravans or caravans fixed to land) should be designed and constructed so that they can be removed in the event of a hazard.
- 14 Development should be set back at least 45 metres from the mean high water mark.

Land Division

- 15 No additional allotment(s) should be created wholly or partly within the zone except where a lease or license agreement is made, granted or accepted under the *Residential Parks Act 2007*.

PROCEDURAL MATTERS

Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

Non-complying Development

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

| Form of Development | Exceptions |
|---|--|
| Amusement machine centre | |
| Bus depot | |
| Cemetery | |
| Commercial forestry | |
| Community centre | Except where in association with and ancillary to tourist accommodation. |
| Consulting room | |
| Crematorium | |
| Dairy | |
| Dam | |
| Dwelling | Except for a manager's / caretaker's residence in association with and ancillary to tourist accommodation. |
| Farming | |
| Fuel depot | |
| Horse keeping | |
| Horticulture | |
| Hospital | |
| Hotel | |
| Indoor recreation centre | Except where in association with and ancillary to tourist accommodation. |
| Industry | |
| Intensive animal keeping | |
| Land division which results in the creation of additional allotment(s) either wholly or partly within the zone. | Except where a lease or licence agreement is made, granted or accepted under the <i>Residential Parks Act 2007</i> . |
| Marina | |
| Motor repair station | |
| Nursing home | |

| Form of Development | Exceptions |
|---|--|
| Office | Except where in association with and ancillary to tourist accommodation. |
| Petrol filling station | |
| Place of worship | |
| Pre-school | |
| Prescribed mining operations | |
| Public service depot | |
| Residential flat building | |
| Restaurant | Except where it is both: (a) less than 500 square metres in gross floor area (b) in association with and ancillary to tourist accommodation. |
| Road transport terminal | |
| Service trade premises | |
| Shop or group of shops | Except where it is both: (a) less than 500 square metres in gross floor area (b) in association with and ancillary to tourist accommodation. |
| Stadium | |
| Stock sales yard | |
| Stock slaughter works | |
| Store | Except where in association with and ancillary to tourist accommodation. |
| Warehouse | |
| Waste reception, storage, treatment or disposal | Except where in association with and ancillary to tourist accommodation. |
| Wrecking yard | |

Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development (except where the development is non-complying) are designated:

| Category 1 | Category 2 |
|---|------------------------|
| Amenity block, including shower, toilet, laundry and kitchen facilities | Interpretive centre |
| Cabin | Restaurant |
| Camping ground | Shop or group of shops |
| Caravan park | |
| Caravan permanently fixed to land | |
| Recreation area | |
| Swimming pool | |
| Tourist accommodation (excluding motel) | |
| Tourist park | |

Coastal Conservation Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

OBJECTIVES

- 1 To enhance and conserve the natural features of the coast including visual amenity, landforms, fauna and flora.
- 2 To protect the dunes and adjoining residential development from long term changes such as erosion, and sea level rise which may affect the coastline.
- 3 To contribute to the Metropolitan Open Space System (MOSS).
- 4 Development that contributes to the desired character of the zone.

DESIRED CHARACTER

The role of this zone is to preserve and protect the natural features of the coast, including sand dunes to ensure the conservation of coastal features and scenic quality, to enable appropriate public access, to protect sites of Aboriginal Significance and to ensure that development is not subject to coastal hazards.

Public access will be via a clearly defined and continuous off road, shared use, cycle and pedestrian path that preserves the natural values and features of the dunes and native vegetation. Development within the zone should be subservient to the conservation of the coastal environment in order to ensure that the fragile coastal environment is protected and biodiversity is maintained.

This zone supports the establishment and maintenance of appropriate coastal protection strategies. Parts of the zone are at risk of coastal flooding and erosion, which will increase with sea level rise. Development will address sea level rise.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the zone:
 - conservation work, including sand management programs
 - interpretive signage and facility
 - public pathway
 - small scale tourism/visitor facility (excluding accommodation).
- 2 Development listed as non-complying is generally inappropriate.
- 3 Buildings and structures should mainly be for essential purposes, such as shelters and toilet facilities associated with public recreation, navigation purposes or necessary minor public works.
- 4 Development involving the removal of shell grit or sand, other than for coastal protection works purposes, or the disposal of domestic and industrial waste should not be undertaken.
- 5 Aquaculture inlet and outlet pipes should not be developed unless one or more of the following applies:
 - (a) the adjoining land is located in an aquaculture zone

- (b) the environmental impacts will be minimal.

Form and Character

- 6 Development should be designed and sited to be compatible with conservation and enhancement of the coastal environment and scenic beauty of the zone.
- 7 Development should:
- (a) not adversely impact on the ability to maintain the coastal frontage in a stable and natural condition
 - (b) minimise vehicle access points to the area that is the subject of the development
 - (c) be landscaped with locally indigenous plant species to enhance the amenity of the area and to screen buildings from public view
 - (d) utilise external low reflective materials and finishes that will minimise glare and blend in with the features of the landscape.
- 8 Where public access is necessary in sensitive locations, walkways and fencing should be provided to effectively control access.

Land Division

- 9 Land division should only occur where:
- (a) no additional allotments are created wholly or partly within the zone
 - (b) there is no increase in the number of allotments with direct access to the coast or a reserve including by creation of land under rights of way or community titles.

PROCEDURAL MATTERS

Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

No other forms of development are complying in the zone.

Non-complying Development

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

| Form of Development | Exceptions |
|---|---|
| Advertisement and/or advertising hoarding | Except in association with conservation works for tourist information purposes. |
| Amusement machine centre | |
| Bus depot | |
| Caravan park | |
| Cemetery | |
| Commercial forestry | |
| Community centre | |

| Form of Development | Exceptions |
|------------------------------|--|
| Consulting room | |
| Crematorium | |
| Dairy | |
| Dam | |
| Dwelling | Except where used for the purposes of administering either or both of the: (a) <i>National Parks and Wildlife Act 1972</i> (b) <i>Wilderness Protection Act 1992</i> . |
| Educational establishment | |
| Farming | |
| Fuel depot | |
| Horse keeping | |
| Horticulture | |
| Hospital | |
| Hotel | |
| Indoor recreation centre | |
| Industry | |
| Intensive animal keeping | Except inlet and outlet pipes in association with aquaculture. |
| Land division | Except where all of the following apply: (a) no additional allotments are created wholly or partly within the zone (b) there is no increase in the number of allotments with frontage or direct access to the coast. |
| Marina | |
| Motel | |
| Motor repair station | |
| Nursing home | |
| Office | Except where used for the purposes of administering the <i>National Parks and Wildlife Act 1972</i> . |
| Petrol filling station | |
| Place of worship | |
| Pre-school | |
| Prescribed mining operations | |
| Public service depot | |
| Residential flat building | |
| Road transport terminal | |

| Form of Development | Exceptions |
|---|------------|
| Service trade premises | |
| Shop | |
| Stadium | |
| Stock sales yard | |
| Stock slaughter works | |
| Store | |
| Tourist accommodation | |
| Warehouse | |
| Waste reception, storage, treatment or disposal | |
| Water tank | |
| Wrecking yard | |

Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development (except where the development is non-complying) are designated:

| Category 1 | Category 2 |
|--------------------------------------|------------|
| Interpretive and directional signage | |
| Public pathway | |
| Public shelter | |

Coastal Open Space Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

OBJECTIVES

- 1 Coastal land protected from development other than that necessary for conservation, recreational activity and public facilities.
- 2 Preservation and upgrading of the scenic character of the coastal landscape and foreshore areas fronting urban areas, townships or settlements.
- 3 Development of foreshore areas for a range of passive and active outdoor recreation activities and open space development, conservation and revegetation, in a parkland setting.
- 4 Land subject to inundation or susceptible to erosion kept free of development.
- 5 Maintenance of open space links with the various local and regional open space networks and corridors to ensure passive recreational linkages.
- 6 Development that contributes to the desired character of the zone.

DESIRED CHARACTER

The zone provides for passive outdoor recreational activities (predominantly walking and cycling) that take advantage of the attractive coastal environment. It is located along the entire length of the Council's coastline from Glenelg southwards. It will accommodate limited public facilities and amenities to support these public passive activities, such as yacht and surf lifesaving clubs, showers and taps, ramps and steps, and coastal protection infrastructure.

Public infrastructure will achieve high standards of design that reflect the significance of the coastal location, its aesthetic qualities and its use by local residents, tourists and users from the broader metropolitan area. Development including public infrastructure will be designed and constructed to withstand the harsh coastal environment.

This zone supports the establishment and maintenance of appropriate coastal protection strategies. Parts of the zone are at risk of coastal flooding and erosion, which will increase with sea level rise. Development will address sea level rise.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the zone:
 - coastal protection works
 - community recreation facility directly related to water activities (such as sailing clubs, boat ramps)
 - conservation works
 - jetty and boat ramp
 - recreation area
 - toilet blocks and barbeque facilities
 - public car parking.

- 2 Development listed as non-complying is generally inappropriate.
- 3 The provision of facilities should be related to the demand for such facilities so as to prevent oversupply and inappropriate siting.
- 4 Development should be for public purposes and use.

Form and Character

- 5 Development should not be undertaken unless it is consistent with the desired character for the zone.
- 6 Development should not diminish the ability of the public to use and enjoy the coast or to gain access to the foreshore.
- 7 Development should be of a high standard of coordinated design with an emphasis on the creation of pedestrian areas.
- 8 Community facilities including shelters, boat ramps, public conveniences and kiosks, should be sited in convenient and accessible locations linked to the surrounding vehicular and pedestrian movement networks.
- 9 Development should be designed and sited to be compatible with conservation and enhancement of the coastal environment and scenic beauty of the zone.

Land Division

- 10 Land division should not be undertaken except where:
 - (a) it will facilitate an envisaged use within the zone
 - (b) no additional allotments are created
 - (c) there is no increase in the number of allotments with frontage or direct access to the coast.

PROCEDURAL MATTERS

Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

Non-complying Development

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

| Form of Development | Exceptions |
|---------------------|------------|
| Bus depot | |
| Caravan park | |
| Cemetery | |
| Commercial forestry | |
| Community centre | |
| Consulting room | |
| Crematorium | |

| Form of Development | Exceptions |
|------------------------------|---|
| Dairy | |
| Dam | |
| Dwelling | |
| Educational establishment | |
| Farming | |
| Fuel depot | |
| Horse keeping | |
| Horticulture | |
| Hospital | |
| Hotel | |
| Industry | |
| Intensive animal keeping | |
| Land division | Except where both of the following apply: (a) no additional allotments are created wholly or partly within the zone (b) there is no increase in the number of allotments with frontage or direct access to the coast. |
| Motel | |
| Motor repair station | |
| Nursing home | |
| Office | |
| Petrol filling station | |
| Place of worship | |
| Pre-school | |
| Prescribed mining operations | |
| Public service depot | |
| Residential flat building | |
| Road transport terminal | |
| Service trade premises | |
| Shop or group of shops | Except where the gross leasable area is less than 250 square metres. |
| Stadium | |
| Stock sales yard | |
| Stock slaughter works | |

| Form of Development | Exceptions |
|---|------------|
| Store | |
| Tourist accommodation | |
| Warehouse | |
| Waste reception, storage, treatment or disposal | |
| Water tank | |
| Wrecking yard | |

Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development (except where the development is non-complying) are designated:

| Category 1 | Category 2 |
|--------------------------------------|-------------------------------|
| Coastal protection works | Boat ramps |
| Conservation works | Car park |
| Interpretive and directional signage | Community/recreation facility |
| Public pathway | Jetty |
| Public shelter | Toilet block |

Commercial Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

OBJECTIVES

- 1 A zone accommodating a range of commercial and business land uses, including:
 - (a) commercial, servicing and office activities
 - (b) offices and consulting rooms between Anzac Highway and Augusta Street
 - (c) local service and commercial activities fronting Brighton Road (south of Tassie Street).
- 2 Development that minimises any adverse impacts upon the amenity of the locality within the zone.
- 3 The establishment and maintenance of a visual and acoustic buffer between development and land in the abutting zones.
- 4 Development having an appearance which recognises the important role of Brighton Road and Anzac Highway as an entrance to the central part of the City of Holdfast Bay.
- 5 Consolidation of allotments into suitable sites, where comprehensive redevelopment takes place.
- 6 Redevelopment of the existing residential built form for commercial purposes.
- 7 Development that contributes to the desired character of the zone.

DESIRED CHARACTER

This is a unique zone that comprises five small clusters of commercial activities dispersed across the Council area within the suburbs of Glenelg East, Glenelg, Glenelg South and Brighton. Due to the dispersed nature of the zone, development type, form and scale will vary to suit its localised setting - sensitive to its siting in close proximity to residential areas.

Development will be limited to two storeys in height, with commercial development designed and sited to minimise impacts on residential land abutting the zone. It will comprise a range of commercial activities from local service industry and offices, to consulting rooms, small-scale retail and bulky good outlets.

Landscaping will enhance the appearance of sites by screening large expanses of solid walls and visually enhancing front property boundaries. Landscaping will also provide opportunities for water sensitive design (including storm-water capture and dispersal), encourage biodiversity, reduce the heat island effect and provide a noise-absorbing buffer to improve the amenity of the adjacent **Residential Character Zone**.

Development on Anzac Highway will provide an attractive entrance to the Council area through quality design elements, construction materials and finishes and the location of parking behind the built form, to improve the appearance of the site as viewed from Anzac Highway.

Development in areas along Brighton Road will be consistent in form and scale, and sites will be consolidated, enabling coordinated parking areas to the rear of buildings. This will reduce access points to Brighton Road and promote attractive landscaped frontages, free from visual clutter of parked cars and enhanced through quality building materials.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the zone:
 - bulky goods outlet
 - consulting room
 - motor vehicle related business other than wrecking yard
 - office
 - petrol filling station
 - service trade premises
 - shop with a gross leasable area less than 150 square metres
 - store
 - warehouse.
- 2 Development listed as non-complying is generally inappropriate.
- 3 Retail development in the zone should not hinder the development or function of any centre zone.
- 4 Shops, other than a bulky goods outlet, should have a gross leasable area less than 150 square metres.

Form and Character

- 5 Development should not be undertaken unless it is consistent with the desired character for the zone.
- 6 Buildings should not exceed two storeys in height.
- 7 Offices and consulting rooms should not exceed a total floor area of 450 square metres.
- 8 Development should present a landscaped frontage to Anzac Highway and Brighton Road with car parking areas screened by dense planting or by buildings.
- 9 Development adjacent to the **Residential Zone** or **Residential Character Zone** should:
 - (a) provide a visual buffer in the form of landscaping, fencing, buildings or similar between any car parking, service area or outdoor storage area and the adjacent zone
 - (b) provide acoustic treatment between any excessive noise generating part of the development and the adjacent zone.

Land Division

- 10 Land division should create allotments that vary in size and are suitable for a variety of commercial activities.

PROCEDURAL MATTERS

Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

Non-complying Development

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

| Form of development | Exceptions |
|--|---|
| Amusement machine centre | |
| Dairy | |
| Dwelling | Other than an alteration of or addition to an existing dwelling on its existing site. |
| Educational establishment | |
| Farm building | |
| Farming | |
| Fuel depot | |
| General industry | |
| Horticulture | |
| Hospital | |
| Intensive animal keeping | |
| Major public service depot | |
| Nursing home | |
| Place of worship | |
| Pre-school | |
| Prescribed mining operations | |
| Residential flat building | |
| Road transport terminal | |
| Shop or group of shops | Except where it achieves one of the following: (a) the gross leasable area is less than 150 square metres (b) the shop is a bulky goods outlet. |
| Special industry | |
| Stadium | |
| Stock sales yard | |
| Stock slaughter works | |
| Waste reception, storage, treatment, or disposal | |
| Winery | |
| Wrecking yard | |

Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Community Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

OBJECTIVES

- 1 A zone accommodating community, educational and recreational facilities for the general public's benefit.
- 2 Development that is integrated in function and provides a coordinated base to promote efficient service delivery.
- 3 Development that contributes to the desired character of the zone.

DESIRED CHARACTER

The zone caters for a range of community land-uses and public facilities including public recreation, community centres, sporting areas, passive landscaped spaces and cemeteries.

The zone comprises eight sites, seven of which are owned by the Council, including two cemeteries (the North Brighton and St Jude's Cemeteries), the Glenelg North Community Centre (and Kibby Avenue Reserve), Partridge House, and in **Recreation Policy Area 1**, the Brighton and Glenelg Oval complexes, John Matthwin Reserve and sporting area, and the Seacliff Youth Centre.

Development achieve and maintain a high level of presentation, and be welcoming to the public through clearly identified facilities, way-finding signage and clearly marked and well-designed road and pedestrian areas. Landscaping, high quality structures (including artwork) and fencing that minimises barriers and creates an inviting ambience will also enhance these community facilities, as will the screening of waste, service and storage areas. Bicycle parking and storage facilities will also be provided.

Development will enhance the functionality, shared and adaptable use of the sites and incorporate crime-prevention design techniques including lighting, and building materials that deter and minimise the occurrence of vandalism and anti-social behaviour.

Open spaces will be well maintained at all times to uphold a high level of amenity and to provide a safe and functional zone for community use that enhances its surroundings.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the zone:
 - cemetery
 - child care centre
 - community centre
 - consulting room
 - educational establishment
 - emergency services facility
 - hall
 - health facility
 - hospital
 - library
 - office associated with community service

- place of worship
 - public administration office
 - recreation area
 - recreation centre
 - reserve
 - theatre
 - welfare institution.
- 2 Development listed as non-complying is generally inappropriate.
- 3 Development should not be undertaken if it would inhibit or prejudice the integrated development of land within the zone for further community and institutional uses.

Form and Character

- 4 Development should not be undertaken unless it is consistent with the desired character for the zone.

Recreation Policy Area 1

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 A policy area accommodating sporting, entertainment, cultural and recreational activities and associated spectator and administrative facilities.
- 2 Development of integrated recreational areas and facilities that accommodate a range of activities accessible to the community.
- 3 Buildings, facilities and car parks located and designed to blend in with existing or additional trees, vegetation and landscaping.
- 4 Development that contributes to the desired character of the policy area.

DESIRED CHARACTER

This policy area contains four sites and each has a different focus. The Glenelg and Brighton Oval complexes provide for regional sporting uses, including club, spectator and social activities (and ancillary storage), as well as passive recreational opportunities. The John Mathwin Reserve and sporting area will accommodate tennis and grass-hockey, as well as a range of passive recreational uses and local community uses (including music). The Seacliff Youth Centre site will mainly provide for indoor recreation uses and general community uses.

Upgrades to existing sporting facilities, and redevelopment of clubrooms and infrastructure will be consistent with Council Master Plans (in particular Brighton Oval and John Mathwin Reserve) and will create quality areas (in both amenity and functionality) for community use. Upgrades may also include lighting to allow use of the sites in the evening with issues of noise and light-spill appropriately managed, particularly when adjacent to residential areas.

Development will maintain and improve walking and cycling movement within and through the sites and to nearby sites of interest. For example, links from the Glenelg Oval and the Glenelg Primary School to the Glenelg tram stop, links from the Brighton Oval to the Brighton Primary School and Hove (and/or Brighton) train station, and links from the John Mathwin Reserve to the Les Scott Reserve and future redevelopment south of Scholefield Road at the cement hill site located within the City of Marion.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the policy area:
 - car parking
 - clubroom associated with a sports facility
 - community centre
 - community hall
 - entertainment, cultural and exhibition facility
 - indoor and outdoor recreation facility
 - lighting for night use of facilities
 - office associated with community or recreation facility
 - playground
 - showground
 - special event

- spectator and administrative facilities ancillary to recreation development
 - swimming pool.
- 2 A shop or group of shops should only be developed where:
- (a) it is ancillary to recreation and sport development
 - (b) the total gross leasable area is less than 80 square metres.

Form and Character

- 3 Development should not be undertaken unless it is consistent with the desired character for the policy area.

Land Division

- 4 No additional allotments should be created wholly or partly within the policy area.

PROCEDURAL MATTERS

Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

Non-complying Development

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

| Form of development | Exceptions |
|--|--|
| Amusement machine centre | |
| Bus depot | |
| Caravan park | |
| Consulting room | |
| Crematorium | |
| Dwelling | Except where it achieves both of the following: (a) it is ancillary to and in association with an envisaged use (b) it is located on the same allotment. |
| Fuel depot | |
| Hospital | |
| Horticulture | |
| Hotel | |
| Industry | |
| Intensive animal keeping | |
| Land division where located within the Recreation Policy Area 1 | Except where no additional allotments are created partly or wholly within the policy area. |
| Motel | |
| Public service depot | |
| Motor repair station | |
| Nursing home where located in the Recreation Policy Area 1 | |
| Office | Except where associated with and ancillary to community or recreation facilities. |
| Office and dwelling | |
| Petrol filling station | |
| Racecourse | |
| Residential flat building | |
| Road transport terminal | |

| Form of development | Exceptions |
|---|--|
| Service trade premises | |
| Shop or group of shops | Except where the gross leasable area is 250 square metres or less and within the Recreation Policy Area 1 . |
| Stock sales yard | |
| Stock slaughter works | |
| Store | Except where ancillary to and in association with a recreation land use or activity. |
| Telecommunications facility above 30 metres in height | |
| Warehouse | |
| Waste reception, storage, treatment or disposal | |
| Wrecking yard | |

Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development (except where the development is classified as non-complying) are designated:

| Category 1 | Category 2 |
|--|--|
| Store ancillary to and in association with a recreation land use or activity | All other development where located within the Recreation Policy Area 1 |
| Public conveniences | |
| Shelters | |

District Centre Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

OBJECTIVES

- 1 Centres that accommodate a range of retail facilities, offices, consulting rooms, and cultural, community, public administration, entertainment, educational, religious and residential facilities to serve the community and visitors within the surrounding district.
- 2 Development of visually and functionally cohesive and integrated district centres.
- 3 Centres accommodating medium to high-density residential development in conjunction with non-residential development.
- 4 District centres that have a distinctive and attractive shopping strip character, with a predominance of shop windows and entrances, sited close to street frontages.
- 5 Uses and activities providing, in convenient and accessible groupings, convenience goods and comparison goods to serve the day-to-day, weekly and tourist needs of the community.
- 6 Development of public and private car parking areas located and designed to be convenient to the users of the centres with a minimum of impact upon adjoining residential areas.
- 7 The provision of facilities to enhance the function and amenity of the zone for pedestrians.
- 8 Development that contributes to the desired character of the zone.

DESIRED CHARACTER

Development of the **District Centre Zone** will achieve lively, vibrant places that provide for a wide range of retail, commercial, administrative, and community uses, services and functions. It will create inviting pedestrian and cycle friendly centres distinguished by historic architecture, high-quality functional spaces and ease of access.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the zone:
 - affordable housing
 - bank
 - child care centre
 - civic centre
 - consulting room
 - discount department store
 - dwelling in conjunction with non-residential development
 - educational establishment
 - emergency services facility
 - entertainment facility
 - health facility
 - hospital

- hotel
 - indoor games centre
 - library
 - office
 - place of worship
 - playing field
 - pre-school
 - primary school
 - public transport terminal
 - residential flat building in conjunction with non-residential development
 - restaurant
 - shop
 - supermarket
 - swimming pool.
- 2 Development listed as non-complying is generally inappropriate.
- 3 Development comprising a variety of medium density residential (including affordable housing) and non-residential uses should be developed only if it does not prejudice the operation of existing or future non-residential activity within the zone.

Form and Character

- 4 Development should not be undertaken unless it is consistent with the desired character for the zone.
- 5 Development should be sited and designed to promote linkages between the various developments within the centre and adjoining main roads.
- 6 Facilities within the **zone** should be sited and designed with a view to promoting after-hours use to reinforce the centres as the **foci** of social activity in the district.
- 7 Dwellings **and residential flat buildings** should be located only behind or above non-residential uses on the same allotment.
- 8 **Buildings should display:**
- (a) articulated surfaces to visible walls
 - (b) a traditional pattern of wall areas and window spaces
 - (c) use of materials and colour finishes which blend with or complement, without copying, those used on the predominant historic buildings
 - (d) clearly and appropriately defined public entry points.
- 9 Development should incorporate verandas, parapet wall treatments and traditional window display frontages consistent with a traditional, linear shopping centre, providing public walkways and sheltered public and private spaces.
- 10 Outdoor advertisements should be designed and located to be in keeping with the building to which they relate, the character of the area, and the streetscape of a traditional linear shopping centre.
- 11 Advertisements should be designed and erected as follows:
- (a) under canopy signs should comprise the predominant form of signage in the zone and should be rectangular in shape
 - (b) flat wall signs located above canopy level should be of a size and shape relative to the building facade so as not to dominate the facade or appear disproportionate with respect to sign/wall area ratios

- (c) fascia signs should not obscure the alignment of curved or bull nose verandas nor if they project beyond the alignment of the structure to which they are affixed
 - (d) projecting signs above canopy level should only serve to identify the predominant use, name or function of a building and should be in a form which conserves and reinforces the character of the building and the streetscape
 - (e) pylon signs should not be erected within the zone.
- 12 Advertisements should be not erected upon:
- (a) public footways, veranda posts or public utility poles located on public footways
 - (b) roof tops or on the skyline
 - (c) a vehicle adapted and exhibited primarily as an advertisement
 - (d) a building so as to extend above the silhouette of the building
 - (e) land or a building not in use for or related to the message denoted on the advertisement unless located on a building fascia not fronting Jetty Road Glenelg or Moseley Square where the advertisement provides discrete directional and identification signage to an adjacent premises.
- 13 Development should, wherever practicable, provide or make adequate provision for all of the following:
- (a) pedestrian linkages from public to private spaces
 - (b) usable, pleasant and safe, open areas
 - (c) street furniture including seating and rubbish bins
 - (d) appropriate plantings
 - (e) bicycle parking.

Vehicle Parking

- 14 Vehicle parking should be provided in accordance with the rates set out in [Table HoB/1 - Off Street Vehicle Parking Requirements](#) or [Table HoB/1A - Off Street Vehicle Parking Requirements for Designated Areas](#) (whichever applies).

Land Division

- 15 Land division in the zone is appropriate provided new allotments are of a size and configuration to ensure the objectives of the zone can be achieved.

Glenelg Policy Area 2

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 A policy area that has a distinctive and attractive shopping strip character based on low-scale buildings, with a predominance of shop windows and entrances, sited close to street frontages.
- 2 An integrated district shopping and business centre that enhances the metropolitan significance of Moseley Square and the foreshore as seaside recreation, entertainment and leisure places for the broader population and visitors to Adelaide.
- 3 A high degree of pedestrian activity and a vibrant street-life with well lit and engaging shop fronts and business displays including alfresco seating and dining facilities and licensed areas.
- 4 Development that contributes to the desired character of the policy area.

DESIRED CHARACTER

Development within the **Glenelg Policy Area 2** will reinforce and integrate the dining, district shopping and business status of Jetty Road with the seaside recreation, entertainment, dining and leisure function of Moseley Square and the foreshore - maintaining the area as Adelaide's premier seaside tourist destination.

Development within the policy area will integrate with the existing wide range of activities within this unique setting, providing for the day-to-day and weekly shopping needs and commercial functions of the surrounding community, to high-end and specialty retail, offices, consulting rooms, restaurants and entertainment establishments servicing the southern and western regions of Greater Metropolitan Adelaide. Development of the policy area will evolve to meet the demand for new attractions, activities and uses that support the district and its role as terminus of the Glenelg tram line and a major tourist destination, while balancing those metropolitan roles with its local function and the amenity of adjacent residential areas.

Ground floor uses in the parts of a development that directly abut Jetty Road will be non-residential with upper levels utilised for residential, serviced apartments, tourist accommodation and compatible non-residential uses.

Buildings will provide visual interest for pedestrians and contain frequent pedestrian entries and clear windows to the street. Buildings will continue the established width, rhythm and pattern of façades that support a variety of tenancies generally with narrow footprints, while the height of buildings will increase. Buildings will be built with zero set back from the main street, with the occasional section of building set back to create outdoor dining areas, visually interesting building entrances and intimate but active spaces. The footpath will be sheltered with verandas and shelters to enhance the pedestrian experience. Safe, landscaped pedestrian walkways between buildings will encourage permeability to neighbouring streets and enhance the sense of place and identity of the policy area.

Basement or undercroft car parking is contemplated where site circumstances allow appropriate design and integration with the streetscape / built form. Where ventilation is required for basement car parks, vehicles should be screened and landscaped.

The boundaries of the policy area should create a clearly defined centre thereby minimising negative impacts on adjacent residential areas. All centre type activities will be contained within the defined policy area.

Residential development will generally occur above ground level and will incorporate a range of dwelling sizes and a portion of affordable housing. Short term residential accommodation in the form of serviced apartments and tourist accommodation is also desired in locations where it does not compromise the amenity of longer term residents.

Development will achieve the highest possible standards of urban design. In particular, development will acknowledge the increasing importance of pedestrian movements within the centre and access to the centre by bicycle through the provision of services, facilities and amenities aimed at specifically satisfying these users. Car parking will be enhanced progressively through the development of a number of strategically located sites, in both public and private ownership, to meet the car parking needs of the centre whilst encouraging more sustainable modes of transportation.

Strategic Development Sites

Strategic development sites provide opportunities for integrated developments on large sites to assist in the transformation of a locality. Such developments will facilitate growth in the residential population of the Policy Area, while also activating the public realm and creating a vibrant street feel.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

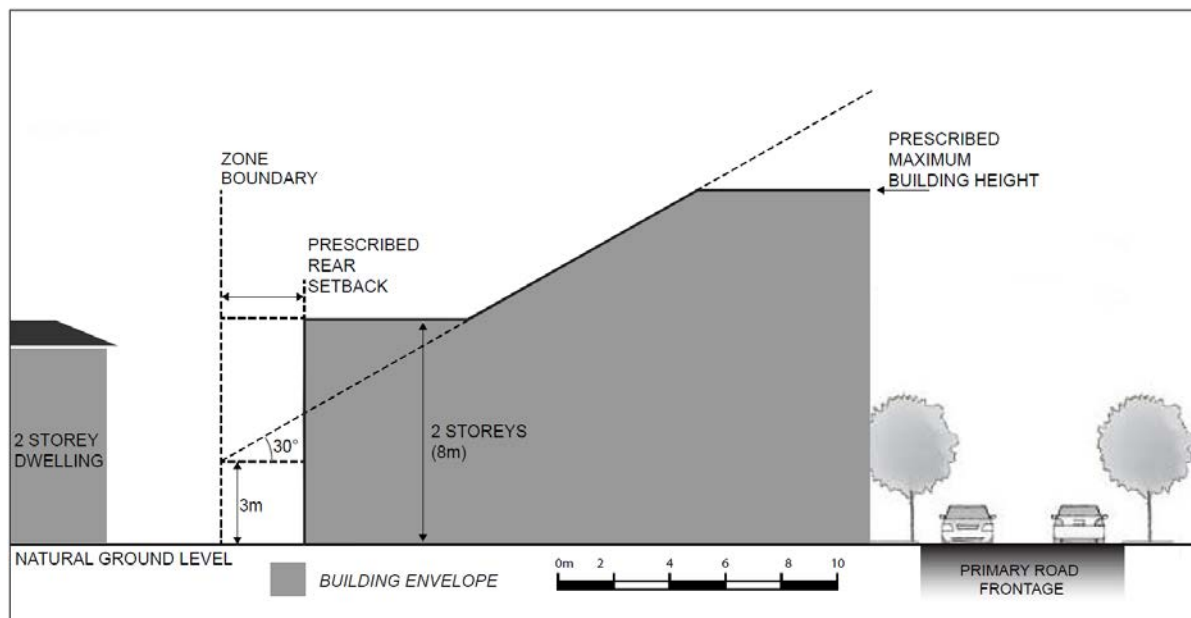
- 1 Amusement machine centres and purpose built gaming and coin operated facilities should not occur within the policy area.

Form and Character

- 2 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 3 The built-form of development should be compatible with and reinforce the traditional shopping strip character of the zone, including where appropriate, features that complement and conserve existing buildings which are of historic or streetscape significance.
- 4 Buildings should be sited to achieve continuity of building alignments to the street frontages.
- 5 Development should be undertaken in accordance with the [Concept Plan Map HoB/1 - Jetty Road and Moseley Square](#).
- 6 Buildings should maintain a pedestrian scale at street level, and should:
 - (a) include a clearly defined podium or street wall fronting the main road and side streets where appropriate, of a height consistent with traditional one and two storey facades and no greater than two storeys or 8.5 metres in height
 - (b) have levels above the defined podium or street wall setback a minimum of 2 metres from that wall.
- 7 Development should not exceed 5 storeys (or 18.5 metres) in height above natural ground level, except:
 - (a) development for residential purposes located above a public car parking structure on the south western corner of Moseley Street and Elizabeth Street (as far west as, but not including, the existing Grand Hotel car parking structure), which should not exceed 5 storeys (or 18.5 metres) in height above the top level of the car park
 - (b) where located on the northern side of Moseley Square, where new buildings should be of a scale and height that does not exceed that of existing adjacent development
 - (c) where located on land bounded by Brighton Road, Augusta Street, Rosa Street and Jetty Road, where new buildings can be built up to 6 storeys (or 22 metres) in height above natural ground level, with the exception of parts of buildings directly adjacent to Jetty Road which should comprise a front wall height of 5 storeys (or 18.5 metres) before stepping up to 6 storeys
 - (d) where located on Strategic Development Sites (sites greater than 1500 square metres [which may include one or more allotments] and have a frontage to Jetty Road or Brighton Road).

- 8 Any portion of a development above 2 storeys (8 metres) in height should be constructed within a building envelope provided by a 30 degree plane measured from a point 3 metres above natural ground level at the zone boundary with the **Residential / Residential Character Zone** (except where this boundary is a primary road corridor i.e. Brighton Road), as illustrated in Figure 1, unless it is demonstrated that the proposed development minimises interface impacts including from building massing, overshadowing and overlooking with adjoining residential development:

Figure 1



- 9 Development of land fronting Torrens Square should be designed and sited to achieve the following:
- (a) enhance the spatial qualities of the square by preserving its sense of enclosure and ensure the harmonious relationship of buildings with St Peters Church
 - (b) maximise views over the square
 - (c) not provide vehicular access onto the square
 - (d) enclose and define the square utilising minimal building setbacks.
- 10 Development within the area marked 'Jetty Road Core Shopping Area' on [Concept Plan Map HoB/1 - Jetty Road and Moseley Square](#) should be designed and sited to achieve the following:
- (a) create a transit-orientated primary retail and business centre, including speciality shops aimed at servicing the tourist industry and visitors
 - (b) locate shopping and service related activities (including restaurants and banks), in ground floor tenancies that have a frontage to Jetty Road
 - (c) primarily locate offices, consulting rooms and personal service establishments above the ground level and on side streets within the centre in a manner compatible with development in adjacent residential zones
 - (d) locate residential uses on upper floor levels and on side streets within the centre in a manner compatible with development in adjacent residential zones.
- 11 Development within the area marked 'Moseley Square Civic/Tourist Area' on [Concept Plan Map HoB/1 - Jetty Road and Moseley Square](#) should:

- (a) consist of land uses that specifically service the needs of tourists and visitors to the foreshore area along with comprising the civic centre for the City
 - (b) comprise primarily of restaurants and take-away food outlets on the southern side of the square and civic functions and community/entertainment uses (including hotels and cinemas) on the northern side of the square.
- 12 Buildings on the northern side of Moseley Square should respect the civic character of existing buildings and reinforce the contrast of built form with open spaces in Moseley Square.
 - 13 Wherever practicable, and particularly on the southern side of the Moseley Square Civic/Tourist Area, development should utilise existing building stock and should upgrade and restore frontages in a manner sympathetic to the original building form.
 - 14 Development should provide pedestrian, bicycle and vehicular access in accordance with [Concept Plan Map HoB/1 - Jetty Road and Moseley Square](#) and may extend beyond the 'Commercial' area.
 - 15 Development should provide on-site car parking areas to the rear of the site and where possible, be linked to form shared parking areas which have access from side streets.
 - 16 Public car parking areas should be developed in suitable locations but not restricted to those, both private and public, as shown on [Concept Plan Map HoB/2 - Car Parking Areas](#).
 - 17 Development should provide car parking within the policy area in accordance with [Table HoB/1 - Off Street Vehicle Parking Requirements](#), [Table HoB/1A - Off-street Vehicle Parking Requirements for Designated Areas](#) (whichever applies), or [Table HoB/1B - Off Street Vehicle Parking Requirements for the Residential High Density Zone or for Residential Uses in the District Centre Zone Glenelg Policy Area 2](#) (whichever applies).
 - 18 Whilst residential development should provide sufficient car parking having regard to the above standards either on the site of the development or on another convenient and accessible site in the locality of the development site, a lesser amount of parking may be appropriate dependent upon:
 - (a) the nature of the development and the past use of the development site
 - (b) the design merits of the development, particularly with respect to the provision of public facilities such as sheltered pedestrian facilities, open space, bicycle facilities and public conveniences
 - (c) existing built-form on or adjacent to the development site which dictates the development of the site in a manner which hampers the provision of onsite car parking
 - (d) the opportunity to exploit shared car parking areas between uses based upon compatible hours of peak operation
 - (e) suitable arrangements for any parking short-fall to be met elsewhere or by other means, being entered into.
 - 19 Buildings should be sited with little or no set-back from public streets and spaces in order to achieve continuity of building alignments to the street frontages, with the occasional section of building set back at ground level to create outdoor dining areas and opportunities for display of goods, visually interesting building entrances and intimate but active spaces.

Strategic Development Sites

- 20 Development on strategic development sites (sites with a frontage to Jetty Road or Brighton Road and over 1500 square metres in area, which may include one or more allotment) should:
 - (a) be constructed in accordance with the building envelope requirement specified in the policy area

- (b) not exceed 7 storeys (or 25.5 metres) in height, or 8 storeys (or 29 metres) in height on land bounded by Brighton Road, Augusta Street, Rosa Street and Jetty Road.
- (c) ensure the massing of taller building elements above 5 storeys (or 18.5 metres) in height are distributed across the site to minimise impacts on the streetscape and residential uses in adjacent zones.

Brighton and Hove Policy Area 16

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 The development of the Brighton Centre in accordance with [Structure Plan Map HoB/2 – Brighton and Hove District Centre](#) to achieve:
 - (a) a multi-purpose centre that incorporates and integrates a range of community and business activities with medium density residential development
 - (b) a strong sense of locality by the coordinated development of consolidated sites and built form achieving a continuous built form edge and development at landmark sites as gateways to the District Centre
 - (c) vibrant and connected pedestrian environment through activated building frontages, co-ordinated pedestrian cover, street furniture and widened footpaths.
- 2 Progressive redevelopment of sites with poor layout and design and underutilised sites.

DESIRED CHARACTER

The Brighton and Hove District Centre will be the focus of retail and community activities and services as well as commercial, office and after-hours activities. The most intensive retail development will be clustered around the existing supermarkets and will develop to service the surrounding community. Retail and commercial activities are to be less intensive in other areas of the zone, particularly south of Edwards Street.

Sites that are underutilised or have poor layout and design will be progressively amalgamated and redeveloped in a coordinated manner to promote a vibrant and community oriented hub.

Residential development above or behind non-residential activities is encouraged throughout the zone. The Centre will provide for a variety of medium density and affordable housing developments, built in conjunction with the non-residential activities of the Centre. Residential development will not prejudice the operation of existing or future non-residential activity within the zone.

Brighton Road and side street frontages will be activated by maximising the extent of fenestration along a continuous built form edge, providing legible entrances and providing pedestrian cover over footpaths with canopies and awnings. Buildings fronting Brighton Road will promote high quality design through active frontages and an attractive environment for pedestrians through setbacks that enable landscaping and streetscape spaces as well as providing set-back consistency with historic buildings. Per [Structure Plan Map HoB/2 – Brighton and Hove District Centre](#), the centre will develop to include gateway buildings at each end of the centre, landmark structures at corner sites, and equally, sensitive development adjacent heritage buildings. Development will be cognisant of the interface with sensitive adjacent land uses, particularly residential areas. Buildings will be in the order of four storeys.

Landscaping along Brighton Road, wider footpaths and the provision of street furniture and art will ensure that the centre is an attractive and pleasant destination at all times. Landscaping will also be utilised to provide a buffer between non-residential developments and adjacent residential areas.

Car parking will be provided as undercroft or to the rear of developments to maximise the centre's presentation to Brighton Road. Car parking will gradually be consolidated and linked to improve overall centre access. The centre will also ensure a network of pedestrian pathways to link key destinations outside of the centre including the Brighton and Hove train stations, Brighton Oval, Brighton Primary School, Brighton Secondary School, Marymount School, St Teresa's School and the beach.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

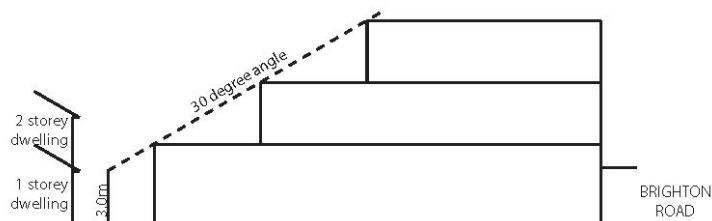
- 1 The following additional forms of development are envisaged in the policy area:
 - community centre.
- 2 Bulky goods outlets and service trade premises that require large bulky buildings or external areas for display should not be developed within the zone.

Form and Character

- 3 Buildings should be developed to a maximum height in the order of 4 storeys, although 5 storeys may be permitted on landmark sites.
- 4 Buildings (excluding verandas, canopies and the like) should be set back from the Brighton Road frontage so as to create a safe and pleasant pedestrian environment and to encourage a consistent built form edge that supports a pedestrian centred environment.
- 5 The ground level street frontages of buildings should contribute to the appearance and retail function of the area by providing at least 5 metres or 60 per cent of the street frontage (whichever is greater) as an entry/ foyer or display window to a shop (including a café or restaurant) or other community or commercial use to provide pedestrian interest and activation.
- 6 Supermarkets should be grouped with small scale specialty retail tenancies along road frontages to maintain an activated edge.
- 7 Development on major corner or landmark sites, should:
 - (a) be designed to emphasise the importance of the street corner including through chamfering of the building façade, prominent entrances, artwork or sculptural features and roof design
 - (b) define and create a landmark building which addresses all street frontages and public spaces by incorporating elements such as verandas, balconies, windows and other articulation in the design of the building
 - (c) incorporate a variety of high quality materials and finishes
 - (d) incorporate paving and landscaping that integrates with those within the public realm, whilst also assisting in framing or terminating a vista.
- 8 Dwellings located above ground level should provide private open space in accordance with the following table:

| Dwelling type | Minimum area of private open space |
|---|------------------------------------|
| Studio (where there is no separate bedroom) | 8 square metres |
| One bedroom dwelling | 8 square metres |
| Two bedroom dwelling | 11 square metres |
| Three or more bedroom dwelling | 15 square metres |

- 9 Development adjacent a residential zone boundary should:
 - (a) be set back within the 30° angle from the zone boundary when measured from a height of three metres above natural ground level (and as shown in figure below):



- (b) locate and design servicing and loading areas to minimise disturbance to residential amenity
 - (c) provide and maintain a landscaped buffer adjacent the property boundary to provide a visual screen to adjacent residential developments
 - (d) include acoustic treatments to building plant and equipment, service and loading areas, and fencing to minimise disturbance to residential amenity as a result of activities on the site.
- 10 Vehicle parking should be located at the rear or underneath buildings in the form of a full basement car park or screened multi-level structures.
- 11 No vehicle access or egress is to be provided directly or indirectly from or to Rutland Avenue.
- 12 Development should minimise the number of access points onto Brighton Road by providing vehicle access:
- (a) from side streets or rear access ways as identified on [Structure Plan Map HoB/2 – Brighton and Hove District Centre](#)
 - (b) via co-ordinated through-property access rights-of-way or common rear vehicle parking areas.
- 13 Development should encourage and facilitate cycling as a mode of transport by incorporating end-of journey facilities including:
- (a) changing facilities and secure lockers for staff
 - (b) signage indicating the location of bicycle facilities
 - (c) bicycle parking facilities provided at the rates set out in the Table below:

| Form of development | Employee / resident (bicycle parking spaces) | Visitor / shopper (bicycle parking spaces) |
|---|---|---|
| Residential component of mixed use building | 1 for every 4 dwellings | 1 for every 10 dwellings |
| Office | 1 for every 200 square metres of gross leasable floor areas | 2, plus 1 per 1000 square metres of gross leasable floor area |
| Shop | 1 for every 300 of gross leasable floor areas | 1 for every 600 square metres of gross leasable floor area |

PROCEDURAL MATTERS

Complying Development

Complying developments are prescribed in schedule 4 of the *Development Regulations 2008*.

In addition, the following forms of development (except where the development is non-complying) are complying:

A change of use to a shop, office, consulting room or any combination of these uses where all of the following are achieved:

- (a) the area to be occupied by the proposed development is located in an existing building and is currently used as a shop, office, consulting room or any combination of these uses
- (b) the development is located inside any of the following area(s):
 - 'Jetty Road Core Shopping Area' as shown on [Concept Plan Map HoB/1 – Jetty Road and Moseley Square](#) of **Glenelg Policy Area 2**
 - **Brighton and Hove Policy Area 16**
- (c) the building is not a State heritage place
- (d) it will not involve any alterations or additions to the external appearance of a local heritage place as viewed from a public road or public space
- (e) if the proposed change of use is for a shop that primarily involves the handling and sale of foodstuffs, it achieves either (i) or (ii):
 - (i) all of the following:
 - (A) areas used for the storage and collection of refuse are sited at least 10 metres from any Residential Zone boundary or a dwelling (other than a dwelling directly associated with the proposed shop)
 - (B) if the shop involves the heating and cooking of foodstuffs in a commercial kitchen and is within 30 metres of any Residential Zone boundary or a dwelling (other than a dwelling directly associated with the proposed shop), an exhaust duct and stack (chimney) exists or is capable of being installed for discharging exhaust emissions
 - (ii) the development is the same or substantially the same as a development, which has previously been granted development approval under the *Development Act 1993* or any subsequent Act and Regulations, and the development is to be undertaken and operated in accordance with the conditions attached to the previously approved development
- (f) if the change in use is for a shop with a gross leasable floor area greater than 250 square metres and has direct frontage to an arterial road, it achieves either (i) or (ii):
 - (i) the primary vehicle access (being the access where the majority of vehicles access / egress the site of the proposed development) is from a road that is not an arterial road
 - (ii) the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared
- (g) off-street vehicular parking is provided in accordance with the rate(s) specified in [Table HoB/1 - Off Street Vehicle Parking Requirements](#) or the desired minimum rate in [Table HoB/1A - Off Street Vehicle Parking Requirements for Designated Areas](#) (whichever table applies) to the nearest whole number, except in any one or more of the following circumstances:

- (i) the building is a local heritage place
- (ii) the development is the same or substantially the same as a development, which has previously been granted development approval under the *Development Act 1993* or any subsequent Act and Regulations, and the number and location of parking spaces is the same or substantially the same as that which was previously approved
- (iii) the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared.

Non-complying Development

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

| Form of development | Exceptions |
|---|---|
| Bus depot | |
| Caravan park | |
| Cemetery | |
| Crematorium | |
| Dwelling | Except where in conjunction with a non-residential development. |
| Electricity generating station | |
| Fuel depot | |
| Gas infrastructure | |
| Industry | |
| Major public service depot | |
| Motor repair station | |
| Residential flat building | Except where in conjunction with a non-residential development. |
| Road transport terminal | |
| Service trade premises | |
| Store | |
| Timber yard | |
| Telecommunications facility above 30 metres in height | |
| Warehouse | |
| Waste reception, storage, treatment or disposal | |
| Water treatment plant | |
| Wrecking yard | |

Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Glenelg Foreshore and Patawalonga Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

OBJECTIVES

- 1 A major tourist and recreational focus for Glenelg and the State including family entertainment facilities, amusements, a range of tourist accommodation facilities including a hotel and apartments, residential housing, marinas, ferry terminal and associated facilities, public plazas, and complementary retail and food outlets.
- 2 A range of tourism and accommodation facilities and activities associated with the Glenelg foreshore and environs.
- 3 Development that is integrated in function and complements the character of adjacent zones.
- 4 Retention of Colley and Wigley Reserves as focal public open space areas, maintained and landscaped for general public recreation.
- 5 Retention of Colley and Wigley Reserves and Patawalonga Recreation and Boat Haven Reserve as the focus of recreational activities.
- 6 Development of the Patawalonga Recreation and Boat Haven Reserve as an area for mooring, incorporating marinas, and use of boats with limited areas for the servicing and repairing of boats.
- 7 Conservation and improvement of the landscape features of the Patawalonga and the coast.
- 8 Development that contributes to the desired character of the zone.
- 9 Maintain the Patawalonga Recreation and Boat Haven Reserve as an area for mooring (incorporating marinas) and use of boats with limited areas for servicing and repairing of boats.

DESIRED CHARACTER

This zone covers an area along the Glenelg foreshore extending from Moseley Square in the south to the Michael Herbert (Wardlipulthu Waadlakatha) Bridge over the Patawalonga in the north and east to Colley Terrace and Adelphi Terrace. The zone has three main functions which are separated spatially including passive recreational uses, high density residential development, and visitor retail and entertainment uses.

The zone is a major seaside tourist and recreational focus for the City of Holdfast Bay and the State. It contains a variety of land uses including recreational, residential, a variety of tourist accommodation, retail, commercial, family entertainment, community, boating, marina and limited service industrial marine related land uses.

The zone contains the recreational focal point of Glenelg outside Moseley Square, being the Glenelg beach (between the Glenelg Jetty and breakwater) the entertainment facilities adjacent to the beach and Colley Reserve at the Holdfast Shores development, and associated entertainment venues and kiosks. The zone also contains community facilities such as the surf lifesaving club, the Glenelg Community Centre and public car parking underground and integrated with the Marina Pier complex.

New development should also have regard to the **Glenelg Foreshore and Patawalonga Zone** provisions as detailed in the Land Not Within a Council Area (Metropolitan) Development Plan and shown on Map LNWCA (MA)/4 and on [Concept Plan Map HoB/5 Extent - Foreshore and Patawalonga Zone](#).

Colley and Wigley Reserves are important open space areas within the zone and buildings and structures will enhance the appearance of these reserves (and the Patawalonga) and encourage their use for recreation.

Buildings and structures will be of a high standard of building material, considering the coastal environment and full lifecycle costs for maintenance and replacement, and designed with regard to external appearance, colours, siting and landscaping.

Development will integrate with adjacent public spaces to produce high quality public environments conducive to public activity, including walking and cycling, and having particular regard to personal safety, sunlight access and visual interest.

Retail, restaurant and entertainment uses will be integrated at ground level with public open spaces and pedestrian routes. Public amenities will be incorporated into the design of commercial and community development.

Public spaces will provide visual and tactile interest, the ability to sit or to participate, a sense of vitality and elements of public art.

A network of linked public pathways will provide public pedestrian and bicycle accessibility to and along the coast, reserves, and to key recreation and other facilities, and will be of a practical and usable width and level.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the zone:
 - family entertainment facilities and amusements
 - ferry terminal and associated facilities
 - marina
 - public plaza
 - residential development
 - retail and food outlets on the southern side of the Patawalonga Basin
 - tourist accommodation, such as hotel and apartment.
- 2 Residential development should incorporate a range of dwelling sizes and types for permanent, temporary and tourist accommodation.
- 3 Stand-alone car parking buildings should not be developed within the zone.

Form and Character

- 4 Development should not be undertaken unless it is consistent with the desired character for the zone.
- 5 Development should be undertaken in accordance with [Concept Plan Map HoB/3 - Foreshore and Patawalonga](#) and [Concept Plan Map HoB/5 - Extent - Foreshore and Patawalonga Zone](#).
- 6 Buildings north of the Patawalonga Basin should not exceed three storeys in height above existing natural ground level.
- 7 Buildings south of the Patawalonga Basin should not contain more than six storeys above undercroft car parking and should not exceed a maximum building height of 28.5 metres above the finished plaza level.
- 8 The average minimum site area per dwelling should measure 80 square metres.
- 9 Development should not restrict public access to or along the beach, or reserve area frontages.

- 10 Basement or undercroft car parking areas and multi-level car parking areas should only occur where contained within commercial/apartment buildings.
- 11 All facilities associated with the supply and maintenance of public utilities should be underground or otherwise screened from view.
- 12 Shopping, commercial, entertainment, marine related and transport uses should have a floor space commensurate with catering for the reasonable needs of tourists, visitors, the boating community, and residents within the zone, and should complement the services and facilities along Jetty Road and Anzac Highway.
- 13 Retail, restaurant and entertainment uses should be integrated at ground level with public open spaces and pedestrian routes, and may extend in to these areas.
- 14 Public and community facilities should be located to conveniently serve visitors and local resident needs, including the provision of recreational facilities, open space, pedestrian walkways and spaces, boat ramp, beach access, public conveniences, required clubs, navigational aids, emergency service facilities and associated car parking. Public toilets should be incorporated into the design of commercial development.
- 15 The appearance of land, buildings, fencing and other structures should be of contemporary design, but not excessively ornate, with empathy for the traditional design elements of the locality and a style complementary to the coastal/seaside environment.
- 16 Construction of breakwaters and any channel to sea should include adequate sand management provisions to minimise or avoid adverse impacts on adjacent coasts and beaches, and to maintain a navigable channel appropriate to the intended use.

Land Division

- 17 Land division should facilitate uses and activities envisaged in the zone and safe and convenient pedestrian access to and along the entire coast and the Patawalonga.

PROCEDURAL MATTERS

Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

Non-complying Development

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

| Form of development | Exceptions |
|------------------------|------------|
| Bulky goods outlet | |
| Industry | |
| Motor repair station | |
| Petrol filling station | |
| Service trade premises | |
| Warehouse | |

Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development (except where the development is classified as non-complying) are designated:

| Category 1 | Category 2 |
|--|------------|
| Dwelling additions and alterations | |
| Outbuildings | |
| Public conveniences | |
| Residential flat building additions and alterations | |
| Shelters (including shade sails) | |
| Store in association with and ancillary to recreational land use or activity | |
| Special events including associated entertainment activities | |

Light Industry Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

OBJECTIVES

- 1 A zone accommodating a range of light industrial, storage and warehouse land uses.
- 2 An area fronting Brighton Road primarily accommodating a range of small-scale business and commercial activities to support the service and industrial functions of the area.
- 3 Development within 60 metres of a residential zone should be designed and developed to minimise any potentially negative noise or visual impacts through:
 - (a) the establishment of landscaped buffers between industrial and residential areas
 - (b) building sound attenuation.
- 4 Development that contributes to the desired character of the zone.

DESIRED CHARACTER

Development will facilitate light industrial land uses which capitalise on the zone's unique geographical position in the centre of the Council area and the close proximity to arterial roads, employment and trade opportunities within the local area.

Development will enhance the distinct zone edge along Brighton Road and Oaklands Road through quality building materials, consistent form and scale, and the coordination of car parking. This will reduce the need for access points to Brighton Road and promote attractive landscaped frontages, free from visual clutter of parked cars and enhanced appearance of buildings and sites.

Development along the suburban edges of the zone of Bowker Street, Vincent Street, Paringa Street and Muriel Avenue (to the south and east) will address the interface between the industrial and residential land uses. In this regard, development will limit operational noise, light, dust, odour, emissions and other nuisances to residential development at the edges of the zone, with intensive industrial land uses located towards the centre of the zone.

Buildings will be designed and include materials which attenuate noise from within buildings and between sites, including through appropriate separation and siting of buildings. Buildings will also have clearly delineated customer entry points and service entrances to ensure safety and convenience, and capitalise on these building elements to provide visual interest to the building façade by way of verandas or porticos.

Buildings will comprise environmentally sustainable design measures and facilitate adaptive renewal energy features such as solar photovoltaic cells and stormwater reuse.

Buildings will be functional and industrial in form, but will take design cues from the character of the area through the introduction of differing materials and finishes to break up expanses of concrete.

Landscaping will break up large expanses of hardstand surfaces and will enhance the appearance of sites by screening unbroken solid walls and visually enhancing front property boundaries. Landscaping will also provide opportunities for water sensitive urban design (including storm-water capture and dispersal), encourage biodiversity, reduce the 'heat island' effect and provide a noise-absorbing buffers at the southern and eastern peripheries of the zone.

Purpose built commercial construction will progressively replace existing remnant building stock. There will be no expansion of the industrial uses envisaged by the zone into the adjacent residential areas.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the zone:
 - light industry
 - office in association with and ancillary to industry
 - service industry
 - store
 - warehouse.
- 2 Development listed as non-complying is generally inappropriate.

Form and Character

- 3 Development should not be undertaken unless it is consistent with the desired character for the zone.
- 4 Advertisements and advertising hoardings should not include any of the following:
 - (a) flashing or animated signs
 - (b) bunting, streamers, flags, wind vanes and similar
 - (c) roof-mounted advertisements projected above the roofline
 - (d) parapet-mounted advertisements projecting above the top of the parapet.
- 5 Turning areas should be paved and constructed with an all-weather surface.
- 6 Storage and other activities associated with industrial development should be contained within buildings or screened in clearly defined areas to avoid an untidy appearance.
- 7 Within 60 metres of a residential zone boundary development should achieve appropriate acoustic performance by:
 - (a) ensuring that all noise sources including machinery, loading, unloading and other service areas are located within the building
 - (b) ensuring buildings are constructed of materials to minimise the transmission of noise, with openings located away from residential areas.
- 8 To minimise the nuisance to householders where the site abuts a residential zone:
 - (a) low impact light industrial development should be located adjacent to the boundary with the residential zone
 - (b) no building in the zone should be located within 10 metres of the boundary of a residential zone.
- 9 A landscaped strip of at least 3 metres should be provided immediately adjacent all public road frontages.
- 10 Buildings should have a floor level of at least 100 millimetres above the level of a 1-in-100 year average return interval flood event.
- 11 Development fronting Brighton Road should be limited to small-scale, low traffic-generating, commercial activities.
- 12 Development fronting Brighton Road should:
 - (a) not create any further access points onto Brighton Road

(b) seek access from adjoining side streets to rear service and parking areas.

13 Retail development should not be undertaken on sites not presently used for that purpose.

14 Site coverage (the proportion of a site covered by ground floor level buildings and structures including outbuildings) should not exceed 50 per cent.

Land Division

15 Land division should create allotments that are of a size and shape suitable for the intended use.

PROCEDURAL MATTERS

Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

Non-complying Development

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

| Form of development | Exceptions |
|----------------------------|--|
| Amusement machine centre | |
| Animal keeping | |
| Animal race track | |
| Caravan park | |
| Community centre | |
| Consulting room | |
| Crematorium | |
| Dairy | |
| Drive-in theatre | |
| Dwelling | Except where: (a) ancillary to and in association with industrial development (b) located on the same allotment. |
| Educational establishment | |
| Emergency service facility | |
| Entertainment venue | |
| Fuel depot | |
| General industry | |
| Hall | |
| Horticulture | |
| Hospital | |

| Form of development | Exceptions |
|--|--|
| Hotel | |
| Intensive animal keeping | |
| Library | |
| Motel | |
| Motor racing or testing venue | |
| Nursing home | |
| Office | Except where: (a) ancillary to and in association with industrial development (b) located on the same allotment. |
| Petrol filling station | |
| Place of worship | |
| Plant nursery | |
| Prescribed mining operations | |
| Pre-school | |
| Primary school | |
| Racecourse | |
| Residential flat building | |
| Restaurant | |
| Road transport terminal | |
| Shop or group of shops | Except where the gross leasable area is less than 250 square metres. |
| Showground | |
| Special industry | |
| Stadium | |
| Stock sales yard | |
| Stock slaughter works | |
| Supported accommodation | |
| Tourist accommodation | |
| Waste reception, storage, treatment or disposal other than in the form of a recycling collection depot | |
| Wrecking yard | |

Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Local Centre Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

OBJECTIVES

- 1 A centre accommodating small-scale convenience shopping, office, medical and community facilities to serve the local community.
- 2 A centre characterised by traditional corner stores or small groups of shops located within easy walking distance of the population they serve.
- 3 A centre accommodating residential development in conjunction with non-residential development.
- 4 Development on sites formed from the consolidation of allotments.
- 5 The establishment and maintenance of visual and acoustic buffers between development in the zone and land in adjacent residential zones.
- 6 Development that contributes to the desired character of the zone.

DESIRED CHARACTER STATEMENT

The zone comprises a number of local centres that are generally surrounded by, or adjacent to, residential development and provide for the daily needs of the local community.

Development in the zone will be compatible in height, scale and form to the immediate locality and create a high standard of presentation to main roads and contribute to a cohesive residential urban form. The provision of sufficient off-street car parking will ensure that surrounding residential amenity is not impacted by patrons parking on the street.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the zone:
 - advertisement
 - community facility
 - consulting room with a maximum gross leasable floor space in the order of 450 square metres
 - office with a maximum gross leasable floor space in the order of 450 square metres
 - parking area
 - shop or group of shops with a maximum gross leasable floor space in the order of 450 square metres.
- 2 Development listed as non-complying is generally inappropriate.

Form and Character

- 3 Development should not be undertaken unless it is consistent with the desired character for the zone.
- 4 Buildings should have a maximum height of no more than one storey above natural ground level.

- 5 Signs and advertising displays should be limited to a message content of only the name and nature of the business conducted on the land on which the sign is displayed to avoid clutter and visual confusion.
- 6 Access from Brighton Road should be limited to minimise disruptions to the flow of traffic. Wherever possible, access should be obtained from adjoining side streets to rear service and parking areas, and all vehicle manoeuvring should be suitably accommodated within the development.

Vehicle Parking

- 7 Vehicle parking should be provided in accordance with the rates set out in [Table HoB/1 - Off Street Vehicle Parking Requirements](#) or [Table HoB/1A - Off Street Vehicle Parking Requirements for Designated Areas](#) (whichever applies).

PROCEDURAL MATTERS

Complying Development

Complying developments are prescribed in schedule 4 of the *Development Regulations 2008*.

In addition, the following forms of development (except where the development is non-complying) are complying:

A change of use to a shop, office, consulting room or any combination of these uses where all of the following are achieved:

- (a) the area to be occupied by the proposed development is located in an existing building and is currently used as a shop, office, consulting room or any combination of these uses
- (b) the building is not a State heritage place
- (c) it will not involve any alterations or additions to the external appearance of a local heritage place as viewed from a public road or public space
- (d) if the proposed change of use is for a shop that primarily involves the handling and sale of foodstuffs, it achieves either (i) or (ii):
 - (i) all of the following:
 - (A) areas used for the storage and collection of refuse are sited at least 10 metres from any Residential Zone boundary or a dwelling (other than a dwelling directly associated with the proposed shop)
 - (B) if the shop involves the heating and cooking of foodstuffs in a commercial kitchen and is within 30 metres of any Residential Zone boundary or a dwelling (other than a dwelling directly associated with the proposed shop), an exhaust duct and stack (chimney) exists or is capable of being installed for discharging exhaust emissions
 - (ii) the development is the same or substantially the same as a development, which has previously been granted development approval under the *Development Act 1993* or any subsequent Act and Regulations, and the development is to be undertaken and operated in accordance with the conditions attached to the previously approved development
- (e) if the change in use is for a shop with a gross leasable floor area greater than 250 square metres and has direct frontage to an arterial road, it achieves either (i) or (ii):
 - (i) the primary vehicle access (being the access where the majority of vehicles access / egress the site of the proposed development) is from a road that is not an arterial road

- (ii) the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared
- (f) off-street vehicular parking is provided in accordance with the rate(s) specified in [Table HoB/1 - Off Street Vehicle Parking Requirements](#) or the desired minimum rate in [Table HoB/1A - Off Street Vehicle Parking Requirements for Designated Areas](#) (whichever table applies) to the nearest whole number, except in any one or more of the following circumstances:
 - (i) the building is a local heritage place
 - (ii) the development is the same or substantially the same as a development, which has previously been granted development approval under the *Development Act 1993* or any subsequent Act and Regulations, and the number and location of parking spaces is the same or substantially the same as that which was previously approved
 - (iii) the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared.

Non-complying Development

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

| Form of development | Exceptions |
|---------------------------|------------|
| Amusement machine centre | |
| Bulky goods outlet | |
| Dwelling | |
| Educational establishment | |
| Fuel depot | |
| Hall | |
| Hospital | |
| Hotel | |
| Indoor recreation centre | |
| Industry | |
| Motel | |
| Motor repair station | |
| Nursing home | |
| Petrol filling station | |
| Place of worship | |
| Pre-school | |
| Public service depot | |

| Form of development | Exceptions |
|---|------------|
| Residential club | |
| Residential flat building | |
| Road transport terminal | |
| Service trade premises | |
| Store | |
| Telecommunications facility above 30 metres in height | |
| Warehouse | |
| Waste reception, storage, treatment or disposal | |
| Water treatment plant | |
| Welfare institution | |
| Wrecking yard | |

Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Mixed Use Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

OBJECTIVES

- 1 A functional and diverse zone accommodating a mix of commercial, medium density residential land use in association with non-residential land use, office, facilities for tourists and small-scale shop land uses.
- 2 Accommodation of a range of bulky goods outlets on the northern side of Anzac Highway.
- 3 Residential development, including a range of tourist accommodation forms, above ground level on the southern side of Anzac Highway.
- 4 Development that maintains and reinforces the historic character of the area and reflects the role of the Anzac Highway as a principal gateway to the Glenelg foreshore area.
- 5 Consolidation, co-ordination and integration of development on adjoining allotments and for land on the northern side of Anzac Highway.
- 6 Development that minimises any adverse impacts upon the amenity of the locality within the zone.
- 7 Development that contributes to the desired character of the zone.

DESIRED CHARACTER

The zone encourages a mix of land uses including medium density housing, offices, consulting rooms and facilities for tourists. Development will include a mix of these uses on single sites which will complement the role and function of the nearby **District Centre Zone**, and are compatible in scale and form with neighbouring residential areas.

Residential development will promote medium densities predominantly in the form of residential flat buildings with ground floors occupied by non-residential land uses. In this regard, development that achieves amalgamation of allotments or shared access to parking arrangements is particularly desirable.

A cohesive character will be created through complementary building scales, setbacks, colours and materials. Likewise, conflict between residential and non-residential development will be limited by acoustic treatment, and siting and separation of built forms where possible.

Development will provide an attractive presentation to the road frontage and public realm through the incorporation of landscaping and by locating storage (including goods storage), waste, loading and service areas to the rear of sites where less visible to public view or screened through quality landscaping.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the zone:
 - bulky goods outlet on the northern side of Anzac Highway
 - consulting room
 - dwelling above non-residential land use

- office
- residential flat building **above non-residential land use**
- service trade premises
- shop or group of shops, where the maximum gross leasable area is in the order of 150 square metres (other than bulky goods outlets north of Anzac Highway)
- warehouse.

2 Development listed as non-complying is generally inappropriate.

Form and Character

3 Development should not be undertaken unless it is consistent with the desired character for the zone.

4 Development located on the northern side of Anzac Highway between Old Tapleys Hill Road and Pasquin Street should be developed in a manner as shown on [Concept Plan Map HoB/4 - Buckle Street](#).

5 The bulk and scale of development should be compatible with adjoining land uses.

6 Buildings should be sited close to or abutting the Anzac Highway frontage and side boundaries where such a streetscape character prevails.

7 Buildings constructed to the street alignment should provide canopies over the footway to provide shelter for pedestrians.

8 Buildings should not exceed three storeys in height above natural ground level and the total vertical wall height at any point, excluding gables, should not exceed 12 metres above natural ground level.

9 Car parking areas should be provided at the rear or side of premises.

10 Development should provide the opportunity for shared use of service access and car parking and integration of car parking areas with adjoining development.

11 Development should be of a low traffic generating nature.

12 Advertisements and advertising hoardings should not include any of the following:

- (a) flashing or animated signs
- (b) bunting, streamers, flags, or wind vanes
- (c) roof-mounted advertisements projected above the roofline
- (d) parapet-mounted advertisements projecting above the top of the parapet.

Vehicle Parking

13 Vehicle parking should be provided in accordance with the rates set out in [Table HoB/1 - Off Street Vehicle Parking Requirements](#) or [Table HoB/1A - Off Street Vehicle Parking Requirements for Designated Areas](#) (whichever applies).

Land Division

14 Land division in the zone is appropriate provided new allotments are of a size and configuration to ensure the objectives of the zone can be achieved.

PROCEDURAL MATTERS

Complying Development

Complying developments are prescribed in schedule 4 of the *Development Regulations 2008*.

In addition, the following forms of development (except where the development is non-complying) are complying:

A change of use to a shop, office, consulting room or any combination of these uses where all of the following are achieved:

- (a) the area to be occupied by the proposed development is located in an existing building and is currently used as a shop, office, consulting room or any combination of these uses
- (b) the building is not a State heritage place
- (c) it will not involve any alterations or additions to the external appearance of a local heritage place as viewed from a public road or public space
- (d) if the proposed change of use is for a shop that primarily involves the handling and sale of foodstuffs, it achieves either (i) or (ii):
 - (i) all of the following:
 - (A) areas used for the storage and collection of refuse are sited at least 10 metres from any Residential Zone boundary or a dwelling (other than a dwelling directly associated with the proposed shop)
 - (B) if the shop involves the heating and cooking of foodstuffs in a commercial kitchen and is within 30 metres of any Residential Zone boundary or a dwelling (other than a dwelling directly associated with the proposed shop), an exhaust duct and stack (chimney) exists or is capable of being installed for discharging exhaust emissions
 - (ii) the development is the same or substantially the same as a development, which has previously been granted development approval under the *Development Act 1993* or any subsequent Act and Regulations, and the development is to be undertaken and operated in accordance with the conditions attached to the previously approved development
- (e) if the change in use is for a shop with a gross leasable floor area greater than 250 square metres and has direct frontage to an arterial road, it achieves either (i) or (ii):
 - (i) the primary vehicle access (being the access where the majority of vehicles access / egress the site of the proposed development) is from a road that is not an arterial road
 - (ii) the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared
- (f) off-street vehicular parking is provided in accordance with the rate(s) specified in [Table HoB/1 - Off Street Vehicle Parking Requirements](#) or the desired minimum rate in [Table HoB/1A - Off Street Vehicle Parking Requirements for Designated Areas](#) (whichever table applies) to the nearest whole number, except in any one or more of the following circumstances:
 - (i) the building is a local heritage place

- (ii) the development is the same or substantially the same as a development, which has previously been granted development approval under the *Development Act 1993* or any subsequent Act and Regulations, and the number and location of parking spaces is the same or substantially the same as that which was previously approved
- (iii) the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared.

Non-complying Development

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

| Form of development | Exceptions |
|------------------------------|------------|
| Amusement machine centre | |
| Bus depot | |
| Community centre | |
| Detached dwelling | |
| Educational establishment | |
| Emergency service facility | |
| Fuel depot | |
| Hall | |
| Horse keeping | |
| Horticulture | |
| Industry | |
| Intensive animal keeping | |
| Motor repair station | |
| Petrol filling station | |
| Place of worship | |
| Prescribed mining operations | |
| Public service depot | |
| Road transport terminal | |
| Showground | |
| Special industry | |
| Stadium | |
| Stock sales yard | |
| Stock slaughter works | |
| Supported accommodation | |

| Form of development | Exceptions |
|---|------------|
| Telecommunications facility above 30 metres in height | |
| Waste reception, storage, treatment or disposal | |
| Wrecking yard | |

Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development (except where the development is classified as non-complying) are designated:

| Category 1 | Category 2 |
|---|--|
| <p>Any of the following forms of development where not located adjacent to a residential zone boundary:</p> <ul style="list-style-type: none"> (a) bulky goods outlet where it is located on the northern side of Anzac Highway (b) consulting room (c) office (d) shop with a gross leasable area of 150 square metres or less. <p>Dwelling other than a detached dwelling Residential flat building</p> | <p>All forms of development not listed as category 1</p> |

Neighbourhood Centre Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

OBJECTIVES

- 1 A centre providing a range of shopping, community, business, and recreational facilities for the surrounding neighbourhood.
- 2 A centre that provides the main focus of business and community life outside a district centre, and provides for the more frequent and regularly recurring needs of a community.
- 3 A centre accommodating residential development in conjunction with non-residential development.
- 4 Development that contributes to the desired character of the zone.

DESIRED CHARACTER

The zone seeks to accommodate a variety of uses, including supermarkets, shops, consulting rooms, offices, restaurants, cafes and hotels.

A significant proportion of the zone shares an interface with residential areas. Activity in the zone will take account of adjacent residential development and development will use appropriate means of screening the activity of the centre from adjacent residential areas wherever possible.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the zone:
 - bank
 - child care facility
 - consulting room
 - dwelling in conjunction with non-residential land use
 - library
 - health centre
 - office
 - petrol filling station
 - place of worship
 - playing field
 - pre-school
 - primary school
 - recreation area
 - restaurant
 - shop
 - supermarket.
- 2 Development listed as non-complying is generally inappropriate.
- 3 Development comprising a variety of residential and non-residential uses should only be undertaken if such development does not prejudice the operation of existing or future non-residential activity within the zone.

- 4 The existing gross leasable area used for retail purposes within the centre at Hove should not be extended.

Form and Character

- 5 Development should not be undertaken unless it is consistent with the desired character for the zone.
- 6 Dwellings should be located only behind or above non-residential uses on the same allotment.
- 7 Within the neighbourhood centre at Hove, existing dwellings converted to commercial uses should retain external residential scale, form and character.

Vehicle Parking

- 8 Vehicle parking should be provided in accordance with the rates set out in [Table HoB/1 - Off Street Vehicle Parking Requirements](#) or [Table HoB/1A - Off Street Vehicle Parking Requirements for Designated Areas](#) (whichever applies).

Land Division

- 9 Land division in the zone is appropriate provided new allotments are of a size and configuration to ensure the objectives of the zone can be achieved.

PROCEDURAL MATTERS

Complying Development

Complying developments are prescribed in schedule 4 of the *Development Regulations 2008*.

In addition, the following forms of development (except where the development is non-complying) are complying:

A change of use to a shop, office, consulting room or any combination of these uses where all of the following are achieved:

- (a) the area to be occupied by the proposed development is located in an existing building and is currently used as a shop, office, consulting room or any combination of these uses
- (b) the building is not a State heritage place
- (c) it will not involve any alterations or additions to the external appearance of a local heritage place as viewed from a public road or public space
- (d) if the proposed change of use is for a shop that primarily involves the handling and sale of foodstuffs, it achieves either (i) or (ii):
 - (i) all of the following:
 - (A) areas used for the storage and collection of refuse are sited at least 10 metres from any Residential Zone boundary or a dwelling (other than a dwelling directly associated with the proposed shop)
 - (B) if the shop involves the heating and cooking of foodstuffs in a commercial kitchen and is within 30 metres of any Residential Zone boundary or a dwelling (other than a dwelling directly associated with the proposed shop), an exhaust duct and stack (chimney) exists or is capable of being installed for discharging exhaust emissions

- (ii) the development is the same or substantially the same as a development, which has previously been granted development approval under the *Development Act 1993* or any subsequent Act and Regulations, and the development is to be undertaken and operated in accordance with the conditions attached to the previously approved development
- (e) if the change in use is for a shop with a gross leasable floor area greater than 250 square metres and has direct frontage to an arterial road, it achieves either (i) or (ii):
 - (i) the primary vehicle access (being the access where the majority of vehicles access / egress the site of the proposed development) is from a road that is not an arterial road
 - (ii) the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared
- (f) off-street vehicular parking is provided in accordance with the rate(s) specified in [Table HoB/1 - Off Street Vehicle Parking Requirements](#) or the desired minimum rate in [Table HoB/1A - Off Street Vehicle Parking Requirements for Designated Areas](#) (whichever table applies) to the nearest whole number, except in any one or more of the following circumstances:
 - (i) the building is a local heritage place
 - (ii) the development is the same or substantially the same as a development, which has previously been granted development approval under the *Development Act 1993* or any subsequent Act and Regulations, and the number and location of parking spaces is the same or substantially the same as that which was previously approved
 - (iii) the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared.

Non-complying Development

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

| Form of development | Exceptions |
|-----------------------------|---|
| Amusement machine centre | |
| Dwelling | Except where in conjunction with a non-residential development. |
| Emergency services facility | |
| Fuel depot | |
| Hall | |
| Horticulture | |
| Indoor recreation centre | |
| Industry | |
| Major public service depot | |
| Motor repair station | |
| Residential flat building | Except where in conjunction with a non-residential development. |

| Form of development | Exceptions |
|---|------------|
| Road transport terminal | |
| Service trade premises | |
| Store | |
| Theatre | |
| Telecommunications facility above 30 metres in height | |
| Warehouse | |
| Waste reception, storage, treatment or disposal | |
| Wrecking yard | |

Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Open Space Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

OBJECTIVES

- 1 A zone:
 - (a) in which the open space character is preserved to provide a visual contrast to the surrounding urban area
 - (b) comprising open space that accommodates a range of public and private activities in an open and natural setting, including:
 - (i) passive and active recreation land uses
 - (ii) habitat conservation and restoration.
- 2 Maintenance of the Patawalonga as a primary area for recreation and water activities including canoeing, sailing, windsurfing and swimming.
- 3 Development that contributes to the desired character of the zone.

DESIRED CHARACTER

The zone encapsulates a number of Council owned reserves that are scattered across the Council area. These open spaces provide important structured and unstructured recreational opportunities for the community of Holdfast Bay and beyond.

They include local reserves in suburban settings, coastal reserves of regional and local appeal (which may cater for Surf Life Saving functions or boating), the historic Old Gum Tree Reserve in Glenelg North, historic Da Costa Park in Glenelg East, coastal reserves in Somerton Park, Brighton and Seacliff, natural reserves in Seacliff and Kingston Park, and the northern sector of the Patawalonga Basin. The Patawalonga Basin is a notable feature of the zone, providing a safe inland waterway that provides a variety of appropriate low-impact water activities including canoeing, sailing, paddleboats and windsurfing.

Development will ensure that the aesthetic qualities of these reserves are preserved and enhanced, and will be appropriate to the current and envisaged public function of the spaces and their local setting.

Development of the Patawalonga Basin will maintain and enhance the character of the area derived from the use of the waterway and landscaped banks of the Patawalonga; with the area along the Patawalonga Basin primarily used for open space with aesthetic, stormwater management and passive recreation functions. Development will be integral to any of these functions provided that structures do not form a dominant feature or reduce the amenity of the Basin. Development will also provide for the establishment and maintenance of a linear open space and biological corridor based on the existing watercourse channel, associated floodplains and vegetation, whilst protecting the existing water quality of the river and the opportunities for recreation.

Development of the linear park will incorporate pedestrian, cycle and recreation facilities, such as drinking fountains and barbecues. Landscaping will incorporate remnant native vegetation and utilise local and/or indigenous vegetation, wherever possible and will provide shaded areas.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the zone:
 - conservation work
 - outbuilding associated with open space maintenance
 - playground
 - recreation area
 - structure associated with a public facility such as car parking, picnic/barbeque area, shelter and toilet
 - toilet block and barbeque facility.
- 2 Development listed as non-complying is generally inappropriate.
- 3 Development should allow for unstructured passive and active recreation

Form and Character

- 4 Development should not be undertaken unless it is consistent with the desired character for the zone.
- 5 Development should only occur where it is integral to the aesthetic, drainage or recreation function of the zone.
- 6 Buildings should be:
 - (a) restricted in size and number
 - (b) sited so as not to detract from the open natural character of the zone
 - (c) constructed of materials which blend with the landscape.
- 7 Development should ensure coordinated design with an emphasis on the creation of pedestrian areas.
- 8 Landscaped buffers should be provided around the perimeter of recreation facilities.
- 9 Landscaping should comprise locally indigenous species and incorporate existing remnant vegetation.
- 10 The natural features of the Patawalonga should be conserved and rehabilitated.
- 11 Development and activities should not cause pollution or contamination of the Patawalonga Basin.
- 12 Development should not create conditions that attract bird species that are likely to disrupt aircraft movement or result in increased risk of bird strike for aircraft using Adelaide Airport.
- 13 Development should not result in external lighting that is likely to be detrimental to safe aircraft navigation.

Land Division

- 14 Land division should not be undertaken except where:
 - (a) it will facilitate the development of envisaged uses in the zone
 - (b) no additional allotments are created.

PROCEDURAL MATTERS

Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

Non-complying Development

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

| Form of development | Exceptions |
|---|---|
| Advertisement and/or advertising hoarding | |
| Amusement machine centre | |
| Consulting room | |
| Crematorium | |
| Dairy | |
| Dwelling | |
| Educational establishment | |
| Fuel depot | |
| Horticulture | |
| Hospital | |
| Hotel | |
| Industry | |
| Intensive animal keeping | |
| Land division | Except where no additional allotments are created partly or wholly within the zone. |
| Motel | |
| Motor repair station | |
| Nursing home | |
| Office | Except where in association with recreation facilities. |
| Petrol filling station | |
| Place of worship | |
| Pre-school | |
| Prescribed mining operations | |
| Public service depot | |
| Residential flat building | |
| Restaurant | |

| Form of development | Exceptions |
|---|--|
| Road transport terminal | |
| Service trade premises | |
| Shop or group of shops | |
| Stock sales yard | |
| Stock slaughter works | |
| Store | Except where it is ancillary to and in association with a recreational land use or activity. |
| Tourist accommodation | |
| Warehouse | |
| Waste reception, storage, treatment or disposal | |
| Wrecking yard | |

Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development (except where the development is classified as non-complying) are designated:

| Category 1 | Category 2 |
|---|---|
| Any of the following forms of development where not located adjacent to a residential zone boundary: (a) outbuildings (b) public conveniences (c) shelters and/or shade structure. | Any of the following forms of development where located adjacent to a residential zone boundary: (a) outbuildings (b) public conveniences (c) shelters and/or shade structure. |

Residential Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

OBJECTIVES

- 1 A residential zone comprising a range of dwelling types, including a minimum of 15 per cent affordable housing.
- 2 Increased dwelling densities in close proximity to centres, public transport routes and public open spaces.
- 3 Development that contributes to the desired character of the zone.

DESIRED CHARACTER

The zone contains the majority of the city's living areas, which are of predominantly low-density suburban form, but within policy areas include medium-to-high density forms of housing on the coast, along key transport corridors and within Glenelg, as well as coordinated development opportunities within large institutional sites. The zone includes five policy areas, three of which cater for coastal development, one for the City's residential institutions (including Minda and Masonic Homes) and one for medium density development along the key transit routes of Brighton Road, Anzac Highway, Tapleys Hill Road and along sections of the Seaford railway transit corridor.

Development outside of the policy areas will be suburban in nature and evolve in response to progressive infill development of existing individual sites and through consolidation of sites to form larger comprehensive redevelopment opportunities. Infill development outside of the Policy Areas will not compromise the suburban character but will progressively increase dwelling densities through unobtrusive small-scale developments. In this regard, infill development will have a comparable height, mass, scale and setbacks to that of existing dwellings in the relevant locality.

The zone's primarily suburban character outside of the policy areas is defined by detached dwellings on individual allotments. Infill development in these suburban areas will contribute to the city's housing diversity through development opportunities that (in order of preference):

- (a) increase dwelling numbers on allotments that have dual road frontages
- (b) provide low scale dwellings at the rear of large allotments with street frontages wide enough to accommodate appropriate sited and sized driveway access and landscaping
- (c) semi-detached dwellings, where site considerations permit.

Development outside of the policy areas will generally be single storey in height in the areas east of Brighton Road, and up to two storeys in height in the areas west of Brighton Road. Buildings will be both domestic and contemporary in design and character to support and reinforce the essentially suburban character through typical domestic design forms, low front fencing and landscaping. Landscaping will help define the public realm and private property boundaries, and substantial landscaped front yards will contribute to the locality, with the retention of mature trees. Development will have side and rear building setbacks that incorporate an access path on one side, with on-boundary built form limited in height, length and location to the equivalent of typical open carports or garaging. Vehicle garaging will be set back clearly behind the immediately adjacent part of the front building facade. Development will enhance and protect streetscape character by minimising driveway access points and width of crossovers and driveways. Undercroft car parking will also be avoided on flat sites and sites that slope down from the street level. Buildings will be stepped and articulated at the front elevation to achieve visual relief and architectural interest as viewed from the street.

Residential development outside of the policy areas will utilise materials and finishes that respond to the character of the immediate locality and utilise brick, stone and rendered finishes to provide visual interest to facades. Development will also incorporate architectural design and detailing that responds to localised character by way of fenestration, doorways, windows, eaves and roof forms. Development will be setback and orientated to minimise impacts of the privacy of neighbouring residents.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the zone:
 - affordable housing
 - domestic outbuilding in association with a dwelling
 - domestic structure
 - dwelling
 - dwelling addition
 - small scale non-residential use that serves the local community, for example:
 - child care facility
 - health and welfare service
 - open space
 - primary and secondary school
 - recreation area
 - supported accommodation.
- 2 Development listed as non-complying is generally inappropriate.
- 3 Except where specified in a particular policy area, vacant or underutilised land should be developed in an efficient and co-ordinated manner to increase housing choice by providing dwellings at densities higher than, but compatible with adjoining residential development.
- 4 Non-residential development such as shops, schools and consulting rooms should be of a nature and scale that:
 - (a) serves the local community
 - (b) is consistent with the character of the locality
 - (c) does not detrimentally impact on the amenity of nearby residents.
- 5 The use and placement of outbuildings should be ancillary to and in association with a dwelling or dwellings.

Form and Character

- 6 Development should not be undertaken unless it is consistent with the desired character for the zone.

- 7 A dwelling should, **except where specified in a particular policy area or precinct**, have a minimum site area (and in the case of **group dwellings and** residential flat buildings, an average site area per dwelling) and a **minimum** frontage to a public road not less than that shown in the following table:

| Dwelling type | Site area (square metres) | Minimum frontage (Except for allotments in the form of a hammerhead configuration) |
|---------------------------|---------------------------|---|
| Detached | 400 minimum | 12 metres |
| Semi-detached | 350 minimum | 12 metres |
| Group dwelling | 350 average | 12 metres |
| Residential flat building | 350 average | 12 metres |
| Row dwelling | 350 minimum | 10 metres |

- 8 Dwellings and residential flat buildings, **except where specified in a particular policy area or precinct**, should not exceed the maximum heights shown in the following table:

| Location of the dwelling | Maximum wall height above natural ground level | Maximum height above natural ground level |
|--|--|---|
| West of Brighton Road or Tapleys Hill Road | 7 metres | Two storeys |
| East of Brighton Road or Tapleys Hill Road | 3.5 metres | One storey, or two storeys if the second storey is incorporated within the roof space and the floor area of the second storey does not exceed 40 percent of the ground floor footprint of the dwelling, including attached garages. |

- 9 Dwellings and/or residential flat buildings should be setback a minimum of 1 metre from one side boundary to incorporate pedestrian access.

Affordable Housing

- 10 Affordable housing should be distributed throughout the zone to avoid over-concentration of similar types of housing in a particular area.
- 11 Development comprising 20 or more dwellings should include a minimum of 15 percent affordable housing (as defined by Notice under the *South Australian Housing Trust Regulations 2010* as amended).

Central West Policy Area 3

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 A policy area primarily accommodating residential development.
- 2 Development designed and located so that the profiles of buildings complement the slope of the land.
- 3 Preservation of the amenity and recreation value of the coastline.
- 4 Development that contributes to the desired character of the policy area.

DESIRED CHARACTER

The policy area covers a significant area fronting the Esplanade extending from Glenelg South through to Seacliff (and including parts of Kingston Park) and accommodates a variety of low to medium-density dwellings. A range of dwelling types up to three storeys in height are envisaged within the policy area.

The metropolitan coastline of Adelaide is an important recreational resource for the community; both for residents who live on the Esplanade and those from the surrounding beach-side suburbs and beyond. The pleasant environment and recreational opportunities afforded by this coastal setting ensure the popularity of these locations for residential development. The policy area will accommodate a greater number of residents over time to capitalise on the pleasant environment provided by the Holdfast Bay coastline, while maintaining the recreation value and amenity of the location for the enjoyment of the wider community.

While a variety of housing forms are appropriate in the policy area, future development will avoid the creation of continuous facades fronting the Esplanade and buildings will be built in response to the slope of the land by minimising the amount of cutting and filling of the natural ground profile. Building design will be of a high architectural standard and incorporate features that reduce the bulk of the development and add visual interest, such as variations in height, roof form, colour and materials, the provision of balconies and porticos and facade articulation. Appropriate low scale landscaping comprising species tolerant of salt-laden winds will feature in front yards to help with breaking up hard sealed areas such as pathways and driveways, and to contribute to the high amenity of the Esplanade locality.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the policy area:
 - domestic outbuilding in association with a dwelling/residential flat building
 - domestic structure
 - dwelling
 - dwelling/residential flat building addition
 - residential flat building.

Form and Character

- 2 Development should not be undertaken unless it is consistent with the desired character for the policy area.

- 3 A dwelling and/or residential flat building should have a minimum site area and a minimum frontage to a public road not less than that shown in the following table:

| Dwelling type | Site area (square metres) | Minimum frontage including a site in the form of a 'hammerhead' or 'battleaxe' configuration (metres) |
|---------------------------|---------------------------|---|
| Detached | 300 | 5 |
| Semi-detached | 250 | 5 |
| Group dwelling | 200 | 5 |
| Residential flat building | 200 | 5 |
| Row dwelling | 200 | 5 |

- 4 Development should not exceed three storeys in height and a vertical wall height at any point, excluding gables, of 10.5 metres above existing natural ground level.

- 5 Development should be setback the following minimum distances:

| Building height (storeys) | Primary road frontage (metres) | Secondary road frontage (metres) | Rear boundary except from a northern boundary as illustrated by 'Figure 3' within Table HoB/2 - Design Principles (metres) | Side boundary except from a northern boundary as illustrated by 'Figure 3' within Table HoB/2 - Design Principles (metres) |
|---------------------------|--------------------------------|----------------------------------|--|--|
| 1 | 6 | 2 | 1 | 1 |
| 2 | 6 | 3 | 3 | 3 |
| 3 | 6 | 3 | 3 | 3 |

- 6 To allow maximum access to solar energy, the minimum setback of a dwelling and/or residential flat building from its northern boundary, as illustrated by 'Figure 3' within [Table HoB/2 - Design Principles](#), should be 3 metres unless the building does not permit a 3 metre setback (which is the case in respect of a semi-detached, row dwelling and/or residential flat building), in which case, an area of private open space containing a rectangle of 6 metres by 4 metres should be provided immediately adjacent to that northern boundary.
- 7 Garages and carports should be setback a minimum of 6 metres from the front property boundary or the same distance as the associated dwelling, whichever is the greater distance from the front property boundary.
- 8 Up to 15 metres of single storey side walls and/or open sided carports and verandas should only be located on one side boundary of a site, provided that:
- (a) the gradient of the site is less than 1-in-10 in any direction
 - (b) the setback to the other side boundary is a minimum of 1 metre
 - (c) the height of the wall and/or the open sided carport does not exceed 2.75 metres above the existing ground level of the site on the boundary.

- 9 To ensure that development does not create a continuous built-form along the Esplanade, the western elevation of any development greater than one storey in height located on the Esplanade should not extend for a distance greater than 90 per cent of the allotment frontage, and in any event, should not create a continuous façade of more than 20 metres in length, as illustrated by 'Figure 2' within [Table HoB/2 - Design Principles](#). Space or spaces used to break up facades of more than 20 metres in length should constitute not less than 10 per cent of the total width of the façade.
- 10 Development of two storeys or more should incorporate architectural features that reduce the bulk of the development and add visual interest, including:
 - (a) variations in height, roof form, colour and materials
 - (b) the provision of balconies and porticos
 - (c) facade articulation.

Institution Policy Area 4

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 A policy area accommodating educational, community or institutional land uses, dwellings, residential flat buildings and housing for aged persons.
- 2 Retention of the open nature of the Sacred Heart College and Brighton High School grounds, as viewed from Brighton Road.
- 3 Cemetery and school facilities located to the west of Brighton Road.
- 4 A transition in the scale and intensity of development along any interface with the **Residential Zone** and **Residential Character Zone**.
- 5 Land divisions that provide for:
 - (a) a variety of allotment sizes suitable for different types and sizes of dwellings
 - (b) street environments which are pleasant and safe for both pedestrian and vehicle movement
 - (c) residential streets and allotments which are arranged so that dwellings can be readily orientated to take most advantage of sun and shade.
- 6 Development that contributes to the desired character of the policy area.

DESIRED CHARACTER

The policy area covers four separate locations in the Council area and incorporates Sacred Heart College, Brighton High School, Minda Incorporated Brighton Campus, Marymount College (the former Mawson High School), Glenelg Primary School, Masonic Homes and Townsend House. Development in the policy area will generally be residential and include medium-density housing forms whilst facilitating the continuing development and operation of institutional activities.

All of the institutions listed above have major areas of open space associated with their facilities and hence have the potential for institutional expansion and/or residential development/redevelopment. Also, there is the possibility of the eventual closure of such facilities. Given the size of these landholdings and their ability to absorb higher residential densities without impact on surrounding areas, it is appropriate that these sites accommodate medium density housing forms.

The redevelopment of these sites will require a comprehensive planning approach to ensure well designed and integrated residential development is achieved. Sustainable design is a key focus, particularly the application of passive design features to minimise thermal load and achieve good natural lighting and ventilation. Any development on these sites must also give due consideration to the retention of heritage places and the setting in which these items are located.

Development in the policy area will comprise safe and pleasant streets, a layout of residential sites to take advantage of environmental conditions and topography and attractive development of open space and varied building form to create interest and diversity in the street environment.

Two and three storey development will incorporate architectural features that reduce the bulk of the development and add visual interest, such as variations in height, roof form, colours and materials, the provision of balconies and porticos and facade articulation.

Height and density will increase towards the centre of the key development sites (being Sacred Heart College, Brighton High School, Minda Incorporated Brighton Campus, Marymount College (the former Mawson High School), Glenelg Primary School, Masonic Homes and Townsend House and decrease at adjoining zone boundaries. The open areas of the schools will be conserved.

The impacts of non-residential development and high traffic levels detract from the amenity of the living environment in close proximity to Brighton Road. This part of the policy area is a barrier to linear expansion of existing commercial areas to the north and south, the extension of which would be detrimental to the living environment and the traffic flow on Brighton Road.

Minda Incorporated Brighton Campus

The Minda Incorporated Brighton Campus site (which is identified on [Concept Plan Map HoB/6 - Minda Incorporated Brighton Campus](#)) will be developed with low to medium rise buildings mainly for residential accommodation. Buildings will be a variety of heights up to 9 storeys along the foreshore area and up to 6 storeys in the rest of the site. Medium density development within an overall target density across the site of around 35-60 dwellings per hectare net is expected, although some parts of the site may be developed above or below this rate, but on average will be within the target range across the site.

Small scale non-residential land uses including shops, offices and consulting rooms will be developed to support residents, along with Minda Incorporated's operations, on the campus and local community.

Retail development will be small in scale and will primarily comprise cafes, restaurants, convenience stores and other tenancies designed to service local community requirements. The amount of retail area within the site will be in the order of 1500 square metres of gross leasable floor area. Retail development will be of a scale that supports an active, mixed use environment, and should primarily be located within a commercial area as identified on [Concept Plan Map HoB/6 - Minda Incorporated Brighton Campus](#).

Development will achieve a high standard of architectural design through careful building articulation and fenestration, with taller buildings (those over 3 storeys) spaced well apart to maintain a sense of openness across the campus.

In general, the greatest height and mass of development will be focussed away from the site's northern, eastern and southern common boundaries, and will reduce in scale to transition down to the interface with low rise residential development adjacent to the Minda Campus.

Overlooking and overshadowing impacts will be moderated through good design. Impacts on adjoining zones will be minimised through appropriate building envelopes, transition of building heights, design and location of windows and balconies.

Open space areas will incorporate the existing secondary dune area on the western side of the campus. The open space areas across the campus will provide connection across the site to the primary dune area located in the adjacent **Coastal Conservation Zone**. Heritage places on the site will front onto common open space to ensure an integrated, cohesive and attractive setting. Open space on the site should also include useable space that is suitable for active recreation and impromptu sporting activities.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

1 The following forms of development are envisaged in the policy area:

- community centre
- domestic outbuilding in association with a dwelling/residential flat building
- domestic structure
- dwelling
- dwelling/residential flat building addition
- indoor recreation centre associated with the Minda Incorporated Brighton campus
- educational establishment

- function centre associated with the Minda Incorporated Brighton Campus
 - housing for aged persons
 - institutional establishment
 - plant nursery associated with the existing institutional use of land at the Minda Incorporated Brighton Campus
 - pre-school associated with the Minda Incorporated Brighton Campus
 - recreation area associated with the Minda Incorporated Brighton Campus
 - residential flat building
 - retirement village associated with the Minda Incorporated Brighton Campus.
- 2 Development of the Glenelg Primary School should include educational and recreational uses and/or dwellings of one and two storeys, together with open space which maintains linkages between the site and the Glenelg Oval.

Form and Character

- 3 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 4 Except on the Minda Incorporated Brighton Campus (where no minimum site area or frontage applies), a dwelling and/or residential flat building should have a minimum site area, (and in the case of residential flat buildings and group dwellings, an average site area per dwelling) and a frontage to a public road not less than that shown in the following table:

| Dwelling type | Site area excluding the area of the 'handle' in the case of a site in the form of a 'hammerhead' or 'battleaxe' configuration (square metres) | Minimum site frontage (metres) |
|---------------------------|---|--------------------------------|
| Detached dwelling | 300 minimum | 5 |
| Semi detached dwelling | 250 minimum | 5 |
| Group dwelling | 200 average | 5 |
| Residential flat building | 200 average | 5 |
| Row dwelling | 200 minimum | 5 |

- 5 Except on the Minda Incorporated Brighton Campus (where no minimum site area applies), where the site area exceeds 1800 square metres in area and comprises a minimum boundary length of 35 metres, the average site area per group dwelling and/or residential flat building should measure a minimum area of 175 square metres, (excluding the area of the 'handle' in the case of a site in the form of a hammerhead/battleaxe configuration).
- 6 Except on the Minda Incorporated Brighton Campus (where development up to nine storeys in height is allowed), development should not exceed three storeys in height and a vertical wall height at any point, excluding gables, of 10.5 metres above existing natural ground level.
- 7 Development should be setback (and on the Minda Incorporated Brighton Campus only in relation to the Campus Site Boundaries identified on [Concept Plan Map HoB/6 - Minda Incorporated Brighton Campus](#)) a minimum of 6 metres from the primary road frontage and 3 metres from a secondary road frontage where an adjoining dwelling is setback 8 metres or more. Otherwise, the following minimum distances apply:

| Parameter | Primary road frontage (metres) | Secondary road frontage (metres) | Rear boundary except from a northern boundary as illustrated by 'Figure 3' within Table HoB/2 - Design Principles (metres) | Side boundary except from a northern boundary as illustrated by 'Figure 3' within Table HoB/2 - Design Principles (metres) |
|--|--------------------------------|----------------------------------|---|---|
| Single storey development or single storey components of a development | 4.5 | 2 | 1 | 1 |
| Two storey development, or two storey components of a development | 4.5 | 2 | 3 | 3 |
| Three storey development, or three storey components of a development | 6 | 3 | 6 metres where: (i) the adjacent side or rear wall of that development contains windows (ii) the southern boundary (being the boundary oriented between 45 degrees and 135 degrees of true north and incorporating the southern extremity of the site, as illustrated by 'Figure 3' within Table HoB/2 - Design Principles) of the site adjoins another dwelling site. | 6 metres where: (i) the adjacent side or rear wall of that development contains windows (ii) the southern boundary (being the boundary oriented between 45 degrees and 135 degrees of true north and incorporating the southern extremity of the site, as illustrated by 'Figure 3' within Table HoB/2 - Design Principles) of the site adjoins another dwelling site. |
| | | | 3 metres in all other cases. | 3 metres in all other cases. |

- 8 To allow maximum access to solar energy, the minimum setback of a dwelling and/or residential flat building from its northern boundary, as illustrated by 'Figure 3' within [Table HoB/2 - Design Principles](#), should be 3 metres unless the building does not permit a 3 metre setback, (which is the case in respect of a semi-detached, row dwelling and/or residential flat building), in which case, an area of private open space containing a rectangle of 6 metres by 4 metres should be provided immediately adjacent to that northern boundary.
- 9 Garages and carports should be setback a minimum of 6 metres from the front property boundary or the same distance as the associated dwelling, whichever is the greater distance from the front property boundary.
- 10 Up to 15 metres of single storey side walls and/or open sided carports and verandas should only be located on one side boundary of a site, provided that:
 - (a) the gradient of the site is less than 1-in-10 in any direction
 - (b) the setback to the other side boundary is a minimum of 1 metre
 - (c) the height of the wall and/or the open sided carport does not exceed 2.75 metres above the existing ground level of the site on the boundary.
- 11 The driveway width of residential development should not exceed 50 per cent of the frontage of the site.

Advertisement and/or Advertising Hoarding

- 12 An advertisement and/or advertising hoarding should be limited to a message content of only the name and nature of the business conducted on the land on which the advertisement and/or advertisement hoarding will be erected.

Land Division

- 13 A variety of allotment sizes should be provided to meet the diverse housing needs of people including allotments suitable for medium-density housing.
- 14 Each allotment, including land in the form of a hammerhead/battleaxe configuration, should have a minimum frontage of 5 metres to a public road. Where the allotment is irregular in shape, the minimum frontage should be widened to allow for safe vehicle access and egress.

Minda Incorporated Brighton Campus

- 15 Development on the Minda Incorporated Brighton Campus should be carried out in accordance with [Concept Plan Map HoB/6 - Minda Incorporated Brighton Campus](#).
- 16 Shops located within the Minda Incorporated Brighton Campus should be of local scale, comprise a number of individual retail tenancies, not exceed in the order of 1500 square metres gross leasable floor area across the whole Campus and should primarily be located within the commercial area as identified on [Concept Plan Map HoB/6 - Minda Incorporated Brighton Campus](#).
- 17 The existing vegetated buffer adjacent to the southern Campus site boundary should be retained to assist with the screening of new development and minimise overlooking from new development.
- 18 Maximum building heights should be in accordance with the following:

| Designated Area | Maximum building height (metres) |
|--|--|
| 'Foreshore Development Area' as identified on Concept Plan Map HoB/6 - Minda Incorporated Brighton Campus | 9 storeys and no more than 32.5 metres |
| Balance of the site within the Institution Policy Area 4 as identified on Concept Plan Map HoB/6 - Minda Incorporated Brighton Campus | 6 storeys and no more than 22 metres |

- 19 Buildings within the foreshore development area (identified on [Concept Plan Map HoB/6 - Minda Incorporated Brighton Campus](#)) should be built at a variety of heights to create visual interest in the site as viewed from the foreshore and coast.
- 20 Public access over that foreshore land required to complete the Coast Park pathway should be provided in accordance with [Concept Plan Map HoB/6 - Minda Incorporated Brighton Campus](#) and should be provided concurrently with any early development of the Minda Brighton Campus Foreshore Development Area.
- 21 To minimise building massing at the interface with adjoining residential development (identified as interface areas on [Concept Plan Map HoB/6 - Minda Incorporated Brighton Campus](#)), building elements adjacent to the eastern, northern and southern Campus site boundaries above three storeys in height should be setback as follows:

| Building height | Setback (metres) |
|---|------------------|
| 4 th storey component of a development | 21 |
| 5 th storey component of a development | 27 |
| 6 th storey component of a development | 33 |

- 22 Building elements adjacent to the **Coastal Conservation Zone** boundary and located south of the area identified as open space on [Concept Plan Map HoB/6 - Minda Incorporated Brighton Campus](#) should be setback 10 metres from the Coastal Conservation Zone boundary.
- 23 Development involving buildings above 4 storeys within the Foreshore Development Area identified within [Concept Plan Map HoB/6 - Minda Incorporated Brighton Campus](#) should be separated from one another by a minimum distance in the order of 25 metres when viewed from the west.

Medium Density Policy Area 5

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 A residential policy area comprising a range of medium-density dwellings, including a minimum of 15 per cent affordable housing, designed to integrate with areas of open space, neighbouring centres or public transport nodes.
- 2 Development that minimises the potential impact of garaging of vehicles on the character of the area.
- 3 Development that supports the viability of community services and infrastructure and reflects good residential design principles.
- 4 Development that contributes to the desired character of the policy area.

DESIRED CHARACTER

The policy area includes areas with immediate frontage to the major transport corridors and nodes throughout the city, namely Tapleys Hill Road, Anzac Highway and Brighton Road. It also includes Jetty Road Brighton, the land each side of the railway line between Jetty Road and the Brighton railway station, and the area adjacent to the Hove railway station.

The policy area has convenient access to services, shopping and businesses in adjacent zones and it also affords a high level of access to public transport routes, both rail and bus, as well as links to the tram at Glenelg.

Development within the policy area will progressively include small and medium-scale redevelopment at medium densities as opportunities arise to meet demand for smaller, conveniently located housing. Consolidation of land holdings into larger sites that accommodate integrated medium-density housing development is encouraged through density and design that achieves efficient use of sites in appropriate infill locations, and minimises vehicle access points, particularly along the arterial road frontages. There is a reduced need for on-site car parking and private open space for well-located medium density dwellings.

All medium density housing forms are appropriate within the policy area, including accommodation for the aged, boarding homes, student accommodation and affordable housing, which take advantage of the proximity of the policy area to transport services and facilities. This will ultimately result in a mix of housing forms, together with semi-detached and detached dwellings that contribute significantly to the range of housing choice in the Council area. To limit the need for vehicle crossovers onto arterial roads, redevelopment of individual allotments for detached dwellings or semi-detached dwellings is not appropriate on arterial road frontages.

Development will contribute positively to the policy area's image and optimise access to public transport, centres and facilities through well-designed medium density residential buildings up to a maximum of 2 storeys (except along the northern side of Buckle Street Glenelg North, with a maximum height of three storeys) and a variety of dwelling styles and sizes. Building design will be domestic in character particularly in the areas adjacent to the Brighton and Hove railway stations and along Jetty Road Brighton. Building siting and design will minimise negative impacts on the existing residential amenity of adjacent zones.

Development will incorporate reduced front setbacks with intensive landscaping and other building treatments such as solid masonry fencing up to 2.2 metres in height along arterial road frontages to facilitate and optimise the practical use of common on-site areas by dwelling occupants. Access points to arterial roads will be restricted and access to sites will preferably be provided to the rear from side streets, where applicable.

Development will incorporate noise attenuation techniques, including into building facades, to minimise traffic noise of arterial roads and the railway line. Buildings will also be designed and sited to limit impact to the existing residential amenity of adjacent zones. Building design will be domestic in character, particularly in the areas adjacent to the Brighton and Hove railway stations and along Jetty Road Brighton.

Recognising that non-residential development detracts from the amenity of the living environment, the linear expansion of existing commercial development activities in the policy area and further intensification of existing non-residential activity is not appropriate.

Non-residential development along Jetty Road Brighton will be limited to the southern side of Jetty Road between Elm Street and the Esplanade and may include dwellings located above non-residential uses. Development in this defined area will seek to retain the 'village amenity' and comprise small shops, with particular attention given to the co-ordinated design of shop fronts to reflect the character of the area (recessed doorways, well-proportioned shop front windows, maximising the extent of fenestration along a continuous built form edge, providing pedestrian cover with canopies, awnings and verandas over the footpath, minimal non-obtrusive and well-integrated building signage where appropriate). Provision for car parking associated with dwellings and or shops should occur on-site (where possible) and at the rear, along with service vehicles and deliveries. Hours of operation should respect nearby residential development.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the policy area:
 - affordable housing
 - detached dwelling
 - domestic outbuilding in association with a dwelling
 - domestic structure
 - dwelling addition
 - group dwelling
 - veranda in association with a dwelling
 - residential flat building
 - row dwelling
 - semi-detached dwelling
 - supported accommodation.
- 2 The use and placement of outbuildings should be ancillary to and in association with residential purposes.

Form and Character

- 3 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 4 Medium density development that achieves gross densities of between 23 and 45 dwellings per hectare (which translates to net densities of between 40 and 67 dwellings per hectare) should be in the form of [two or three storey buildings on the northern side of Buckle Street, Glenelg North as indicated within Concept Plan Map HoB/4 - Buckle Street](#).
- 5 Upper level balconies may extend 1 metre closer to the road boundary than the associated dwelling.
- 6 In the case of multiple dwellings on one site, access to parking and garaging areas from public streets should primarily be via a minimum number of common driveways.
- 7 Development in Buckle Street, Glenelg North should be undertaken in accordance with the [Concept Plan Map HoB/4 - Buckle Street](#).
- 8 Dwellings sited on land in the form of a hammerhead/battleaxe configuration, should have a minimum site area per dwelling of at least 250 square metres, excluding the area used to accommodate a driveway or access way.

- 9 A dwelling should have a minimum site area (and in the case of **group dwellings** and residential flat buildings, an average site area per dwelling) and a frontage to a public road not less than that shown in the following table:

| Dwelling type | Site area other than sites in the form of battleaxe/hammerhead (square metres) | Minimum frontage other than sites in the form of battleaxe/hammerhead (metres) |
|---------------------------|--|--|
| Detached | 250 minimum | 9 |
| Semi-detached | 200 minimum | 9 |
| Group dwelling | 200 average | 7 |
| Residential flat building | 200 average | 7 |
| Row dwelling | 200 minimum | 7 |

- 10 Allotments in the form of a hammerhead/battleaxe configuration should have a minimum frontage to a public road of either (a) or (b):
- (a) 3 metres where servicing 1 to 2 dwellings
 - (b) 5.5 metres where servicing 3 or more dwellings.
- 11 The minimum site area for row dwellings and group dwellings should only be reduced to 150 square metres, excluding the area used to accommodate a driveway or access way, where either (a) or (b) applies:
- (a) a site has a frontage to an arterial road and any of the following are satisfied:
 - (i) access is provided from the rear of the allotment
 - (ii) access is available from a collector road
 - (iii) access is via a common driveway designed to allow vehicles to enter and exit the site in a forward direction, to avoid a proliferation of access points onto busy main roads
 - (b) for all other sites, access is via a common driveway, which is designed to allow vehicles to enter and exit the site in a forward direction, to improve efficiencies in site usage and enhance streetscapes.
- 12 Development should have a maximum height of:
- (a) three storeys and no more than 10.5 metres in vertical wall height measured at any point (excluding gables) above natural ground level, within that area shown on [Concept Plan Map HoB/4 - Buckle Street](#); or otherwise
 - (b) two storeys and no more than 7 metres in vertical wall height measured at any point (excluding gables) above natural ground level.
- 13 Development involving the establishment of undercroft areas for the garaging of vehicles should not occur with the policy area.
- 14 The total floor area for all individual non-residential tenancies along the southern side of Jetty Road Brighton between Elm Street and the Esplanade should generally not exceed 130 square metres, with frontages to Jetty Road no greater than 7 metres in width.

North West Policy Area 6

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 A policy area accommodating residential development.
- 2 Development that contributes to the desired character of the policy area.

DESIRED CHARACTER STATEMENT

The policy area covers a section of development primarily fronting North Esplanade, located between Anderson Avenue and King Street, Glenelg North. The policy area will accommodate a variety of low-density dwelling types.

The metropolitan coastline of Adelaide is an important recreational resource for the broader community and for residents on the Esplanade and in the surrounding beach-side suburbs. The pleasant environment and recreational opportunities afforded by this coastal setting ensure the popularity of these locations for residential development.

The policy area will accommodate single and two storey dwellings that incorporate architectural features to reduce bulk and add visual interest, such as variations in height, roof form, colour and materials, the provision of balconies, porticos and facade articulation. Garaging and carports will not dominate the streetscape. Appropriate low scale landscaping comprising species tolerant of salt-laden winds will feature in front yards and help with breaking up hard sealed areas such as pathways and driveways, and to contribute to the Esplanade locality.

Development will also incorporate noise attenuation measures to limit the adverse effects of aircraft noise from the nearby Adelaide Airport. Aircraft noise related to the operations of the Airport is likely to increase in frequency in the future and new development should respect the long standing use of this facility as the international, national and regional gateway to South Australia.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the policy area:
 - domestic outbuilding in association with a dwelling
 - domestic structure
 - dwelling
 - dwelling addition.

Form and Character

- 2 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 3 Except for development along North Esplanade and King Street, dwellings should satisfy all of the following:
 - (a) a minimum average site area of 500 square metres
 - (b) a maximum height of 2 storeys above natural ground level.

- 4 A dwelling along North Esplanade and King Street should satisfy all of the following:
 - (a) a minimum average site area of 400 square metres
 - (b) a maximum height of 2 storeys above natural ground level.
- 5 Along North Esplanade, development should:
 - (a) create and/or maintain breaks between a building and a building on an adjoining site
 - (b) be setback a minimum of 6 metres from the boundary of North Esplanade
 - (c) not unduly restrict views of the sea or waterfront as seen from the front of any existing building on adjoining land.

South West Policy Area 7

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 A policy area accommodating residential development, residential tourist accommodation, and a limited range of small scale retail.
- 2 Development that contributes to the desired character of the policy area.

DESIRED CHARACTER

The policy area covers three distinct areas at the southern end of the Council area, one being development surrounding the Esplanade in the vicinity of Jetty Road Brighton, another being the coastal strip from just north of Wheatland Street to the Brighton Caravan Park at Seacliff and the third being development along the southern end of Burnham Road, Kingston Park.

The policy area provides primarily for a range of medium-density housing forms but also for a limited range of short-term tourist accommodation. Residential development will avoid the creation of continuous facades fronting the Esplanade and be designed to respond to the slope of the land in order to minimise the amount of cutting and filling of the natural ground profile.

Tourist accommodation will be in the form of holiday apartments, guesthouses and motels, which generally maintain the residential character. Tourist facilities will not incorporate entertainment venues or other non-accommodation uses that may generate excessive levels of noise and traffic. Small-scale retail and food outlets that serve tourists and the local population are envisaged in appropriate locations.

Development will incorporate architectural features to reduce bulk and add visual interest, such as variations in height, roof form, colour and materials, the provision of balconies, porticos and facade articulation. Appropriate low scale landscaping comprising species tolerant of salt-laden winds will feature in front yards to help with breaking up hard sealed areas such as pathways and driveways, and to contribute to the high amenity of the Esplanade and Burnham Road locality.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the policy area:
 - domestic outbuilding in association with a dwelling/residential flat building
 - domestic structure
 - dwelling
 - dwelling/residential flat building addition
 - residential flat building
 - residential tourist accommodation, including:
 - holiday apartment
 - guest house
 - motel
 - small scale non-residential use that services the local community and tourists, for example:
 - shop
 - food outlet.

- 2 Small scale non-residential uses in the form of kiosks, shops and food outlets should only occur within either of the following locations:
 - (a) The Esplanade, Brighton within 100 metres of its intersection with Jetty Road
 - (b) The Esplanade, Seacliff, within 100 metres of its intersection with Wheatland Street.
- 3 Residential tourist accommodation facilities should not incorporate entertainment venues or other uses which cause unreasonable interference through traffic or noise.

Form and Character

- 4 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 5 A dwelling and/or residential flat building should have a minimum site area and a frontage to a public road not less than that shown in the following table:

| Dwelling type | Site area (square metres) | Minimum frontage including land in the form of a hammerhead/battleaxe configuration (metres) |
|---------------------------|---------------------------|--|
| Detached | 300 | 5 |
| Semi detached | 250 | 5 |
| Group dwelling | 200 | 5 |
| Residential flat building | 200 | 5 |
| Row dwelling | 200 | 5 |

- 6 Except for residential or tourist accommodation, development should not exceed 150 square metres in floor area.
- 7 Development should not exceed 3 storeys in height and a vertical wall height at any point, excluding gables, of 10.5 metres above natural ground level, except in the following locations which are able to take advantage of the sloping topography:

| Parameter | Maximum height |
|---|---|
| North of Wheatland Street | 4 storeys, as illustrated by 'Figure 1' within Table HoB/2 - Design Principles |
| South of Wheatland Street and north of Maitland Terrace | 5 storeys, as illustrated by 'Figure 1' within Table HoB/2 - Design Principles |
| South of the Caravan and Tourist Park Zone | 7 storeys but no more than 3 storeys above ground level as illustrated by 'Figure 1' within Table HoB/2 - Design Principles |
| Between Maitland Terrace and the Caravan and Tourist Park Zone | 3 storeys, as illustrated by 'Figure 1' within Table HoB/2 - Design Principles |

- 8 Development should be setback a minimum of 6 metres from the primary road frontage and 3 metres from a secondary road frontage where an adjoining dwelling is setback 8 metres or more. Otherwise, the following minimum distances apply:

| Parameter | Primary road frontage (metres) | Secondary road frontage (metres) | Rear boundary except from a northern boundary as illustrated by 'Figure 3' within Table HoB/2 - Design Principles (metres) | Side boundary except from a northern boundary as illustrated by 'Figure 3' within Table HoB/2 - Design Principles (metres) |
|--|--------------------------------|----------------------------------|---|---|
| Single storey development or single storey components of a development | 4.5 | 2 | 1 | 1 |
| Two storey development, or two storey components of a development | 4.5 | 2 | 3 | 3 |
| Three storey development, or three storey components of a development | 6 | 3 | <p>6 metres where:</p> <p>(i) the adjacent side or rear wall of that development contains windows</p> <p>(ii) the southern boundary (being the boundary oriented between 45 degrees and 135 degrees of true north and incorporating the southern extremity of the site, as illustrated by 'Figure 3' within Table HoB/2 - Design Principles) of the site adjoins another dwelling site.</p> <p>3 metres in all other cases.</p> | <p>6 metres where:</p> <p>(i) the adjacent side or rear wall of that development contains windows</p> <p>(ii) the southern boundary (being the boundary oriented between 45 degrees and 135 degrees of true north and incorporating the southern extremity of the site, as illustrated by 'Figure 3' within Table HoB/2 - Design Principles) of the site adjoins another dwelling site.</p> <p>3 metres in all other cases.</p> |

- 9 To allow maximum access to solar energy, the minimum setback of a dwelling and/or residential flat building from its northern boundary, (as illustrated by 'Figure 3' within [Table HoB/2 - Design Principles](#), should be 3 metres unless the building does not permit a 3 metre setback, (which is the case in respect of a semi detached, row dwelling and/or residential flat building), in which case, an area of private open space containing a rectangle of 6 metres by 4 metres should be provided immediately adjacent to that northern boundary.
- 10 Garages and carports should be setback not less than 6 metres from the road frontage or the same distance as the associated dwelling, whichever is the greater distance.
- 11 Up to 15 metres of single storey side walls and/or open sided carports and verandas should only be located on one side boundary of a site, provided that:
- (a) the gradient of the site is less than 1-in-10 in any direction
 - (b) the setback to the other side boundary is a minimum of 1 metre
 - (c) the height of the wall and/or the open sided carport does not exceed 2.75 metres above the existing ground level of the site on the boundary.

- 12 Development should attempt to preserve the existing landform through designs that relate to the slope of the land.
- 13 To ensure that development does not create a continuous built-form along the Esplanade, the western elevation of any development greater than 1 storey in height located on the Esplanade should not extend for a distance greater than 90 per cent of the allotment frontage, and in any event, should not create a continuous façade of more than 20 metres in length, as illustrated by 'Figure 2' within [Table HoB/2 - Design Principles](#). Space or spaces used to break up facades of more than 20 metres in length should constitute not less than 10 per cent of the total width of the facade.
- 14 The driveway width of residential development should not exceed 50 per cent of the frontage of the site.

PROCEDURAL MATTERS

Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

Non-complying Development

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

| Form of Development | Exceptions |
|---|--|
| Advertisement and / or advertising hoarding | Except where it is located within the Minda Incorporated Brighton Campus . |
| Amusement machine centre | |
| Consulting room | Except where it is located within the Minda Incorporated Brighton Campus . |
| Crematorium | |
| Dairy | |
| Farming | |
| Fuel depot | |
| Horse keeping | |
| Horticulture | |
| Hospital | |
| Hotel | |
| Indoor recreation centre | Except where it is located within the Institution Policy Area 4 . |
| Industry | |
| Intensive animal keeping | |
| Motel | |
| Motor repair station | |
| Office | Except where it is located within the Institution Policy Area 4 and it achieves one of the following: <ul style="list-style-type: none"> (a) it has a gross leasable area of 100 square metres or less (b) it has a gross leasable area of greater than 100 square metres and it is associated with an existing institution. (c) it is located within the Minda Incorporated Brighton Campus. |
| Office and dwelling | |
| Petrol filling station | |
| Public service depot | |
| Road transport terminal | |
| Service trade premises | |

| Form of Development | Exceptions |
|------------------------|--|
| Shop or group of shops | <p>Except where it achieves one of the following:</p> <ul style="list-style-type: none"> (a) it is located within the Medium Density Policy Area 5 and sited on one or more of the following allotments: <ul style="list-style-type: none"> (i) Lot 48, FP 104037, Volume 5140 Folio 759 (66-74 Jetty Road, Brighton) (ii) Lot 12, FP 104009, Volume 5140 Folio 832 (66-74 Jetty Road, Brighton) (iii) Lot 6, DP 2061, Volume 5533 Folio 365 (79-81 Jetty Road, Brighton) (iv) Lot 7, DP 2061, Volume 5226 Folio 261 (75-77 Jetty Road, Brighton) (v) Lot 80 DP 71536, Volume 5969 Folio 247 (71, 71A, 73 & 73A Jetty Road, Brighton) (vi) Lot 80 DP 71536, Volume 5969 Folio 247 (73A Jetty Road, Brighton) (vii) Lot 81 DP 71536, Volume 5969 Folio 248 (73A Jetty Road, Brighton) (viii) Lot 10, DP 2061, Volume 5211 Folio 69 (69, 67-69 Jetty Road, Brighton) (ix) Lot 11, DP 2061, Volume 5332 Folio 938 (65 Jetty Road, Brighton) (x) Lot 12, DP 2061, Volume 5085 Folio 348 (63 Jetty Road, Brighton) (xi) Lot 13, DP 2061, Volume 5452 Folio 559 (61 Jetty Road, Brighton) (xii) Lot 48, DP 56331, Volume 5844 Folio 139 (57 Jetty Road, Brighton) (xiii) Lot 49, DP 56331, Volume 5844 Folio 140 (57 Jetty Road, Brighton) (xiv) Lot 50, DP 56331, Volume 5844 Folio 141 (57 Jetty Road, Brighton) (xv) Lot 1, CP 25404, Volume 6037 Folio 448 (1/51 Jetty Road, Brighton) (xvi) Lot 2, CP 25404, Volume 6037 Folio 449 (2/51 Jetty Road, Brighton) (xvii) Lot 3, CP 25404, Volume 6037 Folio 450 (1/51 Jetty Road, Brighton) (xviii) Lot 4, CP 25404, Volume 6037 Folio 451 (2/51 Jetty Road, Brighton) (xix) Lot 18, DP2061, Volume 5137 Folio 343 (49 Jetty Road, Brighton) (xx) Lot 19, DP2061, Volume 5137 Folio 342 (49 Jetty Road, Brighton) (xxi) Lot 20, DP2061, Volume 5216 Folio 595 (47 Jetty Road, Brighton) (b) it is located within the Institution Policy Area 4 and it achieves one of the following: <ul style="list-style-type: none"> (i) has a gross leasable floor area of 250 square metres or less (ii) is located within the Minda Incorporated Brighton Campus (c) it is located within the South West Policy Area 7 and it has a gross leasable floor area of 250 square metres or less. |
| Stadium | |
| Stock sales yard | |
| Stock slaughter works | |

| Form of Development | Exceptions |
|---|---|
| Store | |
| Theatre | Except where it is located within the Minda Incorporated Brighton Campus . |
| Transmitting station | |
| Warehouse | |
| Waste reception, storage, treatment or disposal | |
| Wastewater treatment plant | |
| Wrecking yard | |

Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development (except where the development is classified as non-complying) are designated:

| Category 1 | Category 2 |
|---|---|
| <p>Development where it is located within the Central West Policy Area 3 or Institution Policy Area 4 (except within the Minda Incorporated Brighton Campus) or South West Policy Area 7 and is 3 storeys or less in height, above natural ground level, comprising one or a combination of the following:</p> <ul style="list-style-type: none"> (a) detached dwelling (b) group dwelling (c) residential flat building (d) semi-detached dwelling (e) row dwelling. <p>Development where it is located within the Minda Incorporated Brighton Campus and is 3 storeys or less in height comprising one or a combination of the following:</p> <ul style="list-style-type: none"> (a) aged persons accommodation (b) all forms of development that are ancillary and in association with residential development (c) community centre (d) consulting room (e) dwelling (f) function centre (g) indoor recreation centre (h) office (i) plant nursery associated with the existing institutional use of land at the Minda Incorporated Brighton Campus (j) pre-school (k) recreation area associated with the Minda Incorporated Brighton Campus (l) residential flat building (m) retirement village | <p>Development where it is located outside a policy area within the Residential Zone or where located within Medium Density Policy Area 5 and it has a wall located on a side or rear property boundary (other than a common wall of semi-detached or row dwellings) exceeding either of the following:</p> <ul style="list-style-type: none"> (a) 2.75 metres in height above natural ground level (b) 10 metres in length or 40 per cent of the length of the boundary, whichever is the lesser. <p>Development where it is located within the Central West Policy Area 3 or Institution Policy Area 4 (except within the Minda Incorporated Brighton Campus) or South West Policy Area 7 and it exceeds 3 storeys in height, above natural ground level, comprising one or a combination of the following:</p> <ul style="list-style-type: none"> (a) detached dwelling (b) group dwelling (c) residential flat building (d) semi-detached dwelling (e) row dwelling. <p>Development within the Minda Incorporated Brighton Campus comprising one or a combination of the following:</p> <ul style="list-style-type: none"> (a) all forms of development not listed as Category 1 (b) any development listed as Category 1 and is more than 3 storeys in height. |

| Category 1 | Category 2 |
|--|------------|
| <ul style="list-style-type: none"> (n) supported accommodation (o) shop or group of shops not exceeding 1500 square metres in total leasable floor area for the Minda Incorporated Brighton Campus. | |

Residential Character Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

OBJECTIVES

- 1 A residential zone primarily accommodating single storey detached dwellings on individual allotments, while providing opportunities for compatible infill development, and the preservation of the existing development patterns and built form.
- 2 Development that is designed to reflect the traditional character elements of the area, particularly as presented to the streetscape.
- 3 Development that contributes to the desired character of the zone.

DESIRED CHARACTER

The zone comprises a number of policy areas in the areas of Glenelg, Glenelg East, Glenelg North, Glenelg South, Brighton and Seacliff. The zone displays and will continue to display strong heritage and historic character created by original subdivision patterns, low-density character, street layouts and concentrations of early dwelling styles including bungalows, cottages, villas and tudors. Development in the zone also displays and will continue to display mature landscaping and biodiversity.

Development will contribute to the character of the zone through the retention, conservation and enhancement of the existing desirable features as articulated in the various policy areas. Residential development and its landscaping will dominate and be consistent with and contribute to the character of the policy area in which it is located.

Development should retain existing older style dwellings of attractive appearance and sound condition which are consistent with the desired character of the policy area.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the zone:
 - affordable housing
 - domestic outbuilding in association with a dwelling
 - domestic structure
 - dwelling
 - dwelling addition
 - small-scale non-residential use that serves the local community, for example:
 - child care facility
 - open space
 - primary and secondary school
 - recreation area
 - supported accommodation.
- 2 Development listed as non-complying is generally inappropriate.

3 Non-residential development should be of a nature and scale that:

- (a) serves the local community
- (b) is consistent with the character of the locality
- (c) does not detrimentally impact on the amenity of nearby residents.

4 The use and placement of outbuildings should be ancillary to and in association with a dwelling or dwellings.

Form and Character

5 Development should not be undertaken unless it is consistent with the desired character for the zone.

6 Development should preserve and enhance streetscapes within the zone by:

- (a) the incorporation of fences and gates in keeping with the height, scale and type of fences in the locality
- (b) limiting the number of driveway crossovers.

7 The conversion of an existing dwelling into two or more dwellings may be undertaken provided that the building and front yard retain the original external appearance to the public road, and minimum site areas and frontage requirements are adhered to (as specified in the policy area).

8 Where a new dwelling is constructed alongside or within a group of older style residential buildings, the new dwelling should be of a similar height, scale and proportions and be constructed of materials that complement and reinforce the character and design elements of existing buildings.

Affordable Housing

9 Development comprising 20 or more dwellings should include a minimum of 15 percent affordable housing (as defined by Notice under the *South Australian Housing Trust Regulations 2010* as amended)

10 Affordable housing should be distributed throughout the zone and/or policy areas to avoid over-concentration of similar types of housing in a particular area.

Central Glenelg Village Policy Area 8

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 An area of heritage value where heritage character and integrity is conserved and reinforced.
- 2 Accommodation of detached, semi-detached and row dwellings at a scale that complements the predominant Victorian housing styles.
- 3 Preservation and enhancement of the spatial qualities of Torrens Square, the relationship between the St Peters Church and other buildings and pleasant vistas along Nile Street and Augusta Street to Torrens Square.
- 4 The retention of subdivision patterns developed during the 1870s and 1880s.
- 5 Development that contributes to the desired character of the policy area.

DESIRED CHARACTER

The policy area incorporates the early streets of Colonel William Light's plan for Glenelg as drawn in 1839 and features Torrens Square at its centre. The policy area represents the early residential development of the initial subdivision around St. Peter's Anglican Church (which occupies Torrens Square) and the historic village of Glenelg.

The policy area protects examples of buildings constructed in a number of periods, including the earliest period of settlement of South Australia distinguished by their low scale, relatively small dimensions and basic building details. Houses of this period, unless extremely well maintained, are often in poor structural condition and development within the policy area will progressively conserve, enhance and protect these buildings for future generations.

In addition to early settlement buildings, there are a large number of houses of the mid-Victorian period within the policy area, which exhibit typical design characteristics of stone walls with render detailing, verandas with decorative cast iron trim and corrugated iron roofs. These houses were generally built during the 1870s and 1880s following subdivision of the original large allotments.

The policy area also contains some houses constructed in the 1920s, which are indicative of a significant period of residential growth in Glenelg.

Development within the policy area will be sympathetic to its siting in relation to buildings from the three periods discussed above. Development of buildings, or development next to buildings, constructed in the earliest period of settlement will conserve and enhance their distinctive features, including their low scale, small dimensions and basic, unrefined building details.

Development of buildings, or next to buildings, of the mid Victorian period, which characterise the policy area, will conserve and enhance the typical design characteristics including stone walls with elaborate render detailing (particularly around windows and doors), verandas with decorative cast iron trim and corrugated iron roofs.

Development of, or adjacent to, housing constructed in the 1920s will complement the scale, roof forms, setbacks and fencing which characterise such bungalow style housing.

The subdivision pattern in the policy area has been retained. All forms of development will reinforce the earliest subdivision pattern in the policy area and will not detract from the important landscape focus being Torrens Square and the church, additions and mature fig trees residing on the Square. In addition, development along Nile Street will reinforce the vistas between St Peter's Anglican Church and the Congregational Church on Jetty Road, Glenelg through the construction of built form of consistent alignment that ensures the continuity of vistas along Nile Street and within Torrens Square.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

1 The following forms of development are envisaged in the policy area:

- detached dwelling
- domestic outbuilding in association with a dwelling
- domestic structure
- row dwelling
- semi-detached dwelling.

Form and Character

2 Development should not be undertaken unless it is consistent with the desired character for the policy area.

3 The average site area per dwelling should not be less than 300 square metres.

4 A building should not exceed 2 storeys above existing natural ground level.

5 Garages and carports should:

- (a) be setback a minimum distance of 500 millimetres from the main face of the associated dwelling or building
- (b) have a maximum opening width of 6 metres or 50 per cent of the allotment frontage, whichever is the lesser.

6 Development adjacent to Torrens Square (the Square) should:

- (a) enhance the spatial qualities of the Square by preserving its sense of enclosure and the harmonious relationship of buildings with St Peter's Anglican Church
- (b) not provide vehicular access onto the Square
- (c) maximise views over the Square
- (d) consist of minimal setbacks and a height of 2 storeys in order to enclose and define the Square.

Da Costa Park Policy Area 9

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 An area of heritage value where heritage character and integrity is conserved and reinforced.
- 2 Accommodation of detached dwellings designed to complement the typical Tudor Revival and Californian Bungalow housing styles.
- 3 Preservation and enhancement of the distinctive street pattern and reserve design defined by many original fences to dwellings.
- 4 Development that contributes to the desired character of the policy area.

DESIRED CHARACTER

The policy area contains a subdivision created in 1923, which is indicative of 1920s town planning practice and the popular use of angled, diagonal streets with a central reserve focus. The subdivision includes over 200 allotments with consistent frontages created around the Da Costa Park Reserve and east of the Glenelg Oval. The Da Costa Park Reserve forms a triangular landscaped area within the subdivision and displays a range of design elements typical of that period. It is edged with a double row of mature trees and is bisected by another tree lined path.

Development will comprise single storey detached dwellings that comprise design elements that complement the typical Tudor Revival and Californian Bungalow styles of housing in the policy area. This development will conserve and enhance the distinctive features of housing from the 1920s era including their strong masonry walling, terracotta tiled roofs (in the case of bungalows) and corrugated iron (in the case of Tudor style houses), scale, setbacks and fencing to the street (in the traditional styles of the 1920s).

Development along St. Peters Street and on allotments facing Da Costa Park Reserve will reinforce vistas between Hawkes Avenue and Short Avenue.

Development adjacent to Da Costa Park Reserve will enhance the spatial qualities of the Square in terms of housing scale, orientation and setback.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the policy area:
 - detached dwelling
 - domestic outbuilding in association with a dwelling
 - domestic structure.
- 2 The following forms of development should not be developed within the policy area:
 - (a) group dwellings
 - (b) residential flat buildings
 - (c) row dwellings.

Form and Character

- 3 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 4 The minimum site area per dwelling should not be less than 500 square metres.
- 5 A building should not exceed 1 storey above existing natural ground level.
- 6 Garages and carports should:
 - (a) be setback a minimum distance of 500 millimetres from the main face of the associated dwelling or building
 - (b) have a maximum opening width of 6 metres or 50 per cent of the allotment frontage, whichever is the lesser.

Maturin Road Policy Area 10

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 An area of heritage value where heritage character and integrity is conserved and reinforced.
- 2 Accommodation of detached dwellings designed to complement typical 1890s and early 20th Century housing styles.
- 3 Preservation and enhancement of the unique spatial qualities of Maturin Road typified by a narrow street reserve combined with allotments with substantial front garden plantings to the street.
- 4 Development that contributes to the desired character of the policy area.

DESIRED CHARACTER

The policy area comprises an area created by the subdivision of two large properties around the turn of the 20th Century. Allotments on the north side of Maturin Road originally formed part of the land around a property known as 'The Olives', Edward A Wright's home. Land on the southeast end of the street formed part of 'Bromley', owned by Colonel Maturin.

The policy area provides an excellent range of dwellings constructed in the late 1890s to early 20th Century and a high proportion of residences contribute to the heritage character of the zone. Development will enhance and complement the distinctive features of existing dwellings in the policy area through single to two-storey detached dwellings with appropriate setbacks, scale, roof forms and roof materials, external materials and facade details, fencing and landscaping that complement the typical 1890s and early 20th Century dwellings within the policy area.

Development will include substantial garden plantings that compensate for the lack of street planting due to the relatively narrow width of Maturin Road. Development will also preserve and enhance the cohesive streetscape in Maturin Road through fences and gates in keeping with the height, scale and type of fencing in the locality.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the policy area:
 - detached dwelling
 - domestic outbuilding in association with a dwelling
 - domestic structure.
- 2 The following forms of development should not be developed within the policy area:
 - (a) group dwellings
 - (b) residential flat buildings
 - (c) row dwellings.

Form and Character

- 3 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 4 The minimum site area per dwelling should not be less than 500 square metres.
- 5 A building should not exceed 2 storeys above existing natural ground level.
- 6 Garages and carports should:
 - (a) be setback a minimum distance of 500 millimetres from the main face of the associated dwelling or building
 - (b) have a maximum opening width of 6 metres or 50 per cent of the allotment frontage, whichever is the lesser.

New Glenelg Policy Area 11

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 An area of heritage value where heritage character and integrity is conserved and reinforced.
- 2 Accommodation of detached and semi-detached dwellings at a scale that complements the predominant late Victorian and early 20th Century housing styles.
- 3 Preservation and enhancement of vistas particularly along Broadway, Moseley Street and Partridge Street, which are characterised by Norfolk Island Pine street tree plantings.
- 4 Development that contributes to the desired character of the policy area.

DESIRED CHARACTER

The policy area comprises an area subdivided by John Bentham Neales in 1850, with the area on the western side subdivided into large allotments, and smaller, narrow, regularly spaced allotments created in the eastern sections from Hastings Street to Brighton Road. Over time, many of the larger allotments were re-subdivided into smaller allotments, with the 1870s and 1880s being the most intense residential development phase until the 1920s when remaining vacant allotments were developed for housing.

This policy area represents typical residential development in Glenelg based on the early subdivision patterns and containing excellent examples of all domestic architectural types, styles and periods from the largest of seaside mansions to the humblest of workers cottages.

Development within the policy area will primarily be single storey detached dwellings and single storey semi-detached dwellings that complement the late Victorian and early 20th Century housing predominant in the locality. Development of buildings, or in the vicinity of buildings, constructed in the late Victorian and early 20th Century periods will conserve and enhance their distinctive features including their wall heights, roof forms, scale, external detailing, external materials and fencing.

Development will conserve and enhance the cohesive streetscapes of the policy area, particularly those east of Moseley Street, having regard to predominant building setbacks, scale, external materials, fencing and appearance of existing dwellings in the locality. Likewise, the rows of Norfolk Island Pines planted on Broadway and Partridge Street, and the oaks in Moseley Street, are notable elements that also contribute to the character of the policy area.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the policy area:
 - detached dwelling
 - domestic outbuilding in association with a dwelling
 - domestic structure
 - semi detached dwelling.

Form and Character

- 2 Development should not be undertaken unless it is consistent with the desired character for the policy area.

- 3 The site area per dwelling should not be less than 500 square metres.
- 4 A building should not exceed 2 storeys above existing natural ground level.
- 5 Garages and carports should:
 - (a) be setback a minimum distance of 500 millimetres from the main face of the associated dwelling or building
 - (b) have a maximum opening width of 6 metres or 50 per cent of the allotment frontage, whichever is the lesser.
- 6 Development along Broadway should preserve and reinforce the vistas between Brighton Road and the foreshore.

Seacliff Policy Area 12

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 Development that contributes to the desired character of the policy area.

DESIRED CHARACTER

The policy area includes the area between Young Street and Pine Avenue, and extends from the allotments behind the Esplanade eastwards to Brighton Road. The railway line is a significant feature within the policy area, creating a significant north-south movement and landscape corridor but restricting east-west access and movement.

The policy area has unique characteristics, having been subdivided and first settled earlier than the adjoining precincts. It is characterised by a sloping landform overlaid with a rectilinear 19th Century pattern of roads resulting in smaller allotment sizes, narrow streets, reduced setbacks, limited street trees and limited stormwater infrastructure. While there is a diversity of architectural eras and styles evident, the policy area is characterised by small to medium scale detached dwellings constructed in the 1880s to 1920s west of the railway line, and a similar development scale constructed during the later 1930s to 1960s east of the railway line. These dwelling forms typically vary between one and two storeys in height throughout the policy area.

Further infill development will be limited in response to the constraints of the existing road and stormwater infrastructure, and the need to preserve public coastal views and open space.

Development will reinforce the existing historic street pattern, urban form and have regard to the slope of the land. With respect to the sloping topography of the policy area, buildings will be designed and sited to limit extensive cut and fill and be sited (and of a form and scale) to protect significant views and vistas. Development will also include landscape buffers and noise attenuation features for development on sites that abut the railway line.

Development will incorporate side, front and rear building setbacks that provide landscaping opportunities, on-site stormwater retention/detention, complement the predominant setbacks within the locality and limit overlooking into neighbouring property. Notwithstanding, development may achieve coastal views through the utilisation of the sloping topography and may require horizontal views over (but not necessarily into) lower sites. Likewise, site topography may allow for overlooking into neighbouring properties more than normally tolerated in other zones and policy areas.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the policy area:
 - domestic outbuilding in association with a dwelling
 - domestic structure
 - dwelling
 - dwelling addition
 - residential flat building.

Form and Character

- 2 Development should not be undertaken unless it is consistent with the desired character for the policy area.

- 3 Development on the western side of Marine Parade and south of Wheatland Street should preserve the existing land form of the coastal cliffs through design that relates to the slope of the land.
- 4 Development to the west of Marine Parade and south of Wheatland Street should incorporate stormwater disposal systems for roofed and paved areas that will ensure there is no soil erosion on the cliff face.
- 5 Development should be limited to single storey and in any case the vertical wall height at any point, excluding gables, should not exceed 3.5 metres above natural ground level.
- 6 Two storey development should only occur where the second storey is incorporated within the roof space and the floor area of the second storey does not exceed 40 per cent of the ground floor footprint of the dwelling and attached garage and/or carport.
- 7 A dwelling should have a minimum site area (and in the case of group dwellings and residential flat buildings, an average land area per dwelling) and a frontage to a public road not less than that shown in the following table:

| Dwelling type | Site area (square metres) | Minimum frontage except for allotments in the form of a hammerhead configuration (metres) |
|---------------------------|---------------------------|---|
| Detached | 450 minimum | 12 |
| Semi-detached | 400 minimum | 12 |
| Group dwelling | 350 average | 12 |
| Residential flat building | 350 average | 12 |
| Row dwelling | 350 minimum | 12 |

- 8 Allotments in the form of a hammerhead/battleaxe configuration should have a minimum frontage to a public road of not less than (a) or (b):
 - (a) 5 metres where servicing 1 to 2 dwellings
 - (b) 6 metres where servicing 3 or more dwellings.
- 9 The driveway or 'handle' portion of a hammerhead/battleaxe allotment should:
 - (a) be compatible with the prevailing pattern of development that exists in the locality
 - (b) be designed to minimise its impact on the visual amenity of the streetscape
 - (c) avoid multiple access points being created by maximising the separation distance between the proposed driveway and other existing driveways.
- 10 Except where it is required to service a hammerhead or battle axe allotment, no more than one vehicle cross-over point should be provided for each dwelling and it should not exceed 4 metres in width.
- 11 Dependent accommodation should not be developed within the policy area.
- 12 Development should not incorporate the establishment of undercroft areas for the garaging of vehicles.

Streetscape Character (Brighton and Glenelg East) Policy Area 13

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 Preservation of the existing development patterns and built form of the policy area.
- 2 Infill development that is designed to reflect the traditional character elements of the area, particularly as presented to the streetscape.
- 3 Development that contributes to the desired character of the policy area.

DESIRED CHARACTER

The policy area features predominantly detached dwellings on medium to large sized allotments, set within heavily landscaped settings and mature street trees that collectively create very high quality, low-density living environments.

The majority of dwellings were constructed between the 1900s and 1930s. Dwelling configuration is typically double or triple fronted with driveway access down one side of the site and a small set back from the non-driveway side boundary. Development will conserve this configuration. Street setbacks are typically 8 metres from primary road frontages with lesser setbacks to the secondary frontages of corner allotments. Front yards are well established containing mature trees, established landscaping and low, visually permeable fencing that adds to the established high amenity of the public environment, which contains established street trees and wide road pavements.

Relative to other residential areas within Holdfast Bay, development in the policy area will be slow and progressive, resulting in the existing predominant character being maintained, albeit in a modified form, with new dwellings replacing older dwellings with contemporary, sympathetically designed buildings and housing forms that respond to new lifestyle choices. New development will respect and enhance the special character qualities of the policy area, without necessarily replicating existing older building forms to achieve this outcome.

Development will complement and reinforce the established suburban character, which is one of rich architectural style, landscaped streets and openness through single storey building scale that exhibits typical residential design forms. There may be some circumstances where site dimensions enable two-storey development, primarily for detached dwellings, provided that they contribute to the established character qualities of the policy areas through key characteristics (for example roof form and side setbacks).

Front setbacks will be consistent with those of adjacent sites, creating a uniform street elevation and enhanced by landscaped front yards that contribute to the established streetscape character. Building design including site coverage, height, massing and architectural detailing will match or relate closely to those of the established detached dwellings in the locality, and utilise stepping and articulation of the front elevation to achieve visual relief and architectural interest as viewed from the street. Development will utilise visible hip or gable roof forms, and materials such as corrugated iron or shingle roofing that respond to the character of the immediate locality, including brick, stone, timber and part-rendered finish typical of the era of the policy area's development and providing visual interest to facades. Development will also incorporate architectural design and detailing that responds to localised character by way of fenestration, doorways, windows, eaves and roof forms, utilising materials that are generally dark in colour to limit glare and reflection.

Garaging will be located so that it is set back clearly behind the alignment of the adjacent domestic portion of the dwelling with front and side setbacks that reflect predominant setbacks of sites in the locality. Driveway design will also be subsidiary to the domestic built form with the use of a single vehicle crossover point for each dwelling and landscaping that provides breaks to expanses of hardstand surfaces.

Low and visually permeable front fencing will be incorporated to clearly define the boundary between public and private property whilst maintaining an open streetscape. Fencing forms and materials will complement and enhance the associated dwelling, and may include low masonry fencing (often stepped), simple timber paling fencing, a combination of paling and masonry fencing, metal railing and masonry fencing, rock-face sandstone, cyclone mesh and crimped wire fencing. Brush fencing (up to a maximum of 1400 millimetres high with either rolled or metal capping) and hedging may also be appropriate, but picket fences and elaborate masonry fences are not appropriate in the policy area.

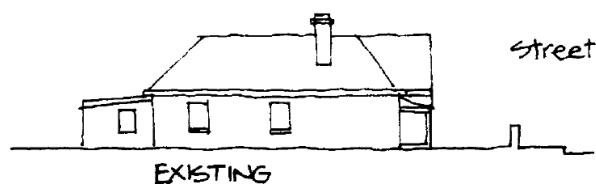
PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the policy area:
 - detached dwelling
 - domestic outbuilding in association with a dwelling
 - domestic structure
 - dwelling addition
 - semi detached dwelling.
- 2 The following forms of development should not be developed within the policy area:
 - (a) group dwellings
 - (b) residential flat buildings
 - (c) row dwellings
 - (d) dependent accommodation
 - (e) development that incorporates the establishment of undercroft areas for the garaging of vehicles
 - (f) development in the form of a hammerhead/battleaxe configuration.

Form and Character

- 3 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 4 Development should be limited to one storey, except where a dwelling faces a public road (i.e. is not sited in a battle-axe allotment or at the rear of a development site) and any of the following is proposed:
 - (a) the development is located east of Brighton Road in which case it must not have a vertical wall height exceeding 3.5 metres at any point above natural ground level
 - (b) sympathetic two-storey additions that use existing roof space or incorporate minor extensions of roof space to the rear of the dwelling (refer to the figure below)



- (c) in new dwellings, a second storey within the roof space where the overall building height, scale and form is compatible with existing single-storey development in the locality (refer to the figure below)



- (d) dormer windows with a total length less than 30 per cent of the total roof length along each elevation
- (e) the floor area of the second storey should be a maximum of 40 percent of the ground floor footprint of the dwelling and attached garage.
- 5 A dwelling should have a minimum site area and a frontage to a public road not less than that shown in the following table:

| Dwelling type | Site area (square metres) | Minimum frontage (metres) |
|---------------|------------------------------|------------------------------|
| Detached | 600 minimum | 14 |
| Semi-detached | 600 minimum | 14 |

- 6 Development should conserve, enhance and complement the distinctive features of existing dwellings in the policy area, including their:
- (a) front, side and rear dwelling setbacks
- (b) scale

- (c) roof forms
 - (d) roof materials
 - (e) external materials
 - (f) facade details
 - (g) fencing.
- 7 Garages and carports facing the street (other than an access lane way) should be designed with a maximum width (including the total width of any support structure) of 6.5 metres or 30 per cent of the building site frontage width, whichever is the lesser distance.

Streetscape Character (Glenelg and Glenelg North) Policy Area 14

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 Preservation of the existing development patterns and built form of the policy area.
- 2 Infill development that is designed to reflect the traditional character elements of the area, particularly as presented to the streetscape.
- 3 Development that contributes to the desired character of the policy area.

DESIRED CHARACTER

The policy area features consistent design themes comprising single storey brick and stone dwellings with substantial hipped or gabled roof forms, consistent building setbacks, landscaped front yards and visually permeable fencing facing public streets. The Glenelg North portion of the policy area is characterised by smaller scale and simplified building forms. While some infill residential flat development has occurred in parts of the policy area, this does not represent the predominant character and is not the form of desired future development.

The Glenelg portion of the policy area is characterised by a variety of single and two storey dwelling forms, typically on individual allotments. This part of the policy area contains some examples of buildings that were constructed in the earliest period of settlement of South Australia, which are characterised by their low scale, relatively small dimensions and simple building details.

The majority of dwellings were constructed between the 1880s and 1920s, are predominantly single storey in height and built on allotments that range in area from 200 to 600 square metres. These dwellings typically comprise semi-detached, small detached and row dwelling forms with small setbacks from the street. Setbacks typically range between 3 and 6 metres (and less from secondary frontages for corner allotments). These setbacks create small front yards that, when added to the low (less than 1 metre in height) and simple fencing forms such as timber picket, paling, woven wire on timber frames or rolled steel fencing, and typical cottage gardens, add to the intimate scale of the established streetscape.

Relative to other residential areas, development in the policy area will be slow and progressive, resulting in the existing predominant character being maintained, albeit in a modified form, with increasing numbers of new dwellings as older dwellings are replaced with contemporary, sympathetically designed buildings and housing forms that respond to new lifestyle choices. Development will respect and enhance the special character qualities of the policy area, without necessarily replicating existing older building forms to achieve this outcome.

Development will reinforce the character of the policy area established by the existing mix of predominantly small-scale traditional dwelling types, and complement those dwelling types through appropriate mass, composition, architectural detailing, materials and colours. New buildings will incorporate pitched roof forms together with veranda and portico elements. Where row dwellings are a dominant feature in the streetscape, new buildings may also incorporate parapets to front facades.

Development will be single storey in scale and exhibit typical residential design forms that are complementary to the predominant established character of the policy area. There may be some circumstances where site dimensions enable two-storey development, primarily for detached dwellings, provided this development contributes to the streetscape through other key characteristics (eg development footprint, height, massing and architectural detail) that will match or relate closely to those of the established detached dwellings in the locality.

Front setbacks will be consistent with those of adjacent sites, creating a uniform street elevation defined by landscaped front yards that contribute to the established streetscape character. Development will utilise visible hip or gable roof forms, and materials and finishes that respond to the character of the immediate locality, including brick, stone and rendered finish providing visual interest to facades. Development will also incorporate architectural design and detailing that responds to localised character by way of fenestration, doorways, windows, eaves and roof forms, utilising materials that are generally dark in colour to limit glare and reflection.

Garaging will be located clearly behind the alignment of the adjacent domestic portion of the dwelling with front and side setbacks that reflect predominant setbacks of sites in the locality. Driveway design will also be subsidiary to the domestic built form with the use of a single vehicle crossover point for each dwelling and landscaping that provides breaks to expanses of hardstand surfaces.

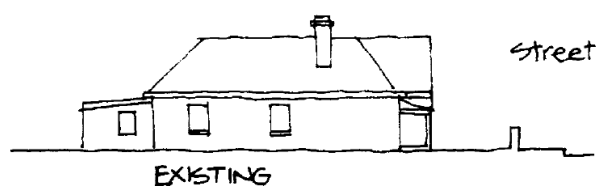
PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the policy area:
 - detached dwelling
 - domestic outbuilding in association with a dwelling
 - domestic structure
 - dwelling addition
 - row dwelling
 - semi detached dwelling.
- 2 The following forms of development should not be developed within the policy area:
 - (a) group dwellings
 - (b) residential flat buildings
 - (c) dependent accommodation
 - (d) development that incorporates the establishment of undercroft areas for the garaging of vehicles
 - (e) development in the form of a hammerhead/battleaxe configuration.

Form and Character

- 3 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 4 Development should be limited to one storey, except where a dwelling faces a public road (ie is not sited on a battleaxe allotment or at the rear of a development site) and any of the following is proposed:
 - (a) the development is located east of Brighton or Tapleys Hill Road in which case it must not have a vertical wall height exceeding 3.5 metres at any point above natural ground level
 - (b) sympathetic two-storey additions that use existing roof space or incorporate minor extensions of roof space to the rear of the dwelling (refer to the figure below)



- (c) in new dwellings, a second storey within the roof space where the overall building height, scale and form is compatible with existing single-storey development in the locality (refer to the figure below)



- (d) dormer windows with a total length less than 30 per cent of the total roof length along each elevation.
- (e) the floor area of the second storey should be a maximum of 40 percent of the ground floor footprint of the dwelling and attached garage
- 5 A dwelling should have a minimum site area and a frontage to a public road not less than that shown in the following table:

| Dwelling type | Minimum site area other than for affordable housing (square metres) | Minimum frontage (metres) |
|---------------|---|---------------------------|
| Detached | 350 | 11 |
| Semi-detached | 300 | 11 |
| Row Dwelling | 250 | 8 |

- 6 Development should conserve, enhance and complement the distinctive features of existing dwellings in the policy area, including their:
- (a) front, side and rear dwelling setbacks

- (b) scale
 - (c) roof forms
 - (d) roof materials
 - (e) external materials
 - (f) façade details
 - (g) fencing.
- 7 Garages and carports facing the street (other than an access lane way) should be designed with a maximum width (including the total width of any support structure) of 6.5 metres or 30 per cent of the building site frontage width, whichever is the lesser distance.

PROCEDURAL MATTERS

Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

Non-complying Development

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

| Form of Development | Exceptions |
|--|------------|
| Advertisement and/or advertising hoarding | |
| Amusement machine centre | |
| Auction room | |
| Bus depot | |
| Bus station | |
| Caravan park | |
| Cemetery | |
| Community centre | |
| Consulting room | |
| Crematorium | |
| Dairy | |
| Farming | |
| Fuel depot | |
| Group dwelling where it is located within one of the following policy areas: | |
| (a) Da Costa Park Policy Area 9 | |
| (b) Maturin Road Policy Area 10. | |
| Hall | |
| Horse keeping | |
| Horticulture | |
| Hospital | |
| Hotel | |
| Indoor recreation centre | |
| Industry | |
| Intensive animal keeping | |
| Motel | |
| Motor repair station | |

| Form of Development | Exceptions |
|---|------------|
| Office | |
| Petrol filling station | |
| Public service depot | |
| Residential flat building where it is located within one of the following policy areas: | |
| (a) Da Costa Park Policy Area 9 | |
| (b) Maturin Road Policy Area 10. | |
| Restaurant | |
| Road transport terminal | |
| Row dwelling where it is located within one of the following policy areas: | |
| (a) Da Costa Park Policy Area 9 | |
| (b) Maturin Road Policy Area 10. | |
| Service trade premises | |
| Shop or group of shops | |
| Stadium | |
| Stock sales yard | |
| Stock slaughter works | |
| Store | |
| Theatre | |
| Telecommunications facility | |
| Warehouse | |
| Waste reception, storage, treatment or disposal | |
| Wrecking yard | |

Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development (except where the development is classified as non-complying) are designated:

| Category 1 | Category 2 |
|------------|--|
| | <p>Development where it is located within the Seacliff Policy Area 12 or Streetscape Character (Brighton and Glenelg East) Policy Area 13 or Streetscape Character (Glenelg and Glenelg North) Policy Area 14 and it has a wall located on a side or rear property boundary (other than a common wall of semi-detached or row dwellings) that exceeds one of the following:</p> <ul style="list-style-type: none">(a) 2.75 metres in height above natural ground level(b) 10 metres in length or 40 per cent of the length of the boundary, whichever is the lesser. |

Residential High Density Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

OBJECTIVES

- 1 A residential zone comprising a range of high density dwellings, including a minimum of 15 per cent affordable housing, primarily in the form of row dwellings and residential flat buildings, designed to integrate with areas of open space, neighbouring centres or public transport nodes.
- 2 Development that supports the viability of community services and infrastructure.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the zone:
 - affordable housing
 - residential flat building
 - row dwelling
 - small scale non-residential use that serves the local community, for example:
 - child care facility
 - open space
 - recreation area
 - shop, office or consulting room
 - supported accommodation.
- 2 Development listed as non-complying is generally inappropriate.
- 3 Non-residential development such as shops, schools and consulting rooms should be of a nature and scale that:
 - (a) serves the local community
 - (b) is consistent with the character of the locality
 - (c) does not detrimentally impact on the amenity of nearby residents.
- 4 The use and placement of outbuildings should be ancillary to and in association with a dwelling or dwellings.

Form and Character

- 5 A residential flat building should provide a variety of dwelling sizes (e.g. bed-sit, one, two and three bedrooms) particularly in larger complexes.
- 6 Medium density development that achieves gross densities of between 23 and 45 dwellings per hectare (which translates to net densities of between 40 and 67 dwellings per hectare) should typically be in the form of 2 to 4 storey buildings.
- 7 High density development that achieves gross densities of more than 45 dwellings per hectare (which translates to net densities of more than 67 dwellings per hectare) should typically be in the form of over 4 storey buildings.

- 8 Development should result in high-quality aesthetic and urban design outcomes, and where possible, allotments should be amalgamated to assist the achievement of this.
- 9 Ground floor dwellings and accommodation should contribute to the desired streetscape of a locality and, where applicable, create active, safe streets by incorporating either or both of the following:
 - (a) front landscaping or terraces that contribute to the spatial and visual structure of the street while maintaining adequate privacy for apartment occupants
 - (b) individual entries for ground floor accommodation.
- 10 All residential development should be designed to ensure the living rooms have an external outlook (an outlook being a short range prospect, as distinct from a view which is more extensive and long range to particular objects or geographic features). Living rooms should not have an outlook only through high level windows and/or a skylight.
- 11 Garages and carports that face and take direct access from the primary street should be avoided. Where there is no practical alternative to their location, garages and carports facing the street (other than an access lane way) should be designed with a maximum width of 6 metres or 50 per cent of the allotment or building site frontage width, whichever is the lesser distance.
- 12 The development of a residential flat building or of group dwellings should include minimum private open space of at least the area shown in the following table:

| Configuration | Open space requirement, other than for affordable housing |
|-----------------------------------|---|
| Studio (without separate bedroom) | No minimum requirement |
| One-bedroom | 8 square metres |
| Two-bedroom | 11 square metres |
| Three-bedroom or greater | 15 square metres |

- 13 Service yards, car parking areas and facilities, service ducting and plant should be designed and located to ensure that the appearance of buildings and land viewed from all abutting roads is attractive.
- 14 Driveway cross-overs in prominent pedestrian footpath areas should maintain the footpath level and incorporate measures to maximise pedestrian safety.
- 15 Development should provide car parking within the zone in accordance with [Table HoB/1B – Off Street Vehicle Parking Requirements for the Residential High Density Zone or for Residential Uses in the District Centre Zone Glenelg Policy Area 2](#).

Urban Glenelg Policy Area 15

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 A policy area comprising tourist accommodation and a range of dwellings and residential flat buildings at medium to high densities.
- 2 Accommodation that provides a diversity of dwelling sizes within residential flat buildings that cater for different household requirements.
- 3 Retention of the heritage character, especially along South Esplanade in **Precinct 4 Five Storey** and **Precinct 5 Twelve Storey**.
- 4 Development that contributes to the desired character of the policy area/precinct.

DESIRED CHARACTER

The policy area provides the Council's premier coastal medium and high density living opportunities. It includes areas of Glenelg North around the foreshore and the Patawalonga, and within Glenelg and Glenelg South along the foreshore and extending into small parts of the suburban landscape, and along Colley Terrace.

The Glenelg District Centre and Glenelg's major foreshore reserves provide important features adjacent to the policy area (including Wigley and Colley Reserves, the beach and Glenelg foreshore and the Anzac Highway streetscape) and entertainment, retail, residential and visitor apartment accommodation provided nearby in the Holdfast Shores site to the western side of Colley and Wigley Reserve.

The policy area is a premier location with excellent accessibility to views, beach, public spaces, centre services, facilities and public transport. The policy area adds to the choice of accommodation within Holdfast Bay and the wider metropolitan area by providing for a variety of medium and higher density dwelling types, including apartments for residential purposes and visitor accommodation.

Small scale non-residential land uses including shops offices and consulting rooms will be developed in appropriate areas to support residents and the local community.

Retail development will be small in scale (and not exceed in the order of 1500 square metres in gross leasable floor area) and will primarily comprise cafes, restaurants, convenience stores and other tenancies designed to service local community requirements. Retail development will also be of a scale that supports an active mixed use environment.

Development will be of the highest architectural standard, contemporary in style and contribute positively to the quality of the public realm. Its built form will contrast with the open character of the adjacent foreshore and reserve public spaces. It will capitalise on the highly desirable location through significant scale, with built form between three and twelve stories in height. This development will demonstrate excellence in urban design. It will create design relationships between buildings at ground level and the street frontage that acknowledge and respect the existing context, ensuring that scale and the built form edge protects and enhances significant visual and movement corridors (including key vistas to the sea and views through to public spaces). Views into and out of development sites will also reinforce visual connectivity and way-finding within the policy area.

Building form and setbacks will vary to provide large-scale articulation within the streetscape. Building form will also use light and shade through articulation, eaves, verandas, canopies and balconies, to provide architectural detail, summer shade and promote greater energy efficiency. Likewise, buildings will use a balanced approach to the use of solid materials and glazing so to provide an attractive backdrop to key public spaces and streets.

The policy area is well provisioned with quality public open spaces and accessible by public transport (in the form of buses and tram). Accordingly, there is a recognised reduced need for provision of private car parking and private open space (when compared to suburban localities in other zones and policy areas). Similarly, a higher degree of overshadowing and loss of privacy is expected in the policy area given the medium-to-high density nature of development (and heights).

Basement or undercroft car parking is contemplated where site circumstances allow appropriate design and integration with the streetscape / built form. Where ventilation is required for basement car parks, vehicles should be screened and landscaped.

Roofs will be designed to be integrated into the overall façade and composition of buildings and provide enclosed places for the screening of plant and service equipment (if not provided in basements) in locations away from living areas that do not visually detract from the amenity of adjoining spaces.

Landscaping will contribute to the high quality of the adjacent public areas, open space and streetscapes. Car parking areas that are not visible from public spaces will be shared and consolidated. Commercial uses in residential developments will be restricted to those associated with the respective building function.

Public promenades will incorporate public art, which is easily identifiable and fully integrated into the public environment.

Precinct 3 Three Storey

Precinct 3 Three Storey will be developed to comprise a mix of residential development including row dwellings, residential flat buildings and tourist accommodation, with buildings in the order of 3 storeys (or 11.5 metres) in height. Development may also include small scale non-residential uses such shops, restaurants and cafes, offices and consulting rooms at ground and first levels where site conditions permit.

Precinct 4 Five Storey

Development within **Precinct 4 Five Storey** will be predominantly in the form of residential flat buildings, serviced apartments and tourist accommodation of up to 5 storeys (or 18.5 metres) in height. Development may also include small scale non-residential uses such shops, restaurants and cafes, offices and consulting rooms at ground and first levels where site conditions permit.

Development will be of the highest architectural standard and contribute positively to the public realm through establishing clearly defined space between buildings on adjoining sites and building design that incorporates articulated facades and built form elements including balconies to create light and shadow. Building design will complement the scale, proportions, siting and materials of the existing heritage places in the locality.

Development on land fronting the South Esplanade immediately adjacent **Precinct 5 Twelve Storey** may accommodate additional building height over 5 storeys to achieve a transition in scale from the taller building anticipated in Precinct 5, down to the 5 storey scale anticipated in Precinct 4, provided buildings are designed to minimise any impacts on adjoining land within Precinct 4 or adjoining residential zones.

Development directly adjoining Sturt Street should not exceed 2 storeys in height to order to achieve a transition down to low scale at the interface with the adjoining **Residential Character Zone**.

Precinct 5 Twelve Storey

Development within **Precinct 5 Twelve Storey** will be predominantly in the form of residential flat buildings, serviced apartments and tourist accommodation of up to 12 storeys (or 43 metres) in height. Development may also include small scale non-residential uses such shops, restaurants and cafes, offices and consulting rooms at ground and first levels where site conditions permit.

Development will be of the highest architectural standard and contribute positively to the public realm through establishing clearly defined space between buildings, incorporating surface articulation using a balanced approach to the use of solid materials and glazed areas and adopting a building design that incorporates design elements that relate to the surrounding buildings, streetscape and public open space.

Building design will complement the scale, proportions, siting and materials of the existing heritage places in the locality.

In the section of this precinct that adjoins the **Coastal Open Space Zone**, development will be designed to provide spaces between adjacent buildings and accommodate pedestrian walkways and visual connections between the **Coastal Open Space Zone** and the developed areas to the east.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 2 Building entrances should satisfy all of the following:
 - (a) be oriented towards the primary street
 - (b) be visible and easily identifiable from the street
 - (c) provide shelter, a sense of personal address and transitional space around the entry.
- 3 Balconies should make a positive contribution to the internal and external amenity of buildings and should:
 - (a) be functional and responsive to the environment
 - (b) be located to predominantly face north, east or west to provide solar access
 - (c) be integrated into the overall architectural form and detail of the building
 - (d) contribute to the safety and liveliness of the street by facilitating casual overlooking of public spaces
 - (e) be located adjacent to the main living areas, such as the living room, dining room or kitchen to extend the dwelling's living space
 - (f) be of a minimum depth of 2 metres in order to be functional and promote indoor/outdoor living
 - (g) be designed to provide residential flat buildings/tourist accommodation with private open space, thereby promoting the enjoyment of outdoor living
 - (h) incorporate balustrades designed to allow views and casual surveillance of the street and public open space while providing for safety and visual privacy through detailing that incorporates a proportion of solid to transparent materials to promote a balance of privacy and casual surveillance and public interaction.
- 4 Fencing and external walls should facilitate the use of private open space abutting the street.
- 5 Building design should minimise the impact of overlooking and overshadowing on existing lower density and scale development in adjoining zones/policy areas/precincts.
- 6 To minimise building massing at the interface with directly abutting residential development outside the zone (ie where not separated by a public street or laneway), new buildings should provide a transition in height and bulk down to a two storey scale at the interface.
- 7 Private open space may be reduced by a maximum of 10 per cent if the equivalent amount of space provided the annexed portion is appended to communal open space accessible to all occupants of the development.

- 8 The incorporation of roof top gardens should only occur on multi storey buildings.
- 9 Where access to the site is available from a side, rear or private road, or via a right of way, development should not incorporate vehicle access to the Esplanade.
- 10 For development along St John's Row, within **Precinct 3 Three Storey** and/or **Precinct 5 Twelve Storey**:
 - (a) space between buildings on adjoining sites should be created or, where existing, maintained
 - (b) the walls of buildings nearest to the boundary of that road should be parallel to and setback a minimum distance of 4 metres from the boundary of that road
 - (c) building elements in excess of 3 storeys should be setback from the external walls below to create a 'podium' effect.
- 11 For development along Colley Terrace:
 - (a) building elements in excess of 3 storeys (or 11.5 metres) should be set-back from the external walls below to create a 'podium' effect
 - (b) the facade of a building or part of a building up to three-storeys should have predominantly horizontal proportions and architectural features, such as banding
 - (c) the facade of buildings should be of masonry construction with a high proportion of solids to voids
 - (d) space between buildings on adjoining sites should be created or, where existing, maintained
 - (e) the facades of buildings should be parallel to Colley Terrace and set-back at least 4 metres from Colley Terrace
 - (f) the site of a building should have a minimum frontage of 25 metres where that building has more than 5 storeys (external wall height greater than 18.5 metres) above natural ground level.
- 12 For development along Anzac Highway within **Precinct 4 Five Storey** and/or **Precinct 5 Twelve Storey**:
 - (a) the facade of a building or part of a building with an external wall height up to 11.5 metres above natural ground level should have predominantly horizontal proportions and architectural features such as banding to reinforce the horizontal emphasis
 - (b) building elements in excess of 3 storeys (or 11.5 metres) should be set-back from the external walls of the lower levels to create a 'podium' effect
 - (c) buildings should be constructed close to the Anzac Highway frontage and present a continuous facade to Anzac Highway with minimal breaks between buildings on adjoining sites
 - (d) vehicular access onto Anzac Highway should be minimised.
- 13 Development in the form of a battleaxe/hammerhead configuration should not be undertaken within the policy area.
- 14 Development above 5 storeys (or 18.5 metres) in height should incorporate spaces between buildings or other design techniques that enable sunlight access and avoid wide continuous building walls.
- 15 Shops should be of a local scale and not exceed in the order of 1500 square metres gross leasable floor area.

PRECINCT SPECIFIC PROVISIONS

Refer to the [Map Reference Tables](#) for a list of the maps that relate to the following precincts.

Precinct 3 Three Storey

- 16 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 17 Development should not exceed an external wall height of 11.5 metres above natural ground level (excluding lift service levels and gables).

Precinct 4 Five Storey

- 18 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 19 Development should not exceed an external wall height of 18.5 metres above natural ground level (excluding lift service levels and gables).
- 20 For development along South Esplanade:
 - (a) space between buildings on adjoining sites should be created or, where existing, maintained
 - (b) the modulated form of buildings along South Esplanade should be retained by continuing the predominant, regular, building alignment with stepped facades
 - (c) immediately adjacent **Precinct 5 Twelve Storey**, additional building height above 5 storeys in height is appropriate in order to achieve a transition in scale from the taller building anticipated in **Precinct 5 Twelve Storey**, down to the 5 storey scale anticipated in **Precinct 4 Five Storey**, provided buildings are designed to minimise any impacts on adjoining land within **Precinct 4 Five Storey** or adjoining residential zones.
- 21 Development directly adjoining Sturt Street should not exceed 2 storeys in height to order to achieve a transition down to low scale at the interface with the adjoining **Residential Character Zone**.

Precinct 5 Twelve Storey

- 22 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 23 Development should not exceed an external wall height of 43 metres above natural ground level (excluding lift service levels and gables).
- 24 Buildings should be setback on a podium that is designed to be a maximum height of 11.5 metres above natural ground level.

PROCEDURAL MATTERS

Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

Non-complying Development

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

| Form of Development | Exceptions |
|---|---|
| Advertisement and/or advertising hoarding | |
| Amusement machine centre | |
| Community centre | |
| Crematorium | |
| Dairy | |
| Detached dwelling | Except for additions and alterations to the existing building where located within Precinct 3 Three Storey . |
| Entertainment venue | |
| Farming | |
| Fuel depot | |
| Group dwelling | |
| Horse keeping | |
| Horticulture | |
| Hospital | |
| Hotel | |
| Indoor recreation centre | |
| Industry | |
| Intensive animal keeping | |
| Motor repair station | |
| Petrol filling station | |
| Public service depot | |
| Road transport terminal | |
| Semi detached dwelling | Except for additions and alterations to an existing building where located within Precinct 3 Three Storey . |
| Service trade premises | |
| Stock sales yard | |

| Form of Development | Exceptions |
|--|------------|
| Stock slaughter works | |
| Store | |
| Telecommunications facility | |
| Warehouse | |
| Waste reception, storage, treatment or disposal | |
| Wrecking yard | |

Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development (except where the development is classified as non-complying) are designated:

| Category 1 | Category 2 |
|--|---|
| Development with an overall height of 11.5 metres or less (excluding gables) measured from the natural ground level. | Development with an overall height exceeding 11.5 metres (excluding gables) measured from the natural ground level. |

Suburban Neighbourhood Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone

OBJECTIVES

- 1 A predominantly medium density residential area that comprises a range of dwelling types, together with a neighbourhood activity centre that is located within a walkable distance of residents.
- 2 Provision of medium density residential development adjacent to an activity centre, public transport stops and public open space.
- 3 A neighbourhood activity centre that provides a range of shopping, community, business and recreational facilities for the surrounding neighbourhood.
- 4 Sustainable development outcomes through innovation in stormwater management, waste minimisation, water conservation, energy efficiency and urban biodiversity.
- 5 Development that contributes to the desired character of the zone.

DESIRED CHARACTER

This zone will be developed predominantly as a medium density residential area focused around a neighbourhood scale activity centre. The layout of the area will support integration of activities, an active public realm and provide convenient pedestrian, cycling and vehicular access to public open space, shops, a range of community services and adjacent education facilities.

Development across the zone will take advantage of scenic views of the Adelaide coastline and cityscape in the arrangement of streets, open spaces and the orientation of buildings. Buildings of up to six storeys are envisaged.

Public open space will provide a high level of amenity for local residents and will be primarily designed for local use. Existing tall trees will be retained, where possible, and all new species planted to provide canopy cover, cooling, habitat and improved air quality and stormwater management. Reserves will support a network of pedestrian and cycling linkages throughout to the zone, encouraging access to nearby recreation and sporting facilities, public transport nodes and the activity centre. Some reserves will also provide a joint stormwater management function.

Stormwater, both from the upstream catchment and generated within the zone, will be carefully managed to ensure that flows do not exceed the capacity of the downstream system. A variety of Water Sensitive Urban Design mechanisms will be integrated throughout the zone at the neighbourhood, street and site level. Where practical, harvested stormwater will be used for irrigation to improve the aesthetic and functional value of open spaces.

Sensitive development will be sited and designed so as to not affect the ongoing operation of the Linwood quarry to the south of the zone. Air quality issues will be mitigated by a vegetated landscape buffer within the zone to ensure both an appropriate living environment for residents and protection of the quarry activities.

Due to former industrial uses within the zone, development is expected to occur on a precautionary basis where a site contamination audit verifies that a site is suitable for its intended uses, particularly where it involves a sensitive use such as residential development.

A variety of dwelling types and densities, and a range of allotment sizes, will be provided across the zone, catering for different household sizes, life cycle stages and housing preferences. The average net residential site density will be in the order of 35 to 70 dwelling units per hectare across the zone, with pockets of

development that may be lesser or greater than this target. Buildings of up to 6 storeys in height are envisaged in some parts of the zone.

In the residential area, setbacks to local streets will be used to provide opportunities for landscaping to soften the built form. A cohesive built form will be achieved through design elements such as roof forms, articulated buildings, recessed vehicle garaging, and landscaped spaces between buildings and the public road. Buildings will include balconies, windows and doors that overlook the street to promote community interaction and safer streets.

The residential area public realm will include unique and interesting themes achieved through landscaping, appropriate species of trees, surface treatments, street furniture, building design and other elements. Garaging and associated entry points will not dominate the appearance of a building from a local street (including a laneway). Street patterns and walkways will be designed to minimise the need for local vehicle trips, promote low vehicle speeds and maximise shared street opportunities in local streets. These networks, and the canopy cover, will encourage walking and cycling to local facilities and public transport services.

The neighbourhood activity centre will incorporate a mixture of services, providing for the daily and weekly shopping, business and community needs of the surrounding community. It will contain in the order of 6000 square metres floor area for retail activities and in the order of 2000 square metres floor area for other non-residential land uses. It will comprise generally multi-storey, mixed use buildings where the street level uses are primarily non-residential. Upper floor uses will primarily comprise residential development with some complementary non-residential uses such as offices and consulting rooms.

The built form within the activity centre will have a strong contemporary urban character, with active building frontages orientated towards Scholefield Road, adjacent open space to the west and other public areas. A variety of materials, colours and façade articulation will be used to provide interest and amenity. Active ground floor frontages will be provided, with clear connection of the building with public areas and spaces. Features and activities that attract people are encouraged, such as frequent doors and display windows, retail shopfronts and outdoor eating or dining areas spilling out onto footpaths.

The activity centre public realm will include landscaping comprising established upper canopy species of trees, consistent with the scale and height of buildings, to provide shade canopy cover, as well as softening the building form. It will also feature a public plaza with a strong connection to the Scholefield Road frontage. A pedestrian friendly environment will be provided through such means as wide footpaths, colonnades, courtyards, verandahs and awnings and street furniture.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development, or any combination thereof, are envisaged in the zone:
 - affordable housing
 - aged persons accommodation
 - dwelling
 - domestic outbuilding
 - educational establishment
 - pre-school
 - primary school
 - residential flat building
 - supported accommodation.
- 2 The following additional forms of development, or combination thereof, are also envisaged within the designated neighbourhood activity centre identified on [Concept Plan Map HoB/7 – Seacliff Park](#):
 - community centre
 - consulting room
 - indoor recreation centre
 - office

- place of worship
 - pre-school
 - restaurant (excluding those incorporating a drive-through facility)
 - shop (excluding a bulky goods outlet or a retail showroom or a shop associated with a premise that sells and/or trades petrol).
- 3 Development should be in accordance with [Concept Plan Map HoB/7 – Seacliff Park](#).
- 4 Non-residential development should be located within the designated neighbourhood activity centre.
- 5 Development listed as non-complying is generally inappropriate.

Form and Character

- 6 Development should be consistent with the desired character for the zone.
- 7 Development may be up to 6 storeys in height.
- 8 Unless separated by a public road or reserve (open space), the visual massing and height of buildings in the zone should be progressively reduced to a maximum of 3 storeys at the interface with low rise (1 to 2 storey) residential development.
- 9 Garage top apartments should:
- (a) be no more than 2 storeys in height above the garage (a total of 3 storeys)
 - (b) front a street or laneway that provides rear access for vehicles associated with the main dwelling and garage top apartment
 - (c) complement the existing dwelling or mixed use building.
- 10 Development should be sited and designed to:
- (a) not affect the ongoing operation of the adjacent quarry
 - (b) ameliorate noise, vibration and air quality impacts from the adjacent quarry
 - (c) ensure there is no direct line of sight to mining operations associated with the adjacent quarry
- 11 Where required, an acoustic barrier should be constructed within the zone adjacent the boundary with Ocean Boulevard to mitigate noise and vibration issues associated with traffic using the road.
- 12 A landscape buffer of an appropriate width should be constructed adjacent the boundary of the site with Ocean Boulevard (where required) and the boundary to the south, to mitigate air quality issues associated with the road and nearby quarry activities.

Dwellings and Residential Flat Buildings

Building to the Side Boundary

- 13 Walls of dwellings and residential flat buildings sited on side boundaries should be in accordance with at least one of the following:
- (a) be located immediately abutting the wall of an existing or simultaneously constructed building on the adjoining land to the same or lesser length and height
 - (b) constructed in accordance with any approved building envelope plan
 - (c) the exposed section of the wall is less than 8 metres in length and 3.5 metres in height above reference level, where reference level means where the natural ground level is readily apparent or

known, that level, otherwise the pre-existing ground level ignoring any preparatory works done prior to the assessment of the development.

- 14 Dwellings and residential flat buildings developed to both side boundaries should provide ground level access to the rear of the site via a carport, garage, access way, service lane or the like.

Setbacks from the Side Boundary

- 15 Walls of dwellings and residential flat buildings set back from the side boundary should be designed in accordance with the following:

| Wall height (measured from reference level, where reference level means where the natural ground level is readily apparent or known, that level, otherwise the pre-existing ground level ignoring any preparatory works done prior to the assessment of the development) | Minimum setback from side boundaries (metres) |
|---|---|
| For any portion of the wall less than or equal to 7 metres | 0.9 |
| For any portion of the wall greater than 7 metres | 1.5 |

Front Setbacks

- 16 Dwellings and residential flat buildings (excluding verandahs, porticos and the like) should be set back from road frontages in accordance with the following parameters:

| Minimum setback | Value (metres) |
|--|--|
| From the primary road frontage of an arterial road | 8 or the average of any existing dwellings on any adjoining allotments with the same primary frontage (or, if there is only one such dwelling, the setback of that dwelling) |
| From the primary road frontage of all other roads | 3 or the average of any existing dwellings on any adjoining allotments with the same primary frontage (or, if there is only one such dwelling, the setback of that dwelling) |
| From a secondary road frontage of an arterial road | 8 or the average of any existing dwellings on any adjoining allotments with the same primary frontage (or, if there is only one such dwelling, the setback of that dwelling) |
| From a secondary road frontage of all other roads | 1.5 |

Setbacks from Rear Boundaries

- 17 The walls of dwellings and residential flat buildings should be set back from rear boundaries, except where the rear boundary adjoins a laneway, in accordance with the following parameters:
- (a) ground floor minimum setback of 2.5 metres
 - (b) second storey minimum setback of 4 metres
 - (c) third storey or more minimum setback of 4 metres plus any increase in wall height over 6 metres.
- 18 Walls of dwellings and residential flat buildings sited on rear boundaries should be in accordance with at least one of the following:
- (a) be located immediately abutting the wall of an existing or simultaneously constructed building on the adjoining land to the same or lesser length and height
 - (b) constructed in accordance with an approved building envelope plan.

Development Fronting a Laneway

- 19 Laneways should:
- (a) be of adequate dimensions to enable safe and efficient movements for pedestrians, cars and service vehicles (including for waste collection)
 - (b) have a minimum road reserve width of 8 metres
 - (c) be through routes of a straight configuration
 - (d) not be longer than 140 metres without a mid-link lane.
- 20 Development fronting a laneway should enable safe and easy access into/from the laneway and be setback a sufficient distance to:
- (a) avoid the need for people to step directly onto the road reserve when leaving a dwelling
 - (b) avoid the need for doors/gates to protrude into the laneway when open
 - (c) adequately cater for the turning path of a typical motor vehicle accessing the site.
- 21 Built development facing a laneway should be setback from the boundary of the laneway:
- (a) a minimum of 0.5 metres for the ground floor of a dwelling and may be on the boundary for upper levels
 - (b) a minimum of 0.5 metres and a maximum of 1.0 metre for a garage or carport.
- 22 Dwellings and residential flat buildings facing a laneway should provide visible front door access, with a clearly identified house number, letter box and access to metered services.

Private Open Space

- 23 Dwellings and residential flat buildings should include private open space that conforms to the requirements below:

Minimum area of private open space (particularly dwellings with ground level living rooms)

| Site area | Private open space (POS) | | |
|------------------------------|--------------------------|---|------------------|
| | Total | Directly accessible from a living room | Off-ground |
| 175 square metres or greater | 20 per cent of site area | 10 per cent of the site area with a minimum dimension of 5 metres by 5 metres | 10 square metres |
| Less than 175 square metres | 35 square metres | 16 square metres with a minimum dimension of 4 metres by 4 metres | 8 square metres |

Dwellings with ground level habitable rooms should have at least the total amount of private open space specified above. Off-ground areas such as balconies, roof patios, decks or the like may comprise part of the open space if each is at least the size specified.

One part of the private open space should:

- be directly accessible from a living room of the dwelling and no less than the size specified

- have a minimum dimension as specified above
- have a gradient no steeper than 1-in-10.

All other private open space should have a dimension of at least 2.5 metres at ground level and 2 metres off-ground level.

Minimum area of private open space (dwellings located wholly above ground level or without ground level habitable rooms)

| Dwelling type | Private open space |
|------------------------------|--|
| Studio (no separate bedroom) | 6 square metres |
| One bedroom dwelling | 8 square metres |
| Two bedroom dwelling | 11 square metres |
| Three + bedroom dwelling | 11 square metres plus 4 square metres for each bedroom after the first two |

Dwellings without ground level habitable rooms should have at least the amount of private open space specified above. All private open space should have a dimension of at least 2 metres, and be directly accessible from a living room of the dwelling.

Site Area

- 24 A dwelling should have a **site area** (and in the case of group dwellings and residential flat buildings, an average site area per dwelling), a frontage to a public road and a **site depth** of not less than that shown in the following table:

| Dwelling type | Minimum site area (square metres) | Minimum frontage (metres) | Minimum site depth (metres) |
|---|-----------------------------------|---------------------------|-----------------------------|
| Detached dwelling (except where constructed boundary to boundary) | 270 | 7 | 20 |
| Semi-detached dwelling | 220 | 6 | 20 |
| Row dwelling and detached dwelling constructed boundary to boundary | 220 | 5 | 20 |
| Group dwelling / residential flat building (1 and 2 storey) | 200 | 15 | 45 |

Minimum Dwelling Areas for Residential Flat Buildings

- 25 Residential flat buildings should contain dwellings with internal floor areas of not less than the following:
- (a) studio (where there is no separate bedroom): 35 square metres
 - (b) 1 bedroom dwelling/apartment: 50 square metres
 - (c) 2 bedroom dwelling/apartment: 65 square metres
 - (d) 3+ bedroom dwelling/apartment: 80 square metres plus an additional 15 square metres for every additional bedroom over 3 bedrooms.

“Internal floor areas” includes internal storage areas but does not include balconies or car parking as part of the calculation.

Affordable Housing

- 26 Affordable housing should be distributed throughout the zone to avoid over-concentration of similar types of housing in a particular area.

Neighbourhood Activity Centre

- 27 The designated neighbourhood activity centre should:
- (a) maximise its role as a neighbourhood focus by including shops, a range of community facilities and residential development
 - (b) comprise buildings of a human scale, addressing the street and other areas accessible to the public
 - (c) be physically connected with surrounding residential areas by:
 - (i) avoiding large expanses of vehicle parking that physically separates the activity centre from surrounding residential areas
 - (ii) including pedestrian and cycle linkages that enable residents to comfortably walk and cycle directly from residential areas to and also within activity centre facilities
 - (d) orientate development towards and near to public spaces and street frontages
 - (e) incorporate, where possible, mixed use development along the edges to provide a transition from activity centre uses to residential areas
 - (f) include shelter for pedestrians along public streets and internal access ways
 - (g) ensure building façades create diversity of interest and have the appearance of an aggregation of small buildings
 - (h) ensure roof forms are varied and do not include large expanses of roofline that are visible from the public domain
 - (i) ensure buildings address the street frontage and open spaces with service areas generally accessed via rear lanes or internal to the centre and not visible from public streets or residential development.
- 28 A range of setbacks should be used within the activity centre to:
- (a) support active frontages
 - (b) accommodate activities within the wider public realm (i.e. the streets, open spaces and other areas accessible to the public).
- 29 Development with larger floor areas and typically large frontages (e.g. such as a supermarket) within the activity centre should be designed to present a small frontage to the public area, which is integrated with the smaller scale frontages of other development by such means as 'wrapping or capping' the larger shop frontage with smaller shops fronting the external environment.
- 30 Upper floors of multi-storey mixed use buildings should primarily comprise of residential development, with some complementary non-residential uses such as offices or consulting rooms.
- 31 Outdoor storage, loading and service areas should be:
- (a) screened from public view by a combination of built form, solid fencing and/or landscaping
 - (b) conveniently located and designed to enable the manoeuvring of service and delivery vehicles

- (c) sited away from sensitive land uses.
- 32 Undercroft garaging of vehicles should only occur when:
- (a) the overall height and bulk of the development does not adversely impact on streetscape character of the locality or the amenity of adjacent properties
 - (b) vehicles can safely enter and exit from the site without compromising pedestrian or cyclist safety or causing conflict with other vehicles
 - (c) the site slopes up from the street
 - (d) driveway gradients provide for safe and functional entry and exit
 - (e) driveways and adjacent walls, fencing and landscaping are designed to provide adequate sightlines from vehicles to pedestrians using the adjacent footpath
 - (f) openings to undercroft areas are integrated with the main building so as to minimise visual impact
 - (g) landscaping, mounding and/or fencing is incorporated to improve its presentation to the street and to adjacent properties
 - (h) the overall streetscape character of the locality is not adversely impaired (e.g. visual impact, building bulk, front setbacks relative to adjacent development)
 - (i) the height of the car park ceiling does not exceed 1 metre above the finished ground level.
- 33 Buildings with 4 storeys or more above natural surface level should include provision for undercroft parking.
- 34 Semi-basement or undercroft parking should be suitably integrated with the building form.
- 35 In the case of semi-basement or undercroft car parks where cars are visible from public areas, adequate screening and landscaping should be provided.

Stormwater Management and Water Quality

- 36 Development should include stormwater management systems designed to achieve the following stormwater runoff outcomes:
- (a) Protection from the 1 in 100 year average recurrence interval flows generated by the upstream catchment. This should include a safe overland flow path and could include a suitably sized pipe system for these floodwaters through or around the zone, discharging to a suitably sized detention basin which discharges at a rate no greater than 0.1m³/sec.
 - (b) Stormwater runoff generated by development from rainfall events having up to a 1 in 5 year average recurrence interval should be retained/reused within the zone and discharged at a rate no greater than 0.03m³/sec to the downstream drainage system, in lieu of soil infiltration into underlying contaminated soil. However, use of on-site Water Sensitive Design treatments should be maximised.
 - (c) The peak 1 in 100 year average recurrence interval flow discharged from the zone should be reduced to the existing 5 year average recurrence interval peak flow of 0.66m³/sec.
- 37 The amenity of the proposed development should be protected from polluted upstream catchment stormwater discharged through the zone.
- 38 Development should include stormwater management systems designed to achieve the following catchment runoff quality outcomes compared to an equivalent urban catchment with no water quality management:

- (a) suspended solids - 80% reduction in average annual pollutant load
- (b) total phosphorous - 60% reduction in average annual pollutant load
- (c) total nitrogen - 45% reduction in average annual pollutant load
- (d) litter/gross pollutants– 90% reduction in average annual pollutant load
- (e) oil and grease – no visible oils for flows up to the 3 month ARI Peak Flow
- (f) flow-run off rates that do not exceed the rate of discharge from the site that existed during pre-development.

Bicycle Parking

39 Development should encourage and facilitate cycling as a mode of transport by incorporating end-of journey facilities including:

- (a) changing facilities and secure lockers for staff
- (b) signage indicating the location of bicycle facilities
- (c) bicycle parking facilities provided at the rates set out in the Table below:

| Form of development | Employee / resident (bicycle parking spaces) | Visitor / shopper (bicycle parking spaces) |
|--|---|---|
| Residential component of multi-storey building / residential flat building | 1 for every 4 dwellings | 1 for every 10 dwellings |
| Office | 1 for every 200 square metres of gross leasable floor areas | 2, plus 1 per 1000 square metres of gross leasable floor area |
| Shop | 1 for every 300 square metres of gross leasable floor area | 1 for every 600 square metres of gross leasable floor area |

PROCEDURAL MATTERS

Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

Non-complying Development

The following forms of development and any development which includes one or more or a combination of the following forms of development (in any fashion, including as an integrated component), are non-complying:

| Form of development | Exceptions |
|-------------------------------------|------------|
| Adult products and services premise | |
| Amusement machine centre | |

| Form of development | Exceptions |
|--|--|
| Advertisement and/or advertising hoarding where one or more of the following applies: | |
| Neighbourhood activity centre area of the zone: (a) it moves, flashes or rotates (b) it projects above the roof line when roof mounted (c) when attached to a building, has any part projecting above the walls, fascia or parapet (d) it displays third party content relating to services, messages or products that are not directly related to the primary activity on the site on which the advertisement is being displayed (e) has a height greater than 8 metres above reference level (f) it exceeds a display area or panel size of 12 square metres in area (6 square metres if double sided) | Except in regard to subclause (d), advertisements that display third party content where integrated with a bus shelter or public telephone booth located on a primary arterial road. |
| Residential area of the zone: (a) it is illuminated (internally, externally or indirectly) (b) it moves, flashes or rotates (c) when freestanding, exceeds 4 metres above reference level at any point (d) when attached to a building, has any part protruding above the highest level of that building (e) when on a non-residential site it exceeds a display area or panel size of 4 square metres in area (2 square metres if double sided) (f) when on a residential site it exceeds a display area or panel size of 0.4 square metres in area (0.2 square metres if double sided) (g) it displays third party content relating to services, messages or products that are not directly related to the primary activity on the site on which the advertisement is being displayed | Except in regard to subclause (g), advertisements that display third party content where integrated with a bus shelter. |
| Bulky goods outlet or retail showroom | |
| Car wash facility | |
| Fuel depot | |
| Hospital | |
| Industry | |
| Motor repair station | |
| Petrol filling station | |
| Premise that incorporates a facility for the fuelling of vehicles | |
| Public service depot | |
| Restaurant incorporating a drive-through facility | |
| Road transport terminal | |
| Service trade premise | |
| Store | |

| Form of development | Exceptions |
|---|------------|
| Warehouse | |
| Waste reception, storage, treatment or disposal | |
| Wrecking yard | |

Public Notification

Categories of public notification are prescribed in Schedule 9 of the Development Regulations 2008.

Further, the following forms of development (except where the development is classified as non-complying) are designated:

| Category 1 | Category 2 |
|---|---|
| Advertisement | All forms of development not listed as Category 1 |
| Aged persons accommodation | |
| All forms of development that are ancillary and in association with residential development | |
| Dwelling | |
| Nursing home | |
| Office that is ancillary and in association with a dwelling | |
| Residential flat building | |
| Retirement village | |
| Supported accommodation | |
| Within the neighbourhood activity centre: | |
| (a) community centre | |
| (b) consulting room | |
| (c) indoor recreation centre | |
| (d) office | |
| (e) place of worship | |
| (f) pre-school | |
| (g) shop | |

Table Section

Table HoB/1 - Off Street Vehicle Parking Requirements

The following off street vehicle parking requirements apply, except where otherwise stated in [Table HoB/1A - Off Street Vehicle Parking Requirements for Designated Areas](#) or [Table HoB/1B - Off Street Vehicle Parking Requirements for the Residential High Density Zone or for Residential Uses in the District Centre Zone Glenelg Policy Area 2](#).

| Form of Development | Number of Required Car Parking Spaces (the resultant number of car parks rounded to the nearest whole number) |
|---|--|
| Parking for people with a disability - minimum rate for reserved spaces | 1 car parking space in every 30 spaces provided with any form of development (other than residential development) should function as a car parking space reserved for the exclusive use of people with a disability. |
| Amusement hall Amusement machine centre Bowling alley Clubrooms Community centre Dance hall Exhibition hall Gymnasium Indoor recreation centre Stadium | 1 per 10 square metres of total floor area. |
| Bank Building society Post Office | 1 per 25 square metres of total floor area, with a minimum number of 5 car parking spaces. |
| Billiard Saloon | 1 per 15 square metres of total floor area. |
| Boarding house Lodging house | 1 per 4 beds. |
| Bowling club | 30 per bowling green. |
| Bulky goods outlet | 4 per 100 square metres of gross leasable area of that shop. |
| Café located within the District Centre Zone Glenelg Policy Area 2 | 1 per 5 seats able to be accommodated. |
| Concert hall Meeting hall Place of worship | 1 per 5 seats provided or capable of being provided. |
| Consulting rooms (other than where located within the District Centre Zone Glenelg Policy Area 2 or the Suburban Neighbourhood Zone) | 4 for the first consulting room, plus 2 per each additional consulting room. |
| Consulting rooms located within the District Centre Zone Glenelg Policy Area 2 | 4 per 100 square metres of gross leasable floor area. |
| Consulting rooms located within the Suburban Neighbourhood Zone | 4 for the first consulting room, plus 1 per each additional consulting room. |

| Form of Development | Number of Required Car Parking Spaces (the resultant number of car parks rounded to the nearest whole number) |
|--|--|
| Department store within District Centre Zone Glenelg Policy Area 2 | 5 per 100 square metres of gross leasable floor area. |
| Detached dwelling Semi-detached Row dwelling (other than where located within the Suburban Neighbourhood Zone) | 2 on-site parking spaces, 1 of which is covered (the second space can be tandem). |
| Detached dwelling Semi-detached Row dwelling located within the Suburban Neighbourhood Zone | 1 per 1 bedroom dwelling 2 per 2 or more bedroom dwelling. |
| Educational establishment Pre-school Primary school | 1 per full time staff member and the number of part-time staff members equivalent to 1 full time staff member, plus an adequate number of spaces for visitors. |
| Funeral parlour | 1 per 5 chapel seats plus 1 for each vehicle operated by the parlour. |
| Group dwelling or residential flat building located within the Medium Density Policy Area 5 | 0.75 per dwelling, where the dwelling has a maximum of 1 bedroom or a total floor area of less than 75 square metres 1 per dwelling where the dwelling has 2 bedrooms or a total floor area of at least 75 square metres and less than 130 square metres 1.25 per dwelling, where the dwelling has 3 or more bedrooms or a total floor area of 130 square metres or more *Add an additional 0.25 spaces per dwelling for visitor parking. |
| Group dwelling or residential flat building located within the Minda Incorporated Brighton Campus | 1 per dwelling, where the dwelling has a maximum of 2 bedrooms or a total floor area of less than 130 square metres 1.25 per dwelling, where the dwelling has 3 or more bedrooms or a total floor area of 130 square metres or more *Add an additional 0.25 spaces per dwelling for visitor parking. |
| Group dwelling or residential flat building (other than where located within the Medium Density Policy Area 5 , the Minda Incorporated Brighton Campus or the Suburban Neighbourhood Zone) | 1 per dwelling, where the dwelling has a maximum of 1 bedroom or a total floor area of less than 75 square metres 1.5 per dwelling where the dwelling has 2 bedrooms or a total floor area of at least 75 square metres and less than 130 square metres 2 per dwelling, where the dwelling has 3 or more bedrooms or a total floor area of 130 square metres or more *Add an additional 0.25 spaces per dwelling for visitor parking. |
| Group dwelling or residential flat building located within the Suburban Neighbourhood Zone | 1 per 1 bedroom dwelling 1.5 per 2 bedroom dwelling 2 per 3 or more bedroom dwelling *Add an additional 0.25 spaces per dwelling for visitor parking. |
| Guest house | 1 for every 2 beds provided or capable of being provided. |
| Hotel | 1 per 2 square metres of total floor area in a public bar, plus 1 per 6 square metres of total floor area in a dining room, lounge, gaming room and beer garden, plus 1 per 3 guest rooms. |

| Form of Development | Number of Required Car Parking Spaces (the resultant number of car parks rounded to the nearest whole number) |
|---|--|
| Industry Service industry | 1 per 50 square metres of total floor area; or 1 per 2 employees (whichever provides the larger parking area); except where retail sales take place, when the car parking provision relating to shops will apply to that floor area used for the sale or display of items. |
| Motel | 1 per room or residential unit, plus where a restaurant or dining area is provided, an additional 1 per 10 square metres of restaurant floor area. |
| Meeting hall | 1 per 5 seats provided or able to be provided in the hall. |
| Motor repair station | 10 spaces per premises. |
| New and used vehicle lot Motor showroom | 1 for every 10 vehicles displayed or able to be displayed for sale on the vehicle lot or in the vehicle showroom, plus the applicable rate for motor repair station or service industry as appropriate being applied for any area used for servicing/repairing vehicles and/or supplying spare parts to the trade or public. |
| Non-residential club | 1 for every 6 square metres of total floor area used or capable of being used by the members of the club. |
| Office | 1 per 25 square metres of total floor area. |
| Plant nursery (retail) | 1 per 100 square metres of indoor floor area used for display purposes, plus 1 per 100 square metres of outdoor area used for display purposes. |
| Pre-school located within the Suburban Neighbourhood Zone | 1 per employee plus 0.25 spaces per child as drop off/pick up bays plus 1 space for wheelchair access. |
| Restaurant (other than where located within the District Centre Zone Glenelg Policy Area 2 or the Suburban Neighbourhood Zone) | 1 per 10 square metres of total floor area, or 1 for every 3 seats provided or able to be provided, whichever provides the greater number, together with additional car parking spaces if food is able to be taken away from the premises, to ensure that all car parking occurs on the site of the development. |
| Restaurant located within the District Centre Zone Glenelg Policy Area 2 | 1 per 5 seats able to be accommodated. |
| Restaurant located within the Suburban Neighbourhood Zone | 1 per 2 seats able to be accommodated. |
| Service station | 10 per premises for customer and employee use. |
| Service trade premises | 5 per 100 square metres of indoor floor area used for display purposes, plus 1 per 100 square metres of outdoor area used for display purposes. |
| Shop (other than where located within the District Centre Zone Glenelg Policy Area 2 or the Suburban Neighbourhood Zone) | 1 per 15 square metres of total floor area. |
| Shop within the District Centre Zone Glenelg Policy Area 2 | 4.5 per 100 square metres of gross leasable floor area. |

| Form of Development | Number of Required Car Parking Spaces (the resultant number of car parks rounded to the nearest whole number) |
|--|--|
| Shop located within the Suburban Neighbourhood Zone | 5 per 100 square metres of gross leasable floor area. |
| Supermarket within the District Centre Zone Glenelg Policy Area 2 | 5 per 100 square metres of gross leasable floor area. |
| Supported Accommodation located within the Minda Incorporated Brighton Campus | |
| Squash court Tennis court | 3 per court. |
| Store Warehouse | 1 per 150 square metres of total floor space, or 1 per 3 employees (whichever provides the larger parking area). |
| All other non-residential uses located within the Suburban Neighbourhood Zone | 4 per 100 square metres of gross leasable floor area. |

Table HoB/1A - Off Street Vehicle Parking Requirements for Designated Areas

Interpretation

- 1 The vehicle parking rates table applies to Designated Areas listed below except where:
 - (a) any applicable condition(s) is/are not met
 - (b) the zone provisions require a lesser amount of on-site vehicular parking spaces than the amount determined using the vehicle parking rates tables below.

Designated Areas

- 2 The following are Designated Areas:

| Designated Area | Conditions |
|---------------------------|---|
| District Centre Zone | Any part of the development site is located in accordance with at least one of the following: |
| Neighbourhood Centre Zone | |
| Local Centre Zone | |
| Mixed Use Zone | |
| | (a) within 200 metres of any section of road reserve along which a bus service operates as a high frequency public transit service ⁽²⁾ |
| | (b) within 400 metres of a bus interchange ⁽¹⁾ that is part of a high frequency public transit service ⁽²⁾ |
| | (c) within 400 metres of an O-Bahn interchange ⁽¹⁾ |
| | (d) within 400 metres of a passenger rail station ⁽¹⁾ that is part of a high frequency public transit service ⁽²⁾ |
| | (e) within 400 metres of a passenger tram station ⁽¹⁾ |
| | (f) within 400 metres of the Adelaide Parklands. |

⁽¹⁾ Measured from an area that contains any platform(s), shelter(s) or stop(s) where people congregate for the purpose waiting to board a bus, tram or train, but does not include areas used for the parking of vehicles

⁽²⁾ A high frequency public transit service is a route serviced every 15 minutes between 7.30am and 6.30pm Monday to Friday and every 30 minutes at night, Saturday, Sunday and public holidays until 10pm.

Applicable off-street vehicular parking requirements

- 3 Development should provide off-street vehicle parking in accordance with the table(s) below. A lesser number of parking spaces may be provided based on the nature of the development and parking conditions in the wider locality including (but not limited to) the following:
 - (a) the development is a mixed use development with integrated (shared) parking where the respective peak parking demands across the range of uses occurs at different times
 - (b) the development is sited in a locality where the respective peak demands for parking for the range of uses (existing and proposed) occurs at different times and suitable arrangements are in place for the sharing of adjoining or nearby parking areas
 - (c) the development involves the retention and reuse of a place of heritage value, where the provision of on-site parking is constrained
 - (d) suitable arrangements are made for any parking shortfall to be met elsewhere or by other means (including a contribution to a car parking fund)

- (e) generous on-street parking and/or public parking areas are available and in convenient proximity, other than where such parking may become limited or removed by future loss of access, restrictions, road modifications or widening.
- (f) the site of the development is located within distances specified in the conditions applicable to Designated Areas for at least two different public transit modes.

TABLES: VEHICLE PARKING RATES**TABLE 1: Non-residential development excluding tourist accommodation**

| Location of development | Desired minimum number of vehicle parking spaces | Maximum number of vehicle parking spaces |
|---|---|---|
| All Designated Areas (unless otherwise stated) | 3 spaces per 100 square metres of gross leasable floor area | 6 spaces per 100 square metres of gross leasable floor area |

Table HoB/1B - Off Street Vehicle Parking Requirements for the Residential High Density Zone and for Residential Uses in the District Centre Zone Glenelg Policy Area 2

Table HoB/1B - Off Street Vehicle Parking Requirements for the Residential High Density Zone and for Residential Uses in the District Centre Zone Glenelg Policy Area 2

The following vehicle parking requirements apply to development specifically in the **District Centre Zone Glenelg Policy Area 2** and the **Residential High Density Zone**.

- 1 Residential development, in the form of residential flat buildings and residential development in multi-storey buildings should provide vehicle parking in accordance with the following rates:

| Residential Form | Number of required vehicle parking spaces | Plus number of required visitor parking spaces |
|--|--|---|
| Studio (no separate bedroom), 1, or 2 bedroom dwelling | 1 per dwelling | 0.25 per dwelling |
| 3 + bedroom dwelling | 1.25 per dwelling | 0.25 per dwelling |

- 2 Row, semi-detached and detached dwellings should provide off-street vehicle parking in accordance with the following rates:

| Number of bedrooms, or rooms capable of being used as a bedroom | Number of required vehicle parking spaces |
|--|--|
| 1 or 2 bedrooms | 1 |
| 3 + bedrooms | 2 |

- 3 Tourist accommodation should provide off-street vehicle parking in accordance with the following rates:

| Minimum number of required vehicle parking spaces | Maximum number of vehicle parking spaces |
|--|--|
| 1 space for every 4 bedrooms up to 100 bedrooms and 1 space for every 5 bedrooms over 100 bedrooms | 1 space for every 2 bedrooms up to 100 bedrooms and 1 space for every 4 bedrooms over 100 bedrooms |

The following vehicle parking requirements apply to development specifically in the **Residential High Density Zone**.

- 4 Non-residential development excluding tourist accommodation should provide off-street vehicle parking in accordance with the following rates:

| Minimum number of required vehicle parking spaces | Maximum number of vehicle parking spaces |
|---|---|
| 3 spaces per 100 square metres of gross leasable floor area | 6 spaces per 100 square metres of gross leasable floor area |

The following vehicle parking requirements apply to development specifically in the **District Centre Zone Glenelg Policy Area 2** and the **Residential High Density Zone**.

- 5 A lesser car parking rate than prescribed may be applied where justified based on local circumstances, for example where:
- (a) amalgamation of allotments occurs, or an agreement is formed to integrate and share adjoining parking areas, to create larger more functional and efficient parking areas, as follows:
 - (i) on sites of greater than 1500 square metres and providing greater than 50 parking spaces
 - (ii) side road frontage with two-way access provided
 - (iii) convenient flow through two-way accessibility created between side roads
 - (iv) rationalised, minimised or avoidance of vehicle crossovers to roads and optimisation of on-street parking
 - (b) development includes affordable housing, student accommodation, retirement villages or aged persons' accommodation
 - (c) sites are located within 200 metres walking distance of a convenient and frequent service fixed public transport stop
 - (d) mixed use development including residential and non-residential development has respective peak demands for parking occurring at different times
 - (e) the proposed development is on or adjacent to the site of a heritage place, or includes retention of a desired traditional building and its features, which hinders the provision of on-site parking
 - (f) suitable arrangements are made for any parking shortfall to be met elsewhere or by other means
 - (g) generous on-street parking and/or public parking areas are available and in convenient proximity, other than where such parking may become limited or removed by future loss of access, restrictions, road modifications or widening.

Table HoB/2 - Design Principles

Figure 1 – Maximum Height Limit within the South West Policy Area 7

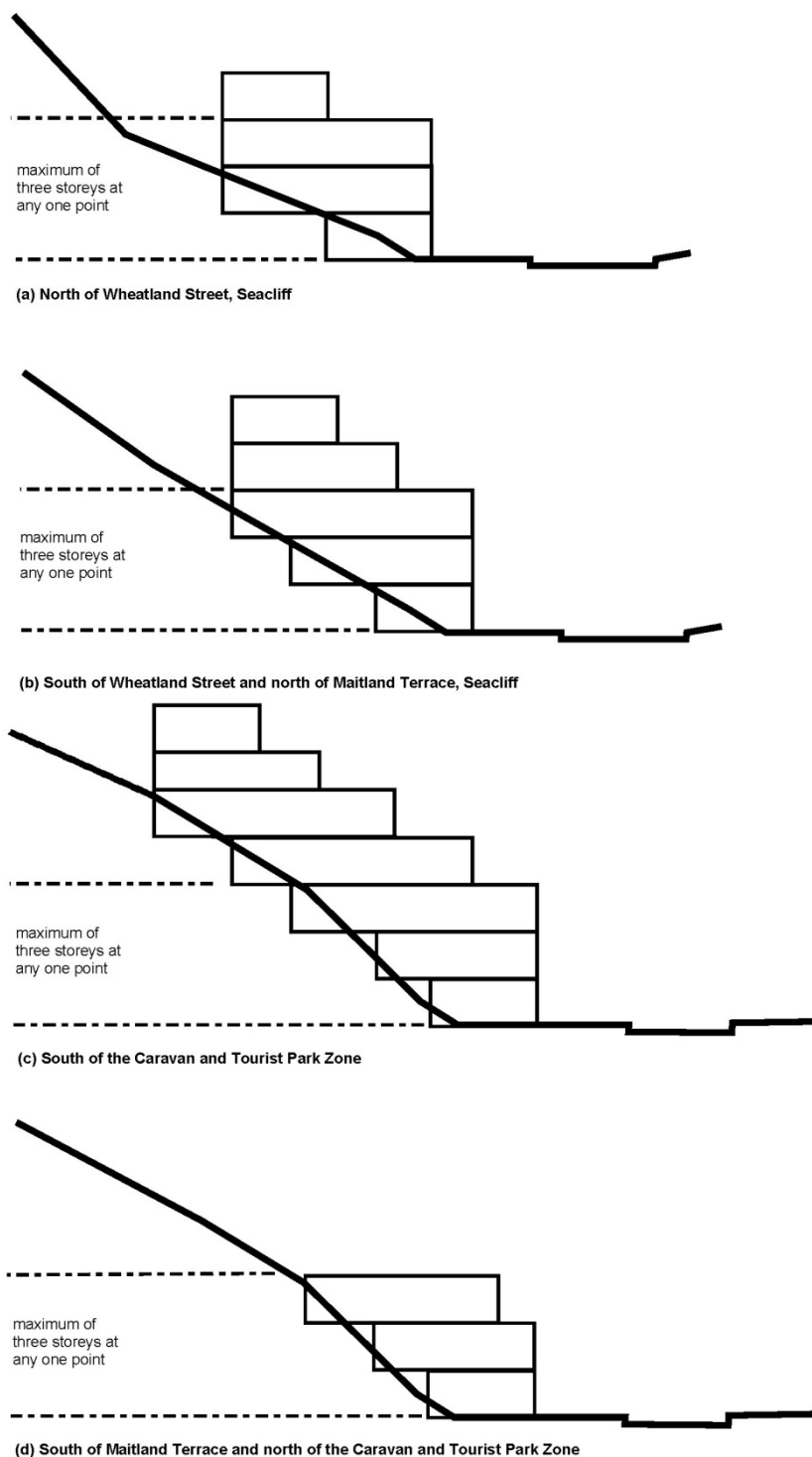


Figure 2 – Facade and Separation Design Approach in the Central West Policy Area 3 and the South West Policy Area 7

Development greater than one storey in height should not have a continuous western facade exceeding the requirements as detailed below:

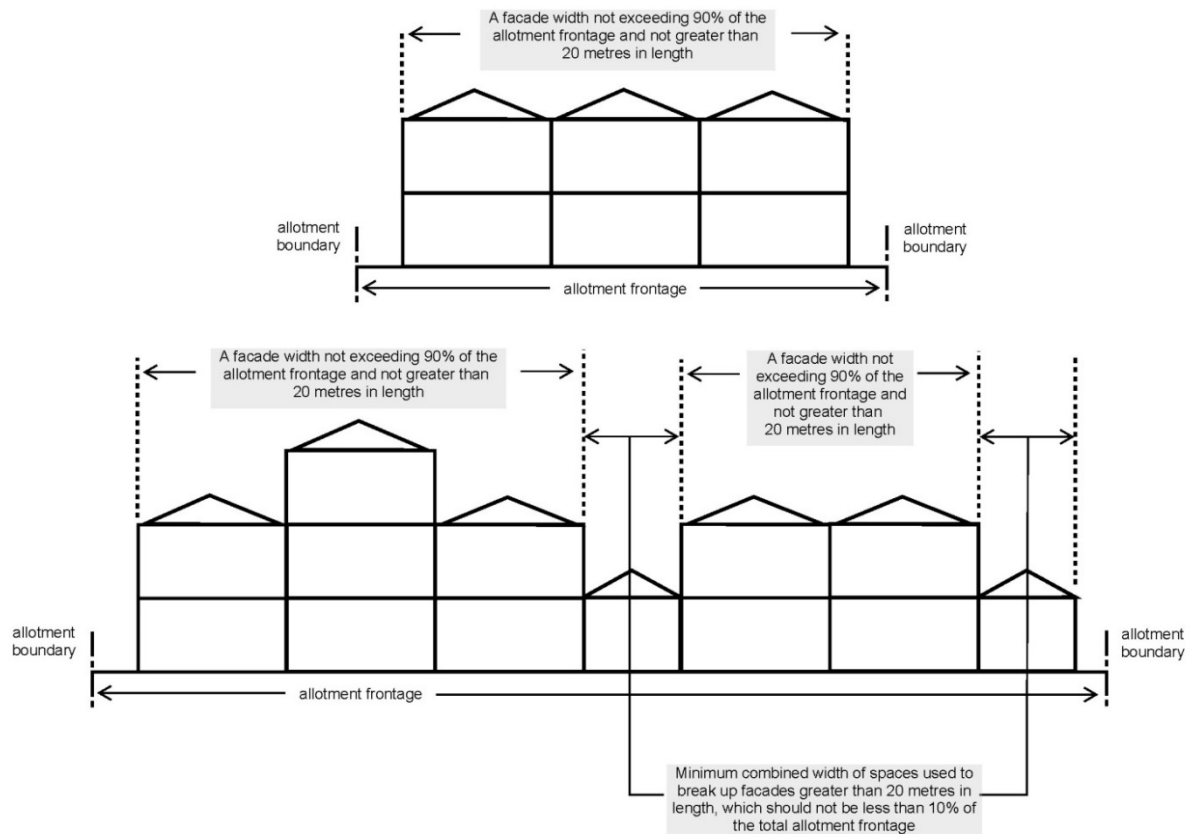
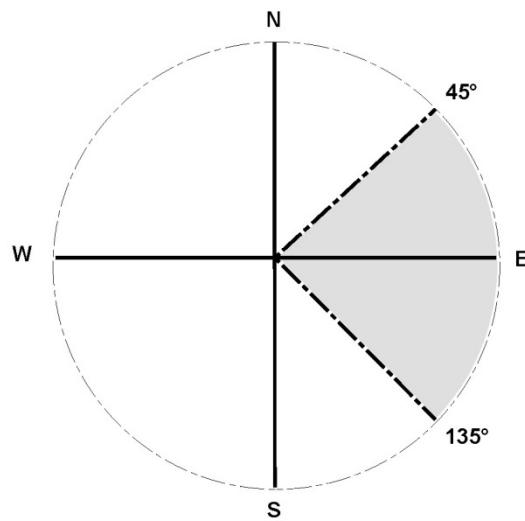


Figure 3 – Definition of the Northern and Southern Boundaries of a Site



Any site boundary having a bearing of between 45° and 135° of true north and located at the northern extremity of the site is defined as the northern boundary. Any site boundary having a bearing of between 45° and 135° of true north and located at the southern extremity of the site is defined as the southern boundary (as shown below):

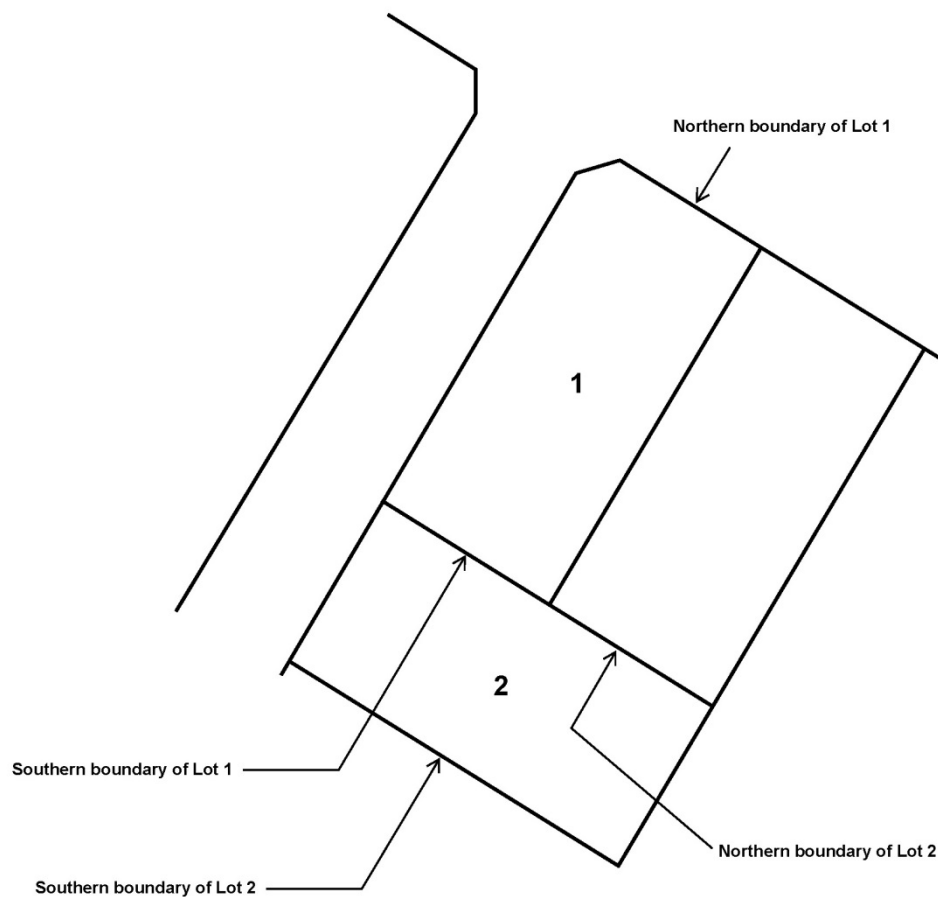


Table HoB/3 - Contributory Items

| Property Address | Description of Contributory item | Certificate of Title | Heritage NR |
|-------------------------------|----------------------------------|----------------------|-------------|
| 17 Augusta Street GLENELG | Dwelling | CT 5095/925 | 26354 |
| 17a Augusta Street GLENELG | Dwelling | CT 5095/925 | 26353 |
| 9 Augusta Street GLENELG | Dwelling | CT 5129/75 | 4638 |
| 11 Augusta Street GLENELG | Dwelling | CT 5488/560 | 4639 |
| 13 Augusta Street GLENELG | Dwelling | CT 5115/59 | 4640 |
| 15 Augusta Street GLENELG | Dwelling | CT 5255/779 | 4641 |
| 1/ 1 Byron Street GLENELG | Dwelling | CT 5025/610 | 26369 |
| 2/ 1 Byron Street GLENELG | Office | CT 5025/611 | 26370 |
| 1/ 20 Byron Street GLENELG | Dwelling | CT 5019/334 | 26372 |
| 2/ 20 Byron Street GLENELG | Dwelling | CT 5019/335 | 26371 |
| 1/ 22 Byron Street GLENELG | Dwelling | CT 5102/766 | 26373 |
| 2/ 22 Byron Street GLENELG | Dwelling | CT 5102/767 | 26374 |
| 2/ 33 Byron Street GLENELG | Dwelling | CT 5017/398 | 26377 |
| 5 Byron Street GLENELG | Office | CT 5063/350 | 4615 |
| 7 Byron Street GLENELG | Dwelling | CT 5038/477 | 4616 |
| 16 Byron Street GLENELG | Dwelling | CT 5720/762 | 4617 |
| 1/ 27-29 Byron Street GLENELG | Dwelling | CT 5020/505 | 26375 |
| 2/ 27-29 Byron Street GLENELG | Dwelling | CT 5020/506 | 26376 |
| 1/ 33 Byron Street GLENELG | Dwelling | CT 5017/397 | 26378 |
| 34 Byron Street GLENELG | Dwelling | CT 5087/52 | 4620 |
| 35-37 Byron Street GLENELG | Dwelling | CT 5504/178 | 4625 |
| 36 Byron Street GLENELG | Dwelling | CT 5087/51 | 4621 |
| 38 Byron Street GLENELG | Dwelling | CT 5212/511 | 4622 |
| 39 Byron Street GLENELG | Dwelling | CT 5394/961 | 4626 |
| 43 Byron Street GLENELG | Dwelling | CT 6037/46 | 4627 |
| 43A Byron Street GLENELG | Dwelling | CT 5813/271 | 26382 |
| 45-45a Byron Street GLENELG | Dwelling | CT 5139/655 | 4628 |
| 34 Durham Street GLENELG | Dwelling | CT 5178/820 | 4662 |
| 36 Durham Street GLENELG | Dwelling | CT 5209/128 | 4663 |

| Property Address | Description of Contributory item | Certificate of Title | Heritage NR |
|---------------------------------|----------------------------------|----------------------|-------------|
| 11 Gordon Street GLENELG | Dwelling | CT 5495/490 | 20719 |
| 1/ 13 Gordon Street GLENELG | Dwelling | CT 5026/110 | 26390 |
| 2/ 13 Gordon Street GLENELG | Dwelling | CT 5026/111 | 26389 |
| 2/ 17 Gordon Street GLENELG | Dwelling | CT 5027/463 | 26392 |
| 1/ 19 Gordon Street GLENELG | Dwelling | CT 5027/462 | 26391 |
| 1/ 21 Gordon Street GLENELG | Dwelling | CT 5028/432 | 26387 |
| 2/ 21 Gordon Street GLENELG | Dwelling | CT 5028/433 | 26388 |
| 1/ 25 Gordon Street GLENELG | Dwelling | CT 5018/119 | 26393 |
| 2/ 25 Gordon Street GLENELG | Dwelling | CT 5018/120 | 26394 |
| 1/ 29 Gordon Street GLENELG | Dwelling | CT 5017/623 | 26396 |
| 2/ 29 Gordon Street GLENELG | Dwelling | CT 5017/624 | 26395 |
| 1/ 33 Gordon Street GLENELG | Dwelling | CT 5008/781 | 26397 |
| 2/ 33 Gordon Street GLENELG | Dwelling | CT 5008/782 | 26398 |
| 35-37 Gordon Street GLENELG | Dwelling | CT 5872/103 | 4610 |
| 39 Gordon Street GLENELG | Dwelling | CT 5895/849 | 26399 |
| 41 Gordon Street GLENELG | Dwelling | CT 5895/848 | 26400 |
| 43 Gordon Street GLENELG | Dwelling | CT 5178/756 | 4612 |
| 45 Gordon Street GLENELG | Dwelling | CT 5178/661 | 4613 |
| 9 Maturin Road GLENELG | Dwelling | CT 5957/647 | 4667 |
| 11 Maturin Road GLENELG | Dwelling | CT 5443/431 | 4668 |
| 12 Maturin Road GLENELG | Dwelling | CT 5778/891 | 4664 |
| 13 Maturin Road GLENELG | Dwelling | CT 5844/450 | 4669 |
| 14 Maturin Road GLENELG | Dwelling | CT 5824/593 | 4665 |
| 15 Maturin Road GLENELG | Dwelling | CT 5856/64 | 4670 |
| 16 Maturin Road GLENELG | Dwelling | CT 5642/857 | 4666 |
| 17 Maturin Road GLENELG | Dwelling | CT 5321/91 | 4671 |
| 19 Maturin Road GLENELG | Dwelling | CT 5498/180 | 4672 |
| 21 Maturin Road GLENELG | Dwelling | CT 5214/788 | 4673 |
| 22 Maturin Road GLENELG | Dwelling | CT 5609/991 | 20720 |
| 1/ 56-58 Moseley Street GLENELG | Dwelling | CT 5139/769 | 26414 |
| 2/ 56-58 Moseley Street GLENELG | Dwelling | CT 5139/770 | 26413 |
| 99 Moseley Street GLENELG | Dwelling | CT 5942/679 | 26427 |

| Property Address | Description of Contributory item | Certificate of Title | Heritage NR |
|-------------------------------|----------------------------------|----------------------|-------------|
| 99A Moseley Street GLENELG | Dwelling | CT 5942/680 | 26426 |
| 1/ 101 Moseley Street GLENELG | Dwelling | CT 5507/623 | 26415 |
| 2/ 101 Moseley Street GLENELG | Dwelling | CT 5507/624 | 26416 |
| 3/ 101 Moseley Street GLENELG | Dwelling | CT 5507/625 | 26417 |
| 4/ 101 Moseley Street GLENELG | Dwelling | CT 5507/626 | 26418 |
| 5/ 101 Moseley Street GLENELG | Dwelling | CT 5507/627 | 26419 |
| 6/ 101 Moseley Street GLENELG | Dwelling | CT 5507/628 | 26420 |
| 7/ 101 Moseley Street GLENELG | Dwelling | CT 5507/629 | 26421 |
| 8/ 101 Moseley Street GLENELG | Dwelling | CT 5507/630 | 26422 |
| 19 Nile Street GLENELG | Office | CT 5173/352 | 4643 |
| 1/ 7 Sussex Street GLENELG | Dwelling | CT 5028/438 | 26438 |
| 2/ 7 Sussex Street GLENELG | Dwelling | CT 5028/439 | 26437 |
| 3/ 7 Sussex Street GLENELG | Dwelling | CT 5028/440 | 26436 |
| 12 Sussex Street GLENELG | Office | CT 5119/196 | 4645 |
| 13 Sussex Street GLENELG | Dwelling | CT 5275/691 | 4656 |
| 16 Sussex Street GLENELG | Dwelling | CT 5508/195 | 4646 |
| 18 Sussex Street GLENELG | Dwelling | CT 5186/621 | 4647 |
| 19 Sussex Street GLENELG | Dwelling | CT 5663/187 | 4657 |
| 1/ 20 Sussex Street GLENELG | Dwelling | CT 5009/38 | 26439 |
| 2/ 20 Sussex Street GLENELG | Dwelling | CT 5009/39 | 26440 |
| 3/ 20 Sussex Street GLENELG | Dwelling | CT 5009/40 | 26441 |
| 4/ 20 Sussex Street GLENELG | Dwelling | CT 5009/41 | 26442 |
| 1/ 21 Sussex Street GLENELG | Dwelling | CT 5005/375 | 26444 |
| 2/ 21 Sussex Street GLENELG | Dwelling | CT 5005/376 | 26443 |
| 27 Sussex Street GLENELG | Dwelling | CT 5157/759 | 4659 |
| 32 Sussex Street GLENELG | Dwelling | CT 5946/441 | 4649 |
| 36 Sussex Street GLENELG | Dwelling | CT 5384/125 | 4650 |
| 37-39 Sussex Street GLENELG | Dwelling | CT 5697/36 | 4660 |
| 38 Sussex Street GLENELG | Dwelling | CT 5275/769 | 4651 |
| 40 Sussex Street GLENELG | Dwelling | CT 5610/692 | 4652 |
| 42 Sussex Street GLENELG | Dwelling | CT 5101/684 | 4653 |
| 44-46 Sussex Street GLENELG | Dwelling | CT 5101/683 | 4654 |

| Property Address | Description of Contributory item | Certificate of Title | Heritage NR |
|---------------------------------|----------------------------------|----------------------|-------------|
| 1/ 2 Waterloo Street GLENELG | office | CT 5028/148 | 26447 |
| 2/ 4 Waterloo Street GLENELG | Consulting Room | CT 5028/149 | 26448 |
| 1/ 6 Waterloo Street GLENELG | Consulting Room | CT 5158/3 | 26449 |
| 2/ 8 Waterloo Street GLENELG | Consulting Room | CT 5158/4 | 26450 |
| 14 Waterloo Street GLENELG | Consulting Room | CT 5410/879 | 4631 |
| 25 Waterloo Street GLENELG | Dwelling | CT 5398/591 | 26452 |
| 27 Waterloo Street GLENELG | Dwelling | CT 5398/590 | 26451 |
| 30 Waterloo Street GLENELG | Dwelling | CT 5857/353 | 4632 |
| 34 Waterloo Street GLENELG | Dwelling | CT 5163/32 | 4633 |
| 43 Waterloo Street GLENELG | Dwelling | CT 5281/384 | 4635 |
| 4 St Peters Street GLENELG EAST | Dwelling | CT 5739/131 | 4734 |
| 7 St Peters Street GLENELG EAST | Dwelling | CT 5081/158 | 4730 |
| 1/ 2 Allen Terrace GLENELG EAST | Dwelling | CT 5147/760 | 26344 |
| 2/ 2 Allen Terrace GLENELG EAST | Dwelling | CT 5913/740 | 26345 |
| 1/ 6 Allen Terrace GLENELG EAST | Dwelling | CT 5063/968 | 26346 |
| 2/ 6 Allen Terrace GLENELG EAST | Dwelling | CT 5063/969 | 26347 |
| 1 Allen Terrace GLENELG EAST | Dwelling | CT 5176/874 | 4740 |
| 5 Allen Terrace GLENELG EAST | Dwelling | CT 5707/24 | 4741 |
| 7 Allen Terrace GLENELG EAST | Dwelling | CT 5129/71 | 4742 |
| 11 Allen Terrace GLENELG EAST | Dwelling | CT 5739/150 | 4743 |
| 12 Allen Terrace GLENELG EAST | Dwelling | CT 5737/802 | 4746 |
| 14 Allen Terrace GLENELG EAST | Dwelling | CT 5711/433 | 4747 |
| 16 Allen Terrace GLENELG EAST | Dwelling | CT 5423/351 | 4748 |
| 18 Allen Terrace GLENELG EAST | Dwelling | CT 5799/573 | 4749 |
| 22 Allen Terrace GLENELG EAST | Dwelling | CT 5010/673 | 4750 |
| 24 Allen Terrace GLENELG EAST | Dwelling | CT 5337/281 | 4751 |
| 32 Allen Terrace GLENELG EAST | Dwelling | CT 5703/752 | 4752 |
| 1/ 18 Farr Terrace GLENELG EAST | Dwelling | CT 5041/919 | 26380 |
| 2/ 18 Farr Terrace GLENELG EAST | Dwelling | CT 5041/920 | 26381 |
| 1 Farr Terrace GLENELG EAST | Dwelling | CT 5600/901 | 4678 |
| 2 Farr Terrace GLENELG EAST | Dwelling | CT 5175/992 | 4700 |
| 3 Farr Terrace GLENELG EAST | Dwelling | CT 5614/96 | 4679 |

| Property Address | Description of Contributory item | Certificate of Title | Heritage NR |
|---------------------------------|----------------------------------|----------------------|-------------|
| 4 Farr Terrace GLENELG EAST | Dwelling | CT 5397/526 | 4701 |
| 5 Farr Terrace GLENELG EAST | Dwelling | CT 5740/405 | 4680 |
| 6 Farr Terrace GLENELG EAST | Dwelling | CT 5740/406 | 4702 |
| 7 Farr Terrace GLENELG EAST | Dwelling | CT 5488/526 | 4681 |
| 8 Farr Terrace GLENELG EAST | Dwelling | CT 5350/833 | 4703 |
| 9 Farr Terrace GLENELG EAST | Dwelling | CT 5298/838 | 4682 |
| 10 Farr Terrace GLENELG EAST | Dwelling | CT 5425/20 | 4704 |
| 11 Farr Terrace GLENELG EAST | Dwelling | CT 5739/45 | 4683 |
| 12 Farr Terrace GLENELG EAST | Dwelling | CT 5739/637 | 4705 |
| 13 Farr Terrace GLENELG EAST | Dwelling | CT 5223/850 | 4684 |
| 14 Farr Terrace GLENELG EAST | Dwelling | CT 5538/450 | 4706 |
| 15 Farr Terrace GLENELG EAST | Dwelling | CT 5448/436 | 4685 |
| 16 Farr Terrace GLENELG EAST | Dwelling | CT 5799/565 | 4699 |
| 17 Farr Terrace GLENELG EAST | Dwelling | CT 5123/10 | 4686 |
| 19 Farr Terrace GLENELG EAST | Dwelling | CT 5798/215 | 4687 |
| 21 Farr Terrace GLENELG EAST | Dwelling | CT 5334/209 | 4688 |
| 23 Farr Terrace GLENELG EAST | Dwelling | CT 5510/900 | 4689 |
| 25 Farr Terrace GLENELG EAST | Dwelling | CT 5731/692 | 4690 |
| 29 Farr Terrace GLENELG EAST | Dwelling | CT 5740/407 | 4691 |
| 1/ 31 Farr Terrace GLENELG EAST | Dwelling | CT 5023/832 | 26384 |
| 2/ 31 Farr Terrace GLENELG EAST | Dwelling | CT 5023/833 | 26383 |
| 33 Farr Terrace GLENELG EAST | Dwelling | CT 5540/780 | 4693 |
| 35 Farr Terrace GLENELG EAST | Dwelling | CT 5725/735 | 4694 |
| 37 Farr Terrace GLENELG EAST | Dwelling | CT 5401/676 | 4695 |
| 39 Farr Terrace GLENELG EAST | Dwelling | CT 5728/3 | 4696 |
| 1/ 41 Farr Terrace GLENELG EAST | Dwelling | CT 5046/592 | 26385 |
| 2/ 41 Farr Street GLENELG EAST | Dwelling | CT 5046/593 | 26386 |
| 1 Hawkes Avenue GLENELG EAST | Dwelling | CT 5128/512 | 4710 |
| 1/ 2 Hawkes Avenue GLENELG EAST | Dwelling | CT 5044/662 | 26410 |
| 2/ 2 Hawkes Avenue GLENELG EAST | Dwelling | CT 5044/663 | 26409 |
| 3 Hawkes Avenue GLENELG EAST | Dwelling | CT 5608/958 | 4712 |
| 4 Hawkes Avenue GLENELG EAST | Dwelling | CT 5799/564 | 4713 |

| Property Address | Description of Contributory item | Certificate of Title | Heritage NR |
|----------------------------------|---|-----------------------------|--------------------|
| 5 Hawkes Avenue GLENELG EAST | Dwelling | CT 5121/55 | 4714 |
| 6 Hawkes Avenue GLENELG EAST | Dwelling | CT 5090/918 | 4715 |
| 8 Hawkes Avenue GLENELG EAST | Dwelling | CT 5090/928 | 4716 |
| 9 Hawkes Avenue GLENELG EAST | Dwelling | CT 5739/432 | 4717 |
| 10 Hawkes Avenue GLENELG EAST | Dwelling | CT 5446/516 | 4718 |
| 11 Hawkes Avenue GLENELG EAST | Dwelling | CT 5753/61 | 4719 |
| 12 Hawkes Avenue GLENELG EAST | Dwelling | CT 6045/998 | 4720 |
| 1 Rugless Terrace GLENELG EAST | Dwelling | CT 5166/707 | 4676 |
| 2 Rugless Terrace GLENELG EAST | Dwelling | CT 5732/791 | 4707 |
| 4 Rugless Terrace GLENELG EAST | Dwelling | CT 5073/360 | 4708 |
| 5 Rugless Terrace GLENELG EAST | Dwelling | CT 5144/314 | 4677 |
| 6 Rugless Terrace GLENELG EAST | Dwelling | CT 5399/621 | 4709 |
| 1-1a Short Avenue GLENELG EAST | Dwelling | CT 5740/408 | 4753 |
| 2 Short Avenue GLENELG EAST | Dwelling | CT 5220/856 | 4761 |
| 3 Short Avenue GLENELG EAST | Dwelling | CT 5157/472 | 4754 |
| 4 Short Avenue GLENELG EAST | Dwelling | CT 5741/879 | 4762 |
| 6 Short Avenue GLENELG EAST | Dwelling | CT 5744/432 | 4763 |
| 8 Short Avenue GLENELG EAST | Dwelling | CT 5113/804 | 4764 |
| 9 Short Avenue GLENELG EAST | Dwelling | CT 5300/808 | 4755 |
| 11 Short Avenue GLENELG EAST | Dwelling | CT 5499/213 | 4756 |
| 13 Short Avenue GLENELG EAST | Dwelling | CT 5749/277 | 4757 |
| 14 Short Avenue GLENELG EAST | Dwelling | CT 5645/423 | 4765 |
| 15 Short Avenue GLENELG EAST | Dwelling | CT 5824/471 | 4758 |
| 16 Short Avenue GLENELG EAST | Dwelling | CT 5566/879 | 4766 |
| 17 Short Avenue GLENELG EAST | Dwelling | CT 5476/281 | 4759 |
| 18-18a Short Avenue GLENELG EAST | Dwelling | CT 5711/435 | 4767 |
| 19 Short Avenue GLENELG EAST | Dwelling | CT 5536/789 | 4760 |
| 20 Short Avenue GLENELG EAST | Dwelling | CT 5730/525 | 4768 |
| 1 St Peters Street GLENELG EAST | Dwelling | CT 5550/851 | 4728 |
| 2 St Peters Street GLENELG EAST | Dwelling | CT 5273/999 | 4733 |
| 5a St Peters Street GLENELG EAST | Dwelling | CT 5248/101 | 4729 |
| 6 St Peters Street GLENELG EAST | Dwelling | CT 5312/161 | 4735 |

Holdfast Bay Council
Table Section
Table HoB/3 - Contributory Items

| Property Address | Description of Contributory item | Certificate of Title | Heritage NR |
|-------------------------------------|----------------------------------|----------------------|-------------|
| 8 St Peters Street GLENELG EAST | Dwelling | CT 5736/295 | 4736 |
| 9 St Peters Street GLENELG EAST | Dwelling | CT 5407/954 | 4731 |
| 10 St Peters Street GLENELG EAST | Dwelling | CT 5416/384 | 4737 |
| 12 St Peters Street GLENELG EAST | Dwelling | CT 5286/930 | 4738 |
| 13 St Peters Street GLENELG EAST | Dwelling | CT 5236/479 | 4732 |
| 14 St Peters Street GLENELG EAST | Dwelling | CT 5724/874 | 4739 |
| 1/ 22A Williams Avenue GLENELG EAST | Dwelling | CT 5380/801 | 26464 |
| 2/ 22A Williams Avenue GLENELG EAST | Dwelling | CT 5380/802 | 26463 |
| 1/ 1 Williams Avenue GLENELG EAST | Dwelling | CT 5055/404 | 26454 |
| 2/ 1 Williams Avenue GLENELG EAST | Dwelling | CT 5055/405 | 26453 |
| 2 Williams Avenue GLENELG EAST | Dwelling | CT 5731/896 | 4769 |
| 3 Williams Avenue GLENELG EAST | Dwelling | CT 5477/404 | 26456 |
| 3A Williams Avenue GLENELG EAST | Dwelling | CT 5426/821 | 26455 |
| 4 Williams Avenue GLENELG EAST | Dwelling | CT 5706/516 | 4770 |
| 5 Williams Avenue GLENELG EAST | Dwelling | CT 5203/582 | 26458 |
| 5A Williams Avenue GLENELG EAST | Dwelling | CT 5082/936 | 26457 |
| 6 Williams Avenue GLENELG EAST | Dwelling | CT 5714/398 | 4771 |
| 7a Williams Avenue GLENELG EAST | Dwelling | CT 5000/836 | 4787 |
| 8-8a Williams Avenue GLENELG EAST | Dwelling | CT 5711/434 | 4772 |
| 9 Williams Avenue GLENELG EAST | Dwelling | CT 5711/443 | 4788 |
| 10 Williams Avenue GLENELG EAST | Dwelling | CT 5375/843 | 26459 |
| 10A Williams Avenue GLENELG EAST | Dwelling | CT 5145/738 | 26460 |
| 12 Williams Avenue GLENELG EAST | Dwelling | CT 5244/119 | 26461 |
| 12A Williams Avenue GLENELG EAST | Dwelling | CT 5244/120 | 26462 |
| 14 Williams Avenue GLENELG EAST | Dwelling | CT 5350/277 | 4776 |
| 16 Williams Avenue GLENELG EAST | Dwelling | CT 5231/818 | 4777 |
| 18 Williams Avenue GLENELG EAST | Dwelling | CT 5086/508 | 4778 |
| 20 Williams Avenue GLENELG EAST | Dwelling | CT 5268/790 | 4779 |
| 24 Williams Avenue GLENELG EAST | Dwelling | CT 5792/572 | 4781 |
| 1/ 26 Williams Avenue GLENELG EAST | Dwelling | CT 5009/896 | 26465 |
| 2/ 26 Williams Avenue GLENELG EAST | Dwelling | CT 5009/897 | 26466 |
| 28 Williams Avenue GLENELG EAST | Dwelling | CT 5081/197 | 4783 |

| Property Address | Description of Contributory item | Certificate of Title | Heritage NR |
|---------------------------------|----------------------------------|----------------------|-------------|
| 1 Wyatt Street GLENELG EAST | Dwelling | CT 5182/393 | 4721 |
| 2 Wyatt Street GLENELG EAST | Dwelling | CT 5118/957 | 4722 |
| 3 Wyatt Street GLENELG EAST | Dwelling | CT 5490/182 | 4723 |
| 5 Wyatt Street GLENELG EAST | Dwelling | CT 5499/598 | 4724 |
| 1/ 6 Wyatt Street GLENELG EAST | Dwelling | CT 5026/155 | 26468 |
| 2/ 6 Wyatt Street GLENELG EAST | Dwelling | CT 5026/156 | 26467 |
| 7 Wyatt Street GLENELG EAST | Dwelling | CT 5739/130 | 4726 |
| 1/ 10 Wyatt Street GLENELG EAST | Dwelling | CT 5030/708 | 26470 |
| 2/ 10 Wyatt Street GLENELG EAST | Dwelling | CT 5030/709 | 26469 |
| 5-5a Young Street GLENELG EAST | Dwelling | CT 5227/842 | 4789 |
| 1/ 6 Young Street GLENELG EAST | Dwelling | CT 5013/768 | 26471 |
| 2/ 6 Young Street GLENELG EAST | Dwelling | CT 2013/769 | 26472 |
| 8 Young Street GLENELG EAST | Dwelling | CT 5409/520 | 4792 |
| 1/ 10 Young Street GLENELG EAST | Dwelling | CT 5023/218 | 26474 |
| 2/ 10 Young Street GLENELG EAST | Dwelling | CT 5023/219 | 26473 |
| 12 Young Street GLENELG EAST | Dwelling | CT 5209/518 | 4794 |
| 15 Young Street GLENELG EAST | Dwelling | CT 5798/201 | 4790 |
| 2/ 10 Alma Street GLENELG SOUTH | Dwelling | CT 5330/786 | 26350 |
| 1/ 4 Alma Street GLENELG SOUTH | Dwelling | CT 5028/539 | 26348 |
| 2/ 4 Alma Street GLENELG SOUTH | Dwelling | CT 5028/540 | 26349 |
| 1/ 8 Alma Street GLENELG SOUTH | Dwelling | CT 5934/92 | 26351 |
| 1 Alma Street GLENELG SOUTH | Dwelling | CT 5066/506 | 4865 |
| 2 Alma Street GLENELG SOUTH | Dwelling | CT 5447/446 | 4869 |
| 3 Alma Street GLENELG SOUTH | Dwelling | CT 5447/87 | 4866 |
| 7 Alma Street GLENELG SOUTH | Dwelling | CT 5097/531 | 4867 |
| 9 Alma Street GLENELG SOUTH | Dwelling | CT 5073/889 | 4868 |
| 12 Alma Street GLENELG SOUTH | Dwelling | CT 5106/665 | 4872 |
| 14 Alma Street GLENELG SOUTH | Dwelling | CT 5201/64 | 4873 |
| 16 Alma Street GLENELG SOUTH | Dwelling | CT 5141/371 | 4874 |
| 18 Alma Street GLENELG SOUTH | Dwelling | CT 5075/812 | 4875 |
| 20 Alma Street GLENELG SOUTH | Dwelling | CT 5213/670 | 4876 |
| 22 Alma Street GLENELG SOUTH | Dwelling | CT 5144/562 | 4877 |

| Property Address | Description of Contributory item | Certificate of Title | Heritage NR |
|------------------------------------|----------------------------------|----------------------|-------------|
| 1 Baker Street GLENELG SOUTH | Dwelling | CT 5450/687 | 26364 |
| 10 Bath Street GLENELG SOUTH | Dwelling | CT 5399/699 | 26355 |
| 12 Bath Street GLENELG SOUTH | Dwelling | CT 5399/699 | 26356 |
| 1/ 14-16 Bath Street GLENELG SOUTH | Dwelling | CT 5000/185 | 26358 |
| 2/ 14-16 Bath Street GLENELG SOUTH | Dwelling | CT 5000/186 | 26357 |
| 1/ 18 Bath Street GLENELG SOUTH | Dwelling | CT 5009/984 | 26359 |
| 2/ 18 Bath Street GLENELG SOUTH | Dwelling | CT 5009/985 | 26360 |
| 22 Bath Street GLENELG SOUTH | Dwelling | CT 5223/685 | 26361 |
| 24 Bath Street GLENELG SOUTH | Dwelling | CT 5153/641 | 26362 |
| 26 Bath Street GLENELG SOUTH | Dwelling | CT 5062/448 | 26363 |
| 8 Bath Street GLENELG SOUTH | Dwelling | CT 5484/286 | 5023 |
| 1 Bath Street GLENELG SOUTH | Dwelling | CT 5478/582 | 5052 |
| 3 Bath Street GLENELG SOUTH | Dwelling | CT 5123/140 | 5051 |
| 5 Bath Street GLENELG SOUTH | Dwelling | CT 5473/257 | 5050 |
| 7 Bath Street GLENELG SOUTH | Dwelling | CT 5196/277 | 5049 |
| 9 Bath Street GLENELG SOUTH | Dwelling | CT 5168/974 | 5046 |
| 13 Bath Street GLENELG SOUTH | Dwelling | CT 5473/14 | 5031 |
| 21 Bath Street GLENELG SOUTH | Dwelling | CT 5089/460 | 5006 |
| 36 Bath Street GLENELG SOUTH | Dwelling | CT 6015/156 | 5009 |
| 38 Bath Street GLENELG SOUTH | Dwelling | CT 5090/650 | 5008 |
| 48 Bath Street GLENELG SOUTH | Dwelling | CT 5479/787 | 5007 |
| 68 Bath Street GLENELG SOUTH | Dwelling | CT 5476/953 | 4978 |
| 70 Bath Street GLENELG SOUTH | Dwelling | CT 6062/127 | 4977 |
| 72 Bath Street GLENELG SOUTH | Dwelling | CT 5094/732 | 4976 |
| 101 Brighton Road GLENELG SOUTH | Dwelling | CT 5441/364 | 4955 |
| 103 Brighton Road GLENELG SOUTH | Dwelling | CT 5876/837 | 4956 |
| 105 Brighton Road GLENELG SOUTH | Dwelling | CT 5400/144 | 4957 |
| 107 Brighton Road GLENELG SOUTH | Dwelling | CT 5438/932 | 4958 |
| 109 Brighton Road GLENELG SOUTH | Dwelling | CT 5172/30 | 4959 |
| 111 Brighton Road GLENELG SOUTH | Dwelling | CT 5251/962 | 4960 |
| 115 Brighton Road GLENELG SOUTH | Dwelling | CT 5190/594 | 4961 |
| 117 Brighton Road GLENELG SOUTH | Dwelling | CT 5466/823 | 4962 |

| Property Address | Description of Contributory item | Certificate of Title | Heritage NR |
|---------------------------------|----------------------------------|----------------------|-------------|
| 119 Brighton Road GLENELG SOUTH | Dwelling | CT 5216/696 | 4963 |
| 121 Brighton Road GLENELG SOUTH | Dwelling | CT 5222/743 | 4964 |
| 123 Brighton Road GLENELG SOUTH | Dwelling | CT 5808/797 | 4965 |
| 8 Bristol Street GLENELG SOUTH | Dwelling | CT 5456/78 | 4800 |
| 12 Bristol Street GLENELG SOUTH | Dwelling | CT 5063/91 | 4801 |
| 14 Bristol Street GLENELG SOUTH | Dwelling | CT 5474/49 | 4802 |
| 10 Broadway GLENELG SOUTH | Dwelling | CT 5474/48 | 26367 |
| 11 Broadway GLENELG SOUTH | Dwelling | CT 5496/556 | 4815 |
| 12 Broadway GLENELG SOUTH | Dwelling | CT 5473/560 | 26366 |
| 19 Broadway GLENELG SOUTH | Dwelling | CT 5248/728 | 4816 |
| 20 Broadway GLENELG SOUTH | Dwelling | CT 5161/304 | 5067 |
| 21 Broadway GLENELG SOUTH | Dwelling | CT 5073/388 | 4817 |
| 25 Broadway GLENELG SOUTH | Dwelling | CT 5733/73 | 5018 |
| 27 Broadway GLENELG SOUTH | Dwelling | CT 5342/251 | 5017 |
| 29 Broadway GLENELG SOUTH | Dwelling | CT 5085/778 | 4834 |
| 35 Broadway GLENELG SOUTH | Dwelling | CT 5439/962 | 4837 |
| 36 Broadway GLENELG SOUTH | Dwelling | CT 6033/591 | 4838 |
| 38 Broadway GLENELG SOUTH | Dwelling | CT 5478/492 | 5066 |
| 40 Broadway GLENELG SOUTH | Dwelling | CT 5455/737 | 5065 |
| 76 Broadway GLENELG SOUTH | Dwelling | CT 5202/577 | 4985 |
| 78 Broadway GLENELG SOUTH | Dwelling | CT 5339/689 | 4907 |
| 80 Broadway GLENELG SOUTH | Dwelling | CT 5087/182 | 4979 |
| 81 Broadway GLENELG SOUTH | Dwelling | CT 5307/434 | 4904 |
| 82 Broadway GLENELG SOUTH | Dwelling | CT 5071/768 | 4969 |
| 83 Broadway GLENELG SOUTH | Dwelling | CT 5440/786 | 4905 |
| 85 Broadway GLENELG SOUTH | Dwelling | CT 5713/33 | 4906 |
| 86 Broadway GLENELG SOUTH | Dwelling | CT 5237/57 | 4968 |
| 88 Broadway GLENELG SOUTH | Dwelling | CT 5128/951 | 4967 |
| 90 Broadway GLENELG SOUTH | Dwelling | CT 5190/595 | 4966 |
| 91 Broadway GLENELG SOUTH | Dwelling | CT 5312/139 | 4908 |
| 23 Broadway GLENELG SOUTH | Dwelling | CT 5459/175 | 5019 |
| 28 Broadway GLENELG SOUTH | Dwelling | CT 5150/34 | 20714 |

| Property Address | Description of Contributory item | Certificate of Title | Heritage NR |
|--------------------------------------|----------------------------------|----------------------|-------------|
| 31-31A Broadway GLENELG SOUTH | Dwelling | CT 5452/593 | 4835 |
| 33 Broadway GLENELG SOUTH | Dwelling | CT 5320/964 | 4836 |
| 42 Broadway GLENELG SOUTH | Dwelling | CT 5092/401 | 5064 |
| 52A Broadway GLENELG SOUTH | Dwelling | CT 5473/898 | 20716 |
| 4 Church Street GLENELG SOUTH | Dwelling | CT 5365/131 | 5015 |
| 6 Farrell Street GLENELG SOUTH | Dwelling | CT 5459/182 | 5068 |
| 9 Farrell Street GLENELG SOUTH | Dwelling | CT 5344/106 | 5070 |
| 10 Farrell Street GLENELG SOUTH | Dwelling | CT 5296/731 | 5069 |
| 14 Farrell Street GLENELG SOUTH | Dwelling | CT 5370/788 | 5071 |
| 17 Farrell Street GLENELG SOUTH | Dwelling | CT 5108/930 | 5074 |
| 18 Farrell Street GLENELG SOUTH | Dwelling | CT 5682/931 | 5072 |
| 19 Farrell Street GLENELG SOUTH | Dwelling | CT 5648/339 | 5075 |
| 20 Farrell Street GLENELG SOUTH | Dwelling | CT 5157/697 | 5073 |
| 21 Farrell Street GLENELG SOUTH | Dwelling | CT 5830/509 | 5076 |
| 23 Farrell Street GLENELG SOUTH | Dwelling | CT 5874/279 | 5077 |
| Lt 224 Hastings Street GLENELG SOUTH | Church | CT 5473/261 | 5002 |
| 4 Hastings Street GLENELG SOUTH | Dwelling | CT 5823/73 | 4890 |
| 5 Hastings Street GLENELG SOUTH | Dwelling | CT 5230/16 | 4878 |
| 6 Hastings Street GLENELG SOUTH | Dwelling | CT 5447/62 | 4891 |
| 9a Hastings Street GLENELG SOUTH | Dwelling | CT 5808/875 | 4879 |
| 1/ 10 Hastings Street GLENELG SOUTH | Dwelling | CT 5933/985 | 26402 |
| 2/ 10 Hastings Street GLENELG SOUTH | Dwelling | CT 5039/184 | 26401 |
| 11 Hastings Street GLENELG SOUTH | Dwelling | CT 5105/635 | 4880 |
| 13 Hastings Street GLENELG SOUTH | Dwelling | CT 5466/60 | 4881 |
| 14 Hastings Street GLENELG SOUTH | Dwelling | CT 5439/380 | 4893 |
| 15 Hastings Street GLENELG SOUTH | Dwelling | CT 5148/81 | 4882 |
| 18 Hastings Street GLENELG SOUTH | Dwelling | CT 5249/47 | 4894 |
| 20 Hastings Street GLENELG SOUTH | Dwelling | CT 5698/620 | 4895 |
| 21 Hastings Street GLENELG SOUTH | Dwelling | CT 5128/854 | 4883 |
| 23 Hastings Street GLENELG SOUTH | Dwelling | CT 5073/432 | 4884 |
| 27 Hastings Street GLENELG SOUTH | Dwelling | CT 5450/724 | 4885 |
| 32 Hastings Street GLENELG SOUTH | Dwelling | CT 5120/461 | 4896 |

| Property Address | Description of Contributory item | Certificate of Title | Heritage NR |
|--------------------------------------|----------------------------------|----------------------|-------------|
| 2/ 34 Hastings Street GLENELG SOUTH | Dwelling | CT 5037/383 | 26403 |
| 1/ 34A Hastings Street GLENELG SOUTH | Dwelling | CT 5037/384 | 26404 |
| 36 Hastings Street GLENELG SOUTH | Dwelling | CT 5665/396 | 4898 |
| 37 Hastings Street GLENELG SOUTH | Dwelling | CT 5157/561 | 5001 |
| 40 Hastings Street GLENELG SOUTH | Dwelling | CT 5227/96 | 4900 |
| 1/ 42 Hastings Street GLENELG SOUTH | Dwelling | CT 5014/385 | 26406 |
| 2/ 42 Hastings Street GLENELG SOUTH | Dwelling | CT 5014/386 | 26405 |
| 47 Hastings Street GLENELG SOUTH | Dwelling | CT 5473/594 | 5003 |
| 48 Hastings Street GLENELG SOUTH | Dwelling | CT 5494/645 | 4902 |
| 49 Hastings Street GLENELG SOUTH | Dwelling | CT 5474/597 | 5004 |
| 51 Hastings Street GLENELG SOUTH | Dwelling | CT 5083/893 | 5005 |
| 54 Hastings Street GLENELG SOUTH | Dwelling | CT 6072/879 | 26408 |
| 56 Hastings Street GLENELG SOUTH | Dwelling | CT 6072/878 | 26407 |
| 58-60 Hastings Street GLENELG SOUTH | Dwelling | CT 5213/156 | 4997 |
| 62 Hastings Street GLENELG SOUTH | Dwelling | CT 5472/368 | 4998 |
| 64 Hastings Street GLENELG SOUTH | Dwelling | CT 5401/429 | 4999 |
| 72 Hastings Street GLENELG SOUTH | Dwelling | CT 5108/444 | 5000 |
| 2 John Street GLENELG SOUTH | Dwelling | CT 6015/157 | 5010 |
| 5 John Street GLENELG SOUTH | Dwelling | CT 5475/763 | 5011 |
| 1/ 1-1A Morley Street GLENELG SOUTH | Dwelling | CT 5179/771 | 26412 |
| 2/ 1-1A Morley Street GLENELG SOUTH | Dwelling | CT 5179/772 | 26411 |
| 3 Morley Street GLENELG SOUTH | Dwelling | CT 5124/120 | 5035 |
| 6 Morley Street GLENELG SOUTH | Dwelling | CT 5064/674 | 5024 |
| 8 Morley Street GLENELG SOUTH | Dwelling | CT 5104/485 | 5025 |
| 9 Morley Street GLENELG SOUTH | Dwelling | CT 5474/83 | 26476 |
| 9A Morley Street GLENELG SOUTH | Dwelling | CT 5474/83 | 26477 |
| 10 Morley Street GLENELG SOUTH | Dwelling | CT 5467/901 | 5026 |
| 11 Morley Street GLENELG SOUTH | Dwelling | CT 5136/349 | 5033 |
| 12 Morley Street GLENELG SOUTH | Dwelling | CT 5473/263 | 5027 |
| 13 Morley Street GLENELG SOUTH | Dwelling | CT 5090/920 | 5032 |
| 14 Morley Street GLENELG SOUTH | Dwelling | CT 5122/376 | 5028 |
| 16 Morley Street GLENELG SOUTH | Dwelling | CT 5372/437 | 5029 |

| Property Address | Description of Contributory item | Certificate of Title | Heritage NR |
|-----------------------------------|----------------------------------|----------------------|-------------|
| 18 Morley Street GLENELG SOUTH | Dwelling | CT 5417/696 | 5030 |
| 46 Moseley Street GLENELG SOUTH | Dwelling | CT 5810/454 | 4820 |
| 48 Moseley Street GLENELG SOUTH | Dwelling | CT 5811/925 | 4821 |
| 50 Moseley Street GLENELG SOUTH | Dwelling | CT 5322/253 | 4822 |
| 52 Moseley Street GLENELG SOUTH | Dwelling | CT 5209/275 | 4823 |
| 54 Moseley Street GLENELG SOUTH | Dwelling | CT 5110/905 | 4824 |
| 57 Moseley Street GLENELG SOUTH | Dwelling | CT 5411/578 | 4806 |
| 59 Moseley Street GLENELG SOUTH | Dwelling | CT 5857/337 | 4807 |
| 60 Moseley Street GLENELG SOUTH | Dwelling | CT 5452/125 | 4826 |
| 61 Moseley Street GLENELG SOUTH | Dwelling | CT 5457/282 | 4808 |
| 63 Moseley Street GLENELG SOUTH | Dwelling | CT 5168/774 | 4809 |
| 64 Moseley Street GLENELG SOUTH | Dwelling | CT 5094/6 | 4827 |
| 65 Moseley Street GLENELG SOUTH | Consulting Room | CT 5459/724 | 4810 |
| 66 Moseley Street GLENELG SOUTH | Dwelling | CT 5302/663 | 4828 |
| 67 Moseley Street GLENELG SOUTH | Dwelling | CT 5459/541 | 4811 |
| 70 Moseley Street GLENELG SOUTH | Dwelling | CT 5986/520 | 4830 |
| 72 Moseley Street GLENELG SOUTH | Dwelling | CT 5276/513 | 4831 |
| 74 Moseley Street GLENELG SOUTH | Dwelling | CT 5067/618 | 4832 |
| 76 Moseley Street GLENELG SOUTH | Dwelling | 5465/902 | 4833 |
| 78 Moseley Street GLENELG SOUTH | Dwelling | CT 5470/652 | 20715 |
| 79 Moseley Street GLENELG SOUTH | Dwelling | CT 5077/232 | 4812 |
| 81 Moseley Street GLENELG SOUTH | Dwelling | CT 5492/552 | 4813 |
| 83 Moseley Street GLENELG SOUTH | Dwelling | CT 5397/791 | 5062 |
| 84 Moseley Street GLENELG SOUTH | Dwelling | CT 5121/523 | 5055 |
| 86 Moseley Street GLENELG SOUTH | Dwelling | CT 5085/958 | 5054 |
| 86A Moseley Street GLENELG SOUTH | Dwelling | CT 5112/447 | 5053 |
| 92 Moseley Street GLENELG SOUTH | Dwelling | CT 5192/126 | 5016 |
| 95 Moseley Street GLENELG SOUTH | Dwelling | CT 5118/701 | 5059 |
| 97 Moseley Street GLENELG SOUTH | Dwelling | CT 5550/703 | 5058 |
| 45 Partridge Street GLENELG SOUTH | Dwelling | CT 5357/680 | 4888 |
| 47 Partridge Street GLENELG SOUTH | Dwelling | CT 5070/55 | 4909 |
| 53 Partridge Street GLENELG SOUTH | Dwelling | CT 5070/92 | 4910 |

| Property Address | Description of Contributory item | Certificate of Title | Heritage NR |
|---|----------------------------------|----------------------|-------------|
| 1/ 55 Partridge Street GLENELG SOUTH | Dwelling | CT 5513/958 | 26424 |
| 2/ 55 Partridge Street GLENELG SOUTH | Dwelling | CT 5882/287 | 26425 |
| 56 Partridge Street GLENELG SOUTH | Dwelling | CT 5452/123 | 4916 |
| 57 Partridge Street GLENELG SOUTH | Dwelling | CT 5289/333 | 4912 |
| 58 Partridge Street GLENELG SOUTH | Dwelling | CT 5230/440 | 4917 |
| 59 Partridge Street GLENELG SOUTH | Dwelling | CT 5698/619 | 4913 |
| 60 Partridge Street GLENELG SOUTH | Dwelling | CT 5301/512 | 4918 |
| 61 Partridge Street GLENELG SOUTH | Dwelling | CT 5093/460 | 20717 |
| 64 Partridge Street GLENELG SOUTH | Dwelling | CT 5450/327 | 4919 |
| 66 Partridge Street GLENELG SOUTH | Dwelling | CT 5126/518 | 4920 |
| 67 Partridge Street GLENELG SOUTH | Dwelling | CT 5397/392 | 4915 |
| 70 Partridge Street GLENELG SOUTH | Dwelling | CT 5198/519 | 4921 |
| 72 Partridge Street GLENELG SOUTH | Dwelling | CT 5494/646 | 4922 |
| 74 Partridge Street GLENELG SOUTH | Dwelling | CT 5176/253 | 4923 |
| 76 Partridge Street GLENELG SOUTH | Dwelling | CT 5478/247 | 4924 |
| 76a Partridge Street GLENELG SOUTH | Dwelling | CT 5433/326 | 4925 |
| 78 Partridge Street GLENELG SOUTH | Dwelling | CT 5082/612 | 4926 |
| 80 Partridge Street GLENELG SOUTH | Dwelling | CT 5514/333 | 4927 |
| 84 Partridge Street GLENELG SOUTH | Dwelling | CT 5080/814 | 4928 |
| 89 Partridge Street GLENELG SOUTH | Dwelling | CT 5472/921 | 4993 |
| 91 Partridge Street GLENELG SOUTH | Dwelling | CT 5104/383 | 4994 |
| 93 Partridge Street GLENELG SOUTH | Dwelling | CT 5104/383 | 4995 |
| 94 Partridge Street GLENELG SOUTH | Dwelling | CT 5178/349 | 4986 |
| 98 Partridge Street GLENELG SOUTH | Dwelling | CT 5397/914 | 4987 |
| 104 Partridge Street GLENELG SOUTH | Dwelling | CT 5366/196 | 4988 |
| 106 Partridge Street GLENELG SOUTH | Dwelling | CT 5476/870 | 4989 |
| 110 Partridge Street GLENELG SOUTH | Dwelling | CT 5110/185 | 4990 |
| 112-112A Partridge Street GLENELG SOUTH | Dwelling | CT 5351/191 | 4991 |
| 35 Penzance Street GLENELG SOUTH | Dwelling | CT 5265/504 | 4929 |
| 37 Penzance Street GLENELG SOUTH | Dwelling | CT 5905/106 | 4930 |
| 38 Penzance Street GLENELG SOUTH | Dwelling | CT 5251/158 | 4944 |
| 39 Penzance Street GLENELG SOUTH | Dwelling | CT 5111/111 | 4931 |

| Property Address | Description of Contributory item | Certificate of Title | Heritage NR |
|-------------------------------------|----------------------------------|----------------------|-------------|
| 40 Penzance Street GLENELG SOUTH | Dwelling | CT 5155/636 | 4945 |
| 41 Penzance Street GLENELG SOUTH | Dwelling | CT 5130/424 | 4932 |
| 43 Penzance Street GLENELG SOUTH | Dwelling | CT 5458/135 | 4933 |
| 44 Penzance Street GLENELG SOUTH | Dwelling | CT 5155/635 | 4946 |
| 45 Penzance Street GLENELG SOUTH | Dwelling | CT 5452/592 | 4934 |
| 47 Penzance Street GLENELG SOUTH | Dwelling | CT 5478/999 | 4935 |
| 48 Penzance Street GLENELG SOUTH | Dwelling | CT 5604/413 | 4947 |
| 1/ 49 Penzance Street GLENELG SOUTH | Dwelling | CT 5005/448 | 26429 |
| 2/ 49 Penzance Street GLENELG SOUTH | Dwelling | CT 5005/449 | 26428 |
| 51 Penzance Street GLENELG SOUTH | Dwelling | CT 5452/586 | 4937 |
| 53 Penzance Street GLENELG SOUTH | Dwelling | CT 5180/33 | 4938 |
| 54 Penzance Street GLENELG SOUTH | Dwelling | CT 5095/824 | 4948 |
| 55 Penzance Street GLENELG SOUTH | Dwelling | CT 5399/642 | 4939 |
| 56 Penzance Street GLENELG SOUTH | Dwelling | CT 5221/476 | 4949 |
| 57 Penzance Street GLENELG SOUTH | Dwelling | CT 5359/32 | 4940 |
| 58 Penzance Street GLENELG SOUTH | Dwelling | CT 5519/926 | 4950 |
| 59 Penzance Street GLENELG SOUTH | Dwelling | CT 5476/858 | 4941 |
| 60 Penzance Street GLENELG SOUTH | Dwelling | CT 5226/877 | 4951 |
| 61 Penzance Street GLENELG SOUTH | Dwelling | CT 5085/716 | 4942 |
| 62 Penzance Street GLENELG SOUTH | Dwelling | CT 5228/320 | 4952 |
| 63 Penzance Street GLENELG SOUTH | Dwelling | CT 5100/340 | 4943 |
| 64 Penzance Street GLENELG SOUTH | Dwelling | CT 5359/586 | 4953 |
| 66 Penzance Street GLENELG SOUTH | Dwelling | CT 5074/122 | 4954 |
| 1/ 73 Penzance Street GLENELG SOUTH | Dwelling | CT 5012/498 | 26430 |
| 2/ 73 Penzance Street GLENELG SOUTH | Dwelling | CT 5012/499 | 26431 |
| 74 Penzance Street GLENELG SOUTH | Dwelling | CT 5312/181 | 4970 |
| 75 Penzance Street GLENELG SOUTH | Dwelling | CT 5480/431 | 4981 |
| 76-78 Penzance Street GLENELG SOUTH | Dwelling | CT 5413/927 | 4971 |
| 79 Penzance Street GLENELG SOUTH | Dwelling | CT 5477/116 | 4982 |
| 82 Penzance Street GLENELG SOUTH | Dwelling | CT 5473/15 | 4972 |
| 84 Penzance Street GLENELG SOUTH | Dwelling | CT 5473/568 | 4973 |
| 85 Penzance Street GLENELG SOUTH | Dwelling | CT 5473/595 | 4983 |

| Property Address | Description of Contributory item | Certificate of Title | Heritage NR |
|----------------------------------|----------------------------------|----------------------|-------------|
| 86 Penzance Street GLENELG SOUTH | Dwelling | CT 5242/10 | 4974 |
| 87 Penzance Street GLENELG SOUTH | Dwelling | CT 5075/8 | 4984 |
| 90 Penzance Street GLENELG SOUTH | Dwelling | CT 5069/173 | 4975 |
| 6 Pier Street GLENELG SOUTH | Dwelling | CT 5207/615 | 4798 |
| 12 Pier Street GLENELG SOUTH | Dwelling | CT 5460/579 | 4799 |
| 22 Pier Street GLENELG SOUTH | Dwelling | CT 5110/805 | 4863 |
| 36 Pier Street GLENELG SOUTH | Dwelling | CT 5233/820 | 4864 |
| 40 Pier Street GLENELG SOUTH | Dwelling | CT 5238/922 | 4886 |
| 44 Pier Street GLENELG SOUTH | Dwelling | CT 5438/905 | 4887 |
| 48 Pier Street GLENELG SOUTH | Dwelling | CT 5248/30 | 4889 |
| 3 Ramsgate Street GLENELG SOUTH | Dwelling | CT 5112/52 | 4845 |
| 5 Ramsgate Street GLENELG SOUTH | Dwelling | CT 5285/824 | 4846 |
| 6 Ramsgate Street GLENELG SOUTH | Dwelling | CT 5077/448 | 4855 |
| 7 Ramsgate Street GLENELG SOUTH | Dwelling | CT 5405/420 | 4847 |
| 8 Ramsgate Street GLENELG SOUTH | Dwelling | CT 5111/904 | 4856 |
| 9 Ramsgate Street GLENELG SOUTH | Dwelling | CT 5592/489 | 4848 |
| 10 Ramsgate Street GLENELG SOUTH | Dwelling | CT 5128/148 | 4857 |
| 11 Ramsgate Street GLENELG SOUTH | Dwelling | CT 5808/866 | 4849 |
| 12 Ramsgate Street GLENELG SOUTH | Dwelling | CT 5072/676 | 4858 |
| 13 Ramsgate Street GLENELG SOUTH | Dwelling | CT 5242/418 | 4850 |
| 14 Ramsgate Street GLENELG SOUTH | Dwelling | CT 5447/404 | 4859 |
| 17 Ramsgate Street GLENELG SOUTH | Dwelling | CT 5447/79 | 4851 |
| 18 Ramsgate Street GLENELG SOUTH | Dwelling | CT 5450/689 | 4860 |
| 19 Ramsgate Street GLENELG SOUTH | Dwelling | CT 5450/676 | 4852 |
| 20 Ramsgate Street GLENELG SOUTH | Dwelling | CT 5088/102 | 4861 |
| 21 Ramsgate Street GLENELG SOUTH | Dwelling | CT 5289/953 | 4853 |
| 22 Ramsgate Street GLENELG SOUTH | Dwelling | CT 5458/171 | 4862 |
| 23 Ramsgate Street GLENELG SOUTH | Dwelling | CT 5331/728 | 4854 |
| 25 Ramsgate Street GLENELG SOUTH | Dwelling | CT 6076/739 | 5048 |
| 26 Ramsgate Street GLENELG SOUTH | Dwelling | CT 5331/503 | 5038 |
| 27 Ramsgate Street GLENELG SOUTH | Dwelling | CT 5473/332 | 5047 |
| 28 Ramsgate Street GLENELG SOUTH | Dwelling | CT 5086/405 | 5039 |

| Property Address | Description of Contributory item | Certificate of Title | Heritage NR |
|-------------------------------------|----------------------------------|----------------------|-------------|
| 30 Ramsgate Street GLENELG SOUTH | Dwelling | CT 5078/325 | 5040 |
| 34 Ramsgate Street GLENELG SOUTH | Dwelling | CT 5478/581 | 5041 |
| 1/ 36 Ramsgate Street GLENELG SOUTH | Dwelling | CT 5024/873 | 26433 |
| 2/ 36 Ramsgate Street GLENELG SOUTH | Dwelling | CT 5024/874 | 26432 |
| 40 Ramsgate Street GLENELG SOUTH | Dwelling | CT 5069/233 | 5043 |
| 42 Ramsgate Street GLENELG SOUTH | Dwelling | CT 5005/800 | 26435 |
| 42A Ramsgate Street GLENELG SOUTH | Dwelling | CT 5005/801 | 26434 |
| 44 Ramsgate Street GLENELG SOUTH | Dwelling | CT 5273/229 | 5045 |
| 1 Robert Street GLENELG SOUTH | Dwelling | CT 5487/439 | 4803 |
| 4 Robert Street GLENELG SOUTH | Dwelling | CT 5898/682 | 4804 |
| 6 Robert Street GLENELG SOUTH | Dwelling | CT 5459/72 | 4805 |
| 27 South Esplanade GLENELG SOUTH | Dwelling | CT 5958/428 | 4795 |
| 28 South Esplanade GLENELG SOUTH | Dwelling | CT 5958/429 | 4796 |
| 29 South Esplanade GLENELG SOUTH | Dwelling | CT 5356/897 | 4797 |
| 36 South Esplanade GLENELG SOUTH | Dwelling | CT 5457/142 | 5078 |
| 1 Union Street GLENELG SOUTH | Dwelling | CT 5330/209 | 5014 |
| 4 Union Street GLENELG SOUTH | Dwelling | CT 5272/859 | 5013 |
| 1/ 6 Union Street GLENELG SOUTH | Dwelling | CT 5015/955 | 26445 |
| 2/ 8 Union Street GLENELG SOUTH | Dwelling | CT 5015/956 | 26446 |
| 2 Victoria Street GLENELG SOUTH | Dwelling | CT 5450/680 | 4839 |
| 3 Victoria Street GLENELG SOUTH | Dwelling | CT 5450/832 | 4842 |
| 4 Victoria Street GLENELG SOUTH | Dwelling | CT 5449/463 | 4840 |
| 5 Victoria Street GLENELG SOUTH | Dwelling | CT 5430/625 | 4843 |
| 6 Victoria Street GLENELG SOUTH | Dwelling | CT 5808/847 | 4841 |
| 7 Victoria Street GLENELG SOUTH | Dwelling | CT 5118/60 | 4844 |

Table HoB/4 - Local Heritage Places

| Property Address | Description and/or Extent of Listed Place | Lot No. or Part Sec | Plan No. | Certificate of Title | Section 23(4) Criteria | Heritage NR |
|-------------------------------|---|---------------------|--------------------|----------------------|------------------------|-------------|
| Alfreda Street BRIGHTON | Row of Eucalypts; Mature trees | Sec 239 | H105500 | | a e f | 4407 |
| 2 Athelney Avenue BRIGHTON | House (Athelney); External form, materials and detail of the earliest sections of the house | A31 | F145859 | CT 5806/820 | a d e | 4423 |
| 411 Brighton Road BRIGHTON | Institute Library; External form, materials and detailing of the original 1929 building | A22 | F145950 | CT 5826/627 | a c e | 4436 |
| 442 Brighton Road BRIGHTON | Brighton Public School and Residence; External form, materials and original detailing of the school building and the Headmaster's residence | A72 | F145200 | CT 5704/936 | a c d f | 4437 |
| 443 Brighton Road BRIGHTON | Uniting Church; External form, materials and original details of the Church | A62 | F145890 | CT 5819/729 | a c d f | 4438 |
| 2/ 444 Brighton Road BRIGHTON | Anglican Church Complex; External form, detail and original materials of French Hall and the earliest rear sections of St Jude's Church | A90 | F15105 | CT 5784/949 | a c d f | 4439 |
| 448 Brighton Road BRIGHTON | Three Moreton Bay Fig Trees; The three trees in their entirety and an appropriate curtilage | A92 | F15105 | CT 5463/944 | a e f | 4581 |
| 507 Brighton Road BRIGHTON | Former Police Station; External form, materials and original detailing | A102 | D32984 | CT 5055/343 | a c | 4440 |
| 20-20a Cedar Avenue BRIGHTON | Attached Shops and Residence; External form, materials and original detailing | A73 | F20754 | CT 5399/221 | a c d | 4451 |
| 40 Cedar Avenue BRIGHTON | House; Overall external form, materials and original detailing of the house | A115 | F41812 | CT 5865/124 | a d e | 4452 |
| 1 Commercial Road BRIGHTON | Windsor Theatre and Freemasons Lodge; Overall external form of the structure | A93 | F145821 | CT 5928/133 | a c f | 4455 |
| Esplanade BRIGHTON | War Memorial & Drinking Fountain; The overall form of the arch and attached tablets and decoration and the entire form of the fountain | 239 240 | H105500 H105500 | | a e f | 4463 |

| Property Address | Description and/or Extent of Listed Place | Lot No. or Part Sec | Plan No. | Certificate of Title | Section 23(4) Criteria | Heritage NR |
|-------------------------------|---|---------------------|------------------|----------------------------|------------------------|-------------|
| 136 Esplanade BRIGHTON | Shop and Flats (Pier Building); External form, material and all original details which remain | A100 | D79393 | CT 6065/742 | a b c f | 4462 |
| 154 Esplanade BRIGHTON | House; Overall external form, materials and original detailing of the house | A23 | D42422 | CT 5373/35 | a d e | 4464 |
| 156 Esplanade BRIGHTON | House; External form, materials and original detailing of the 1900's house | A11 | F145739 | CT 5709/874 | a d | 4465 |
| 158 Esplanade BRIGHTON | House; External form, materials and original detailing of the 1918 structure | A12 | F108512 | CT 5197/598 | a d | 4466 |
| 161 Esplanade BRIGHTON | House; Overall form and original materials and detailing of the circa 1913 house | A81 | F23486 | CT 5629/779 | a d e | 4467 |
| 1-5 Hartley Road BRIGHTON | Telephone Exchange; Overall form and detailing of the Telephone Exchange | A184 | F20754 | CT 5395/876 | a c | 4479 |
| 14 Jetty Road BRIGHTON | Former Residence (Ringwood); Overall external form, original materials and details of the 1924 house | A33 | D18573 | CT 5775/261 | a d e | 4488 |
| 47 Jetty Road BRIGHTON | Shop and Attached Residence; Overall external form and materials and original detailing of the shop and house | A20 | D2061 | CT 5216/595 | a c d | 4489 |
| 49 Jetty Road BRIGHTON | Butcher's Shop; Overall form of the single storey section of the shop | A19 A18 | D2061 D2061 | CT 5137/342 CT 5137/343 | a c d | 4490 |
| 67-69 Jetty Road BRIGHTON | Two Storeyed Shops; Overall external form and detailing | A10 | D2061 | CT 5211/69 | a c d | 4491 |
| 71-73a Jetty Road BRIGHTON | Three Attached Shops; Any original elements which remain from the 1920's | A80 A81 | D71536 D71536 | CT 5969/247 CT 5969/248 | a c | 4492 |
| 11-17 Keelara Street BRIGHTON | Brighton Bowling Club Memorial Gates; The Memorial Gates including their rendered finish | A4 | F128504 | CT 5260/795 | a c | 4589 |
| 15 Margate Street BRIGHTON | House; Overall external form, original materials and detailing of the circa 1889 | A63 | F145591 | CT 5305/935 | a b d | 4505 |
| 21 Old Beach Road BRIGHTON | House; Overall external form and original materials and detailing of the 1840's cottage | A151 | F20754 | CT 5396/852 | a d e | 4527 |

| Property Address | Description and/or Extent of Listed Place | Lot No. or Part Sec | Plan No. | Certificate of Title | Section 23(4) Criteria | Heritage NR |
|---|--|---------------------|--------------------|----------------------------|------------------------|-------------|
| 42 Sturt Road BRIGHTON | House (Wiluna); Overall external form, original materials and detailing | A1 | F11745 | CT 5495/342 | a d e | 4548 |
| 34 The Crescent BRIGHTON | House; Overall external form, original materials and detailing of the 1914 house | A15 | D2129 | CT 6043/398 | a d e | 4559 |
| 44 The Crescent BRIGHTON | Place of Worship - Baptist Church; Original external form, terracotta tile roof, pointed head windows and glazing, face brick and stucco walling of 1918 building and 1927 additions same to rear. Exclusions – 1924 Church hall to rear, 1957 hall to north, all other later structures linking buildings, dating from 1950 onwards | A92 A44 | F145820 F145872 | CT 5367/524 CT 5791/801 | a c | 26246 |
| 12 Yester Avenue BRIGHTON | House (Dunluce); Overall external form and original materials and details | A10 | F102062 | CT 5118/670 | a d e | 4571 |
| 12-16 King George Avenue BRIGHTON NORTH | Frank Hayward House, Minda Home; Overall external form, original materials and detailing which remain of the 1860's residence | 102 S235 | H105500 | CT 5315/870 | a c d e | 20712 |
| 10 Augusta Street GLENELG | St Peters Rectory; external form, original materials and detailing | A102 | D14268 | CT 5668/672 | a c d e | 4424 |
| 33 Brighton Road GLENELG | Telephone Exchange; external form, materials and detailing of the original 1925 section | A100 | D12261 | CT 5544/974 | a c | 4428 |
| 24 Byron Street GLENELG | Dwelling; external form, materials and detailing | A63 | F5422 | CT 5305/79 | a d | 4449 |
| 26-28 Byron Street GLENELG | Dwelling (Rest Home); external form, materials and detailing | A61 A62 | F5422 F5422 | CT 5178/731 CT 5178/731 | a d e | 4450 |
| 2A College Street GLENELG | Coach House; external form, materials and detailing of the former Coach House | F23 | C25026 | CT 6052/714 | a d e | 4453 |

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|--|--|---------------------|----------------|----------------------------|------------------------|-------------|
| 3 College Street GLENELG | Semi-detached dwelling; Original two-storey dwelling under hipped roof, timber verandah balcony with cast iron filigree decoration, stucco faced walls and decoration to eaves and window openings, masonry front fence and cast iron fencing and gates. Exclusions: later side and rear addition, where not under the main hipped roof. | A2 | D40935 | CT 5231/350 | a d | 26231 |
| 5 College Street GLENELG | Semi-detached dwelling; Original two-storey dwelling under hipped roof, timber verandah balcony with cast iron filigree decoration, stucco faced walls and decoration to eaves and window openings, masonry front fence and cast iron fencing and gates. Exclusions: later side and rear addition, where not under the main hipped roof. | A1 | D40935 | CT 5231/349 | a d | 26232 |
| Colley Reserve GLENELG | Colley Reserve; the early elements which remain | Q900 | F48304 | CT 5972/927 | a b c e | 4583 |
| 2 Durham Street GLENELG | Shops (Former Residences); external form, materials and original detailing | A174 A173 | F6526 F6526 | CT 5079/219 CT 5085/911 | a d e | 4460 |
| 17 Giles Avenue GLENELG | Dwelling (Former Stables); The external form, materials and detailing of the former stables to the Bromley' residence. | A2 | D46584 | CT 5824/592 | a d | 26226 |
| GLENELG Jetty Moseley Square GLENELG | Glenelg Jetty ; the full length of the Jetty structure and its approach | | H105500 | CT 5733/344 | a b c e f | 4592 |
| 2/ 14 Gordon Street GLENELG | Former Dwelling; external form and original materials and detailing | | S5406 | CT 5017/31 | a d | 4585 |
| 18 Gordon Street GLENELG | Semi-detached dwelling; The external form and original materials and detailing of the pair of semi-detached dwellings. | A11 | D51722 | CT 5641/959 | a d | 26236 |
| 20 Gordon Street GLENELG | Semi-detached dwelling; The external form and original materials and detailing of the pair of semi-detached dwellings. | A10 | D51722 | CT 5641/958 | a d | 26237 |

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| 22 Gordon Street GLENELG | Former Rechabite Meeting Hall; the rendered frontage to the Hall is the most significant part of the structure | A6 | D966 | CT 5440/428 | a d | 4475 |
| 26 Gordon Street GLENELG | Former Fire Station; original external elements which remain from the Fire Station | A43 | F6529 | CT 5137/121 | a d f | 4476 |
| 28 Gordon Street GLENELG | Garage; the frontages to Gordon Street and Augusta Street and the overall enclosing corrugated iron roof form | A97 | F6530 | CT 5480/294 | a c d | 4477 |
| 36 Gordon Street GLENELG | Dwelling; overall form, detailing and materials | A94 | F6530 | CT 5790/789 | a d | 4478 |
| 1/ 16 Gordon Street GLENELG | Dwelling; external form and original materials and detailing | | S5406 | CT 5017/30 | a d | 4586 |
| 9 High Street GLENELG | MacDonnell Lodge Hall; external form and materials | A229 A216 A215 A214 | F6538 F6538 F6538 F6538 | CT 5437/684 CT 5437/685 CT 5437/687 CT 5437/688 | a c e | 4480 |
| 13-15 High Street GLENELG | Our Lady of Victories Church; external form, materials and detailing of the original church | Q170 | F217003 | CT 5671/950 | a c d f | 4587 |
| 17 High Street GLENELG | St Dominic's Community Centre; external form, materials and detailing of the original c.1900 building | A219 | F6538 | CT 5157/808 | a c | 4481 |
| 19 High Street GLENELG | St Mary's Hall; External form, materials and detailing of St Mary's Hall | Q171 | F217003 | CT 5671/950 | a c d | 4482 |
| 28 High Street GLENELG | Dwelling; external form, materials and detailing of the original front section of the house | A119 | F7181 | CT 5171/500 | a d | 4483 |
| 25 Jetty Road GLENELG | Shops; Those elements of the building which are retained from the 1870's | A170 | F6526 | CT 5434/654 | a d e | 4493 |
| 27 27A 29 31 & 31A Jetty Road GLENELG | Shop; those elements of the building which are retained from the 1870's | A171 | F6526 | CT 5325/632 | a d e | 4494 |
| 33 33a Flats 1 & 2/33 35 Jetty Road GLENELG | Shops; those elements of the building which are retained from the 1870's | A172 | F6526 | CT 5452/153 | a d e | 4495 |
| 42 Jetty Road GLENELG | Shop; external form of the building and the early detailing | A200 | F6538 | CT 5183/254 | a c d | 4496 |

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| 79 Jetty Road GLENELG | Office/Shops; original external form and detailing and materials which remain from the 1922 structure | A2 | F6529 | CT 5199/726 | a d | 4497 |
| 92 & 92A Jetty Road GLENELG | Uniting Church & Hall; external form, materials and detailing | A60 A61 | F5539 F5539 | CT 5268/542 CT 5268/542 | a c d e f | 4535 |
| 97 Jetty Road GLENELG | Office/Shops; overall original external form of building and original detailing | A36 | F6529 | CT 5346/500 | a d | 4499 |
| 118 Jetty Road GLENELG | Office/Shops; overall original external form of building and original detailing | A81 | F6535 | CT 5368/922 | a d | 4500 |
| 4 Kent Street GLENELG | Dwelling; external form and detailing of the building | A293 | F7180 | CT 5856/297 | a d | 4502 |
| 5 Maturin Road GLENELG | Murray Mudge House; the exterior elements and details which remain from the original design of the house | A146 | F7181 | CT 5353/936 | a d | 4591 |
| 18 Maturin Road GLENELG | Dwelling; The external form, materials and detailing of the 1927 dwelling. | A1 | D46584 | CT 5413/191 | a d | 4508 |
| 1-7 Moseley Street GLENELG | Alexandra Terrace; external form, materials and detailing of the original 1880's section | A45 A44 | F6537 F6537 | CT 5162/245 CT 5162/247 | a d e f | 4509 |
| 1-5/ 15 Moseley Street & 15 GLENELG | Art Gallery; external form, materials and detailing of the building itself | A152 | F6859 | CT 5082/173 | a d | 4510 |
| 25 Moseley Street GLENELG | Trinity Church; external form, face brick and terra cotta tiled roof of the 1958 church | A147 A148 | F6859 F6859 | CT 5831/810 CT 5831/810 | a c d | 4511 |
| 1/ 33 Moseley Street GLENELG | Dwelling; external form and original materials of the house and fence | | S4955 S4955 S4955 | CT 5025/95 CT 5025/96 CT 5025/97 | a d | 4512 |
| 2/ 33 Moseley Street GLENELG | Dwelling; external form and original materials of the house and fence | | S4955 S4955 S4955 | CT 5025/95 CT 5025/96 CT 5025/97 | a d | 4513 |
| 37 Moseley Street GLENELG | Dwelling; external form and original detailing of the house | A291 | F7180 | CT 5639/373 | a d | 4514 |
| 39 Moseley Street GLENELG | Dwelling; external form and original detailing | A290 | F7180 | CT 5282/835 | a d | 4515 |
| 42 Moseley Street GLENELG | Dwelling; external form, materials, and original details | A236 | F7182 | CT 5496/492 | a d | 4516 |

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| 51 Moseley Street GLENELG | Dwelling; external form, materials and details of original dwelling | A268 | F7180 | CT 5493/509 | a d | 4517 |
| 29 Partridge Street GLENELG | Dwelling; external form, original materials and detailing | A124 A125 | F7181 F7181 | CT 5071/225 CT 5071/226 | a d | 4528 |
| 37-39 Partridge Street GLENELG | Woodlands Church of England Girls School-St Margaret of Scotland Church; external form of the house as originally constructed-external form, materials and detailing of the chapel | A901 | D52314 | CT 5098/828 | a c d | 4530 |
| 38 Partridge Street GLENELG | Partridge House; external form, detailing and materials of the house as constructed in 1899 | A250 | F7381 | CT 5798/196 | a c d f | 4529 |
| 50 Partridge Street GLENELG | Dwelling; external form of building and any elements which remain from 1874 and 1895 period | A301 | F7589 | CT 5439/745 | a d | 4531 |
| 9-9a Pier Street GLENELG | Dwelling; original form, materials and detailing | A266 | F7180 | CT 5663/11 | a d | 4537 |
| 22-23 South Esplanade GLENELG | Seawall Apartments; original external form and detailing of the dwellings | A254 A255 A256 | F7180 F7180 F7180 | CT 5447/494 CT 5447/494 CT 5447/494 | a d | 4546 |
| 14 St Johns Row GLENELG | Dwelling; overall form of the two storey dwellings including their verandah form | A134 | F6859 | CT 5660/374 | a d e | 4540 |
| 15 St Johns Row GLENELG | Dwelling; overall form of the two storey dwellings including their verandah form | A135 | F6859 | CT 5095/629 | a d e | 4541 |
| 16 St Johns Row GLENELG | Dwelling; overall form of the two storey dwellings including their verandah form | A136 | F6859 | CT 5103/579 | a d e | 4542 |
| 17-17A St Johns Row GLENELG | Dwellings; overall form of the two storey dwellings including their verandah form | A137 | F6859 | CT 5186/306 | a d e | 4543 |
| 18 St Johns Row GLENELG | Dwelling; external form, materials and original detailing | A138 | F6859 | CT 5211/888 | a d | 4544 |
| 17 Sussex Street GLENELG | Rothsay; external form, materials and detailing | A3 | F6274 | CT 5504/370 | a d e | 4549 |
| 22 Sussex Street GLENELG | Terrace Houses; external form, materials and original detailing of the dwellings, particularly the elevations | A200 | F6526 | CT 5167/209 | a d | 4550 |

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| 24 Sussex Street GLENELG | Terrace Houses; external form, materials and original detailing of the dwellings, particularly the elevations | A201 | F6526 | CT 5431/461 | a d | 4551 |
| 26 Sussex Street GLENELG | Terrace Houses; external form, materials and original detailing of the dwellings, particularly the elevations | A202 | F6526 | CT 5093/157 | a d | 4552 |
| 28 Sussex Street GLENELG | Terrace Houses; external form, materials and original detailing of the dwellings, particularly the elevations | A203 | F6526 | CT 5151/734 | a d | 4553 |
| 1-4/ 2 Torrens Square GLENELG | Two storey face brick building; with hipped Marseilles tiled roof, masonry curved external stairs, stucco balcony balustrade & columns, cantilevered masonry balcony, timber windows and doors, brick chimneys. Front fence of brick construction. Exclusions: timber staircases, garages, external plumbing and gutters. | A205 | F6526 | CT 5082/174 | a d | 26247 |
| 37 Vincent Street GLENELG | Good Neighbour Garden Reserve; The full extent of the Garden Reserve area | A4 | F146232 | CT 5709/10 | a e | 4603 |
| Brighton Road GLENELG EAST | Glenelg Oval; overall area of the oval and its associated sporting facilities | A100 | D46662 | CT 5869/949 | a b c d e f | 4578 |
| Brighton Road GLENELG EAST | Glenelg Primary School; external form, materials and detailing of original headmaster's residence and 1929 infant school | A99 | D46662 | CT 5402/627 | a c f | 4430 |
| Adelphi Crescent GLENELG NORTH | Pumping Station; external form, materials and detailing | A147 | D3568 | CT 825/109 | a c d | 4406 |
| 1-3 Alison Street GLENELG NORTH | Railway Cottages; external form, materials and detailing | A112 | F1884 | CT 5350/985 | a c d | 4408 |
| 1A Alison Street GLENELG NORTH | Railway Cottages; external form, materials and detailing | A112 | F1884 | CT 5350/985 | a c d | 4409 |
| 1/ 9 Alison Street GLENELG NORTH | Railway Cottages; external form, materials and detailing | | S5683 | CT 5027/686 | a c d | 4410 |
| 2/ 9 Alison Street GLENELG NORTH | Railway Cottages; external form, materials and detailing | | S5683 | CT 5027/685 | a c d | 4411 |
| 13 15 & 17 Alison Street GLENELG NORTH | Railway Cottages; external form, materials and detailing | A109 | F1884 | CT 5514/166 | a c d | 4412 |

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| 44 Alison Street GLENELG NORTH | Dwelling; external form | A3 | D965 | CT 5477/606 | a d | 4413 |
| 77 Alison Street GLENELG NORTH | Grayleigh; external form, materials and detailing | A4 | D28731 | CT 5314/958 | a d e | 4414 |
| 585 Anzac Highway GLENELG NORTH | Attached Residence; external form, materials and detailing | A78 | F1883 | CT 5498/438 | a d | 4416 |
| 587 Anzac Highway GLENELG NORTH | Attached Residence; external form, materials and detailing | A79 | F1883 | CT 5075/305 | a d | 4417 |
| 589 Anzac Highway GLENELG NORTH | Attached Residence; external form, materials and detailing | A80 | F1883 | CT 5430/675 | a d | 4418 |
| 591 Anzac Highway GLENELG NORTH | Attached Residence; external form, materials and detailing | A81 | F1883 | CT 5191/407 | a d | 4419 |
| 1-7/ 617 Anzac Highway GLENELG NORTH | Berkshire Court; external form, materials and detailing | A40 | F1437 | CT 6006/560 | a c d e | 4420 |
| 619 Anzac Highway GLENELG NORTH | House (Former Shop & House); external form, materials and detailing | A39 | F1437 | CT 5866/642 | a c d | 4421 |
| 621 Anzac Highway GLENELG NORTH | House (Former Shop & House); external form, materials and detailing | A38 | F1437 | CT 5083/33 | a c d | 4422 |
| 2 Darwin Street GLENELG NORTH | Dwelling; external form of the cottage | A27 | F1885 | CT 6007/645 | a d | 4456 |
| 1 Edison Street GLENELG NORTH | Dwelling; external form, materials and detailing of the original section of the Mission | A700 | D57711 | CT 5867/145 | a d | 5079 |
| 15 Fulton Street GLENELG NORTH | Dwelling; external form, materials and detailing | A4 | F7192 | CT 5577/83 | a d | 4474 |
| Jervois Street GLENELG NORTH | St Leonard's School; external form, materials and detailing of the original section | A136 A137 A138 A139 A140 A141 | D828 D828 D828 D828 D828 D828 | CT 5876/393 CT 5876/393 CT 5876/393 CT 5876/393 CT 5876/393 CT 5876/393 | a c d e | 4487 |
| 26 Mary Street GLENELG NORTH | House; The overall external form of the house, including materials and detailing | A101 | F1437 | CT 5355/401 | a d | 20713 |
| 1 Pasquin Street GLENELG NORTH | Attached Residence; external form, materials and detailing | A77 | F1883 | CT 5391/160 | a d | 4532 |
| 3 Pasquin Street GLENELG NORTH | Attached Residence; external form, materials and detailing | A76 | F1883 | CT 5554/18 | a d | 4533 |
| 1/ 5 Pasquin Street GLENELG NORTH | Attached Residence; external form, materials and detailing | U1 | S5501 | CT 5017/467 | a d | 4594 |

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| 2/ 5 Pasquin Street GLENELG NORTH | Attached Residence; external form, materials and detailing | U2 | S5501 | CT 5017/467 | a d | 4595 |
| Patawalonga Frontage GLENELG NORTH | Patawalonga Reserve; the Reserve, including the area occupied by the water body between Adelphi Terrace and the Patawalonga frontage | S1524 A140 A501 A12 A139 Q1100 Q700 Q1100 Q700 A100 A1001 | H105500 D3568 D52221 D53445 D3568 D55022 D55022 D55022 D55022 D55022 D49600 | CR 5753/918 CT 5494/417 CT 5691/367 CT 5711/332 CT 5740/458 CT 5766/778 CT 5766/778 CT 5798/64 CT 5798/64 CT 5798/65 CT 5935/965 | a c f | 4596 |
| Patawilya Reserve GLENELG NORTH | Patawilya Reserve; the full area of the Reserve as indicated on Certificate of Title | A50 | F1194 | CR 5776/93 | a c f | 4534 |
| Wigley Reserve GLENELG NORTH | Wigley Reserve; the early elements which remain | A1001 | D49600 | CT 5935/965 | a b c e | 4600 |
| 30 Broadway GLENELG SOUTH | Dwelling; external 1880's form, materials and detailing | A20 | F14344 | CT 5096/773 | a d | 4444 |
| 32 Broadway GLENELG SOUTH | Dwelling; external 1880's form, materials and detailing | A21 | F14344 | CT 5458/94 | a d | 4445 |
| 61 Broadway GLENELG SOUTH | Broadway Hotel; external form of the earliest part of the hotel, dating from 1878 | A1335 A1336 A1334 | F15391 F15391 F15391 | CT 5450/688 CT 5450/688 CT 5458/693 | a c d | 4446 |
| 40 Bath Street GLENELG SOUTH | Primitive Methodist Church; original form and detailing of the Chapel | A250 | F12522 | CT 5151/360 | a c d | 4426 |
| 83-89 Brighton Road GLENELG SOUTH | Holdfast Hotel; external form, materials and detailing of original 1881 section | A125 A126 A127 | F15385 F15385 F15385 | CT 5999/506 CT 5999/506 CT 5999/507 | a c d f | 4429 |
| 10 Bristol Street GLENELG SOUTH | Dwelling; external form, materials and detailing of the 1880's | A98 | F14344 | CT 5470/753 | a c d | 4443 |
| 71 Moseley Street GLENELG SOUTH | Shop; external form of building and all early materials and detailing | A150 | D32101 | CT 5415/789 | a c d | 4518 |
| 80 Moseley Street GLENELG SOUTH | Kapara Nursing Home; external materials, detailing and form of the major elevation fronting Moseley Street | A287 A288 A289 A290 A300 A301 A302 A303 | F12522 F12522 F12522 F12522 F12522 F12522 F12522 F12522 | CT 5690/555 CT 5690/555 CT 5690/555 CT 5690/555 CT 5690/555 CT 5690/555 CT 5690/555 CT 5690/555 | a c d e | 4519 |

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|---|---|----------------------------|---|---|------------------------|-------------|
| 1-4/ 88 Moseley Street GLENELG SOUTH | Residential Flat Building - Shandon; Original external form, materials and detailing of the c1930 apartments. | U1 U2 U3 U4 CP | S6681 S6681 S6681 S6681 S6881 | CT 5014/565 CT 5014/566 CT 5014/567 CT 5014/568 CT 5014/569 | a d | 26242 |
| 88 88a & 88b Partridge Street GLENELG SOUTH | Shop; external form of original building | A402 | D27335 | CT 5816/859 | a c | 4593 |
| 16 Ramsgate Street GLENELG SOUTH | Dwelling; external form, materials and details | A1319 | F15391 | CT 5545/251 | a d e | 4538 |
| 15 Robert Street GLENELG SOUTH | Restormel; external form, materials and detailing of the two storey, faced stone section of the house | A93 | F14344 | CT 5458/188 | a d | 4539 |
| South Esplanade GLENELG SOUTH | South Esplanade Foreshore Reserve; the Reserve including the area between the western property boundaries along South Esplanade to the Broadway | Q102 | D76687 | CT 6054/23 | a c f | 4545 |
| 9 Weewanda Street GLENELG SOUTH | Two storey face brick building; with hipped tiled roof, timber wrap-around corner windows, timber lined eaves, fluted brick chimneys, brick balcony balustrades. Front fence of brick construction. Exclusions: External timber staircases, garages, external plumbing and gutters. | A6 | D3041 | CT 5082/171 | a d | 26249 |
| 1-4/ 32 Weewanda Street GLENELG SOUTH | Residential Flat Building - Pennsylvania Apartments; Original external form, materials and detailing of the 1938 apartment block. | U1 U2 U3 U4 | S14301 S14301 S14301 S14301 | CT 5637/147 CT 5637/148 CT 5637/149 CT 5637/150 | a d | 26250 |
| 387 Brighton Road HOVE | House; Overall external form and any original materials and details which remain | A1 | D2214 | CT 5203/6 | a d | 4433 |
| 388 Brighton Road HOVE | Former Town Hall; The earliest section of the Hall | A1 | D36909 | CT 5148/889 | a c d e f | 4434 |
| 389 Brighton Road HOVE | Former Post Office; Overall external form and original materials and details of the former Post Office | A2 | D59465 | CT 5890/971 | a c d | 4435 |
| 410-420 Brighton Road HOVE | Row of Stone Pine Trees on Stopford Road, Hove; All sixteen trees in this row | A13 A14 | F40109 F40109 | CT 5733/512 CT 5750/187 | a f | 4580 |

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| 8 Downing Street HOVE | House; Overall external form, original materials and detailing | A6 | D770 | CT 5189/493 | a d | 4457 |
| 58 Downing Street HOVE | Tower House; Overall external form of the 1880's structure including the attached rear extensions | A31 A32 | D770 D770 | CT 5677/46 CT 5677/46 | a d e | 4458 |
| 70-70a Downing Street HOVE | Cottages (Attached); Overall external form and any original materials and detailing that remains | | S13283 S13283 | CT 5241/848 CT 5241/849 | a d | 4459 |
| 48-50 Dunrobin Road HOVE | Alwyndor House and Garden; Overall external form of the 1900 house | A85 A85 | F146113 F146113 | CT 5479/801 CT 5994/987 | a c e | 4584 |
| 2 Hulbert Street HOVE | Cottages; Overall external form and any original detailing which remains | A99 | F146027 | CT 5465/281 | a d | 4484 |
| 21 Illawarra Avenue HOVE | Cottage; Overall external form of cottage | A100 | D42347 | CT 5273/446 | a d | 4486 |
| 16-18, 4-8 Burnham Road KINGSTON PARK | Kingston Park, Monument, Spring and Norfolk Island Pine Trees; The whole of Kingston Park Reserve | S1540 S1662 S1540 | H105500 H105500 H105500 | CT 5217/360 CT 5217/360 CT 5557/560 | a c e f | 4582 |
| 37 Burnham Road KINGSTON PARK | Dwelling; External form, materials and detailing of the c1962 house. The three bay garage is included in the listing. | A3 | F131748 | CT 5909/729 | d | 26230 |
| Esplanade KINGSTON PARK | Stone Jetty ; The projection of the jetty and any evidence of the shipping facility | Offshore 244 | H105500 | | a d e | 4471 |
| 19 Forrest Avenue KINGSTON PARK | Pritchard House; External form, materials and detailing of the 1990 residential structure should be included in the listing | A60 | F146494 | CT 5812/714 | a d f | 4473 |
| 301 Brighton Road NORTH BRIGHTON | North Brighton Cemetery; The earliest sections of the cemetery from Brighton Road through to the cypress row | A107 | F2997 | CT 5791/500 | a c d f | 4431 |
| 305-311 Brighton Road NORTH BRIGHTON | Brighton High School; The 1952 section of the building | A112 | F2997 | CT 5609/930 | a c d | 4579 |
| 313 Brighton Road NORTH BRIGHTON | House (Former Gate House); The overall external form of all sections of the earliest building | A116 | D44651 | CT 5315/873 | a d e | 4432 |

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| 13 Gladstone Road NORTH BRIGHTON | Single storey residence; Including later additions, masonry walls, plate glass walls with timber louvres and raised monitor roof. Exclusions: roof sheeting, rear garage | A53 | F146081 | CT 5768/94 | d | 26235 |
| Ilfracombe Avenue NORTH BRIGHTON | Avenue of River Red Gum Trees ; All mature River Red Gums along Ilfracombe Avenue | Sec 236 | H105500 | | a e f | 4485 |
| 3 Warwick Court NORTH BRIGHTON | House (Woodhurst); The overall external form of the original 1901 house | A4 | D5642 | CT 5197/668 | a d e | 4562 |
| Barwell Avenue SEACLIFF | Educational Establishment - Seacliff Primary School; The original external form, materials and detailing of the 1960 school building. The later additions and single storey infant school building are not included. | A99 A100 A98 A97 A97 A98 A99 A96 | D2823 D2823 D2823 D2823 F146731 F146731 F146731 F146730 | CT 5273/467 CT 5713/966 CT 5726/467 CT 5773/670 CT 5823/148 CT 5823/148 CT 5823/148 CT 5823/149 | a c d | 26243 |
| Brighton Road SEACLIFF | War Memorial ; The War Memorial and plinth and plaques | Sec 244 | H105500 | | a d e f | 4442 |
| Esplanade SEACLIFF | Concrete Sea Wall and Rotunda Relics ; All elements which remain from the original structure | 242 243 | H105500 H105500 | | a c d e f | 4468 |
| 221 Esplanade SEACLIFF | Seacliff Hotel; The remaining overall external form | A130 A131 A132 | F24004 F24004 F24004 | CT 5062/220 CT 5062/220 CT 5062/220 | a b c f | 4469 |
| 241 Esplanade SEACLIFF | Brighton & Seacliff Yacht Club; External form and original details and materials of the corrugated iron 1920's Yacht Club | S1539 | H105500 | CT 5590/814 | a c e | 4470 |
| 29 Kauri Parade SEACLIFF | Seacliff Presbyterian Church Hall; Overall external form and original materials and detailing | A385 | F37416 | CT 5649/669 | a c d | 4588 |
| 40A Kauri Parade SEACLIFF | Centenary Reserve; The whole of the reserve area and current planting | A514 | F35108 | CT 6016/190 | a e f | 4501 |
| Maitland Terrace SEACLIFF | Zig Zag Pedestrian Ramp; The full length of the Zig Zag Pedestrian Ramp | A1 | D41653 | CR 5330/746 | a c f | 4590 |

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| Property Address | Description and/or Extent of Listed Place | Lot No. or Part Sec | Plan No. | Certificate of Title | Section 23(4) Criteria | Heritage NR |
|---------------------------------|--|---------------------|--------------------------------------|--|------------------------|-------------|
| 23-25 Marine Parade SEACLIFF | House (Former Little Company of Mary Convent); The overall external form, materials and detailing of the original building | A4 A5 | D43412 D43412 | CT 5291/453 CT 5291/453 | a d | 4506 |
| 53-53A Marine Parade SEACLIFF | House; Overall external form, materials and detailing | A255 | F38192 | CT 5667/788 | a e | 4507 |
| 40 Myrtle Road SEACLIFF | House; The overall external form and any original materials and details | A56 | F146790 | CT 5708/399 | a d e | 4520 |
| 2-8 Wheatland Street SEACLIFF | Row Housing; Overall external form, original materials and details | | C21777 C21777 C21777 C21777 | CT 5944/772 CT 5944/773 CT 5944/774 CT 5944/775 | a d | 4563 |
| 5 Wheatland Street SEACLIFF | Uniting Church; Overall external form, original materials and detailing of the 1928 Church | A402 | F37690 | CT 5648/332 | a c d | 4564 |
| 9 Wheatland Street SEACLIFF | Community Centre (Former Police Station); Original external form, materials and detailing of the 1937 police station. | A400 | F37690 | CT 5649/713 | a c d | 26251 |
| 10 Wheatland Street SEACLIFF | Former Shop and Residence; Overall external form and detailing of both the shop and attached residence | A583 | F38015 | CT 5683/885 | a c d | 4565 |
| 33 Wheatland Street SEACLIFF | Public Square and Moreton Bay Fig Trees; The whole of the existing allotment/reserve | A338 | F37421 | CT 5647/255 | a e f | 4566 |
| 43 Wheatland Street SEACLIFF | House; Overall external form and original materials and detailing of the 1896 cottage | | C21265 C21265 C21265 C21265 | CT 5871/765 CT 5904/602 CT 5904/603 CT 5904/604 | a d e | 4567 |
| 44-46 Wheatland Street SEACLIFF | Shops; Overall external form of shops | A68 | F146802 | CT 5355/819 | a c d | 4568 |
| 48 Wheatland Street SEACLIFF | Shop; Overall external form of the shop including original materials and detailing which remain | A58 | F146792 | CT 5327/692 | a c d | 4569 |
| 6b Mann Street SEACLIFF PARK | Canary Island Date Palm; Canary Island Date Palm | A11 | D54635 | CT 5792/604 | a e f | 4503 |
| 18 Mann Street SEACLIFF PARK | House; External form of the earliest part of the structure which formed the stables | A36 | F146670 | CT 5397/867 | a e | 4504 |

| Property Address | Description and/or Extent of Listed Place | Lot No. or Part Sec | Plan No. | Certificate of Title | Section 23(4) Criteria | Heritage NR |
|---|---|---------------------|----------|----------------------|------------------------|-------------|
| 2A Angove Road SOMERTON PARK | River Red Gum; overall form of the tree | A242 | F11090 | CT 5887/139 | a f | 4415 |
| 1/ 11 - 13 Angove Road SOMERTON PARK | Semi-detached dwelling; External form, materials and detailing of the pair of semi-detached dwellings. | U1 | S13480 | CT 5269/976 | a d | 26227 |
| 2/ 11 - 13 Angove Road SOMERTON PARK | Semi-detached dwelling; External form, materials and detailing of the pair of semi-detached dwellings. | U2 | S13480 | CT 5269/977 | a d | 26228 |
| 1a Baker Street SOMERTON PARK | Masonic Memorial Village Complex; Corner Facade of the Alexander Russell Hall located on the north eastern corner of the intersection of Diagonal Road and Baker Street- including the entrance, windows, inscription and square and compass. | Q91 | F214422 | CT 5974/555 | a c | 4425 |
| 4 Bickford Terrace SOMERTON PARK | Dwelling and Fence; External form, materials and detailing of the 1939 house, including the front masonry fence. | A59 | D3915 | CT 5130/840 | a d | 26229 |
| 26 Boundary Road SOMERTON PARK | House; Overall form, original materials and details which remain | A11 | F31683 | CT 5426/820 | a d | 4427 |
| 81 College Road SOMERTON PARK | House; Overall external form, original materials and details of the cottage | A101 | D14490 | CT 5343/362 | a d | 4454 |
| 2 North Street SOMERTON PARK | House; Overall form and original materials and detailing of the house should be retained where these still exist | A25 | D494 | CT 5290/352 | a d | 4521 |
| 4 North Street SOMERTON PARK | House; Overall form and original materials and detailing of the house where these still remain | A24 | D494 | CT 5393/563 | a d | 4522 |
| 6 North Street SOMERTON PARK | House; Overall form and original materials and detailing of the house where these still remain | A23 | D494 | CT 5610/354 | a d | 4523 |
| 8 North Street SOMERTON PARK | House; Overall form and original materials and detailing of the house where these still remain | A22 | D494 | CT 5387/885 | a d | 4524 |

Holdfast Bay Council
Table Section
Table HoB/4 - Local Heritage Places

| Property Address | Description and/or Extent of Listed Place | Lot No. or Part Sec | Plan No. | Certificate of Title | Section 23(4) Criteria | Heritage NR |
|---|---|--|---|---|------------------------|-------------|
| 10 North Street SOMERTON PARK | House; Overall form and original materials and detailing of the house where these still remain | A21 | D494 | CT 5837/785 | a d | 4525 |
| 12 North Street SOMERTON PARK | House; Overall form and original materials and detailing of the house where these still remain | A20 | D494 | CT 5270/444 | a d | 4526 |
| 20-22 Phillipps Street SOMERTON PARK | Apartments; Overall external form and original materials and detailing | U1 U2 U3 U4 U5 U6 U7 U8 CP | S5917 S5917 S5917 S5917 S5917 S5917 S5917 S5917 S5917 | CT 5027/663 CT 5027/664 CT 5027/665 CT 5027/666 CT 5027/667 CT 5027/668 CT 5027/669 CT 5027/670 CT 5027/671 | a d | 4597 |
| 21 Phillipps Street SOMERTON PARK | House; Overall external form and original materials and detailing of the 1880's house | A107 | F11328 | CT 5846/955 | a d e | 4536 |
| 42-48 Scarborough Street SOMERTON PARK | Row of Moreton Bay Fig Trees; Entire row of Moreton Bay Fig trees | A200 A223 A201 A202 A206 | F8981 F8981 F8981 F8981 F8981 | CT 5669/917 CT 5797/69 CT 5841/822 CT 5841/822 CT 5841/822 | a e f | 4602 |
| 3 Stimson Grove SOMERTON PARK | House (Former "Fairfield" Quarters); Overall external form and original materials and detailing | A1 | F14192 | CT 5225/846 | a d | 4547 |
| 12-14 Sullivan Street SOMERTON PARK | Stables; The earliest sections of the corrugated iron structures | A17 A18 | D2989 D2989 | CT 5588/642 CT 5588/642 | a b | 4598 |
| Tarlton Street SOMERTON PARK | Pine Trees (Seaforth Park); overall form of both trees | A25 | D49576 | CT 5524/567 | a e f | 4554 |
| 1-5/ 6 Tarlton Street SOMERTON PARK | Apartment; Overall external form and original materials and details of the 1936 block of flats | U1 U2 U3 U4 U5 CP | S11051 S11051 S11051 S11051 S11051 S11051 | CT 5000/931 CT 5000/932 CT 5000/933 CT 5000/934 CT 5000/935 CT 5000/936 | a d | 4555 |
| 21 Tarlton Street SOMERTON PARK | House; Overall external form and original materials and detailing | A35 | F8993 | CT 5286/734 | a d | 4556 |
| 33 Tarlton Street SOMERTON PARK | Pump House; External form and detailing | A41 | F8993 | CT 6055/386 | a c | 4557 |

| Property Address | Description and/or Extent of Listed Place | Lot No. or Part Sec | Plan No. | Certificate of Title | Section 23(4) Criteria | Heritage NR |
|--------------------------------------|--|--|--|--|------------------------|-------------|
| 1-7/ 55 Tarlton Street SOMERTON PARK | Apartment (West Lynton); Overall external form and original face red brick and metal and timber window | U1 U3 U4 U5 U6 U7 CP U2 | S7884 S7884 S7884 S7884 S7884 S7884 S7884 S7884 | CT 5004/565 CT 5004/567 CT 5004/568 CT 5004/569 CT 5004/570 CT 5004/571 CT 5004/572 CT 5996/240 | a d | 4558 |
| 59 Tarlton Street SOMERTON PARK | Semi-detached dwelling; under main hipped roof, stucco finished masonry front porch and associated architectural elements, glazed brick detailing to walls. Exclusions: rear additions beyond main roof, later roof tiles. | A639 | F10243 | CT 5246/890 | a d | 26244 |
| 61 Tarlton Street SOMERTON PARK | Semi-detached dwelling; under main hipped roof, stucco finished masonry front porch and associated architectural elements, glazed brick detailing to walls. Exclusions: rear additions beyond main roof, later roof tiles. | A639 | F10243 | CT 5246/890 | a d | 26245 |
| 23 Walkers Road SOMERTON PARK | House (Inverurie); Overall external form and original materials and detailing of both sections of the house | A12 | D4400 | CT 5254/810 | a d | 4560 |
| 56 Walkers Road SOMERTON PARK | Dwelling; Original external form, materials and detailing of the 1956 residence. | A260 | F11090 | CT 5272/428 | a d | 26248 |
| 65 Walkers Road SOMERTON PARK | House; Overall external form of the house and the original construction materials | A1 | D43645 | CT 5284/66 | a d | 4561 |
| 75 Whyte Street SOMERTON PARK | House (Somerlea); Overall external form of Somerlea including original materials and details | A650 | F10243 | CT 5604/134 | a d e | 4570 |
| 45a Broadway SOUTH BRIGHTON | Dover Square Gardens; The full extent of Dover Square Gardens | A146 | D642 | CT 5494/542 | a c | 4448 |
| 561 Brighton Road SOUTH BRIGHTON | House; External form of the earliest sections of the house and any original materials and detailing | A555 | D55359 | CT 5819/271 | a e | 4441 |

Holdfast Bay Council
Table Section
Table HoB/4 - Local Heritage Places

| Property Address | Description and/or Extent of Listed Place | Lot No. or Part Sec | Plan No. | Certificate of Title | Section 23(4) Criteria | Heritage NR |
|-------------------------------------|---|---------------------|----------|----------------------|------------------------|-------------|
| Broadway SOUTH BRIGHTON | Olive Grove Relics; All the remnant Olive Trees in Broadway | Sec 203 | H105500 | | a b f | 4447 |
| 41-43 Edwards Street SOUTH BRIGHTON | House; Overall external form of the 1917 house | A43 | D66576 | CT 5937/253 | a d | 4461 |

Table HoB/5 - State Heritage Places

| Property Address | Description and/or Extent of Listed Place | Lot No. or Part Sec | Plan No. | Certificate of Title | Section 16 Criteria | SA Heritage Register ID |
|--------------------------------|---|---------------------|----------------------------|---|---------------------|-------------------------|
| 444 Brighton Road BRIGHTON | St Jude's Cemetery | A200 | D54807 | CT 5784/947 | | 14461 |
| 24 Jetty Road BRIGHTON | Brighton Municipal Offices and Council Chambers | A30 | D18573 | CT 5775/258 | | 14464 |
| Colley Reserve GLENELG | Colley Reserve Rotunda | Q900 | F48304 | CT 5972/927 | | 13614 |
| 90 Jetty Road GLENELG | St Andrew's Uniting (former Congregational) Church Hall [original 1859 church] | A60 | F5539 | CT 5268/542 | e g | 12016 |
| 92 Jetty Road GLENELG | St Andrew's Uniting (former Congregational) Church | A61 | F5539 | CT 5268/542 | e g | 12017 |
| 15 Moseley Square GLENELG | Glenelg Post Office & Dwelling | S1510 | H105500 | CT 5845/683 | | 12013 |
| Moseley Square GLENELG | Pioneers Memorial, Glenelg | B1 | H105500 | CT 6054/30 | | 12002 |
| Moseley Square GLENELG | Glenelg Town Hall (originally the Glenelg Institute) | ROAD RESERVE A106 | D71400 D71400 | N/A CT 6054/34 | | 12003 |
| 62 Moseley Street GLENELG | Dwelling | A1155 | F15392 | CT 5160/107 | | 13584 |
| 5 Olive Street GLENELG | Dwelling (formerly 'Russell Court' and 'The Olives') | A133 | F7181 | CT 5548/479 | | 12033 |
| 38 Partridge Street GLENELG | Townsend Drinking Fountain, located at Partridge House (previously located at Colley Reserve) | A250 | F7381 | CT 5798/196 | | 12037 |
| 11 Pier Street GLENELG | Waterworth Hospital (former Dwelling) | A1 A2 A3 | D68118 D68118 D68118 | CT 5951/834 CT 5951/835 CT 5951/836 | | 13583 |
| 6-7 South Esplanade GLENELG | Dwelling - Seafeld Tower | A105 A106 107 | F6859 F6859 | CT 5383/323 CT 5644/657 | | 12029 |
| 14 South Esplanade GLENELG | Dwelling ('Stormont') | A14 | D55876 | CT 5830/290 | | 12559 |

Holdfast Bay Council
Table Section
Table HoB/5 - State Heritage Places

| Property Address | Description and/or Extent of Listed Place | Lot No. or Part Sec | Plan No. | Certificate of Title | Section 16 Criteria | SA Heritage Register ID |
|--|--|-----------------------|--|---|---------------------|-------------------------|
| 16 South Esplanade GLENELG | Albert Hall (former Dwelling) | U1 U2 U3 UCP | C21885 C21885 C21885 C21855 | CT 5902/285 CT 5902/286 CT 5902/287 CT 5902/288 | | 12558 |
| Torrens Square GLENELG | St Peter's Anglican Church, Glenelg | A204 | F6526 | CT 5695/354 | | 12015 |
| 43 MacFarlane Street GLENELG NORTH | Old Gum Tree Site (Supposed site of the proclamation of the establishment of government in 1836) | A45 | F1426 | CT 5388/43 | | 10547 |
| 34 Broadway GLENELG SOUTH | Dwelling ('Blanche Villa') | A1 A2 A3 A4 | C25398 C25398 C25398 C25398 C25398 | CT 6040/958 CT 6040/959 CT 6040/960 CT 6040/962 CT 6040/961 | | 13579 |
| 32 Esplanade GLENELG SOUTH | Dwelling ('Glenara') | A1001 | D31872 | CT 6000/444 | | 10603 |
| 5 Robert Street GLENELG SOUTH | Dwelling ('Colona') | A110 | F14344 | CT 5443/591 | | 12027 |
| 388 Brighton Road HOVE | Former Brighton Town Hall | A1 | D36909 | CT 5148/889 | a g | 26431 |
| 28 King George Avenue HOVE | Townsend House (former South Australian Institution for the Blind, the Deaf and the Dumb) | A65 | D74085 | CT 5996/440 | | 10546 |
| 48 Kingston Crescent KINGSTON PARK | Kingston House (former Marino Homestead, former Dwelling and Marino Inn) | S1541 | H105500 | CR 5757/455 | | 10617 |
| 12-16 King George Avenue NORTH BRIGHTON | Brighton [earlier Avenue, then Sunbeam] House, Minda Home | A12 | D12174 | CT 5431/200 | | 10630 |
| 12-16 King George Avenue NORTH BRIGHTON | Rogerson Building, Minda Home | A104 | F2997 | CT 1563/3 | | 14473 |
| 12-16 King George Avenue NORTH BRIGHTON | Verco Building, Minda Home | A104 | F2997 | CT 1563/3 | | 14474 |

| Property Address | Description and/or Extent of Listed Place | Lot No. or Part Sec | Plan No. | Certificate of Title | Section 16 Criteria | SA Heritage Register ID |
|--|--|---------------------|----------|----------------------|---------------------|-------------------------|
| 44 Maitland Terrace SEACLIFF | Hardy Family House | A54 | D48485 | CT 5527/304 | | 14465 |
| 195-235 Brighton Road SOMERTON PARK | 'Paringa Hall' (former Dwelling), Sacred Heart College | A209 & 211-215 | F8981 | CT 5960/508 | | 13837 |
| 20 Oaklands Road SOMERTON PARK | Modernist Style Dwelling | A16 | F9180 | CT 5091/841 | | 14467 |

Note: This table is an extract from the South Australian Heritage Register established under Section 13 (1) of the *Heritage Places Act 1993*. In the event of a discrepancy between this extract and the South Australian Heritage Register, the South Australian Heritage Register shall prevail.

Mapping Section

Map Reference Tables

Spatial Extent Maps

Structure Plan Maps

Concept Plan Maps

Map Reference Tables

Index Maps

Index Map Type

[Council Index Map](#)

Zone Maps

| Zone Name | Zone Map Numbers |
|--|--|
| Caravan and Tourist Park Zone | HoB/10 |
| Coastal Conservation Zone | HoB/6, HoB/12 |
| Coastal Open Space Zone | HoB/2, HoB/4, HoB/6, HoB/8, HoB/10, HoB/12 |
| Commercial Zone | HoB/4, HoB/5, HoB/8 |
| Community Zone | HoB/2, HoB/4, HoB/6, HoB/8, HoB/9, HoB/10 |
| District Centre Zone | HoB/4, HoB/8 |
| Glenelg Foreshore and Patawalonga Zone | HoB/2, HoB/4 |
| Light Industry Zone | HoB/6, HoB/7 |
| Local Centre Zone | HoB/2, HoB/4, HoB/6 |
| Mixed Use Zone | HoB/4 |
| Neighbourhood Centre Zone | HoB/6, HoB/7, HoB/8, HoB/11 |
| Open Space Zone | HoB/2, HoB/3, HoB/4, HoB/5, HoB/6, HoB/7, HoB/8, HoB/10, HoB/11 |
| Residential Zone | HoB/2, HoB/3, HoB/4, HoB/5, HoB/6, HoB/7, HoB/8, HoB/9, HoB/10, HoB/11, HoB/12 |
| Residential Character Zone | HoB/2, HoB/4, HoB/5, HoB/8, HoB/10 |
| Residential High Density Zone | HoB/2, HoB/4 |
| Suburban Neighbourhood Zone | HoB/10 |

Historic Conservation Area Maps

| Area Name | shown within Overlay Maps - Heritage |
|-------------------------------------|--|
| Historic (Conservation) Policy Area | Overlay Map HoB/4 - Heritage Overlay Map HoB/5 – Heritage |

Policy Area Maps

| Policy Area Name | Policy Area Map Numbers |
|--|--|
| Recreation Policy Area 1 | HoB/4, HoB/8, HoB/9, HoB/10 |
| Glenelg Policy Area 2 | HoB/4 |
| Central West Policy Area 3 | HoB/4, HoB/6, HoB/8, HoB/10, HoB/12 |
| Institutional Policy Area 4 | HoB/4, HoB/6, HoB/7, HoB/8, HoB/12 |
| Medium Density Policy Area 5 | HoB/2, HoB/4, HoB/5, HoB/6, HoB/8, HoB/9, HoB/10 |
| North West Policy Area 6 | HoB/2 |
| South West Policy Area 7 | HoB/8, HoB/10 |
| Central Glenelg Village Policy Area 8 | HoB/4 |
| Da Costa Park Policy Area 9 | HoB/4, HoB/5 |
| Maturin Road Policy Area 10 | HoB/4 |
| New Glenelg Policy Area 11 | HoB/4 |
| Seacliff Policy Area 12 | HoB/8, HoB/10 |
| Streetscape Character (Brighton and Glenelg East) Policy Area 13 | HoB/4, HoB/5, HoB/8 |
| Streetscape Character (Glenelg and Glenelg North) Policy Area 14 | HoB/2, HoB/4 |
| Urban Glenelg Policy Area 15 | HoB/2, HoB/4 |
| Brighton and Hove Policy Area 16 | HoB/8 |

Precinct Maps

| Precinct Name | Precinct Map Numbers |
|--------------------------|----------------------|
| Precinct 3 Three Storey | HoB/2, HoB/4 |
| Precinct 4 Five Storey | HoB/4 |
| Precinct 5 Twelve Storey | HoB/4 |

Overlay Maps

| Overlay Map - Type | Overlay Map Numbers |
|-------------------------|---|
| Location | HoB/1, HoB/2, HoB/3, HoB/4, HoB/5, HoB/6, HoB/7, HoB/8, HoB/9, HoB/10, HoB/11 |
| Transport | HoB/1, HoB/2, HoB/3, HoB/4, HoB/5, HoB/6, HoB/7, HoB/8, HoB/9, HoB/10, HoB/11 |
| Heritage | HoB/2, HoB/4, HoB/5, HoB/6, HoB/7, HoB/8, HoB/9, HoB/10, HoB/11 |
| Development Constraints | HoB/1, HoB/2, HoB/3, HoB/4, HoB/5, HoB/6, HoB/7, HoB/8, HoB/9, HoB/10, HoB/11 |
| Natural Resources | HoB/2, HoB/3, HoB/4, HoB/5, HoB/6, HoB/7, HoB/8, HoB/10, HoB/11 |
| Noise and Air Emissions | HoB/8, HoB/10 |
| Affordable Housing | HoB/2, HoB/4, HoB/10 |

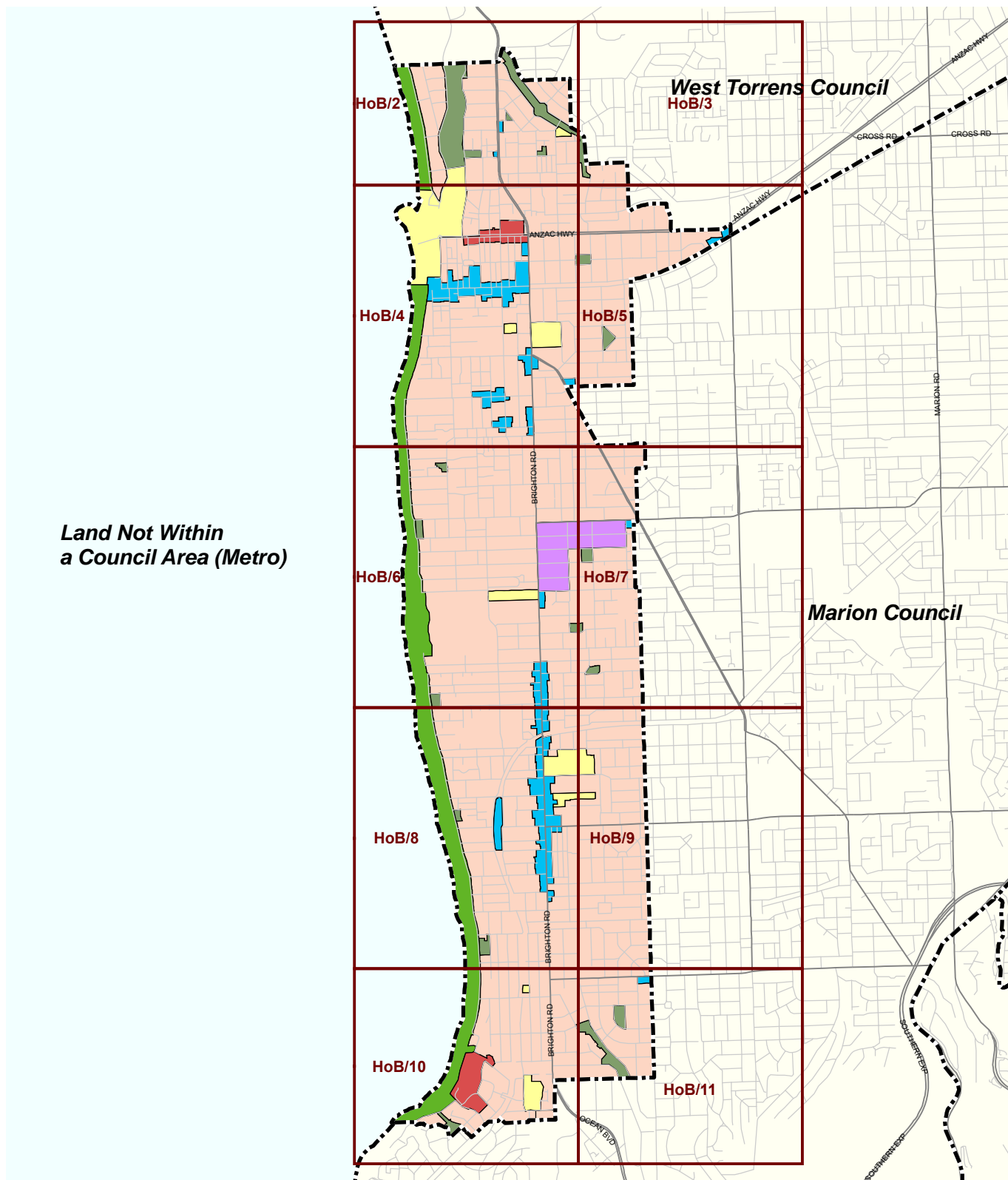
Structure Plan Maps

| Structure Plan Title | Structure Plan Map Numbers |
|-----------------------------------|----------------------------|
| Holdfast Bay | Structure Plan Map HoB/1 |
| Brighton and Hove District Centre | Structure Plan Map HoB/2 |

Concept Plan Maps

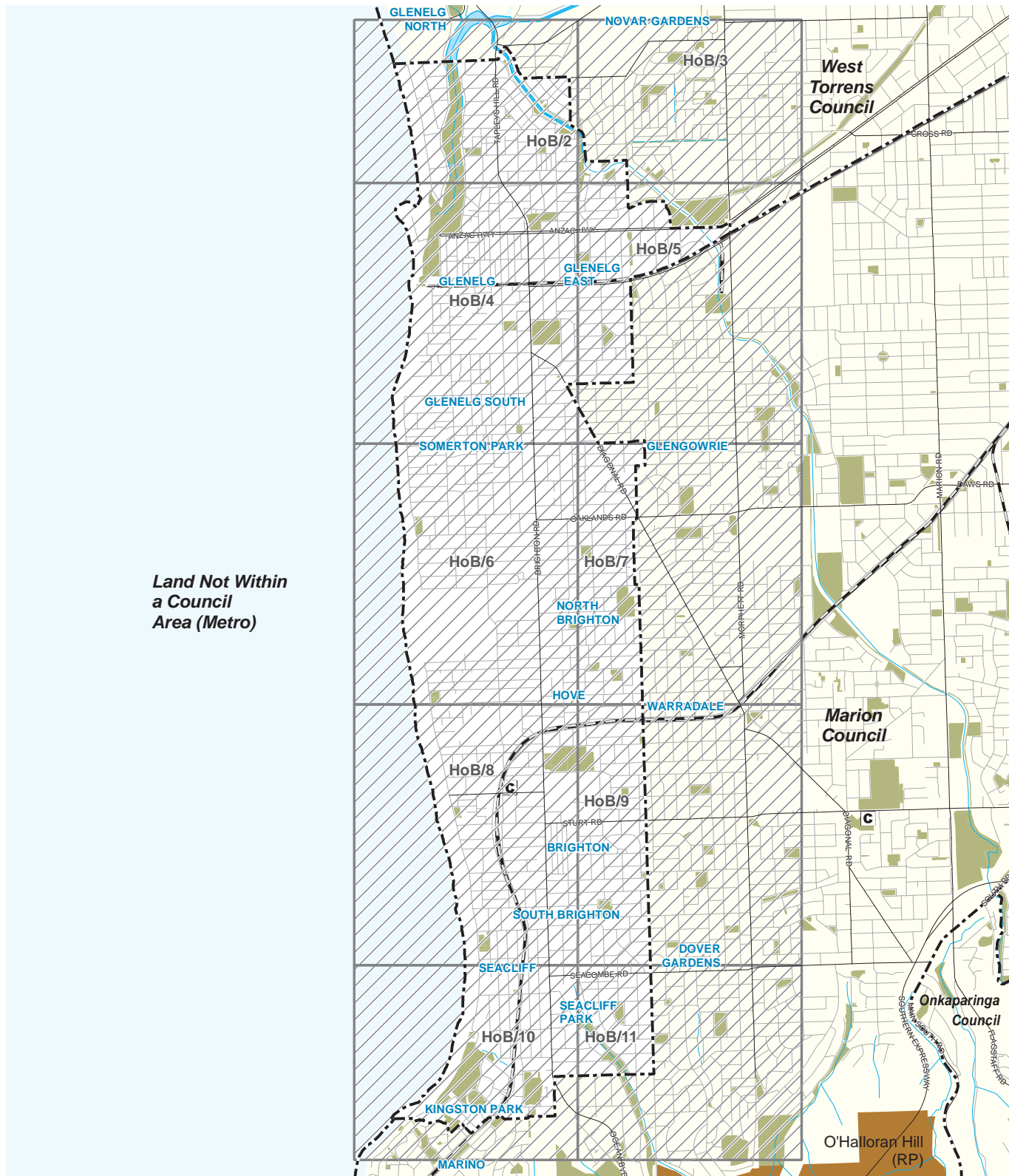
| Concept Plan Title | Concept Plan Map Numbers |
|---|--------------------------|
| Jetty Road and Moseley Square | Concept Plan Map HoB/1 |
| Car Parking Areas | Concept Plan Map HoB/2 |
| Foreshore and Patawalonga | Concept Plan Map HoB/3 |
| Buckle Street | Concept Plan Map HoB/4 |
| Extent – Glenelg Foreshore and Patawalonga Zone | Concept Plan Map HoB/5 |
| Minda Incorporated Brighton Campus | Concept Plan Map HoB/6 |
| Seacliff Park | Concept Plan Map HoB/7 |

Spatial Extent Maps



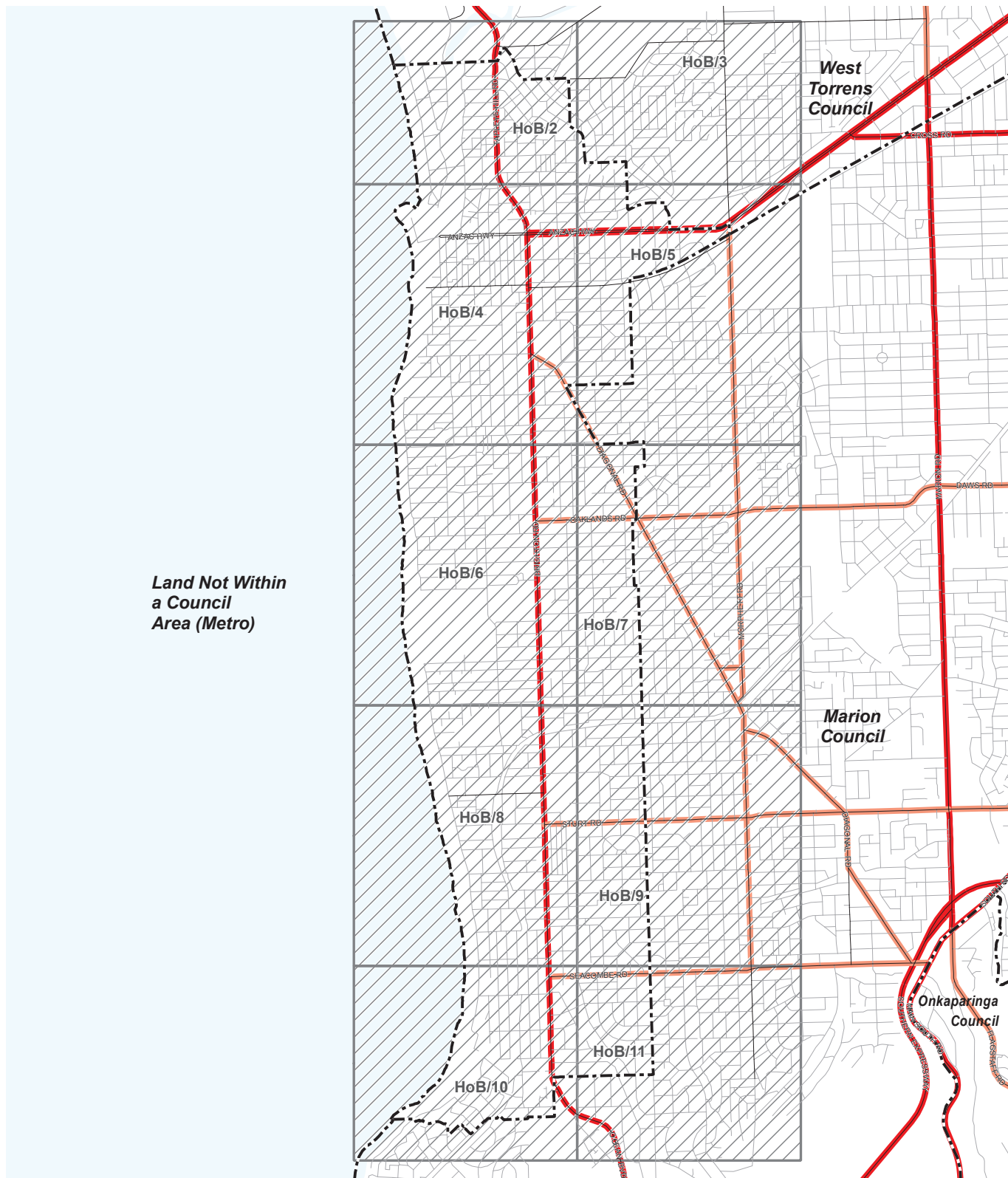
For the purposes of the Development Plan unless otherwise clearly indicated, the zone/policy area/precinct boundaries depicted on or intended to be fixed by Maps HoB/1 to Map HoB/12 inclusive shall be read as conforming in all respects (as the case may require) to the land division boundaries, to the centre line of roads or drain reserves or to the title boundaries, or to imaginary straight lines joining the positions defined by survey or by the measurements shown on the said maps against which the said zone/policy area/precinct boundaries are shown or otherwise indicated.

Council Index Map



- C Council Office
- Railways
- Waterway / drain / channel
- Local Reserves
- Recreation Park
- Development Plan Boundary

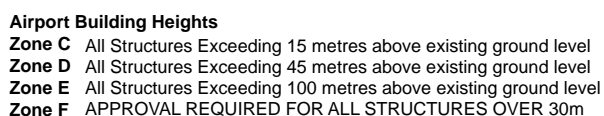
Location Map HoB/1



- Primary Arterial Roads
- Secondary Arterial Roads
- - - - Development Plan Boundary

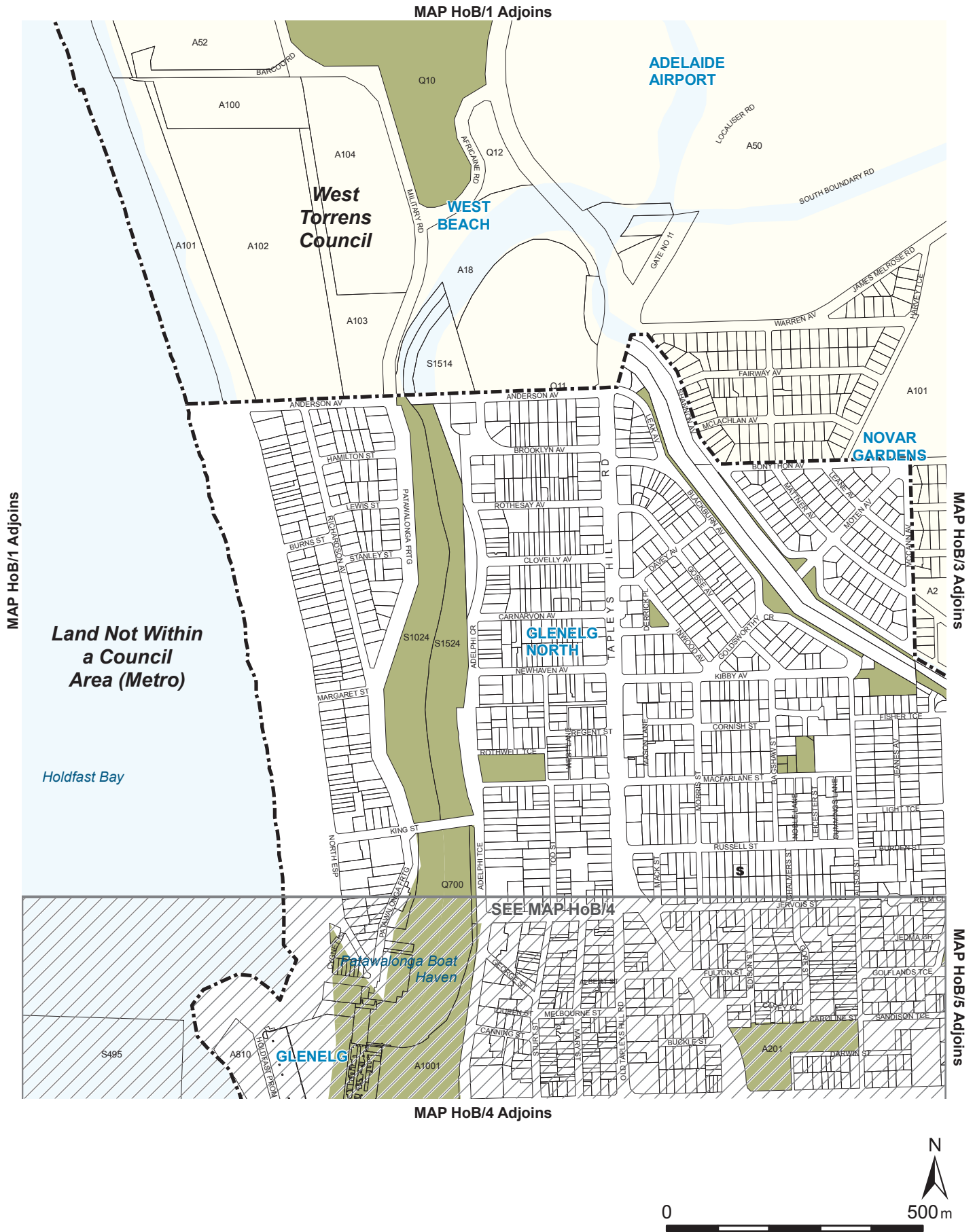
Overlay Map HoB/1

TRANSPORT



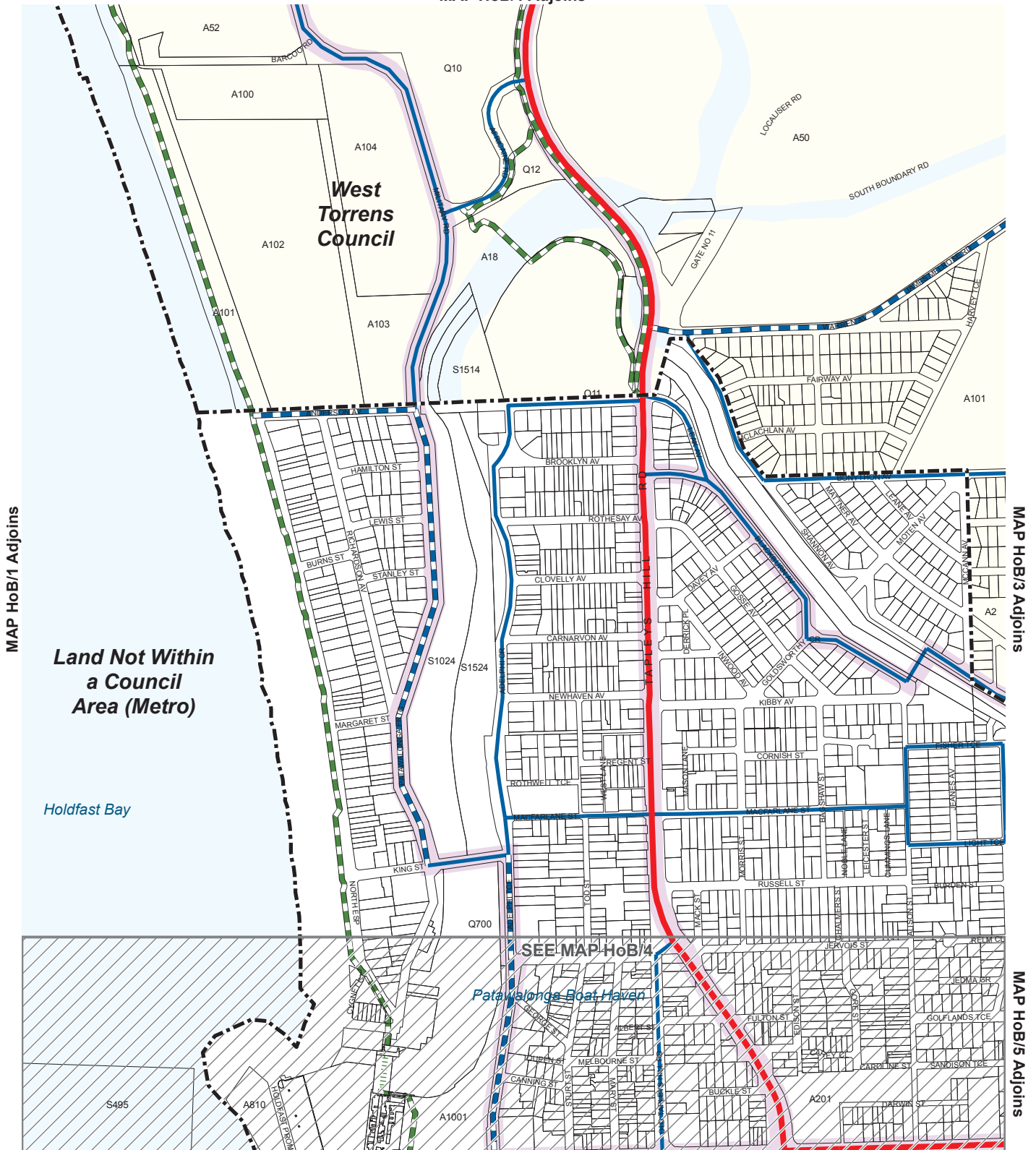
Overlay Map HoB/1

DEVELOPMENT CONSTRAINTS



Location Map HoB/2

MAP HoB/1 Adjoins



MAP HoB/4 Adjoins

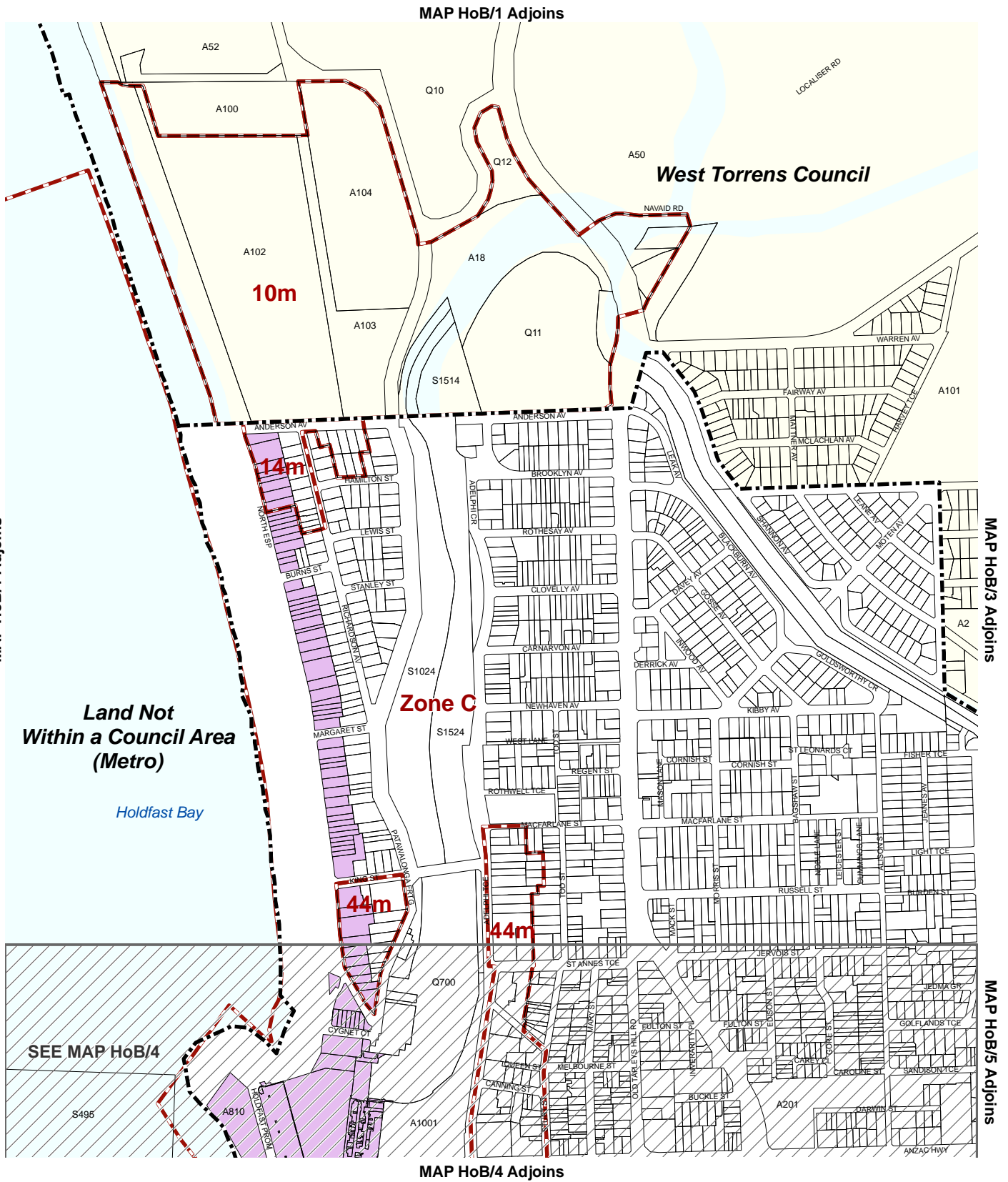
- Primary Arterial Roads
- Bikedirect Network
- Main Road - Bike Lane
- Main Road
- Secondary Road - Bike Lane
- Secondary Road
- Off Road Sealed Path
- Off Road Track
- Public Transport
- - - - Development Plan Boundary



Overlay Map HoB/2

TRANSPORT

HOLDFAST BAY COUNCIL
Consolidated - 26 November 2020



Airport Building Heights
10m - 44m All Structures restricted to height identified on maps
 (above existing ground level, measured from the top of the nearest roadside curb)
Zone C All Structures Exceeding 15 metres above existing ground level

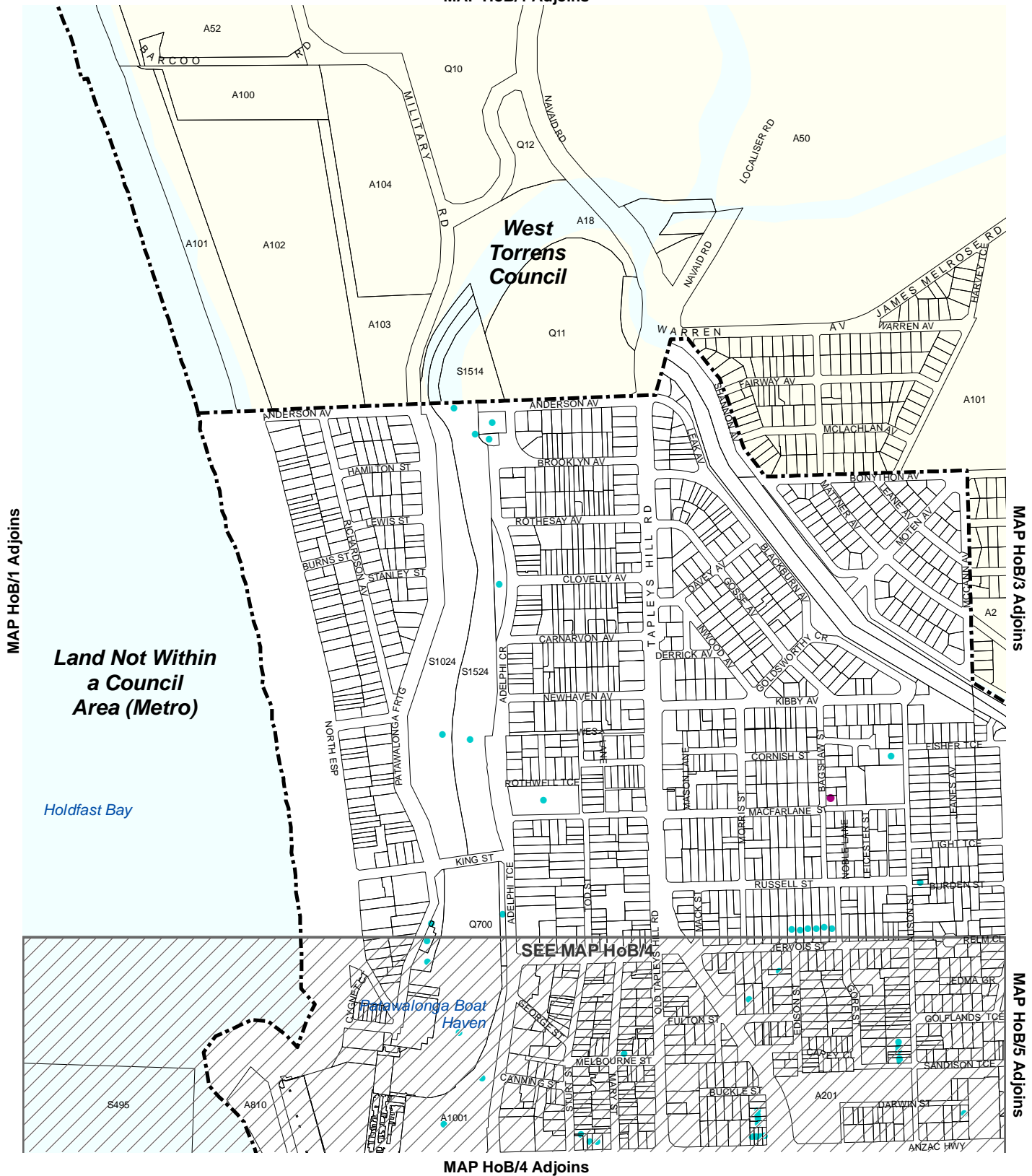


- Airport Building Heights
- Coastal Building and Site Levels (Australian Height Datum)
- Min site level 3.2m - floor level 3.45m
- Development Plan Boundary

Overlay Map HoB/2

DEVELOPMENT CONSTRAINTS

MAP HoB/1 Adjoins



- State heritage place
- Local heritage place

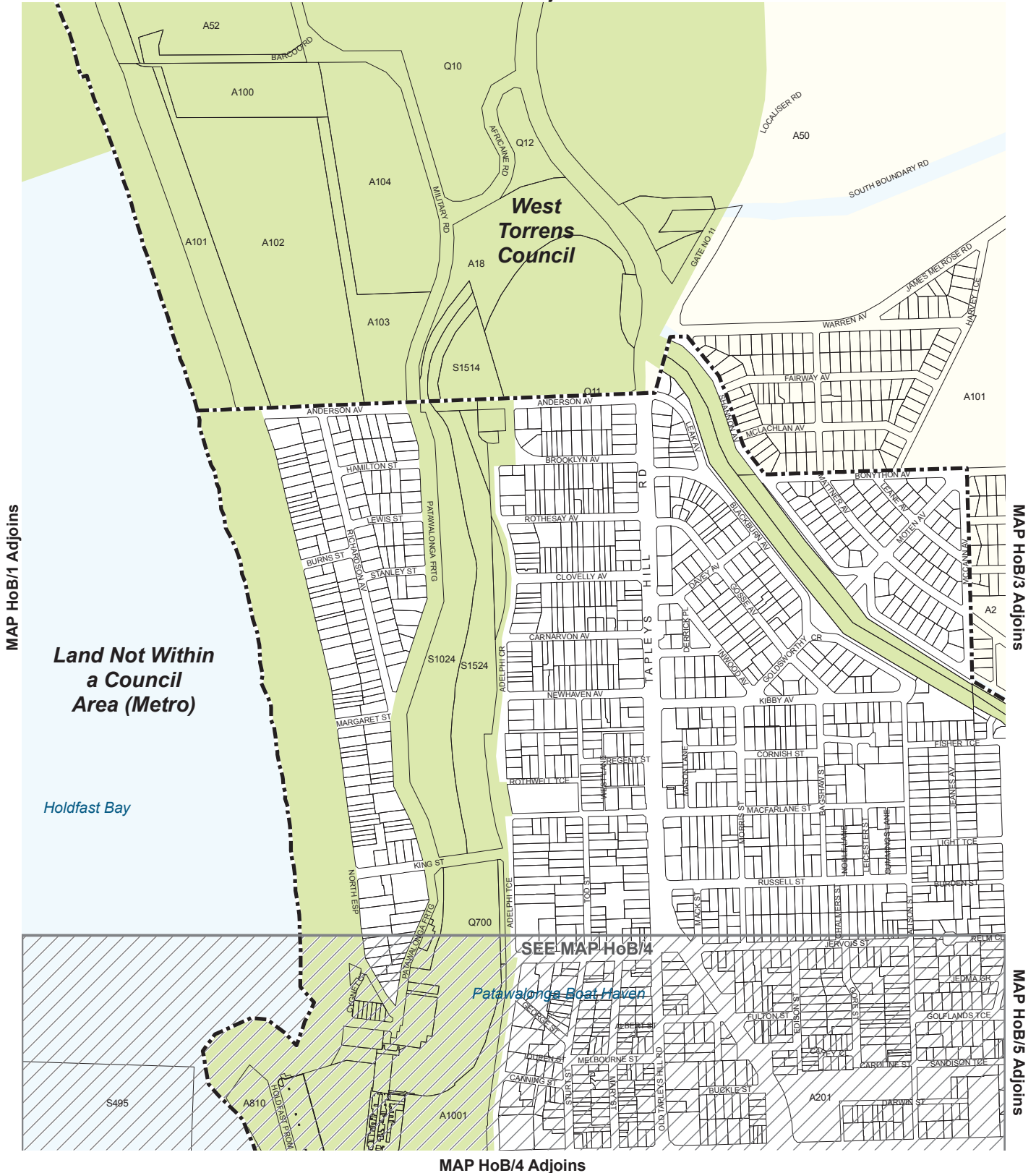
----- Development Plan Boundary

Overlay Map HoB/2

HERITAGE

HOLDFAST BAY COUNCIL
Consolidated - 26 November 2020

MAP HoB/1 Adjoins



MAP HoB/4 Adjoins



Overlay Map HoB/2 NATURAL RESOURCES

- MOSS Study Area
- Development Plan Boundary

[illegible]

MAP HOB/3 Adjoins

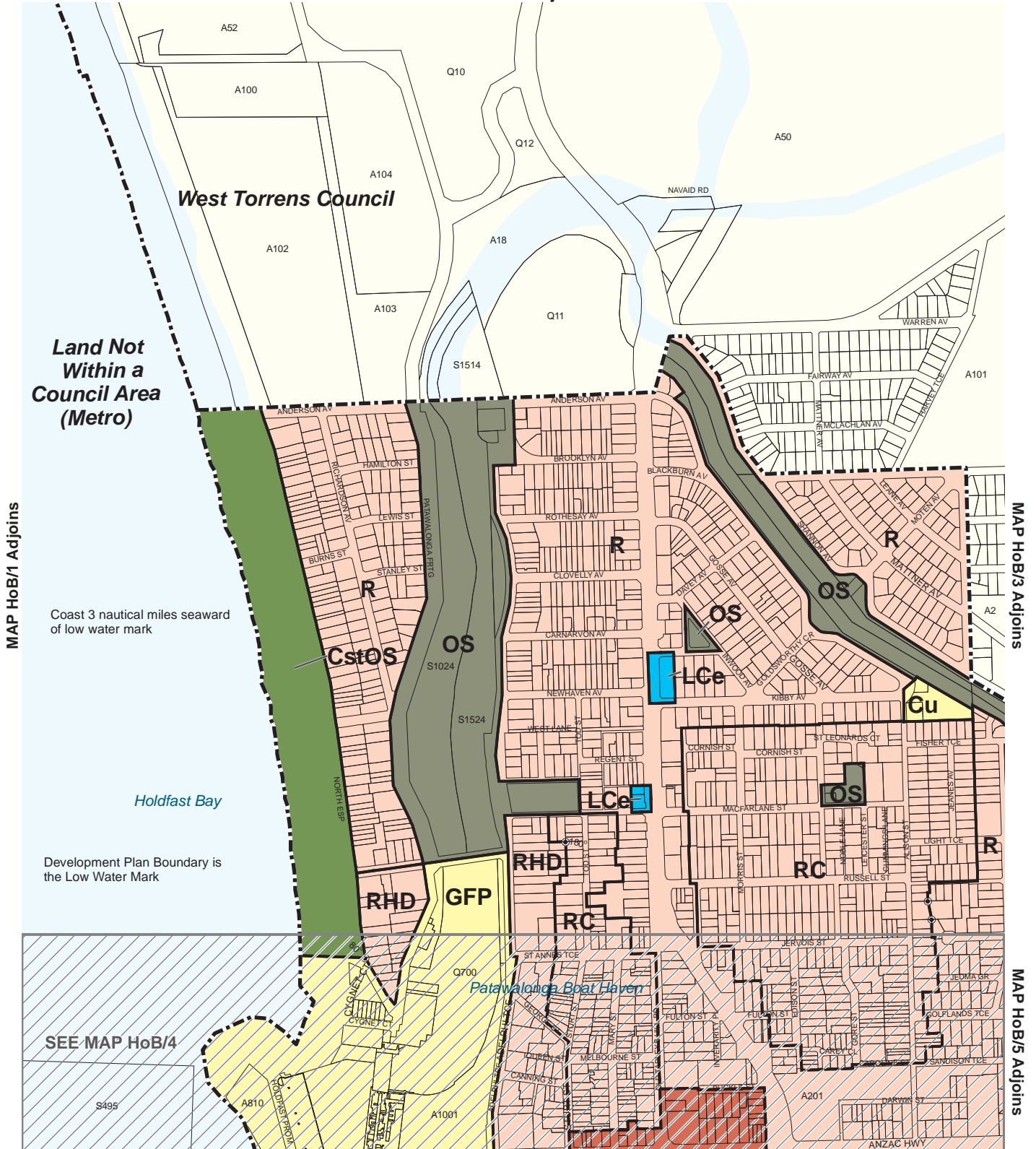
MAP HOB/5 Adjoins

Overlay Map HoB/2

AFFORDABLE HOUSING

HOLDFAST BAY COUNCIL
Consolidated - 26 November 2020

MAP HoB/1 Adjoins



Lamberts Conformal Conic Projection, GDA94

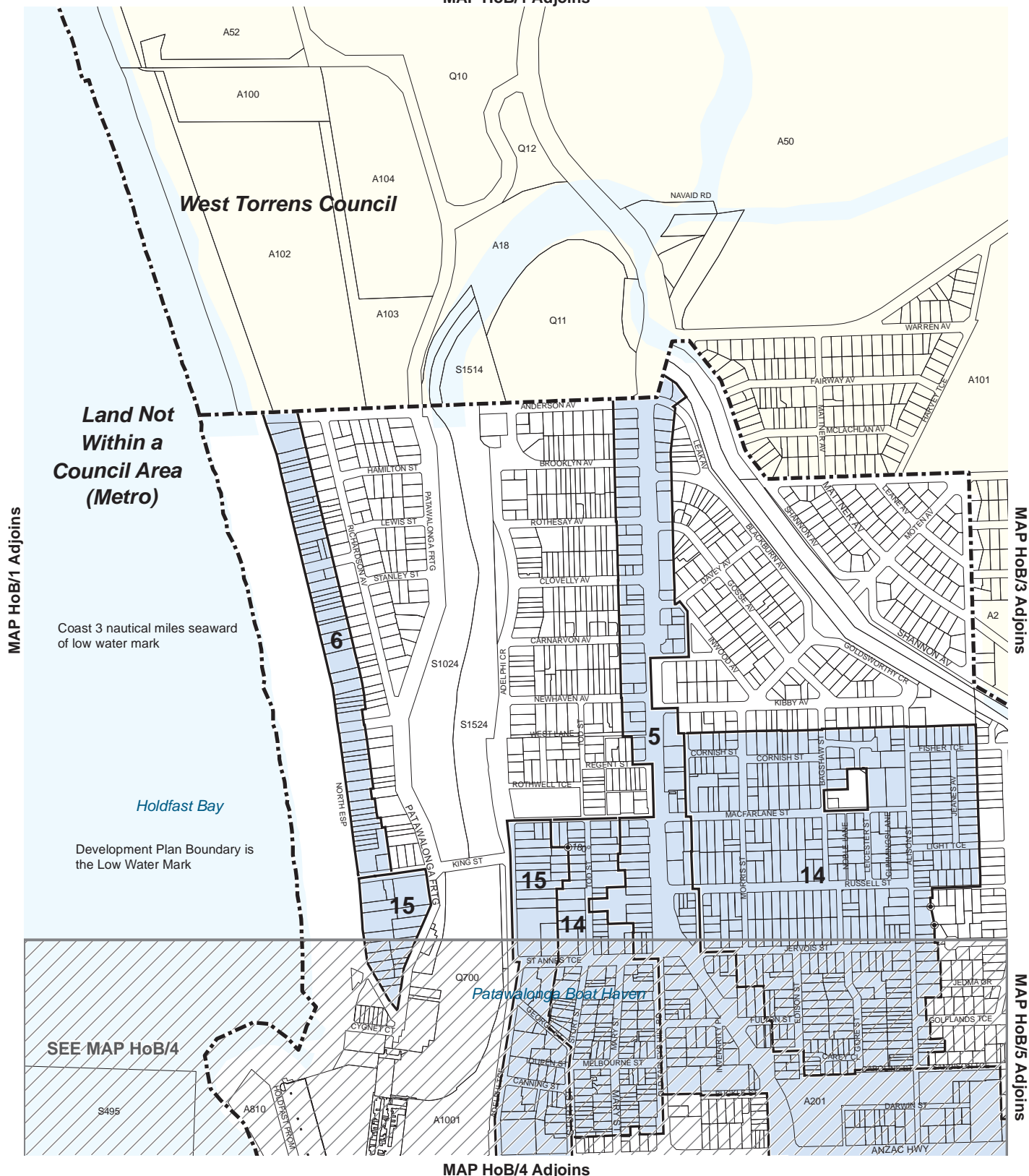
Zones

- CstOS** Coastal Open Space
- Cu** Community
- GFP** Glenelg Foreshore and Patawalunga
- LCe** Local Centre
- OS** Open Space
- R** Residential
- RC** Residential Character
- RHD** Residential High Density
- Zone Boundary**
- Development Plan Boundary**

0 500m

Zone Map HoB/2

MAP HoB/1 Adjoins



Lamberts Conformal Conic Projection, GDA94

Policy Area

- 14 Streetscape Character (Glenelg and Glenelg North)
- 15 Urban Glenelg
- 5 Medium Density
- 6 North West



Policy Area Map HoB/2

- Policy Area Boundary
- Development Plan Boundary

West Torrens Council

Land Not Within a Council Area (Metro)

Coast 3 nautical miles seaward of low water mark

Holdfast Bay

Development Plan Boundary is the Low Water Mark

SEE MAP HoB/4

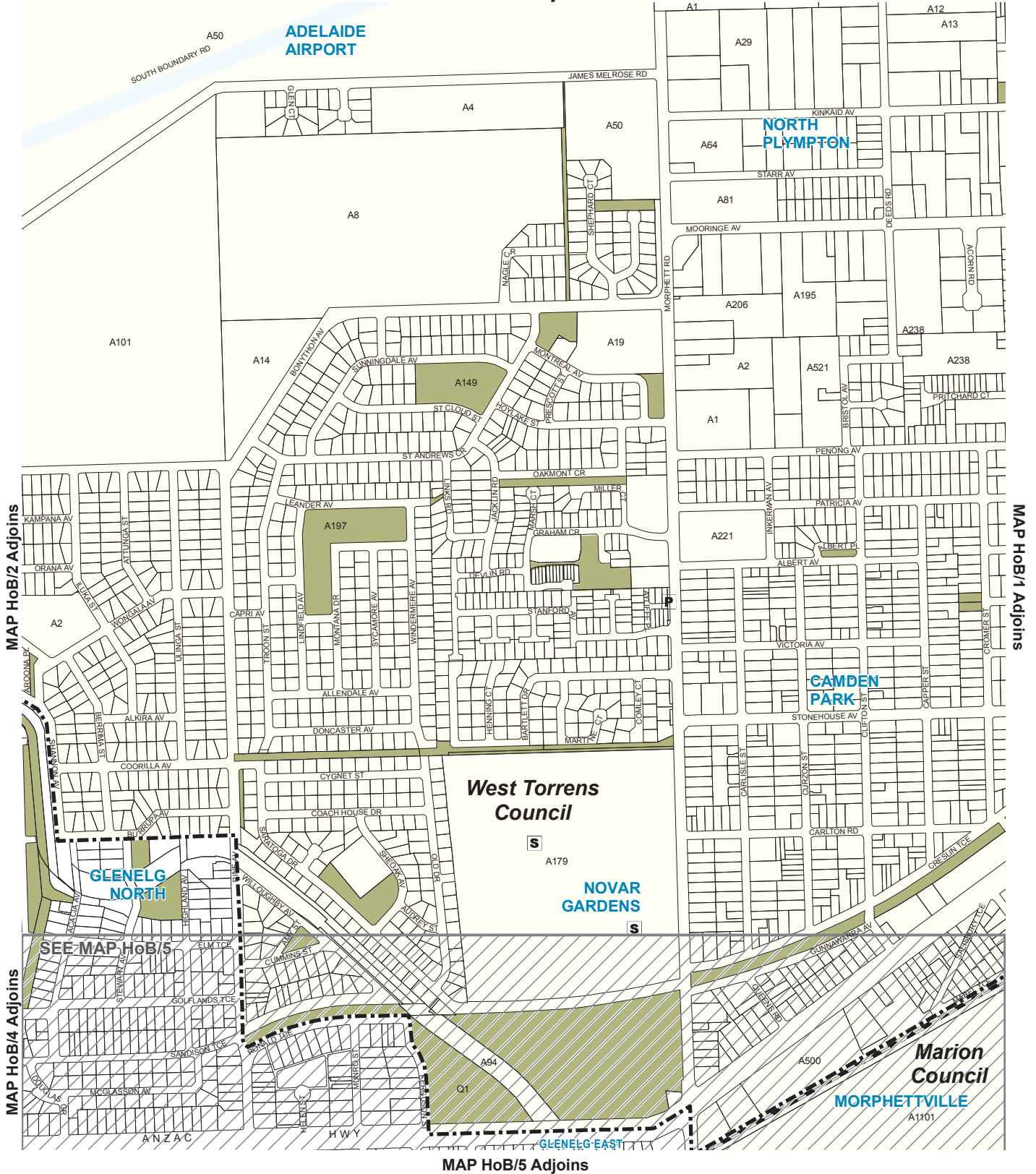
MAP HoB/4 Adjoins

Patawalunga Boat Haven

Map details include street names such as Anderson Av, Hamilton St, Lewis St, Stanley St, Burns St, Richard St, Margaret St, North Esp, King St, West Lane St, Regent St, Rothwell Tce, Macfarlane St, St Ann's Tce, Canning St, Melbourne St, Jervis St, Fulton St, Buckle St, Anzac Hwy, and others. Land parcels are labeled with codes like A52, A100, A104, A102, A103, A18, Q10, Q12, Q11, S1514, S1024, S1524, Q700, A1001, A201, A2, A101, A810, S495, and A201.

- # Precinct Map HoB/2

MAP HoB/1 Adjoins



- S** School
- P** Post Office
- Railways
- Local Reserves
- Development Plan Boundary

Location Map HoB/3

West Torrens Council

MAP HoB/5 Adjoins

Marion Council

SEE MAP HoB/5

South Boundary Rd

West Torrens Council

Marion Council

MAP HoB/5 Adjoins

-

HOLDFAST BAY COUNCIL
Consolidated - 26 November 2020

The map displays the West Torrens Council area, showing various streets, zones, and council boundaries. The map is titled "MAP HoB/5 Adjoins" at the bottom. The map includes labels for "Zone A" (top right), "Zone C" (bottom left), and "Zone B" (bottom center). It also shows the "West Torrens Council" and "Marion Council" boundaries. The map is a detailed street map with various streets labeled, including James Melrose Rd, Kinkaid Av, Starr Av, Moorings Av, Deeds Rd, Acon Rd, Pritchard Ct, Penong Av, Patricia Av, Albert Av, Victoria Av, Stonehouse Av, Carlton Rd, Crescent Tce, Cliffon St, Capier St, Cromer St, Miller St, Oakmont Cr, Jacklin Rd, Devon Rd, Stanford Av, Martin St, Comley Cr, Bartlett Cr, Henning Cr, Leander Av, Allendale Av, Doncaster Av, Cygnet St, Coach House Dr, Shepherd Av, Oldfield St, Audrey St, Cummins St, Golflands Tce, Sandison Tce, McGlasson Av, Anzac, Hwy, and A500. The map also shows the "South Boundary Rd" and "A50" road. The map is a detailed street map with various streets labeled, including James Melrose Rd, Kinkaid Av, Starr Av, Moorings Av, Deeds Rd, Acon Rd, Pritchard Ct, Penong Av, Patricia Av, Albert Av, Victoria Av, Stonehouse Av, Carlton Rd, Crescent Tce, Cliffon St, Capier St, Cromer St, Miller St, Oakmont Cr, Jacklin Rd, Devon Rd, Stanford Av, Martin St, Comley Cr, Bartlett Cr, Henning Cr, Leander Av, Allendale Av, Doncaster Av, Cygnet St, Coach House Dr, Shepherd Av, Oldfield St, Audrey St, Cummins St, Golflands Tce, Sandison Tce, McGlasson Av, Anzac, Hwy, and A500. The map also shows the "South Boundary Rd" and "A50" road.

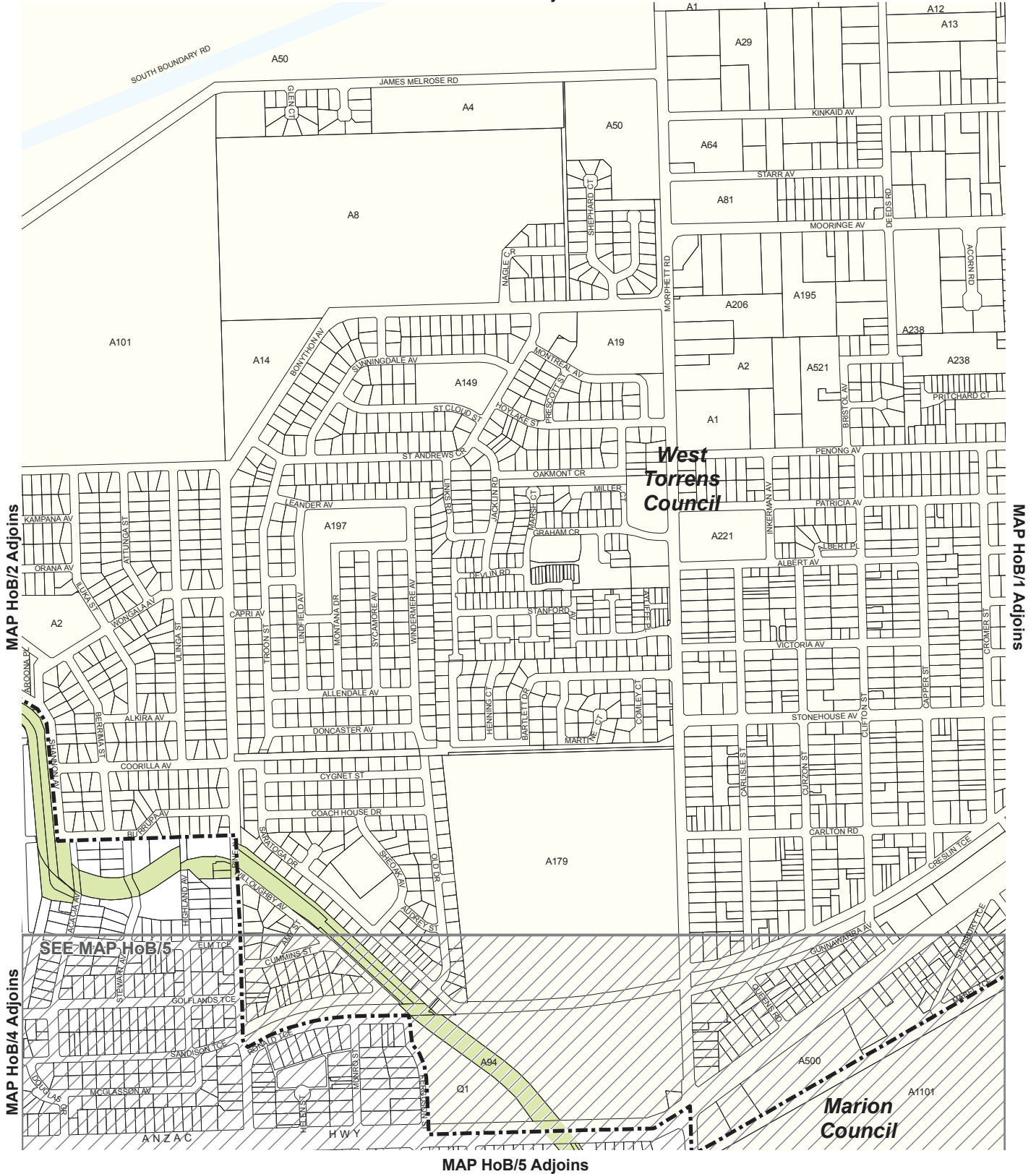
Zone A All Structures
Zone C All Structures Exceeding 15 metres above existing ground level



HOLDFAST BAY COUNCIL
Consolidated - 26 November 2020



MAP HoB/1 Adjoins



Overlay Map HoB/3 NATURAL RESOURCES

- MOSS Study Area
- Development Plan Boundary

[illegible]

Zones

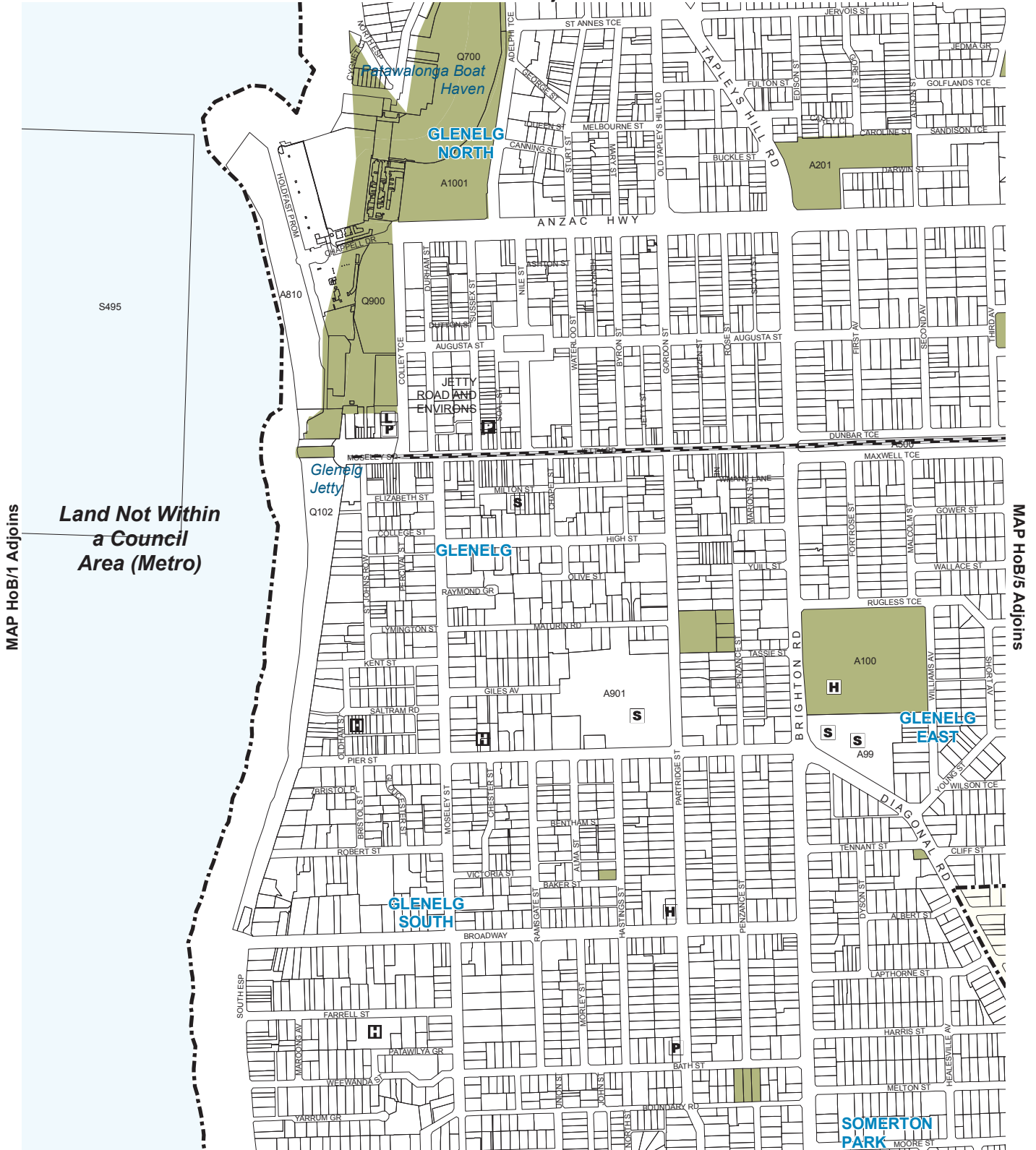
OS Open Space

R Residential

Zone Boundary

Development Plan Boundary

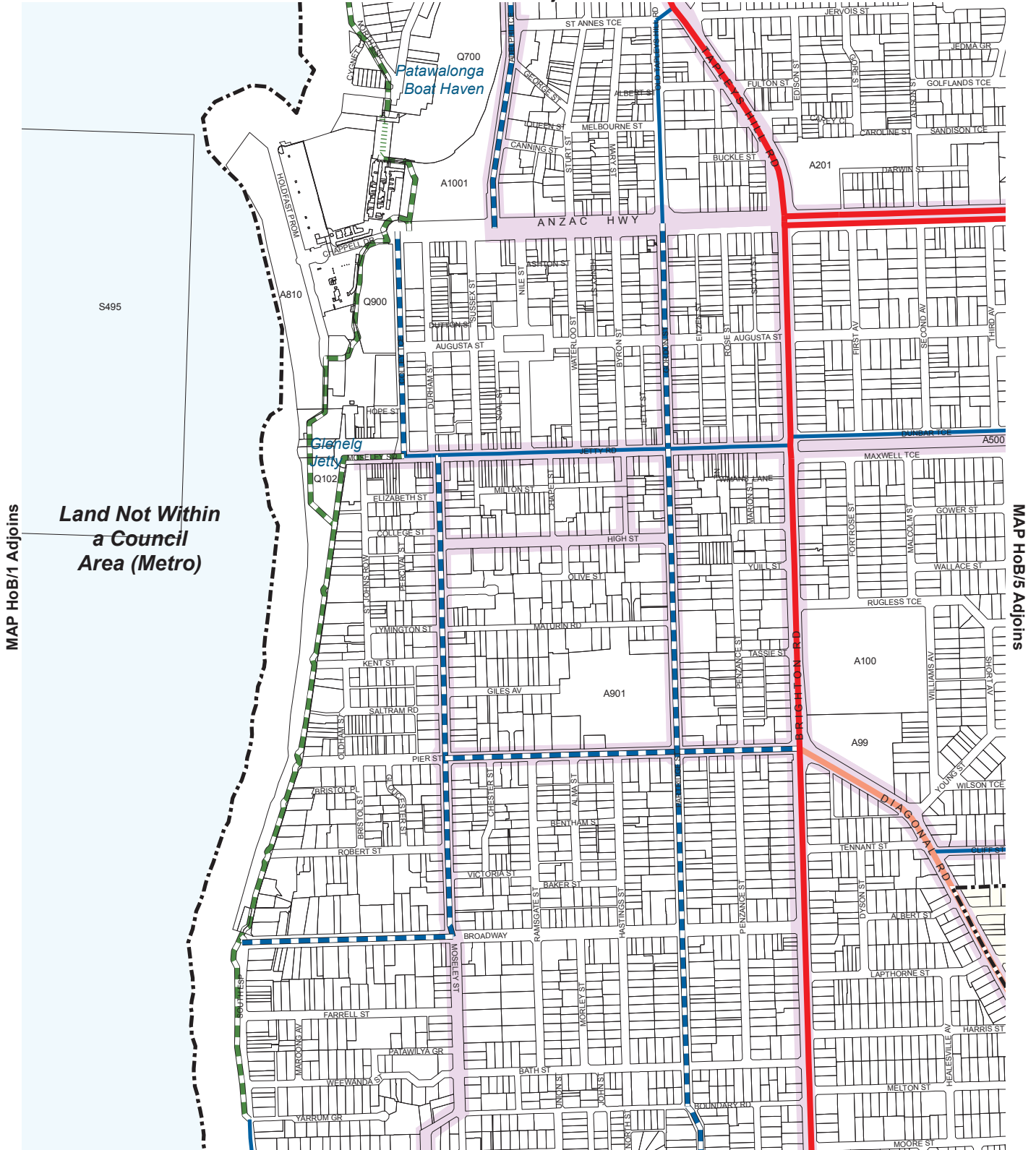
MAP HoB/2 Adjoins



- S** School
- L** Public Library
- P** Post Office
- H** Other Health Services
- H** Hospital
- P** Police Station
- Railways
- Local Reserves
- Development Plan Boundary

Location Map HoB/4

MAP HoB/2 Adjoins



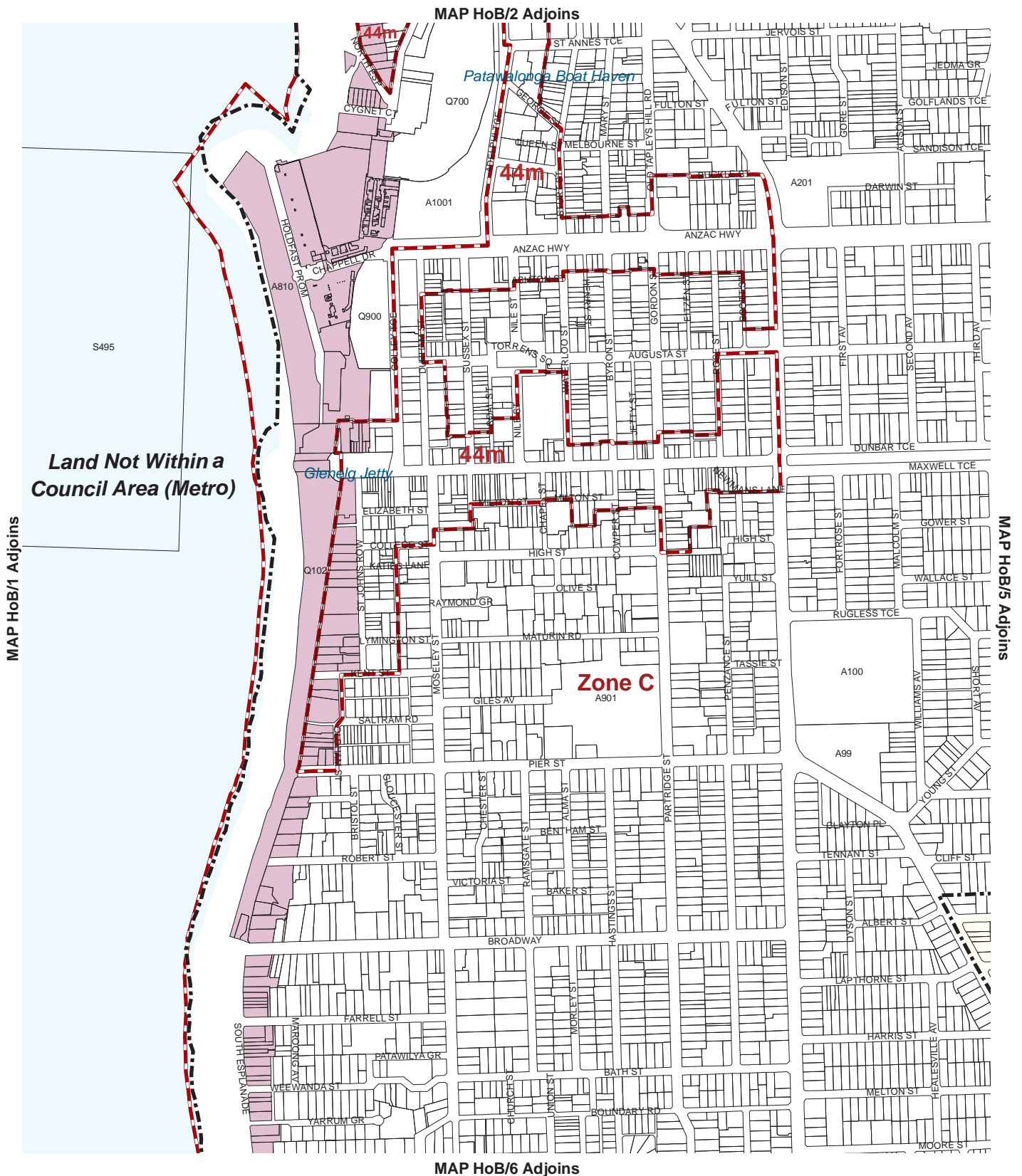
MAP HoB/6 Adjoins

- Primary Arterial Roads
- Secondary Arterial Roads
- Bikedirect Network**
- - - Main Road - Bike Lane
- Main Road
- - - Secondary Road - Bike Lane
- Secondary Road
- - - Off Road Sealed Path
- ||||| Off Road Track
- Public Transport
- - - - - Development Plan Boundary



Overlay Map HoB/4

TRANSPORT



Airport Building Heights

44m All Structures restricted to height identified on maps
(above existing ground level, measured from the top of the nearest roadside curb)

Zone C All Structures Exceeding 15 metres above existing ground level



Airport Building Heights

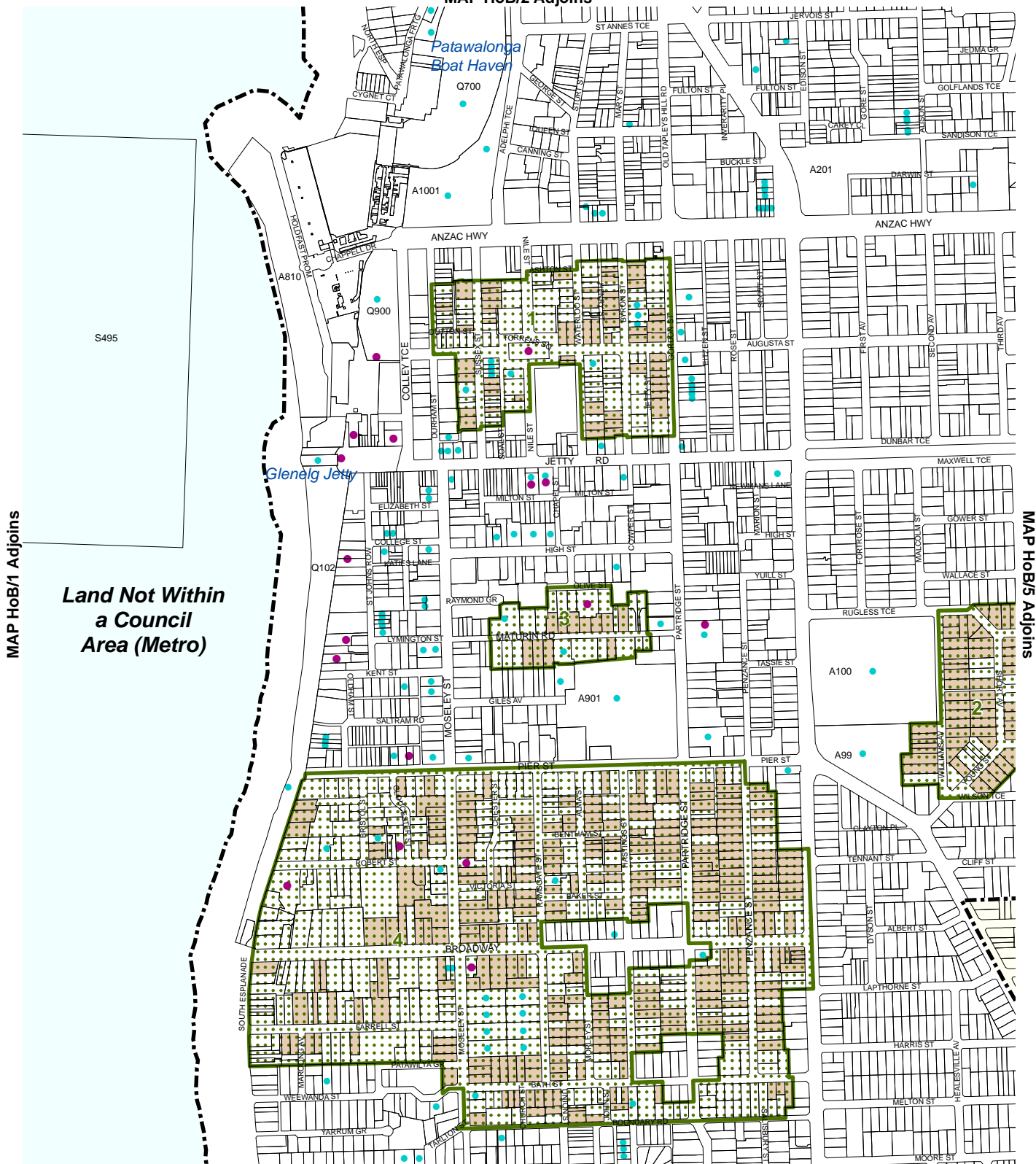
Coastal Building and Site Levels (Australian Height Datum)

Min site level 3.2m - floor level 3.45m

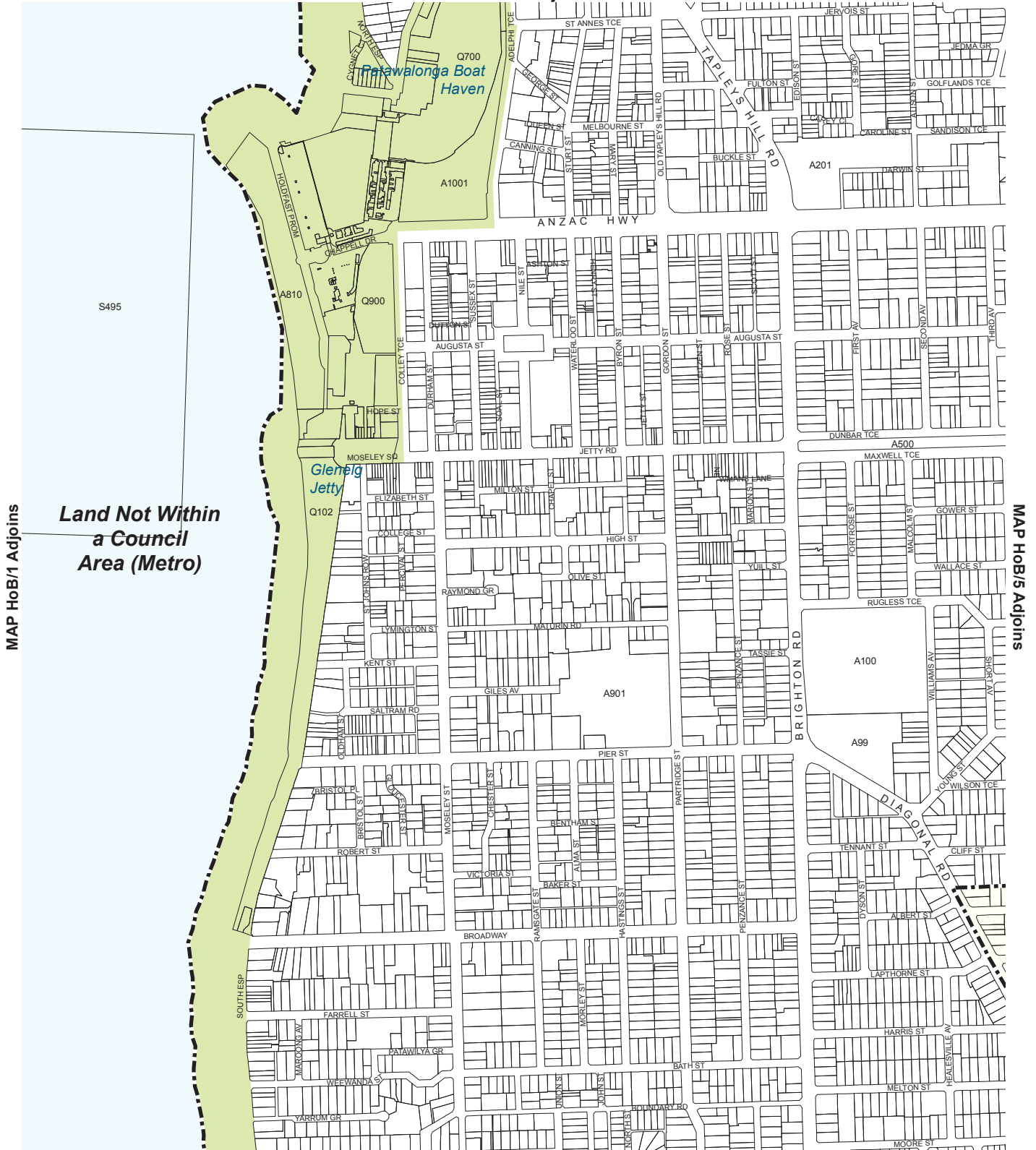
Development Plan Boundary

Overlay Map HoB/4 DEVELOPMENT CONSTRAINTS

MAP HoB/2 Adjoins



MAP HoB/2 Adjoins



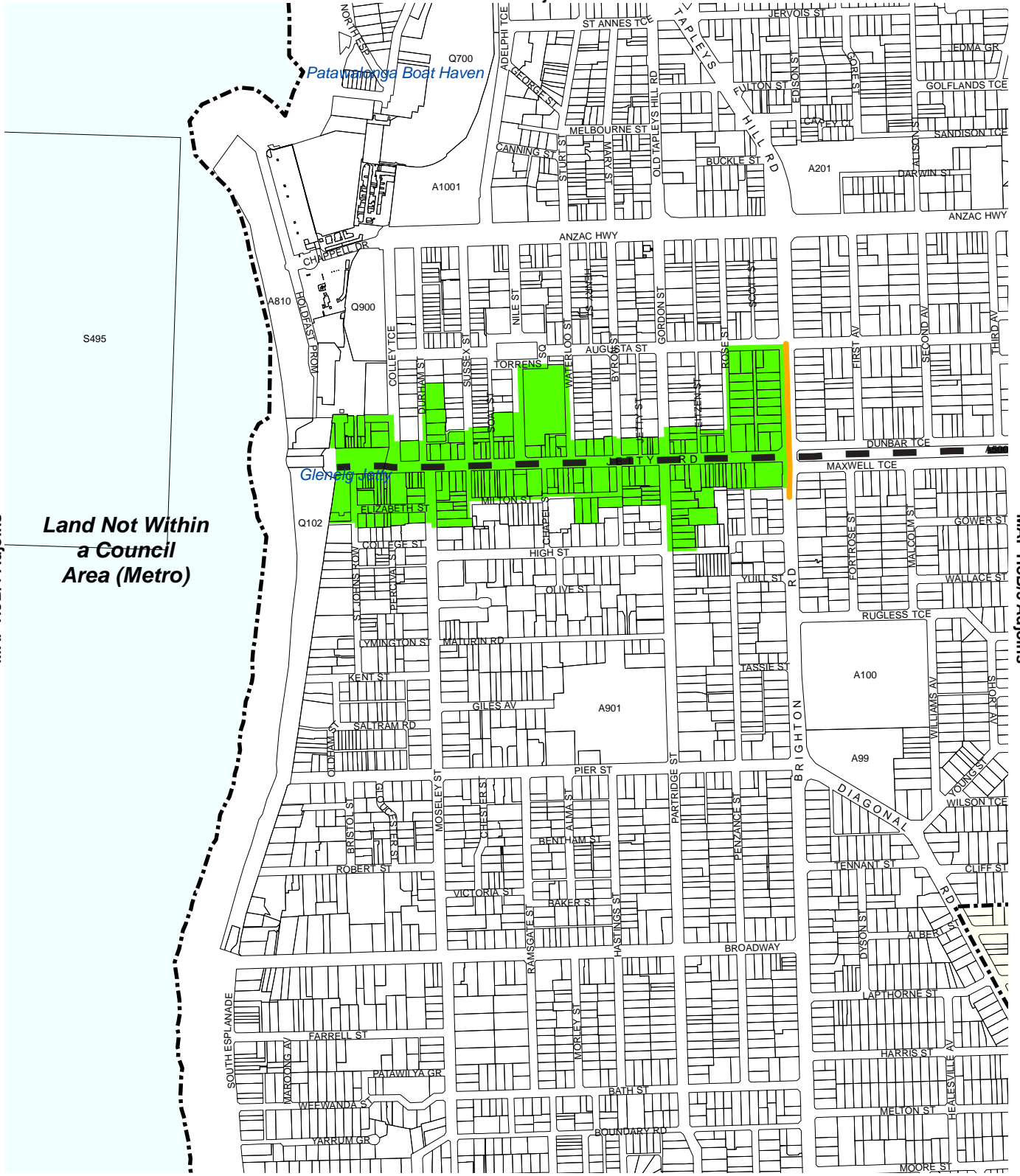
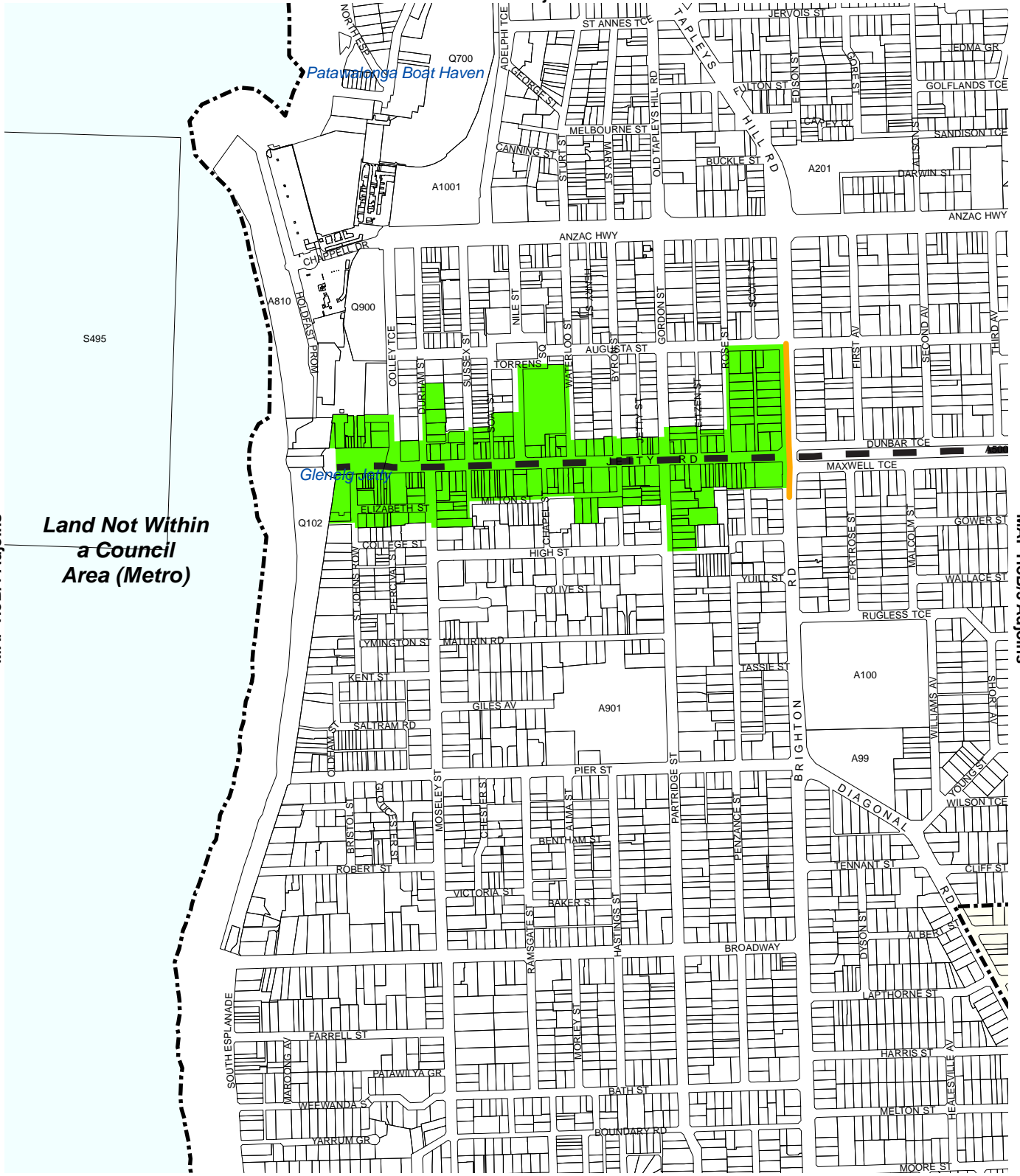
MAP HoB/6 Adjoins



Overlay Map HoB/4 NATURAL RESOURCES

- MOSS Study Area
- Development Plan Boundary

MAP HoB/1 Adjoins

[illegible][illegible]

Land Not Within a Council Area (Metro)

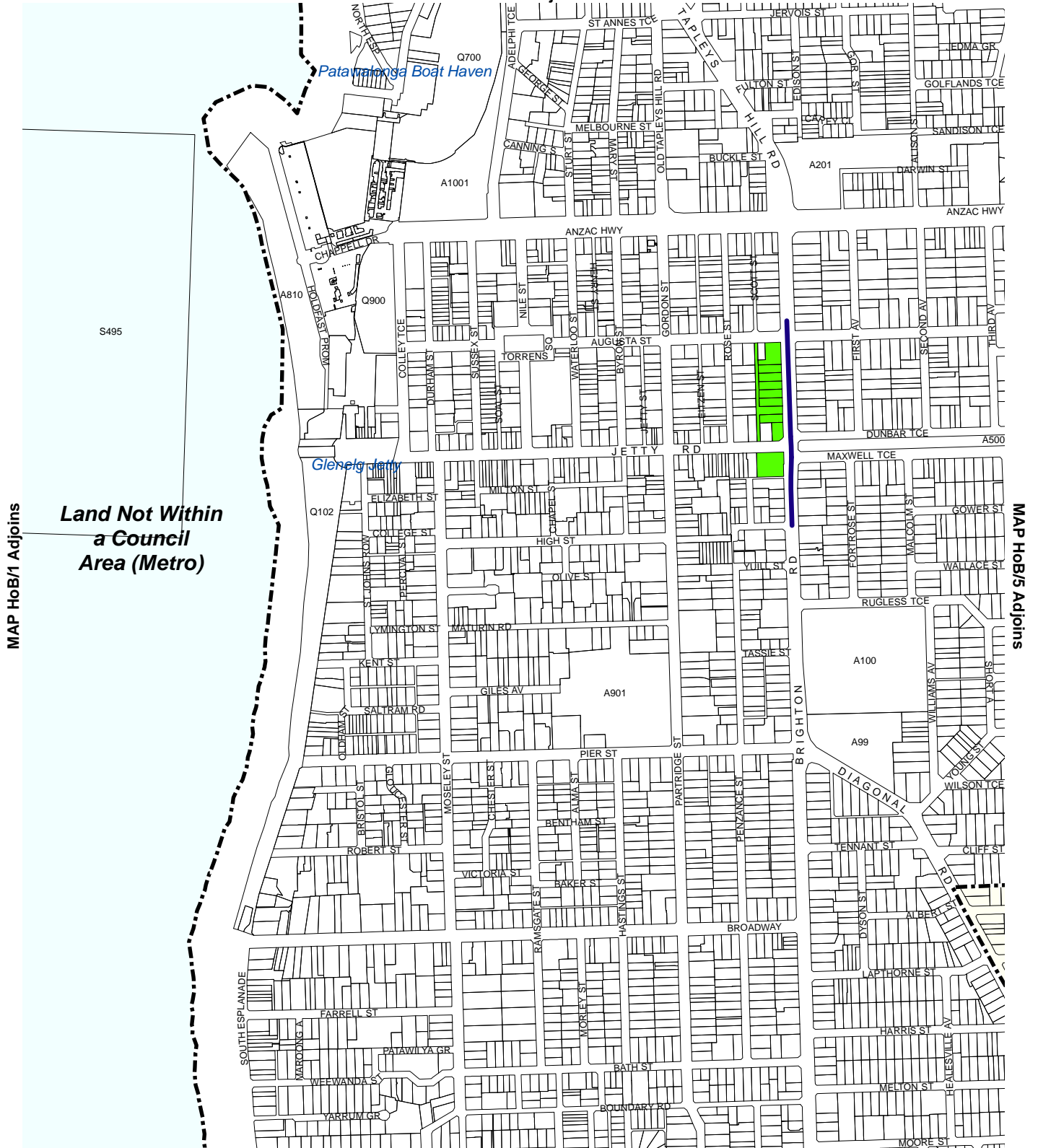
Streets shown include: Adelaide Tce, St Annes Tce, Melbourne St, Canning St, Sturt St, Mary St, Old Tapleys Hill Rd, Buckle St, Hill Rd, Jervois St, East St, Cape St, Sandison Tce, Darwin St, Anzac Hwy, Anzac Hwy, Gordon St, Rose St, Gordon St, First Av, Second Av, Third Av, Maxwell Tce, Portrose St, Malcolm St, Gower St, Wallace St, Rugless Tce, Brighton Rd, A100, A99, Diagonal Rd, Tennant St, Dyson St, Laphorne St, Harris St, Melton St, Moore St, Cliff St, Albert St, Reasville Av, Boundary Rd, Bath St, Morley St, Victoria St, Ramsgate St, Baker St, Hastings St, Pier St, Alma St, Bentham St, Gles Av, Maturin Rd, Ymington St, Kent St, Oldham St, Saltram Rd, Bristol St, Stiles St, Robert St, Moseley St, Chester St, Farrell St, Marong Av, Weewanda St, Yarrum Gr, South Esplanade, and Patawonga Gr.

Other labels: S495, A810, Q700, A1001, Q900, A102, A901, A201.

Land Not Within a Council Area (Metro)

The map displays a network of streets in the Glenelg area. A prominent green-shaded region, outlined by a dashed line, indicates land not within a council area. This region is situated between Adelaide St and Rundle St, extending from the coast towards the center. The map includes numerous street names such as Adelaide St, Rundle St, and others. A dashed line runs along the left side of the map, likely representing a boundary. The map is oriented with North at the top.

MAP HoB/2 Adjoins

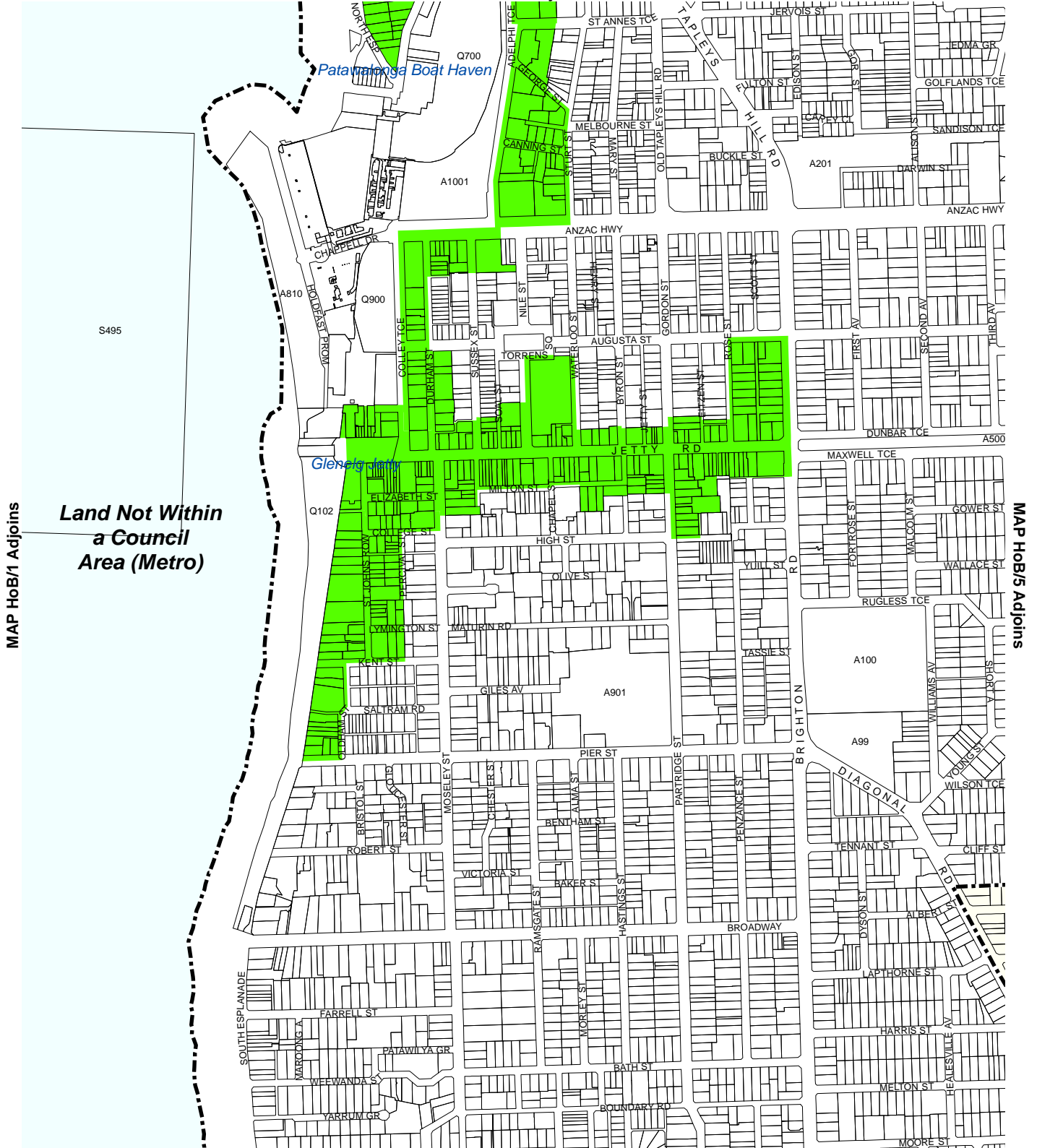


MAP HoB/6 Adjoins

Overlay Map HoB/4 STRATEGIC TRANSPORT ROUTES

- Urban Strategic Transport roads
- Urban Strategic Designated Area
- Development Plan Boundary

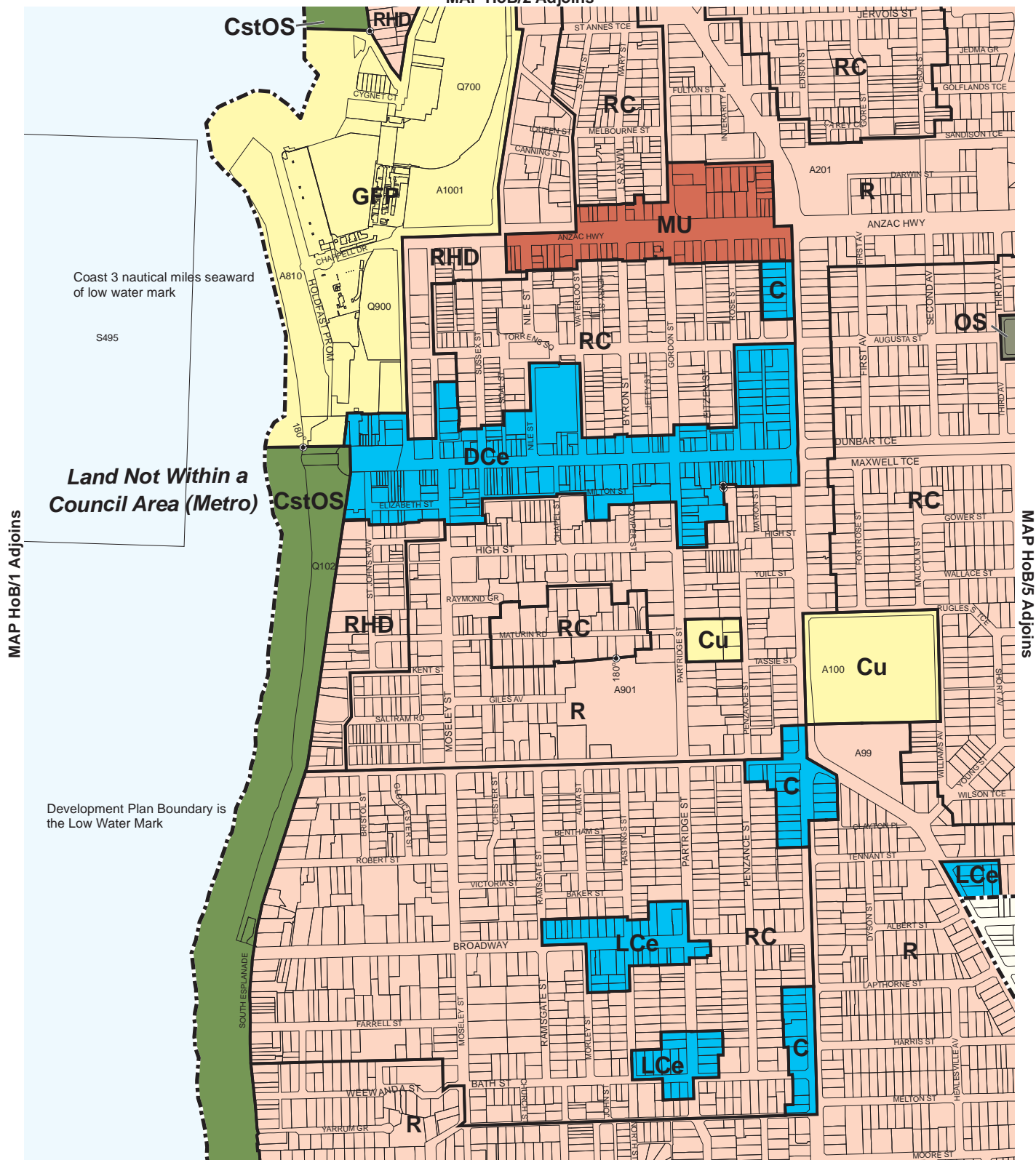
MAP HoB/2 Adjoins



Overlay Map HoB/4 AFFORDABLE HOUSING

- Urban Affordable Designated Area
- Development Plan Boundary

MAP HoB/2 Adjoins



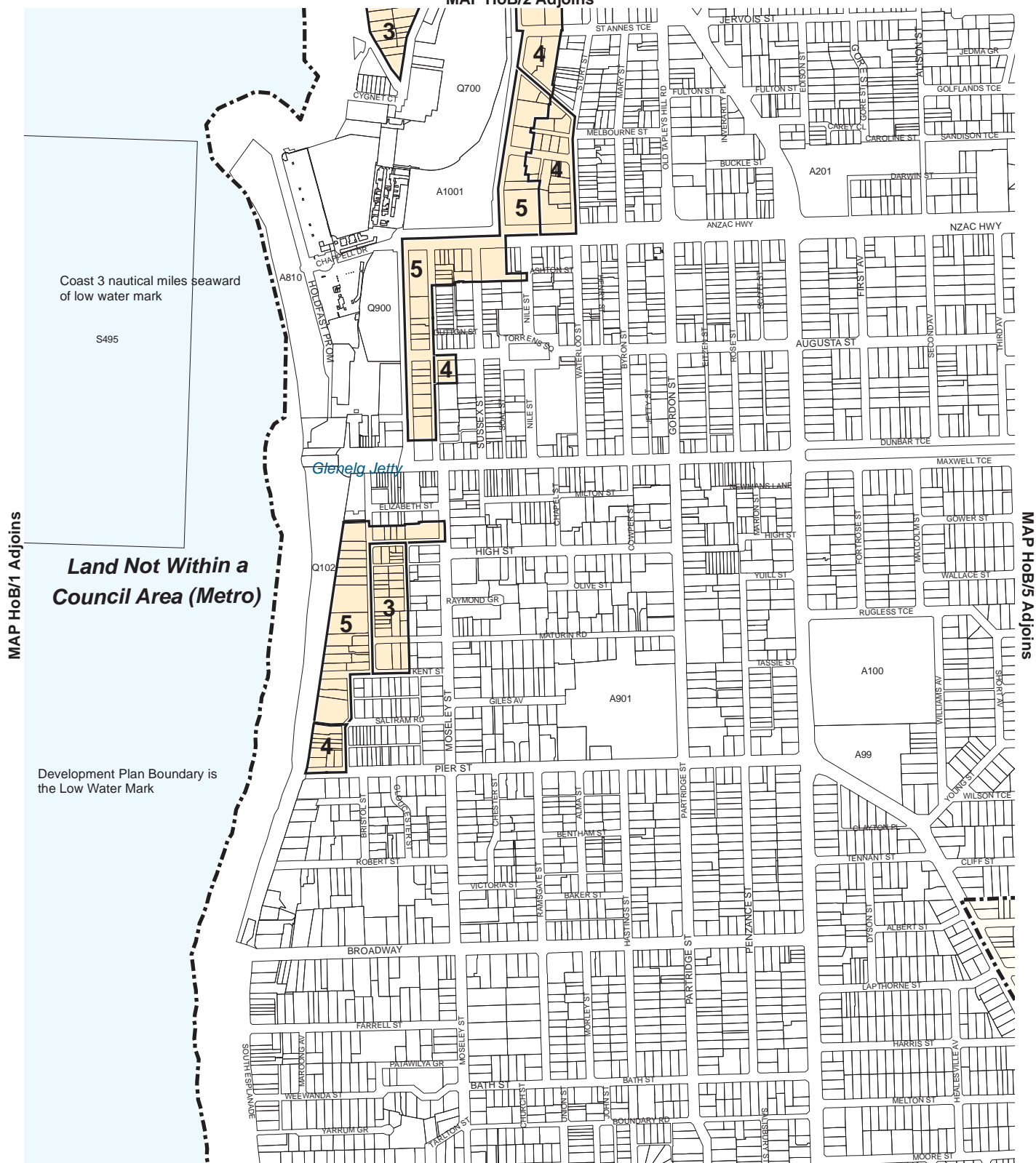
[illegible]

Policy Area

- 1** Recreation
- 10** Maturin Road
- 11** New Glenelg
- 13** Streetscape Character (Brighton and Glenelg East)
- 14** Streetscape Character (Glenelg and Glenelg North)
- 15** Urban Glenelg
- 2** Glenelg
- 3** Central West
- 4** Institution
- 5** Medium Density
- 8** Central Glenelg Village
- 9** Da Costa Park

HOLDFAST BAY COUNCIL
Consolidated - 26 November 2020

MAP HoB/2 Adjoins



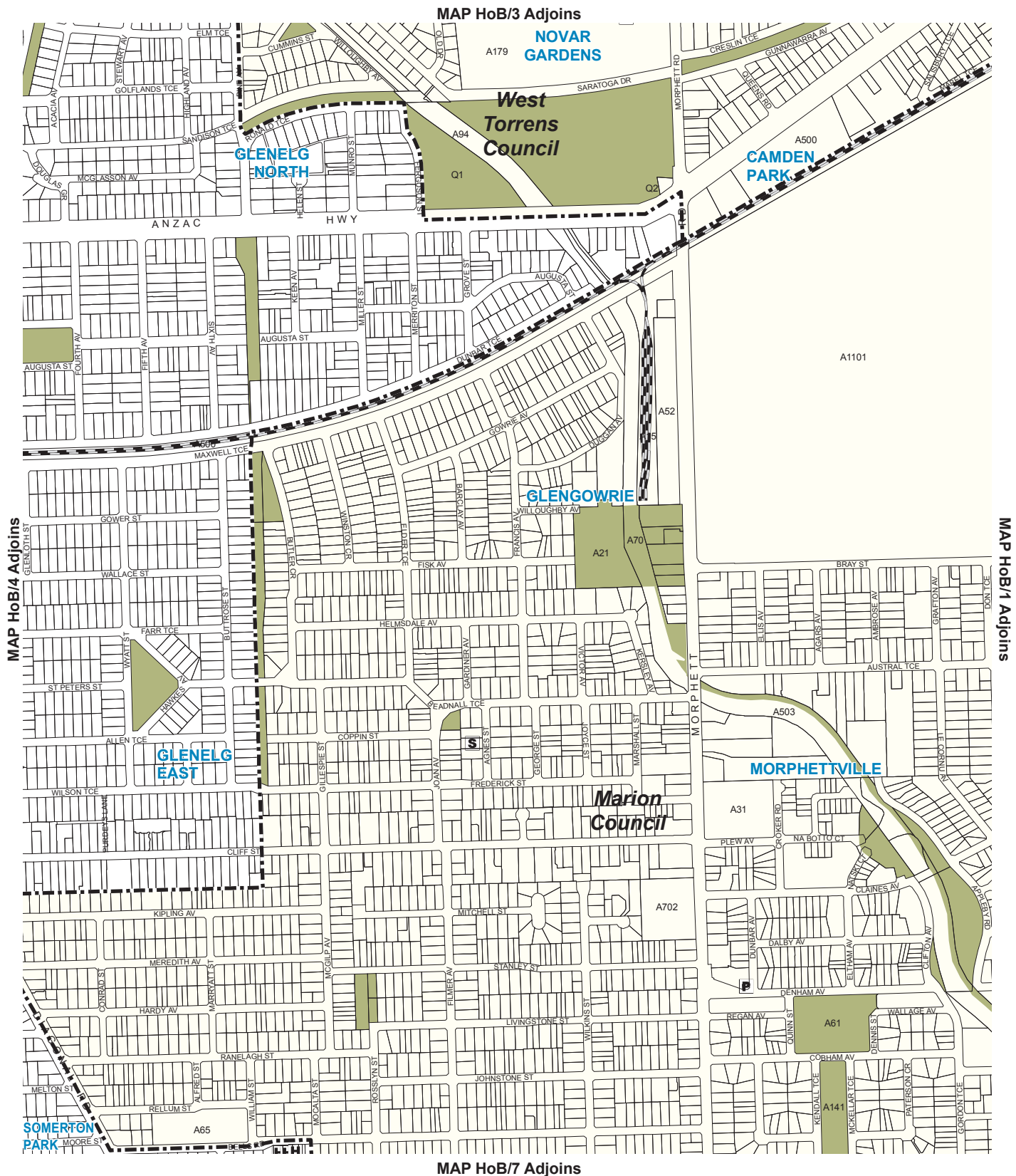
Lamberts Conformal Conic Projection, GDA94

Precinct

- 3 Three Storey
- 4 Five Storey
- 5 Twelve Storey

- Precinct Boundary
- Development Plan Boundary

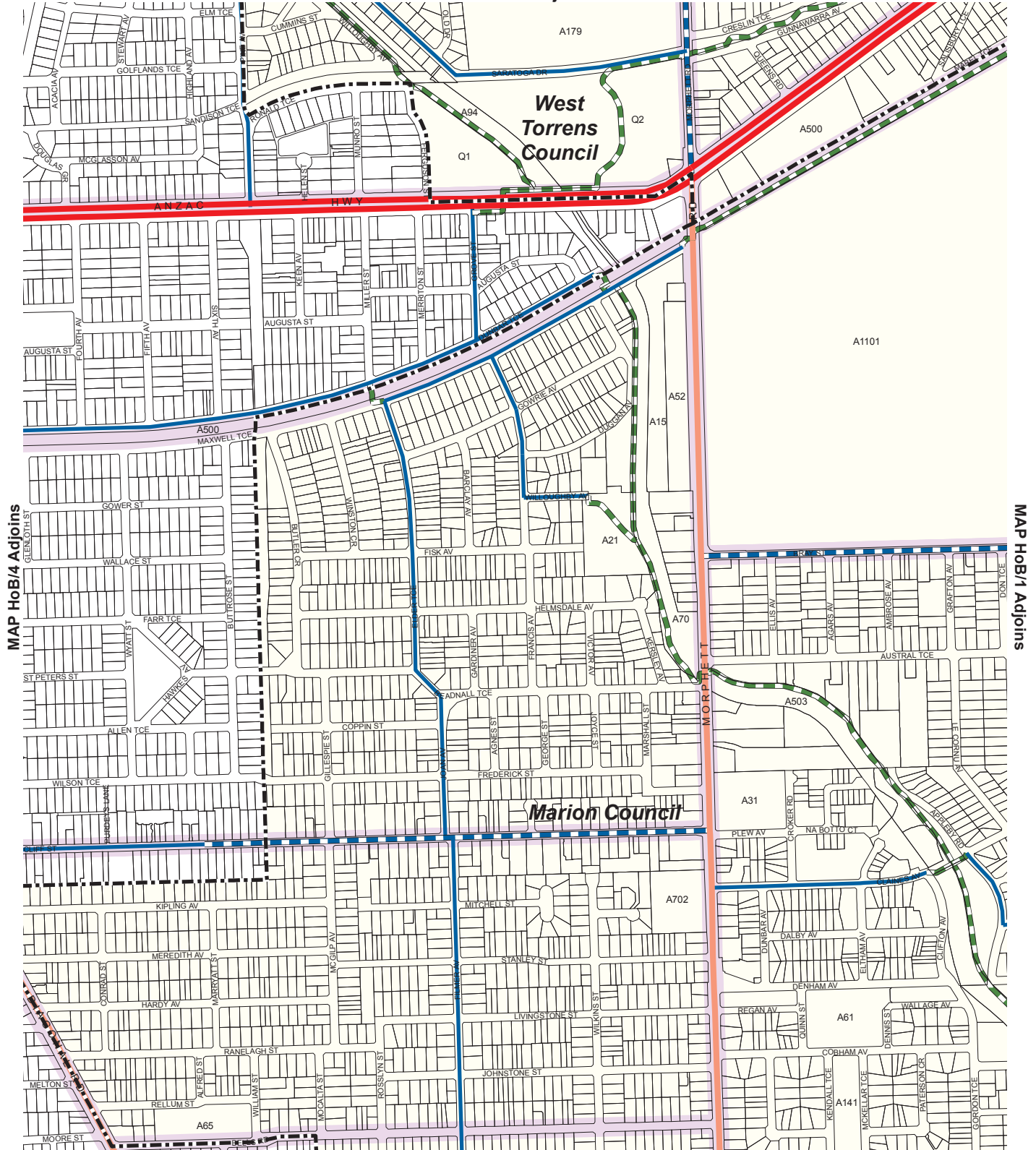
Precinct Map HoB/4



- S** School
- P** Post Office
- H** Other Health Services
- Railways
- Local Reserves
- Development Plan Boundary

Location Map HoB/5

MAP HoB/3 Adjoins



MAP HoB/7 Adjoins

- Primary Arterial Roads
- Secondary Arterial Roads
- Bikedirect Network**
- Main Road - Bike Lane
- Main Road
- Secondary Road - Bike Lane
- Secondary Road
- Off Road Sealed Path
- Public Transport
- - - - - Development Plan Boundary



Overlay Map HoB/5

TRANSPORT

HOLDFAST BAY COUNCIL
Consolidated - 26 November 2020

MAP HoB/3 Adjoins

**West
Torrens
Council**

Zone C

**Marion
Council**

MAP HoB/1 Adjoins

MAP HoB/7 Adjoins

Airport Building Heights

Zone C All Structures Exceeding 15 metres above existing ground level

0

500m



Airport Building Heights

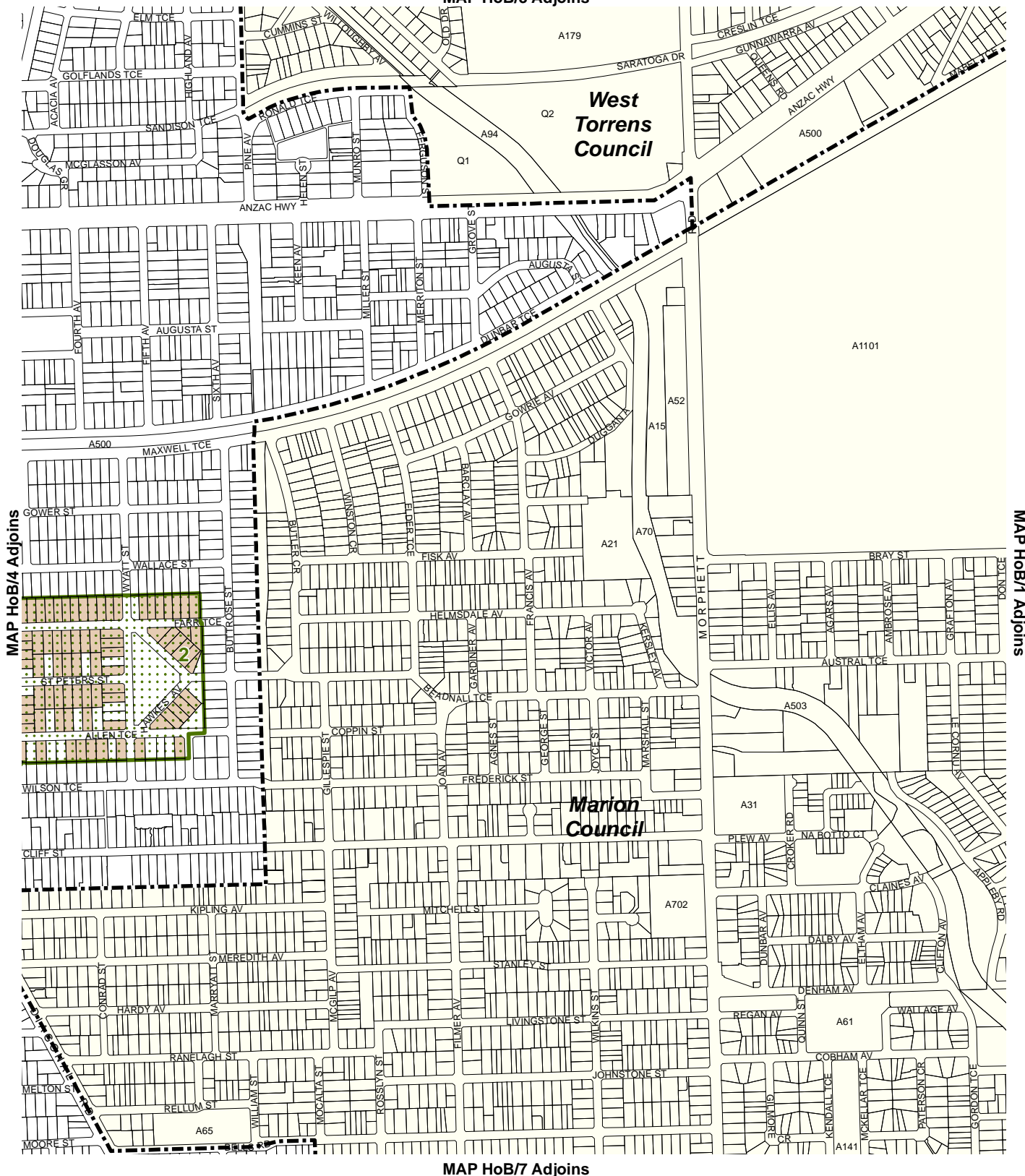


Development Plan Boundary

Overlay Map HoB/5 DEVELOPMENT CONSTRAINTS

HOLDFAST BAY COUNCIL
Consolidated - 26 November 2020

MAP HoB/3 Adjoins



Heritage points are indicative only.
For further information on State and Local Heritage Places and Contributory Items please refer to the relevant tables within this document.

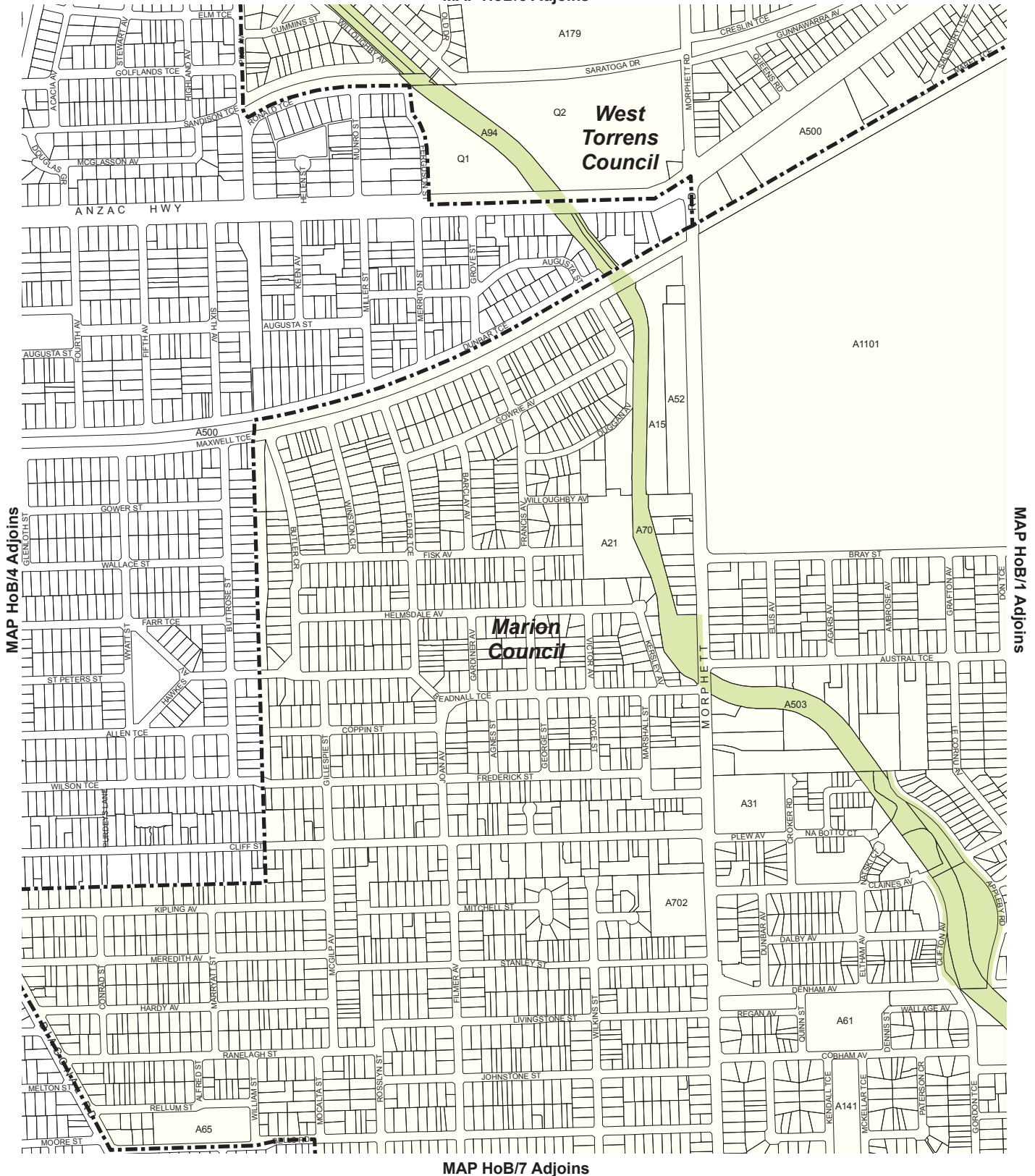
Historic Conservation Area
2 Da Costa Park



Overlay Map HoB/5 HERITAGE

- Contributory item
- Historic Conservation Area
- Development Plan Boundary

MAP HoB/3 Adjoins



MAP HoB/7 Adjoins

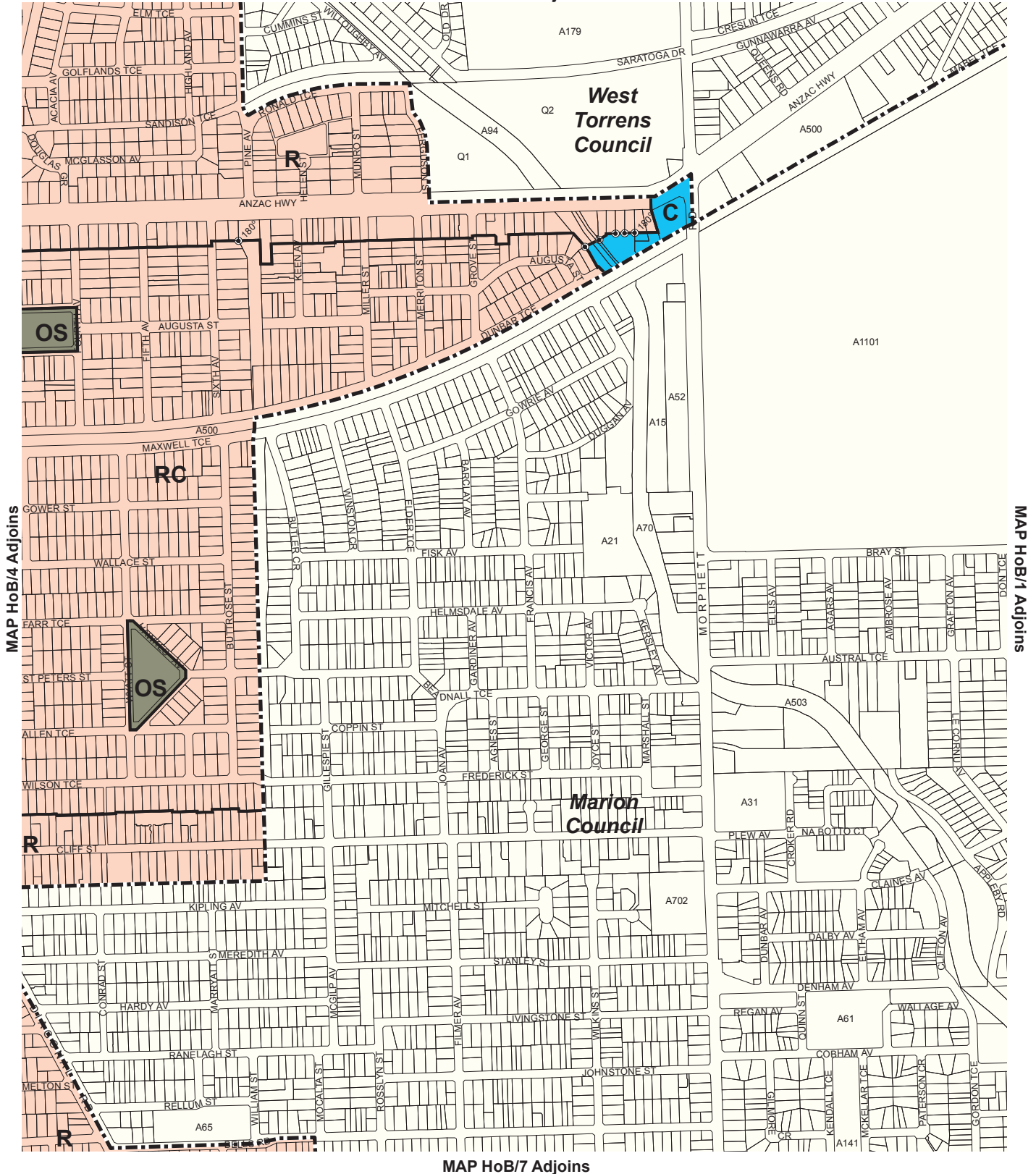


Overlay Map HoB/5 NATURAL RESOURCES

- MOSS Study Area
- Development Plan Boundary

HOLDFAST BAY COUNCIL
Consolidated - 26 November 2020

MAP HoB/3 Adjoins



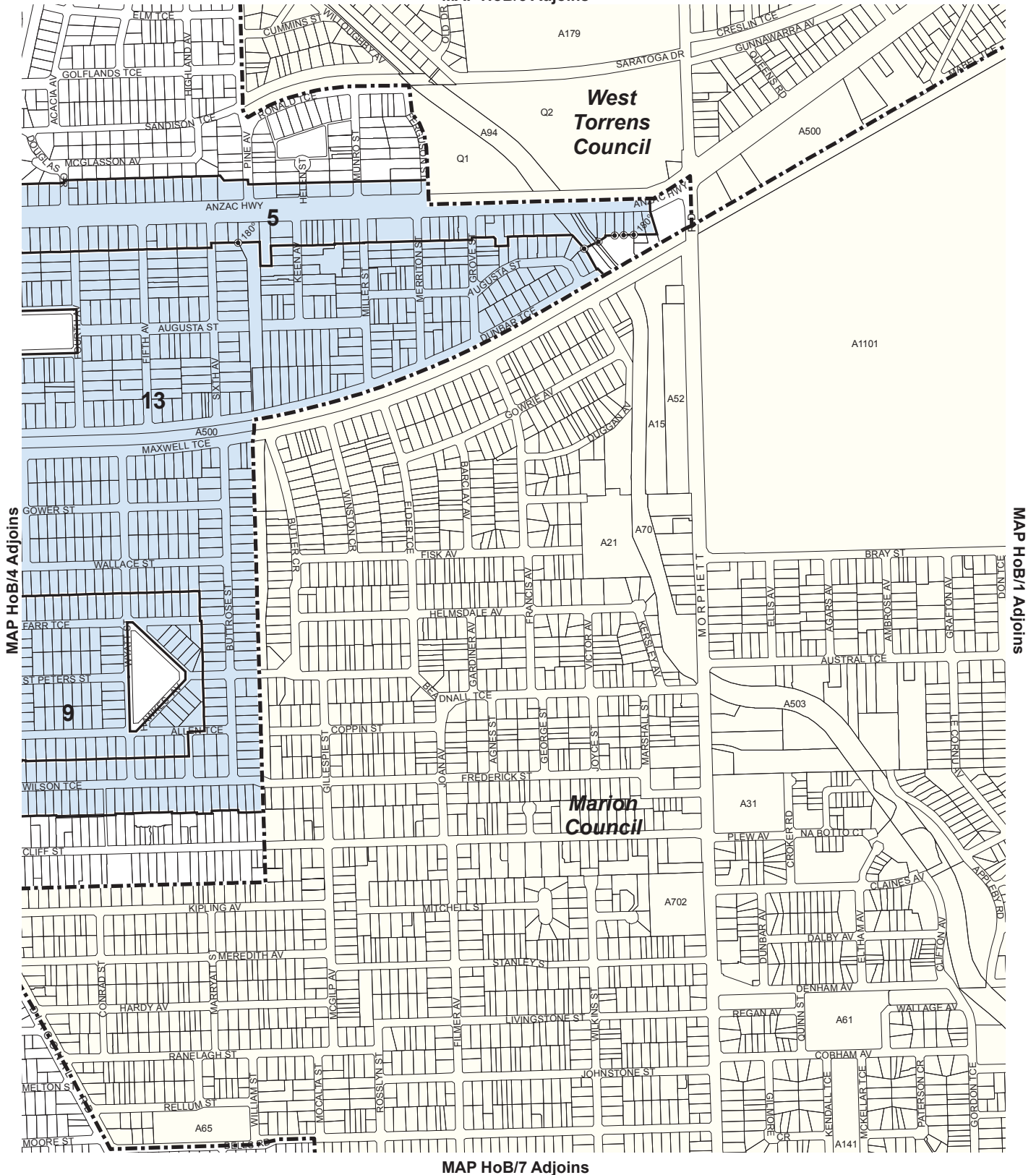
Lamberts Conformal Conic Projection, GDA94



- Zones**
- C Commercial
 - OS Open Space
 - R Residential
 - RC Residential Character
 - Zone Boundary
 - Development Plan Boundary

Zone Map HoB/5

MAP HoB/3 Adjoins



Lamberts Conformal Conic Projection, GDA94

Policy Area

- 13 Streetscape Character (Brighton and Glenelg East)
- 5 Medium Density
- 9 Da Costa Park

Policy Area Map HoB/5

- Policy Area Boundary
- Development Plan Boundary

MAP HoB/1 Adjoins

Land Not Within
a Council
Area (Metro)



MAP HoB/7 Adjoins

MAP HoB/8 Adjoins



- S** School
- P** Post Office
- H** Other Health Services
- H** Hospital
- Local Reserves**
- Development Plan Boundary**

Location Map HoB/6

MAP HoB/4 Adjoins

MAP HoB/7 Adjoins

MAP HoB/1 Adjoins

Land Not Within
a Council
Area (Metro)

MAP HoB/8 Adjoins

0 500m

- Primary Arterial Roads
- Secondary Arterial Roads
- Bikedirect Network
- Main Road
- Secondary Road - Bike Lane
- Secondary Road
- Off Road Sealed Path
- Public Transport
- Development Plan Boundary

Overlay Map HoB/6 TRANSPORT

HOLDFAST BAY COUNCIL
Consolidated - 26 November 2020

MAP HoB/4 Adjoins

Zone C

MAP HoB/7 Adjoins

MAP HoB/8 Adjoins

Land Not Within
a Council
Area (Metro)

0



500m

Airport Building Heights

- Zone C All Structures Exceeding 15 metres above existing ground level
- Zone D All Structures Exceeding 45 metres above existing ground level
- Zone E All Structures Exceeding 100 metres above existing ground level

 Airport Building Heights

Coastal Building and Site Levels (Australian Height Datum)

-  Min site level 3.2m - floor level 3.45m
-  Min site level 3m - floor level 3.25m

 Development Plan Boundary

Overlay Map HoB/6 DEVELOPMENT CONSTRAINTS

HOLDFAST BAY COUNCIL
Consolidated - 26 November 2020

MAP HoB/4 Adjoins

MAP HoB/7 Adjoins

MAP HoB/8 Adjoins

Land Not Within
a Council
Area (Metro)

Heritage points are indicative only.
For further information on State and Local Heritage Places and Contributory
Items please refer to the relevant tables within this document.



Overlay Map HoB/6 HERITAGE

- State heritage place
- Local heritage place
- Development Plan Boundary

MAP HoB/4 Adjoins

MAP HoB/1 Adjoins

MAP HoB/7 Adjoins

**Land Not Within
a Council
Area (Metro)**

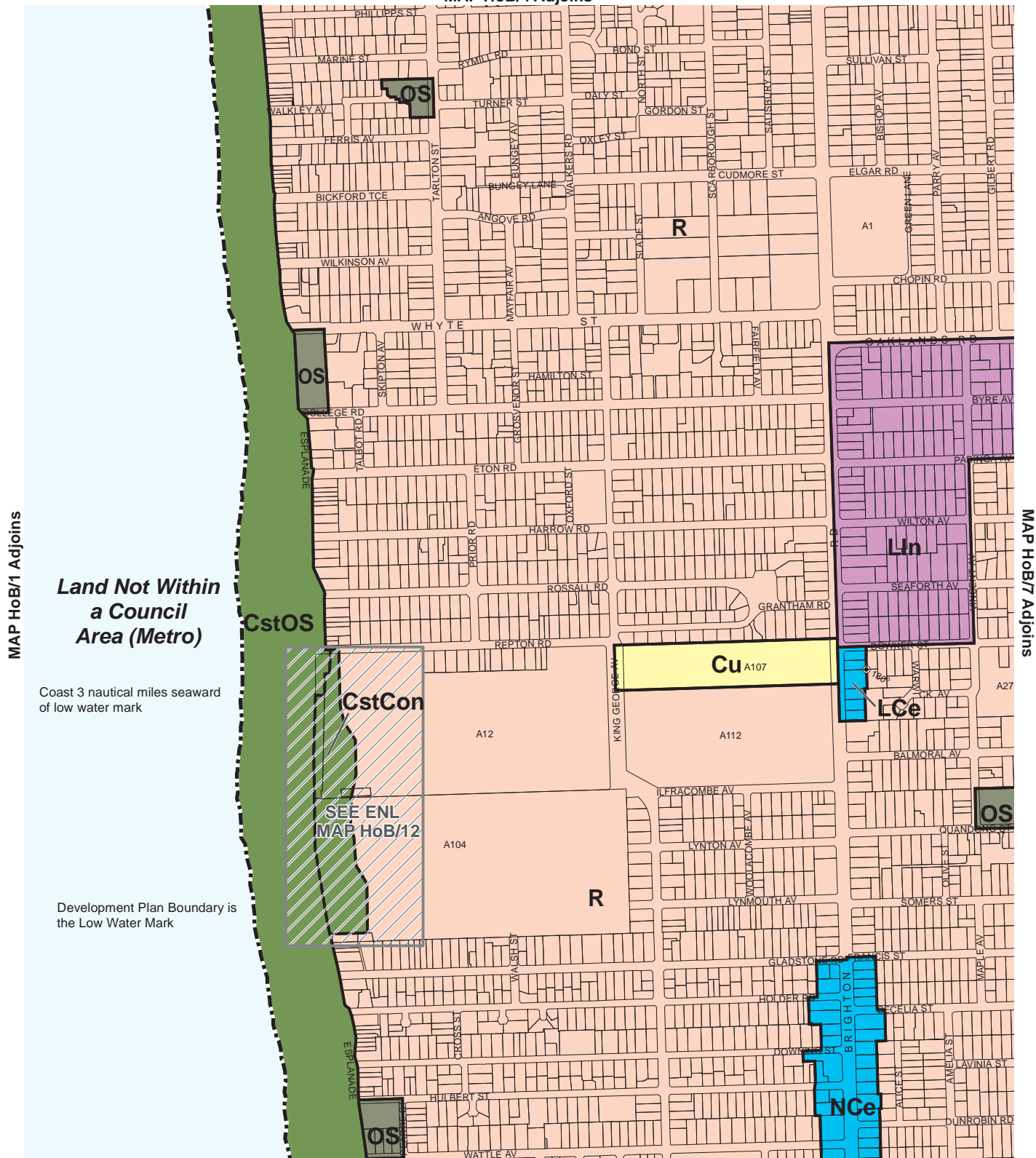
MAP HoB/8 Adjoins



Overlay Map HoB/6 NATURAL RESOURCES

- MOSS Study Area
- Development Plan Boundary

MAP HoB/4 Adjoins



See enlargement map for accurate representation.
Lamberts Conformal Conic Projection, GDA94



Zone Map HoB/6

MAP HoB/1 Adjoins

**Land Not Within
a Council
Area (Metro)**

Coast 3 nautical miles seaward
of low water mark

Development Plan Boundary is
the Low Water Mark

MAP HoB/4 Adjoins

MAP HoB/7 Adjoins

MAP HoB/8 Adjoins

See enlargement map for accurate representation.
Lamberts Conformal Conic Projection, GDA94

Policy Area

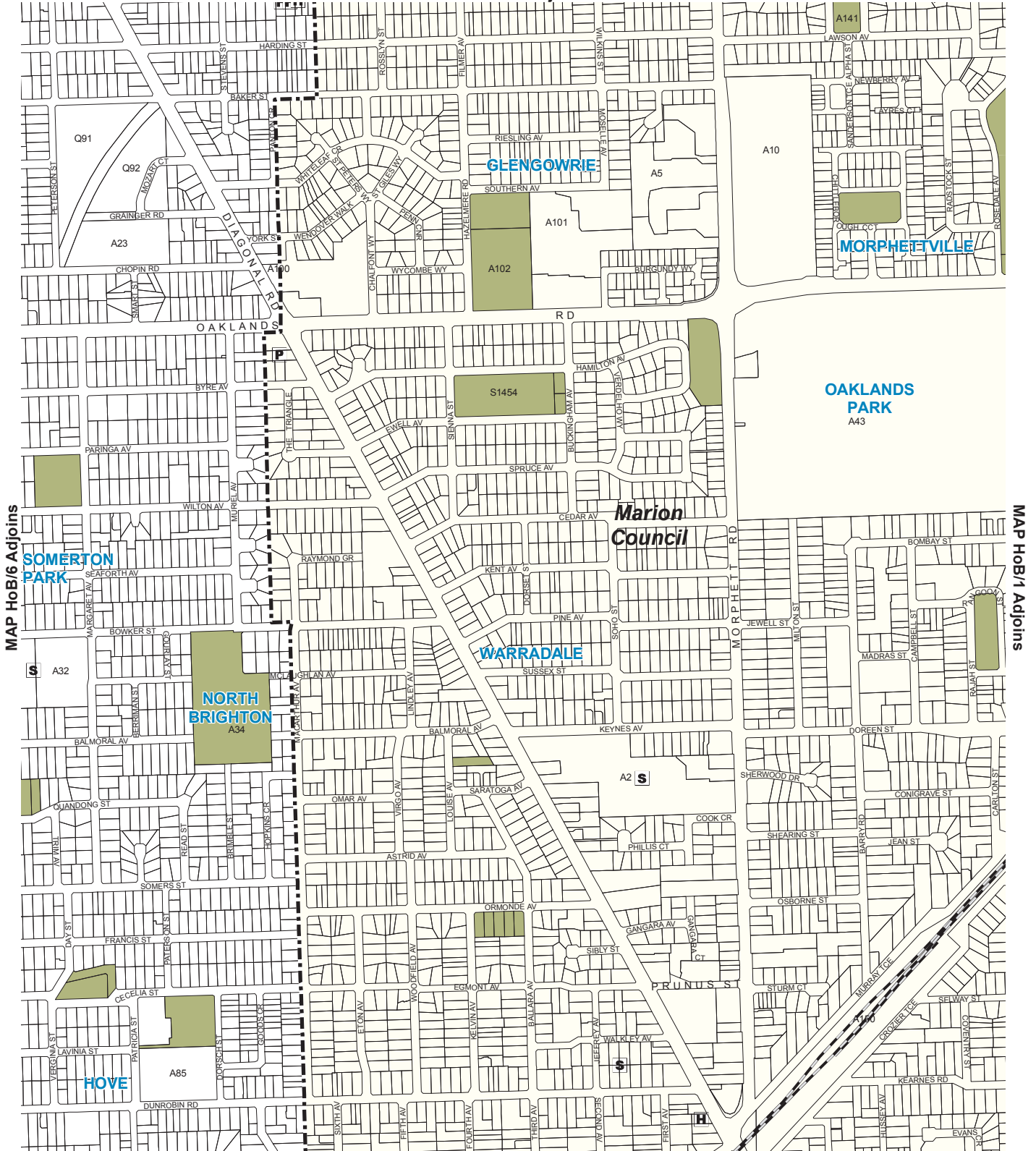
- 3 Central West
- 4 Institution
- 5 Medium Density



Policy Area Map HoB/6

- Policy Area Boundary
- Development Plan Boundary

MAP HoB/5 Adjoins



MAP HoB/9 Adjoins



- S** School
- P** Post Office
- H** Other Health Services
- Railways
- Local Reserves
- Development Plan Boundary





Location Map HoB/7

The map displays a comprehensive street network within the Marion Council jurisdiction. Major roads such as Oaklands Rd and Morphet Rd are highlighted in red. A diagonal road, Prunus St, runs from the top left towards the bottom right. The map is divided into numerous small lots and larger areas labeled with codes like Q91, Q92, A23, A10, A5, A101, A102, A43, A32, A34, A2, A85, and A150. The text "Marion Council" is centered on the map. The map also shows various parks and green spaces, some of which are labeled with numbers like Q91, Q92, A23, A10, A5, A101, A102, A43, A32, A34, A2, A85, and A150. The map is color-coded with various shades of blue, green, and yellow.

MAP HoB/6 Adjoins

MAP HOB/1 Adjoins

**Marion
Council**

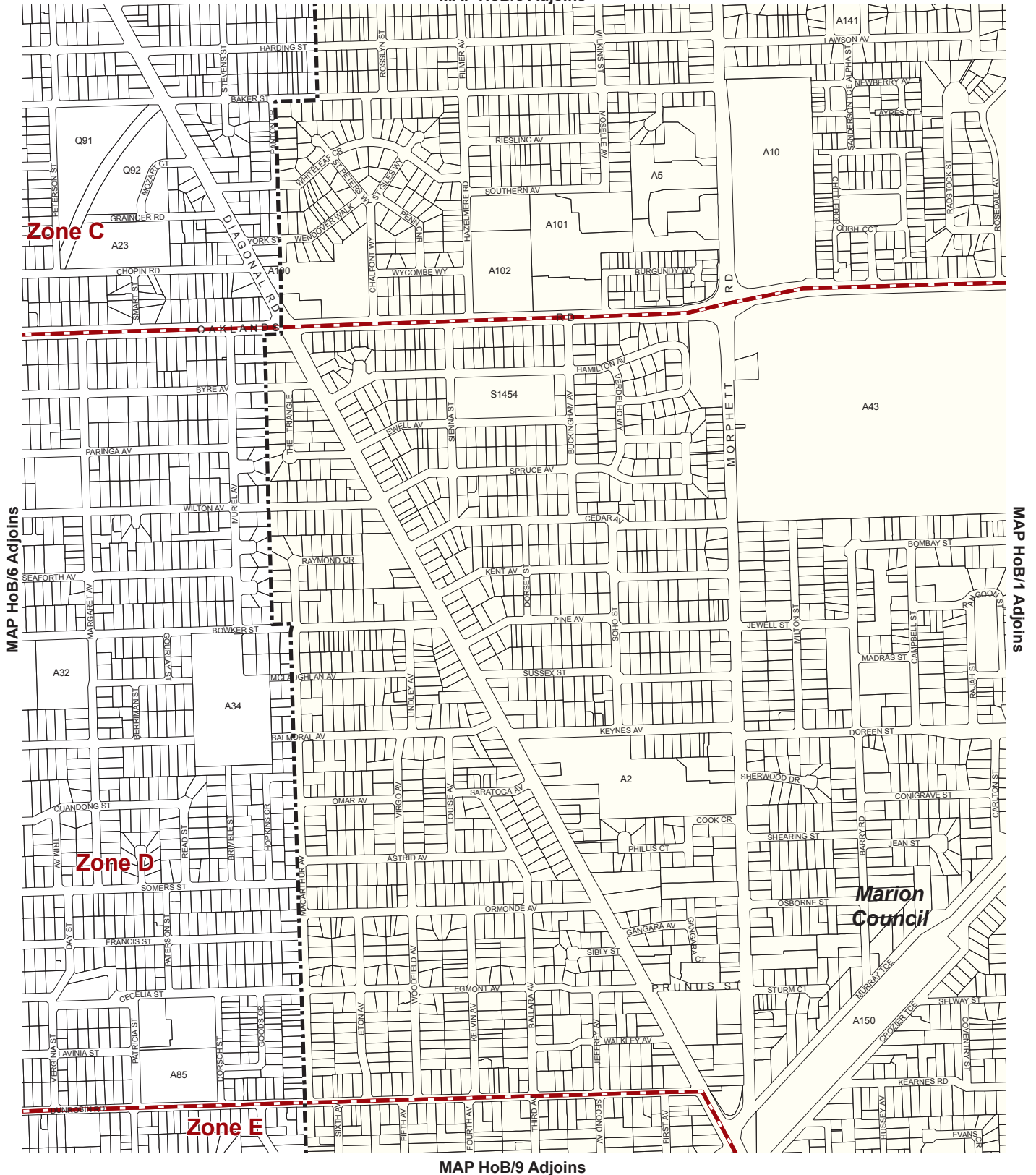
 Secondary Arterial Roads
 Bikedirect Network
 Main Road
 Secondary Road
 Off Road Sealed Path
 Off Road Track
 Public Transport
 Development Plan Boundary



Overlay Map HoB/7

HOLDFAST BAY COUNCIL
Consolidated - 26 November 2020

MAP HoB/5 Adjoins



Airport Building Heights


- Zone C** All Structures Exceeding 15 metres above existing ground level
- Zone D** All Structures Exceeding 45 metres above existing ground level
- Zone E** All Structures Exceeding 100 metres above existing ground level



Overlay Map HoB/7 DEVELOPMENT CONSTRAINTS

- Airport Building Heights
- Development Plan Boundary

HOLDFAST BAY COUNCIL
Consolidated - 26 November 2020

- Local heritage place
 Development Plan Boundary

MAP HoB/9 Adjoins



0 500

N

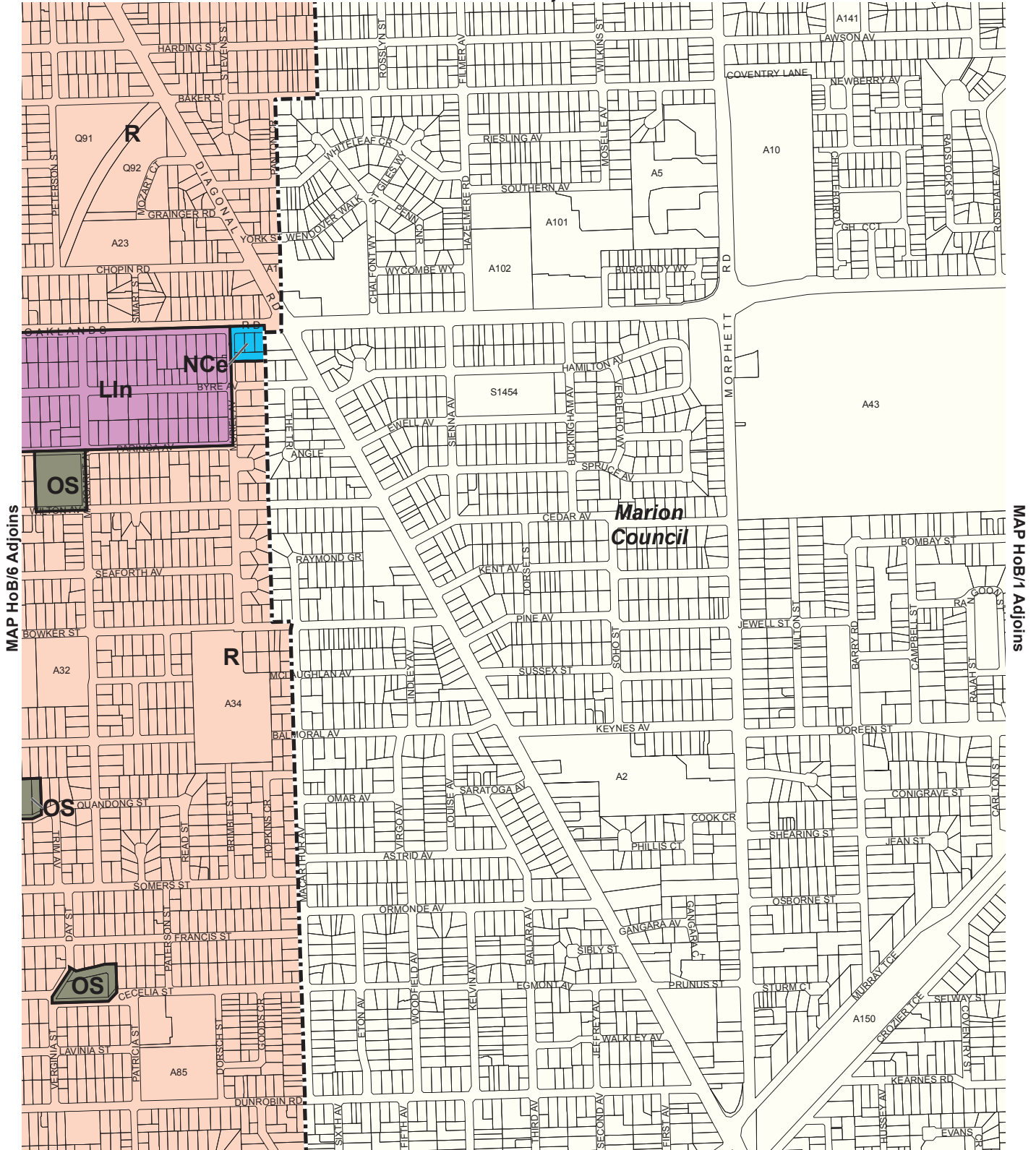
Overlay Map HoB/7

NATURAL RESOURCES

HOLDFAST BAY COUNCIL
Consolidated - 26 November 2020

 MOSS Study Area
 Development Plan Boundary

MAP HoB/5 Adjoins



Lamberts Conformal Conic Projection, GDA94

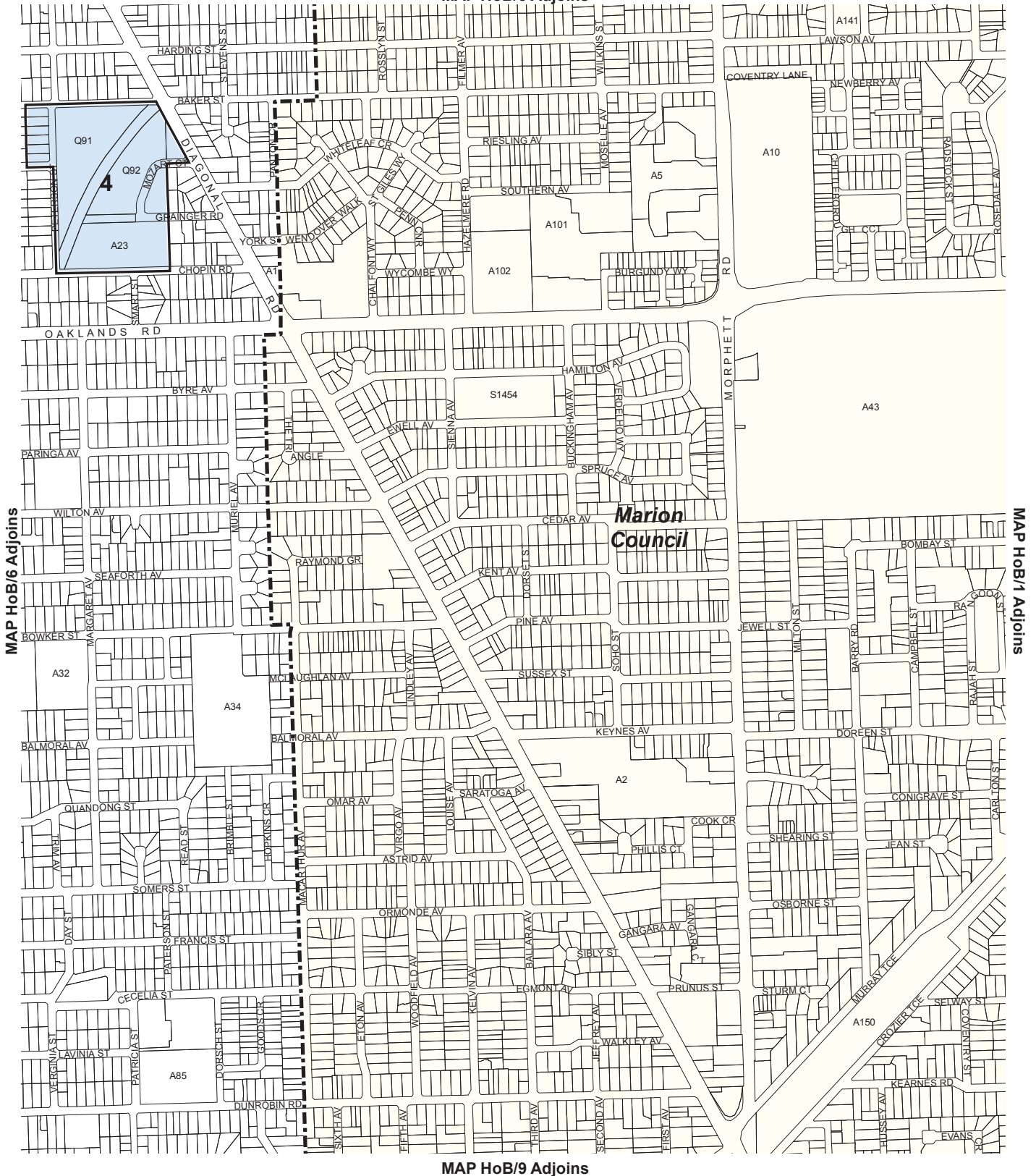


Zones

- LIn Light Industry
- NCe Neighbourhood Centre
- OS Open Space
- R Residential
- Zone Boundary
- Development Plan Boundary

Zone Map HoB/7

MAP HoB/5 Adjoins



Lamberts Conformal Conic Projection, GDA94

Policy Area
4 Institution



Policy Area Map HoB/7

- Policy Area Boundary
- Development Plan Boundary

MAP HoB/1 Adjoins

Land Not Within
a Council
Area (Metro)

MAP HoB/6 Adjoins

MAP HoB/9 Adjoins

MAP HoB/10 Adjoins



- S** School
- L** Public Library
- C** Council Office
- P** Post Office
- Railways
- Local Reserves
- Development Plan Boundary

Location Map HoB/8

MAP HoB/6 Adjoins

MAP HoB/1 Adjoins

Land Not Within
a Council
Area (Metro)

Brighton Jetty

MAP HoB/10 Adjoins

MAP HoB/9 Adjoins

- Primary Arterial Roads
- Secondary Arterial Roads
- Bikedirect Network**
- Main Road
- Secondary Road - Bike Lane
- Secondary Road
- Off Road Sealed Path
- Off Road Track
- Public Transport
- - - - - Development Plan Boundary

0 500m



Overlay Map HoB/8 TRANSPORT

HOLDFAST BAY COUNCIL
Consolidated - 26 November 2020

MAP HoB/6 Adjoins

MAP HoB/9 Adjoins

MAP HoB/10 Adjoins

MAP HoB/1 Adjoins

**Land Not Within
a Council
Area (Metro)**

Brighton Jetty

Zone E

Airport Building Heights

Zone E All Structures Exceeding 100 metres above existing ground level

0

500m



Airport Building Heights

Coastal Building and Site Levels (Australian Height Datum)



Min site level 3m - floor level 3.25m

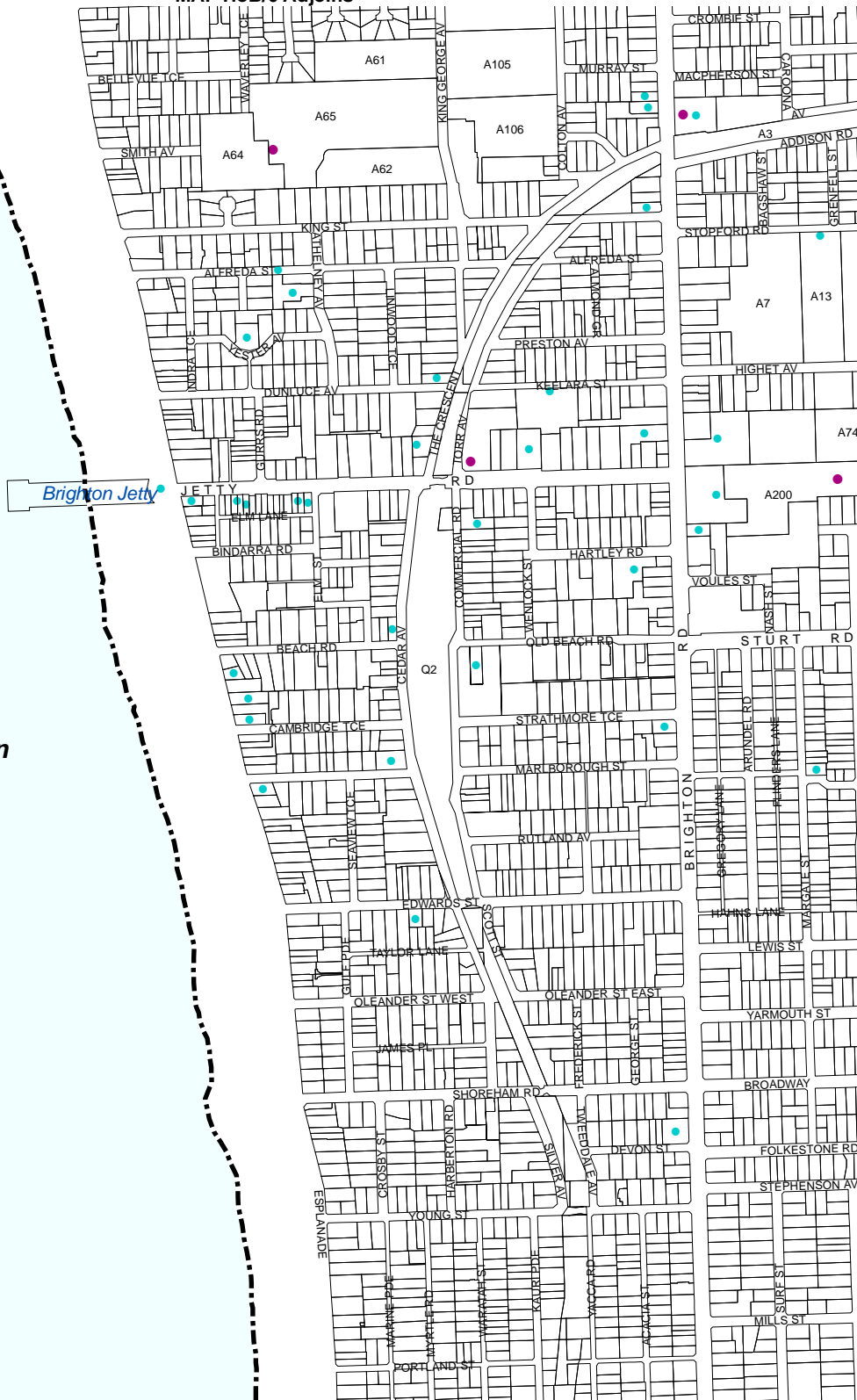


Development Plan Boundary

Overlay Map HoB/8

DEVELOPMENT CONSTRAINTS

MAP HoB/6 Adjoins



**Land Not Within
a Council
Area (Metro)**

Heritage points are indicative only.
For further information on State and Local Heritage Places and Contributory
Items please refer to the relevant tables within this document.



Overlay Map HoB/8 HERITAGE

- State heritage place
- Local heritage place
- Development Plan Boundary

MAP HoB/6 Adjoins

MAP HoB/1 Adjoins

**Land Not Within
a Council
Area (Metro)**

Brighton Jetty

MAP HoB/10 Adjoins

MAP HoB/9 Adjoins

0 500m

Overlay Map HoB/8 NATURAL RESOURCES

- MOSS Study Area
- Development Plan Boundary

HOLDFAST BAY COUNCIL
Consolidated - 26 November 2020

MAP HoB/6 Adjoins

MAP HoB/9 Adjoins

MAP HoB/10 Adjoins

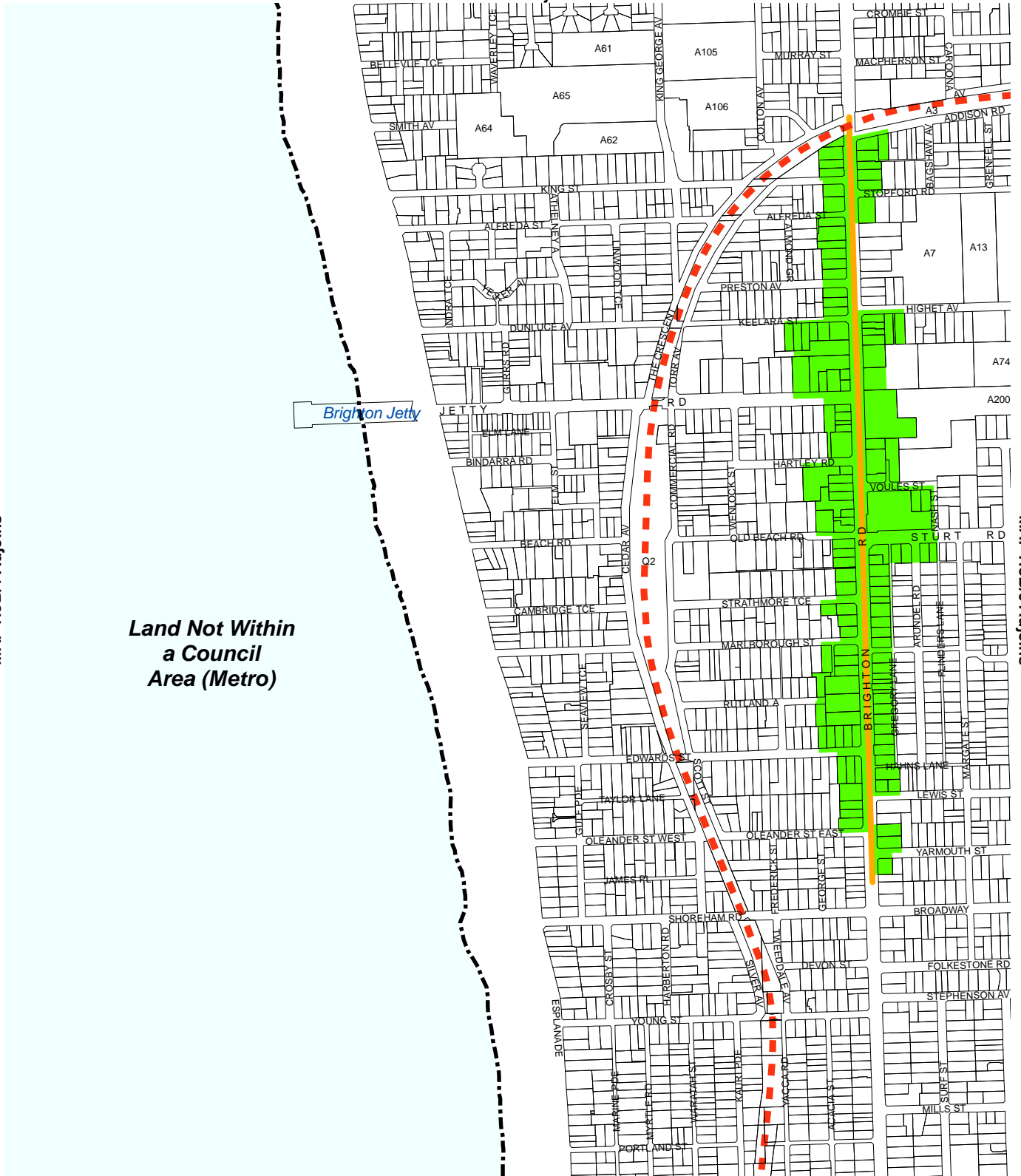
MAP HoB/1 Adjoins

Land Not Within
a Council
Area (Metro)

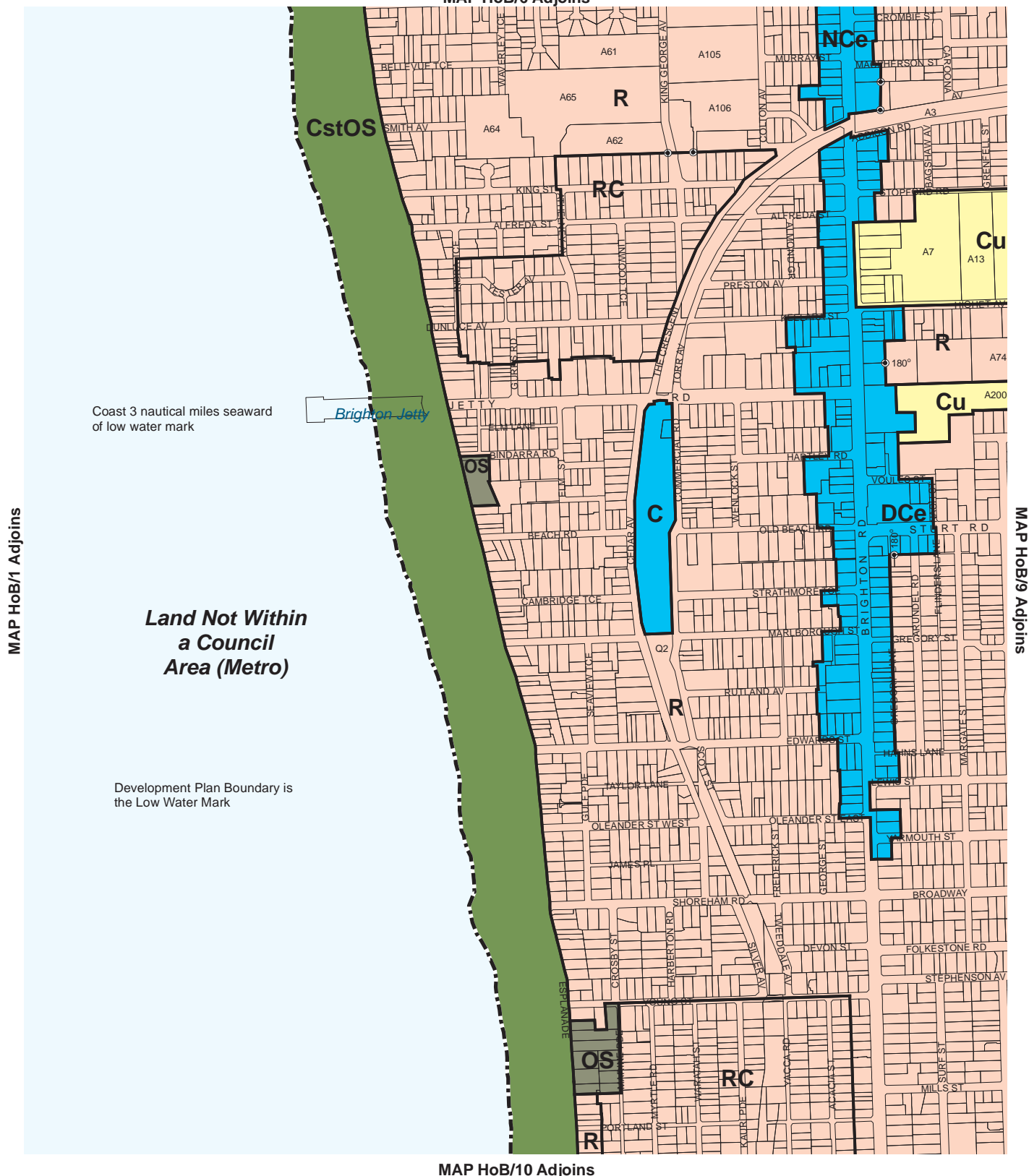
Brighton Jetty

- Train Corridor
- Designated Road: type B road
- Noise and Air Emissions Designated Area
- Development Plan Boundary

Overlay Map HoB/8 NOISE AND AIR EMISSIONS



MAP HoB/6 Adjoins



Lamberts Conformal Conic Projection, GDA94

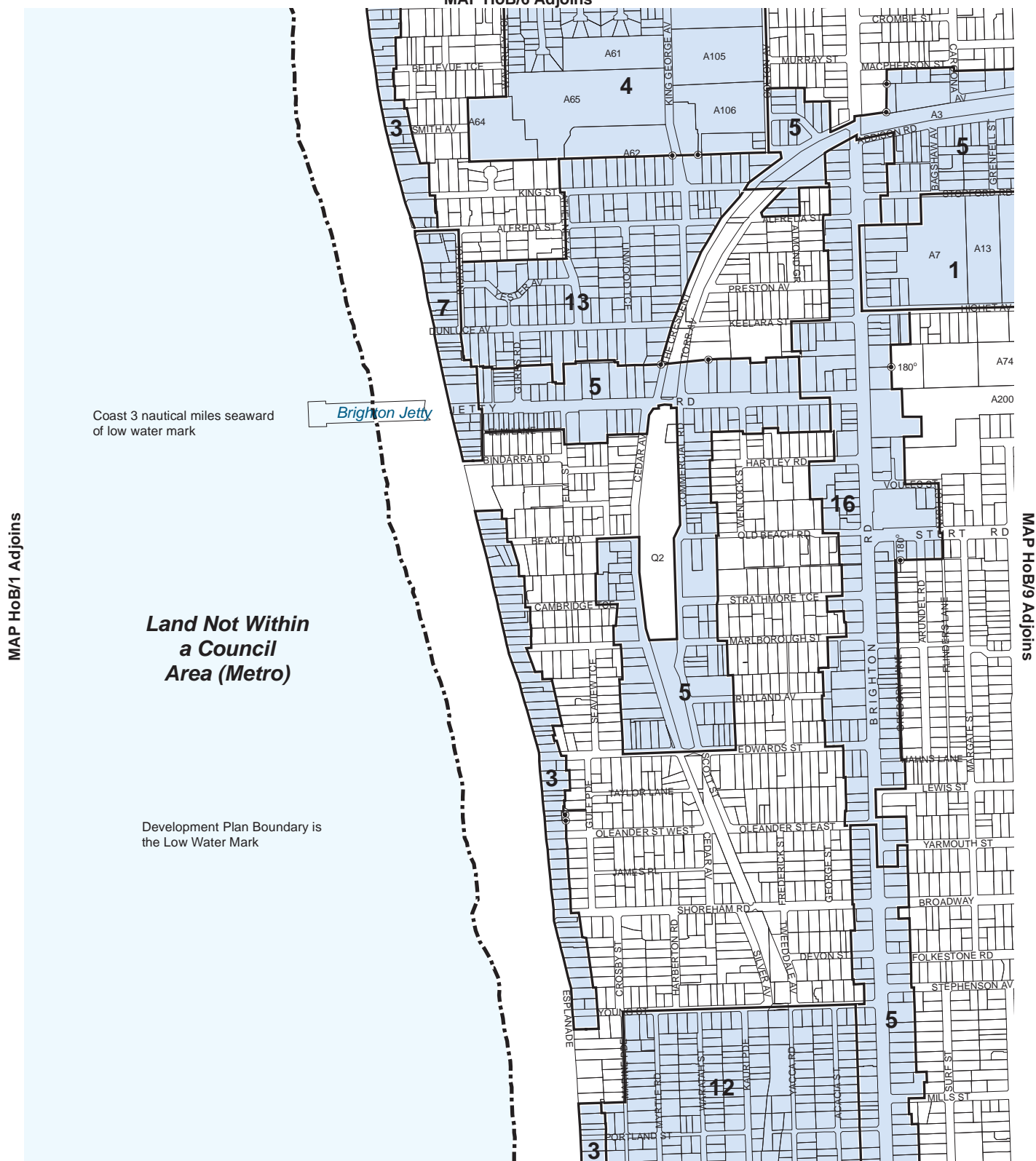
Zones

- CstOS** Coastal Open Space
- C** Commercial
- Cu** Community
- DCe** District Centre
- NCe** Neighbourhood Centre
- OS** Open Space
- R** Residential
- RC** Residential Character
- Zone Boundary**
- Development Plan Boundary**



Zone Map HoB/8

MAP HoB/6 Adjoins



Lamberts Conformal Conic Projection, GDA94

Policy Area

- 1 Recreation
- 12 Seacliff
- 13 Streetscape Character (Brighton and Glenelg East)
- 16 Brighton and Hove
- 3 Central West
- 4 Institution
- 5 Medium Density
- 7 South West



Policy Area Map HoB/8

- Policy Area Boundary
- Development Plan Boundary

[illegible]









- ## Location Map HoB/9

The map displays a comprehensive street network within the Marlon Council area. Key features include:

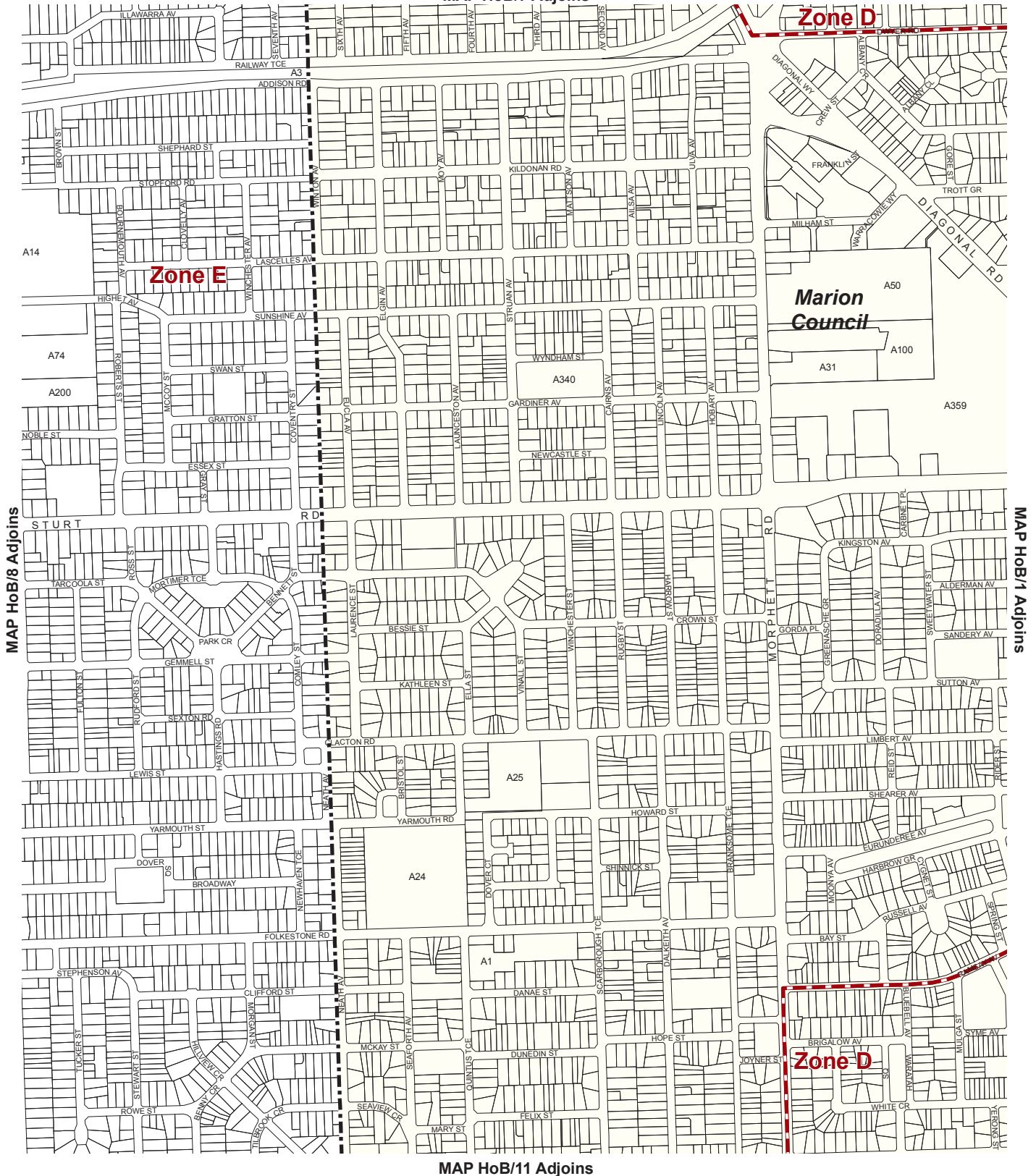
- Street Grid:** A dense network of streets is shown, with major roads like STURT RD and MORPHEIT RD highlighted in orange. Other roads are color-coded in pink or blue.
- Council Areas:** Various regions are labeled with codes such as A14, A74, A200, A340, A25, A24, A1, A50, A31, A100, and A359.
- Geographical Markers:** The text "Marlon Council" is prominently displayed in the center-right. A dashed line runs vertically through the middle of the map.
- Street Names:** Numerous individual streets are labeled, including ILLAWARRA AV, SHEPARD ST, KILDONAN RD, WYNCHAM ST, GARDINER AV, NEWCASTLE ST, BESSIE ST, KATHLEEN ST, YARMOUTH RD, and many others.

HOLDFAST BAY COUNCIL
Consolidated - 26 November 2020



-  Secondary Arterial Roads
 Bikedirect Network
 Main Road - Bike Lane
 Main Road
 Secondary Road
 Off Road Sealed Path
 Public Transport
 Development Plan Boundary

MAP HoB/7 Adjoins



Airport Building Heights

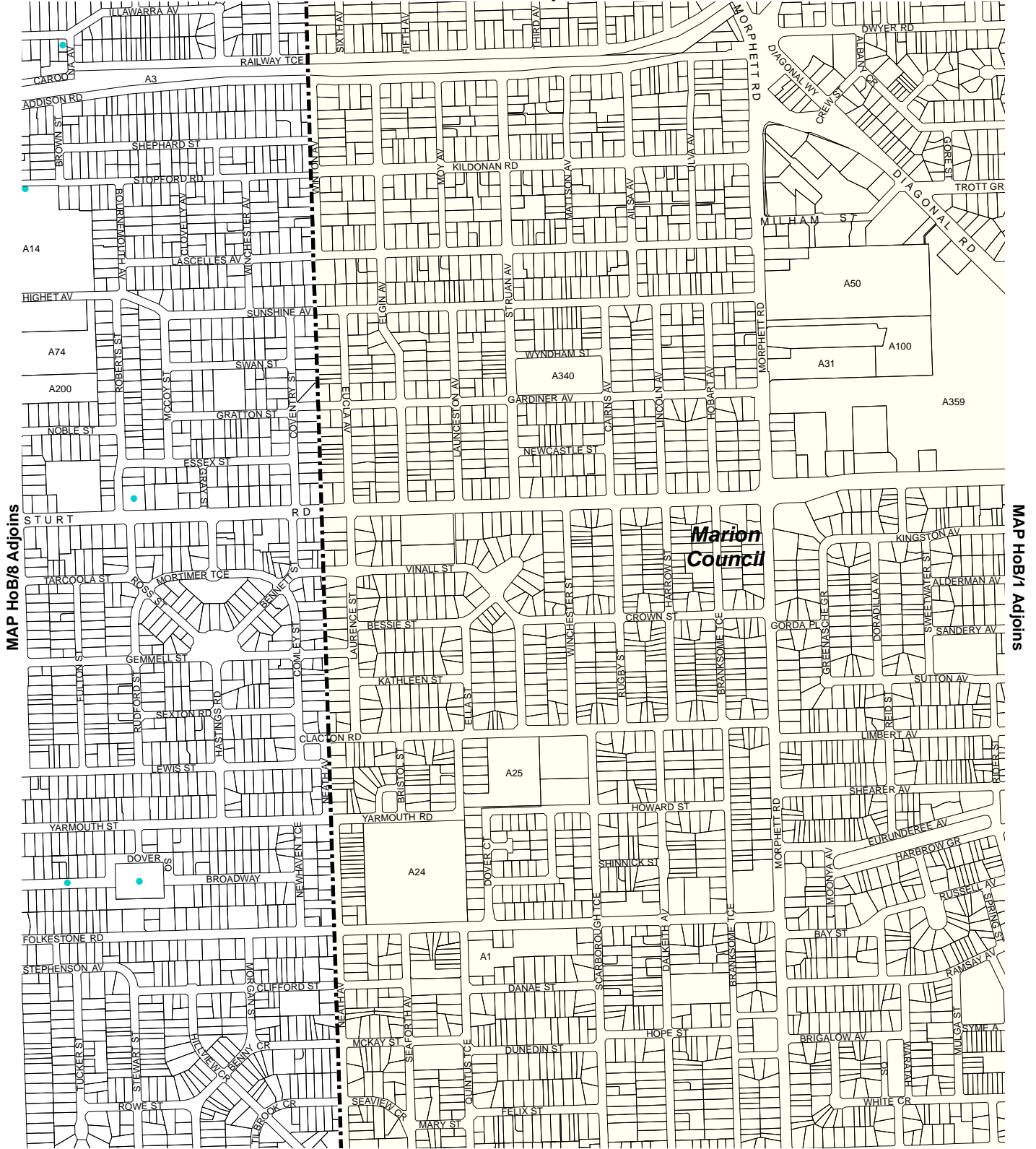
- Zone D** All Structures Exceeding 45 metres above existing ground level
- Zone E** All Structures Exceeding 100 metres above existing ground level



Overlay Map HoB/9 DEVELOPMENT CONSTRAINTS

- Airport Building Heights
- Development Plan Boundary

MAP HoB/7 Adjoins



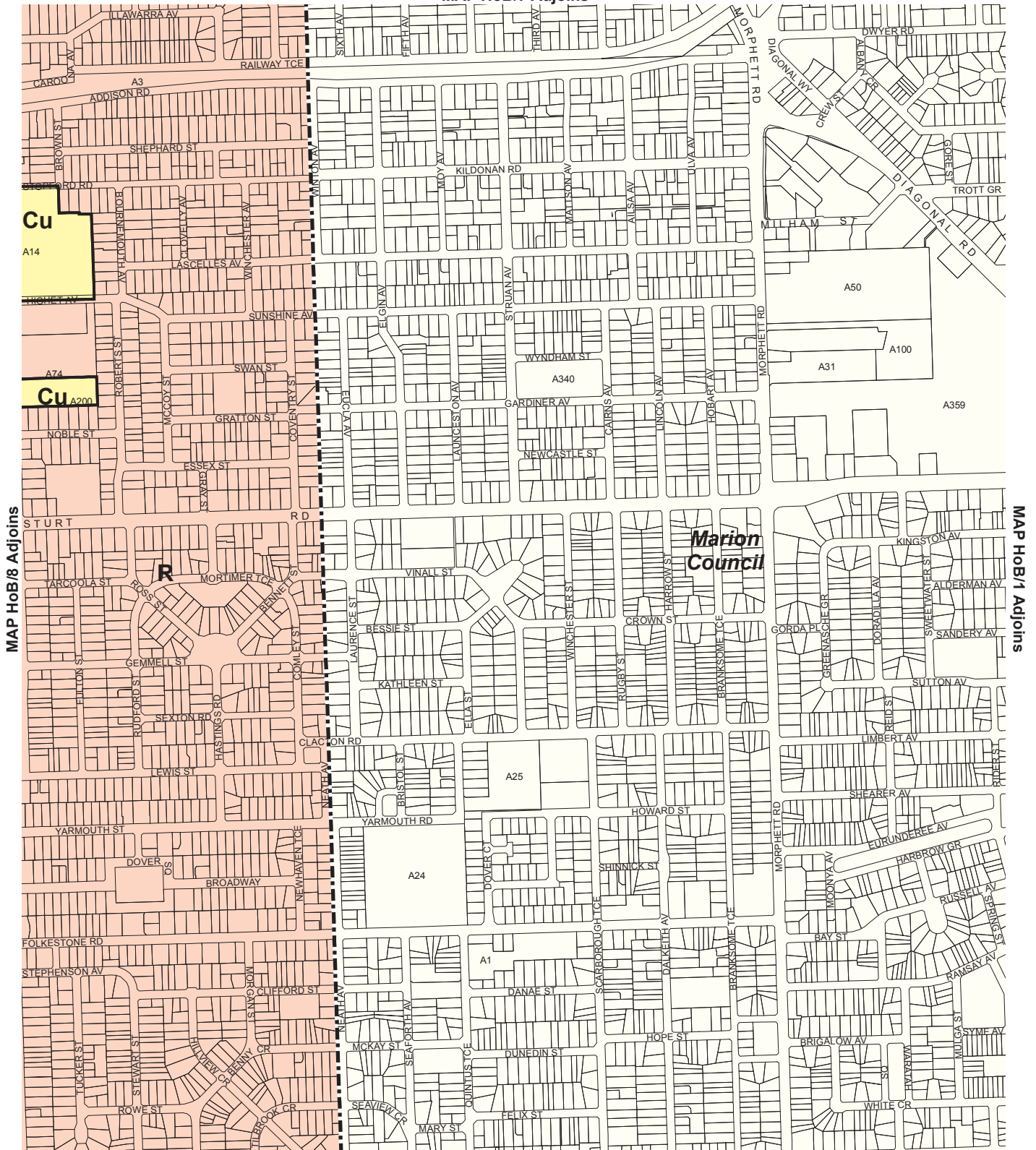
Heritage points are indicative only.
For further information on State and Local Heritage Places and Contributory Items please refer to the relevant tables within this document.



Overlay Map HoB/9 HERITAGE

- Local heritage place
- Development Plan Boundary

MAP HoB/7 Adjoins



Lamberts Conformal Conic Projection, GDA94

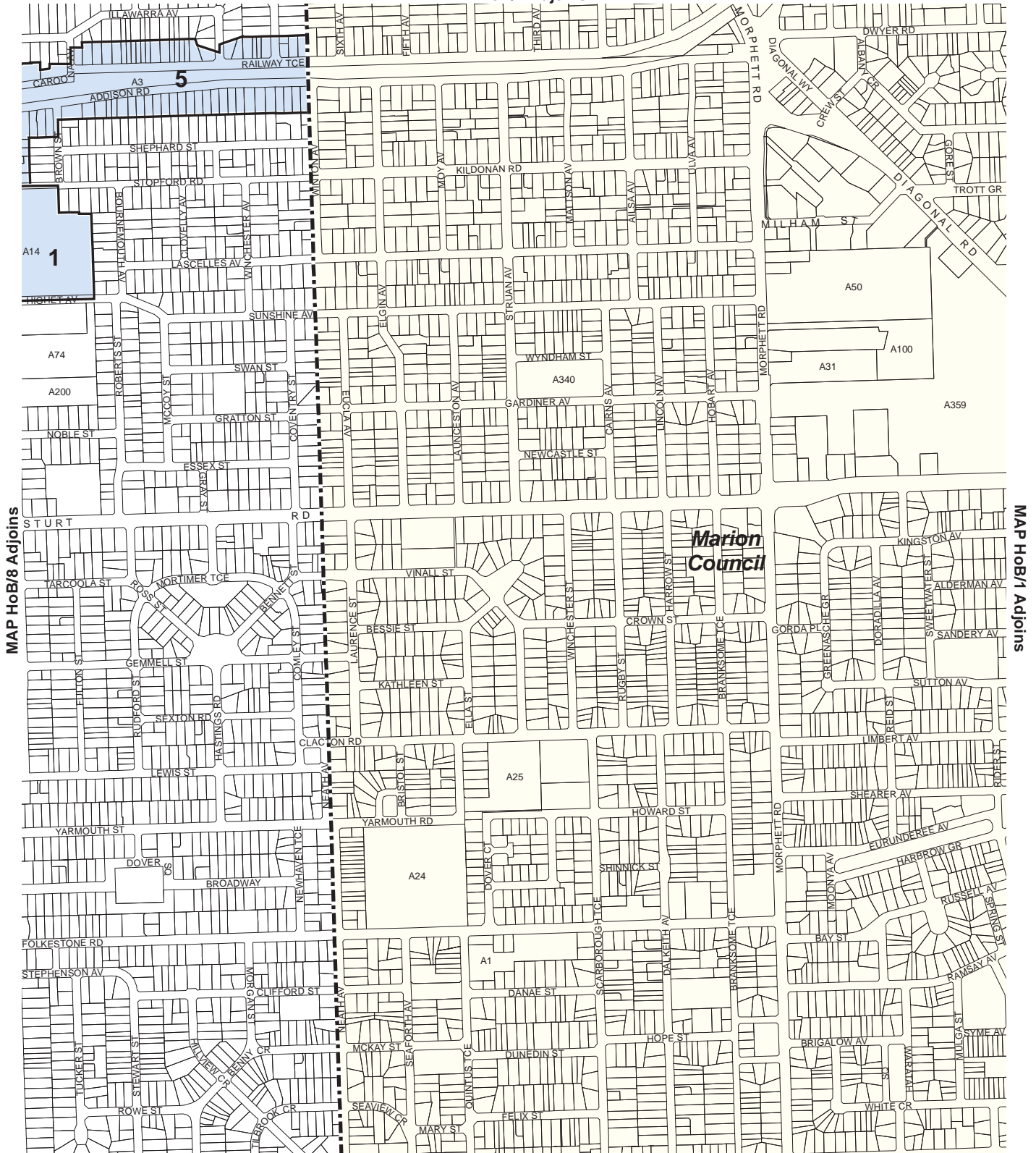


Zones

- Cu Community
- R Residential
- Zone Boundary
- Development Plan Boundary

Zone Map HoB/9

MAP HoB/7 Adjoins



MAP HoB/11 Adjoins

Lamberts Conformal Conic Projection, GDA94

Policy Area

- 1 Recreation
- 5 Medium Density



Policy Area Map HoB/9

- Policy Area Boundary
- Development Plan Boundary

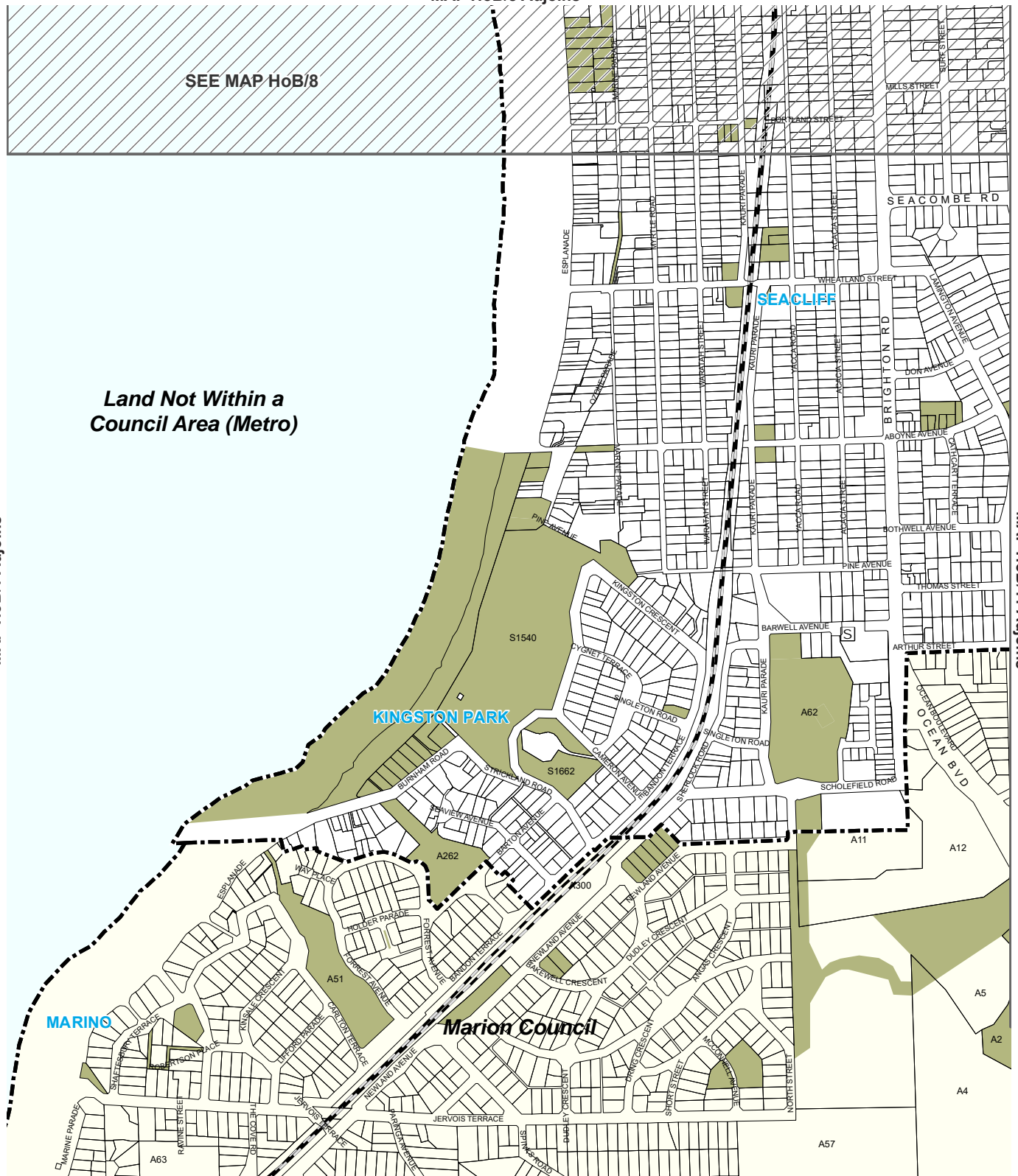
MAP HoB/8 Adjoins

SEE MAP HoB/8

Land Not Within a
Council Area (Metro)

MAP HoB/1 Adjoins

MAP HoB/1 Adjoins



MAP HoB/1 Adjoins



Location Map HoB/10

- School
- Railways
- Local Reserves
- Development Plan Boundary

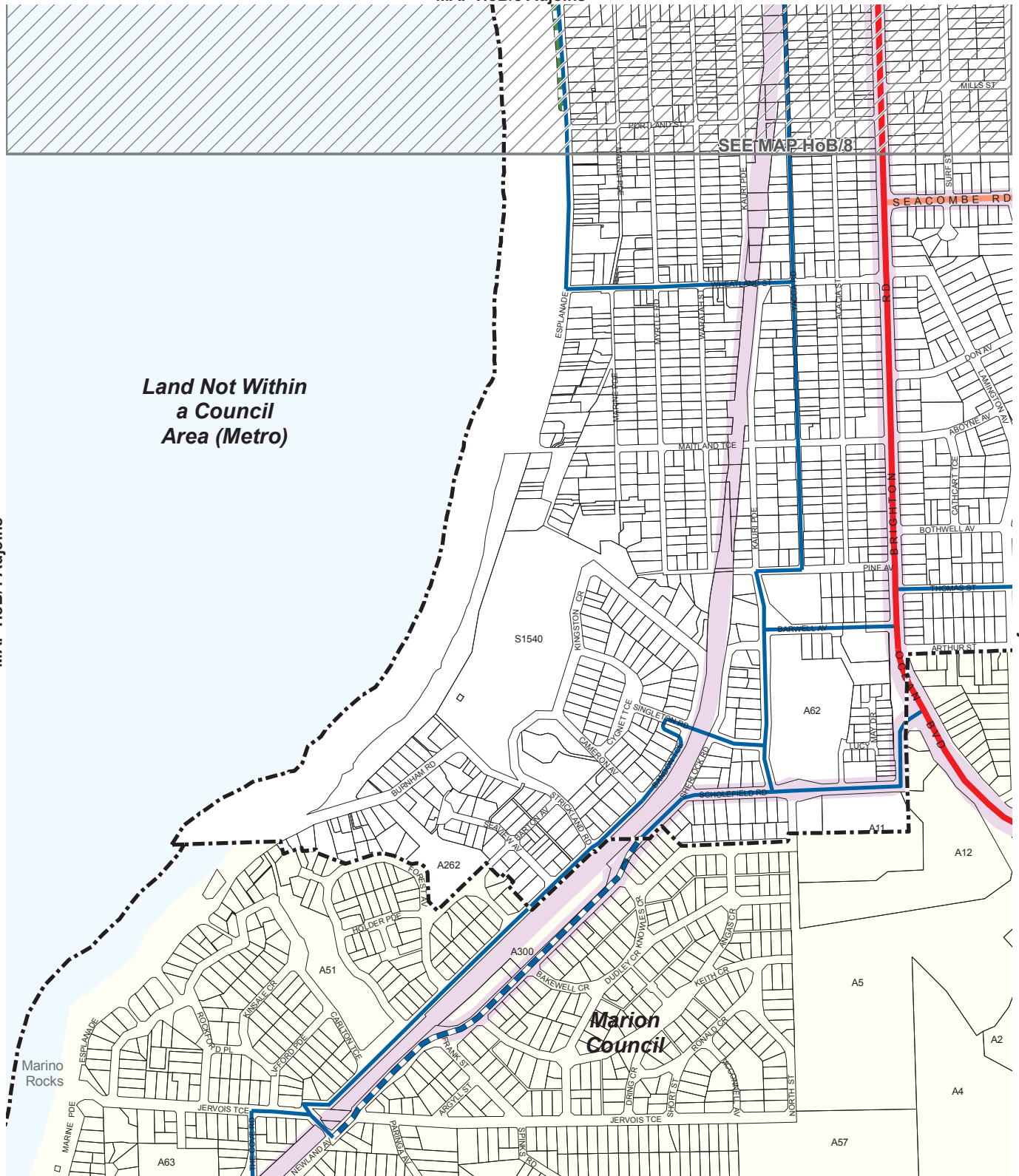
MAP HoB/8 Adjoins

SEE MAP HoB/8

Land Not Within
a Council
Area (Metro)

MAP HoB/1 Adjoins

MAP HoB/11 Adjoins



MAP HoB/1 Adjoins

- Primary Arterial Roads
- Secondary Arterial Roads
- Bikedirect Network**
- Main Road - Bike Lane
- Main Road
- Secondary Road - Bike Lane
- Secondary Road
- Off Road Sealed Path
- Public Transport
- - - Development Plan Boundary



Overlay Map HoB/10

TRANSPORT

HOLDFAST BAY COUNCIL
Consolidated - 26 November 2020

MAP HoB/8 Adjoins

SEE MAP HoB/8

Zone E

Zone D

Zone F

Marion Council

MAP HoB/1 Adjoins

Airport Building Heights

- Zone D All Structures Exceeding 45 metres above existing ground level
- Zone E All Structures Exceeding 100 metres above existing ground level
- Zone F APPROVAL REQUIRED FOR ALL STRUCTURES OVER 30m

0

500m



Airport Building Heights

Coastal Building and Site Levels (Australian Height Datum)



Min site level 3m - floor level 3.25m



Development Plan Boundary

Overlay Map HoB/10 DEVELOPMENT CONSTRAINTS

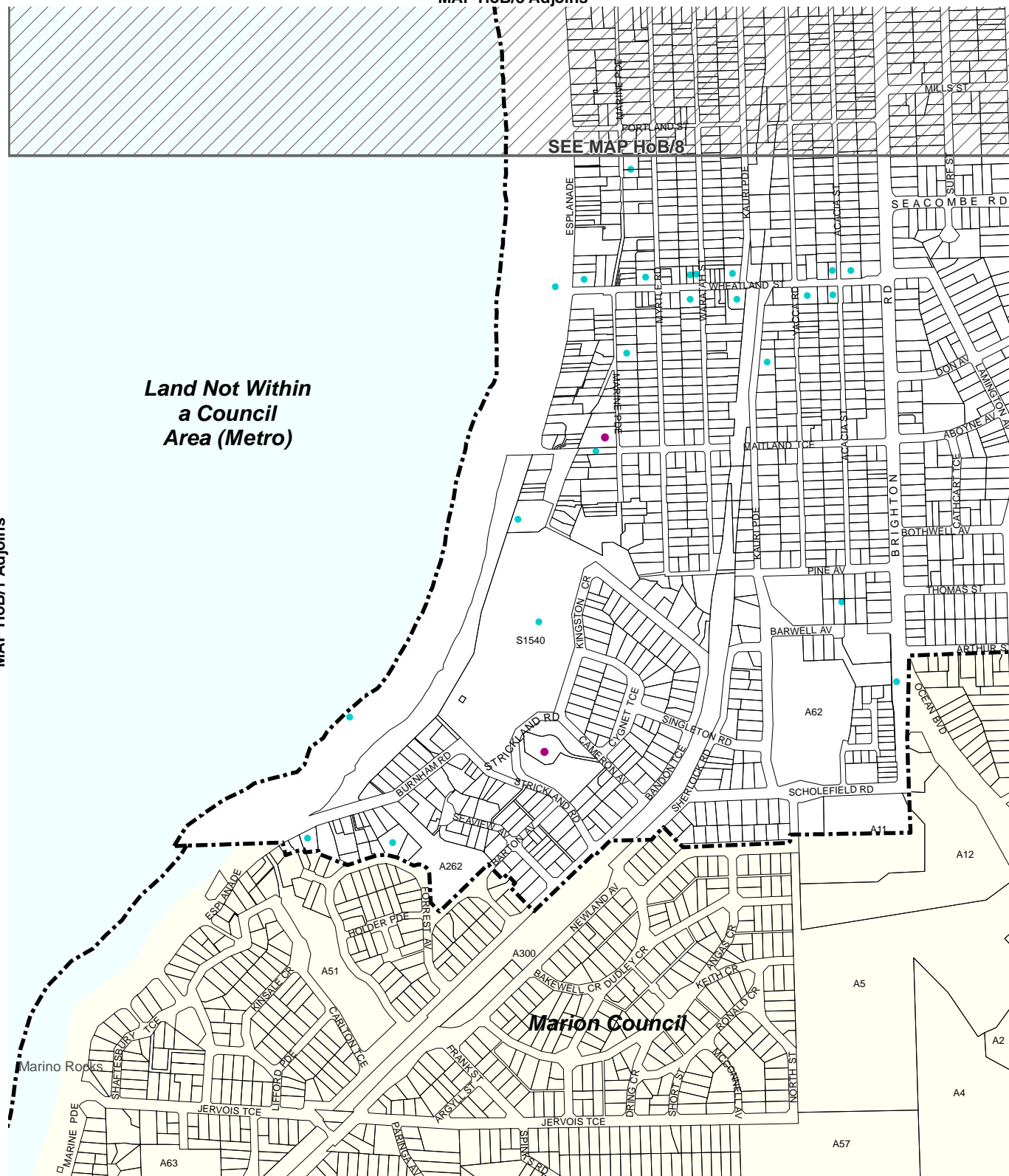
MAP HoB/8 Adjoins

SEE MAP HoB/8

Land Not Within
a Council
Area (Metro)

MAP HoB/1 Adjoins

MAP HoB/1 Adjoins



MAP HoB/1 Adjoins

Heritage points are indicative only.
For further information on State and Local Heritage Places and Contributory
Items please refer to the relevant tables within this document.



Overlay Map HoB/10

HERITAGE

- State heritage place
- Local heritage place
- Development Plan Boundary

MAP HoB/1 Adjoins

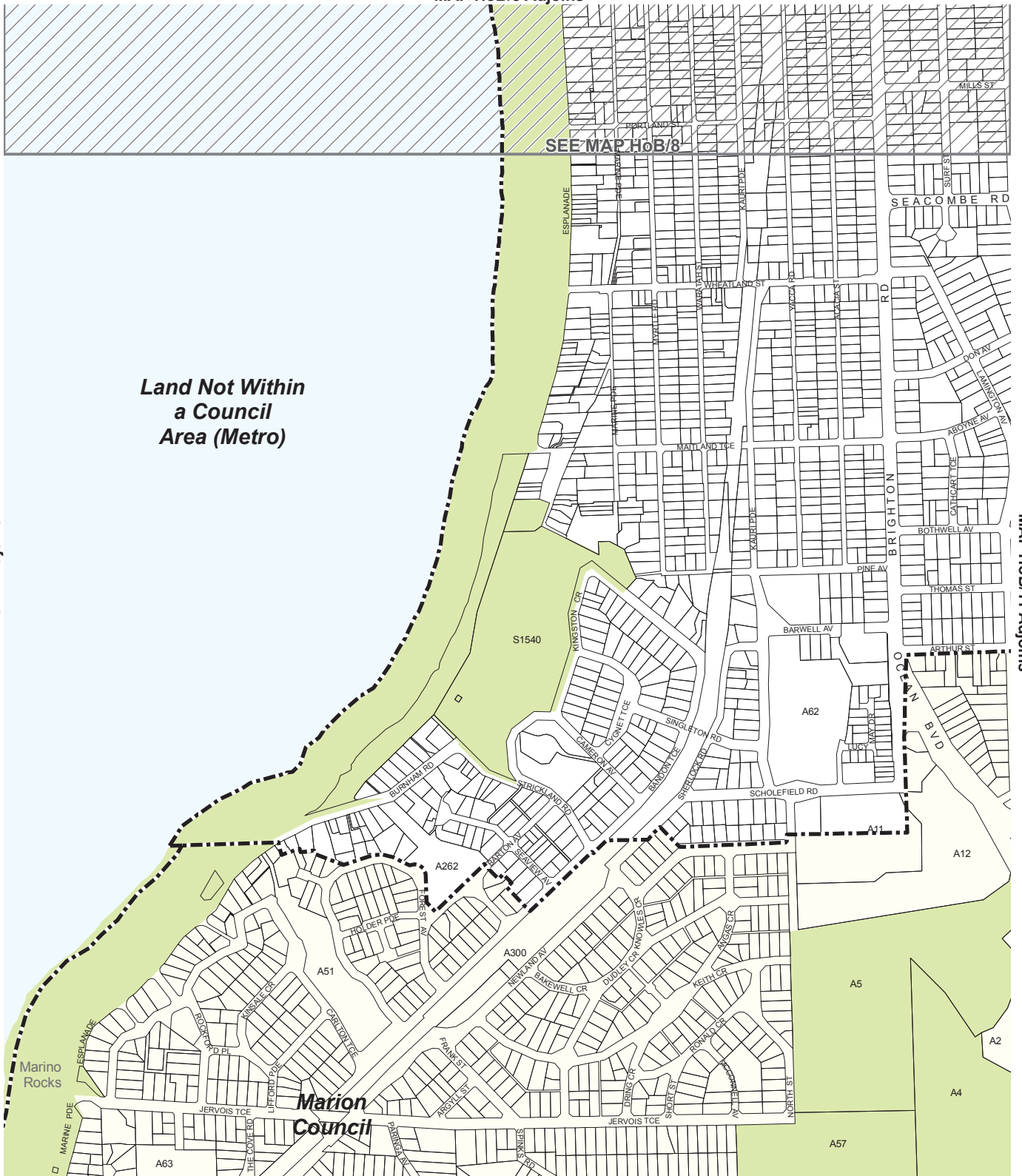
MAP HOB/11 Adinins

MAP HoB/1 Adjoins

Overlay Map HoB/10

NATURAL RESOURCES

HOLDFAST BAY COUNCIL
Consolidated - 26 November 2020



- MOSS Study Area
 Development Plan Boundary

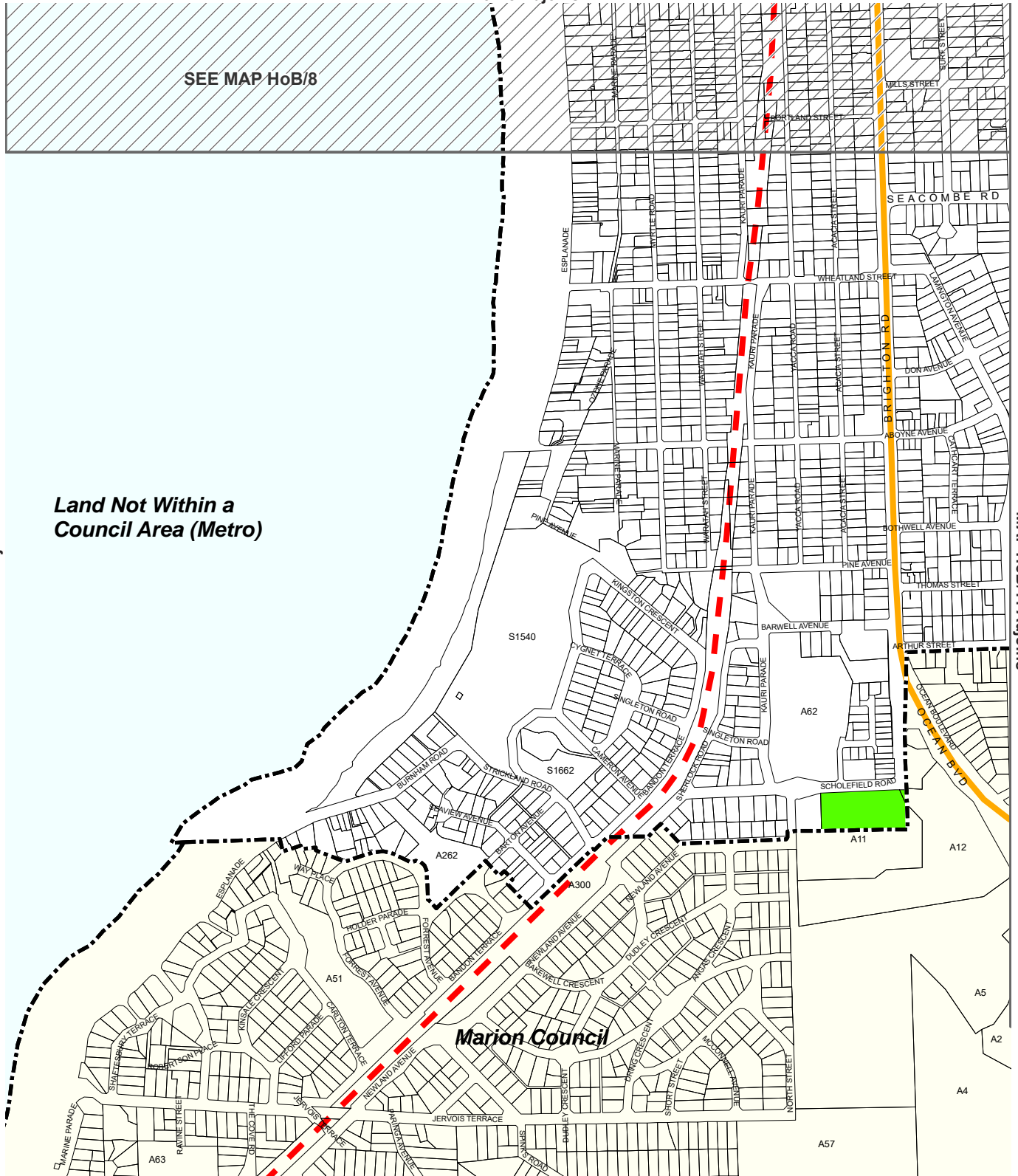
MAP HoB/8 Adjoins

SEE MAP HoB/8

Land Not Within a
Council Area (Metro)

MAP HoB/1 Adjoins

MAP HoB/1 Adjoins



MAP HoB/1 Adjoins



- Train Line
- Designated Road: type B road
- Noise and Air Emissions Designated Area
- Development Plan Boundary

Overlay Map HoB/10 NOISE AND AIR EMISSIONS

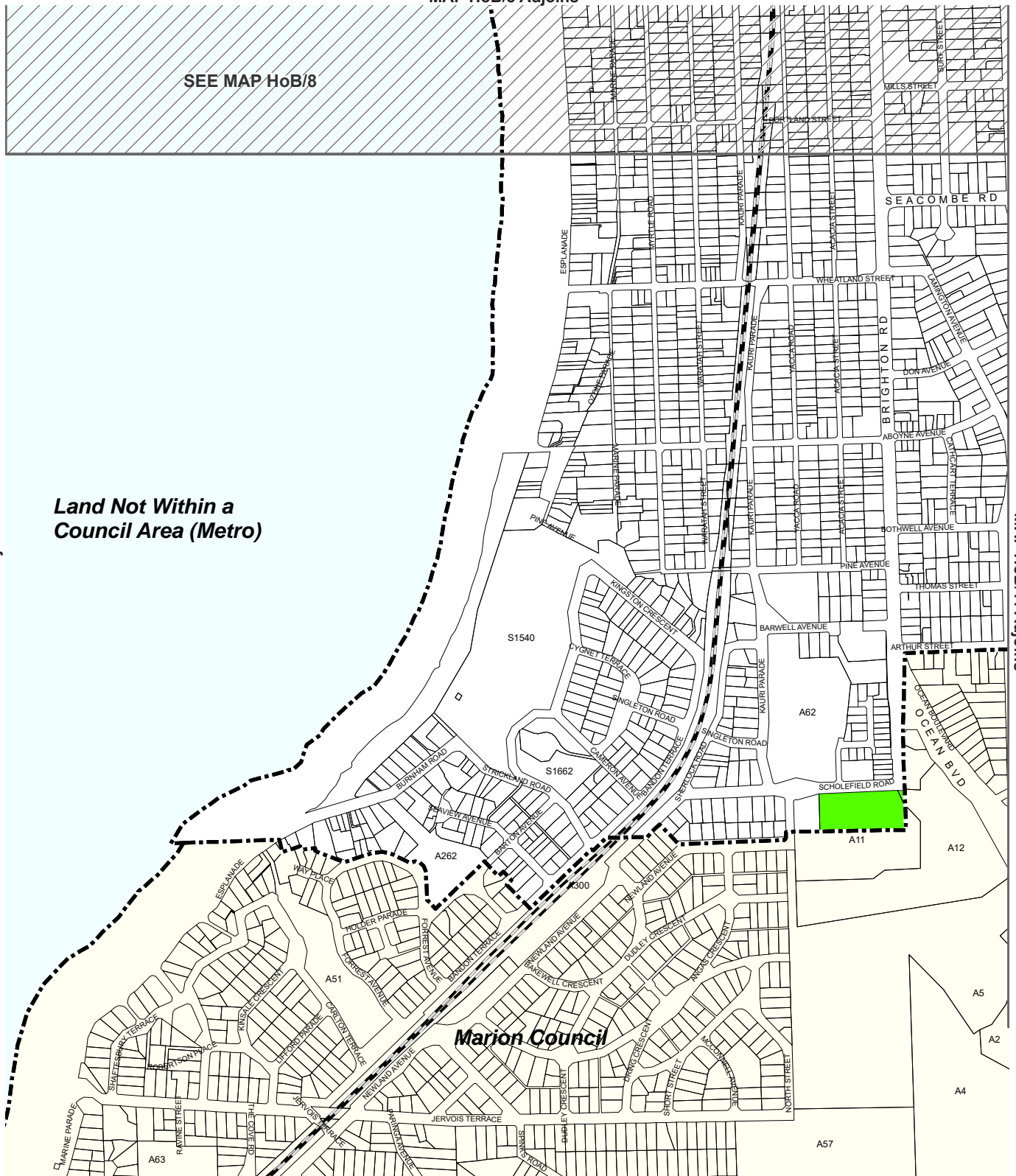
MAP HoB/8 Adjoins

SEE MAP HoB/8

Land Not Within a
Council Area (Metro)

MAP HoB/1 Adjoins

MAP HoB/1 Adjoins



MAP HoB/1 Adjoins



Overlay Map HoB/10

AFFORDABLE HOUSING

- Affordable Housing Designated Area
- Railways
- Development Plan Boundary

MAP HoB/8 Adjoins

SEE MAP HoB/8

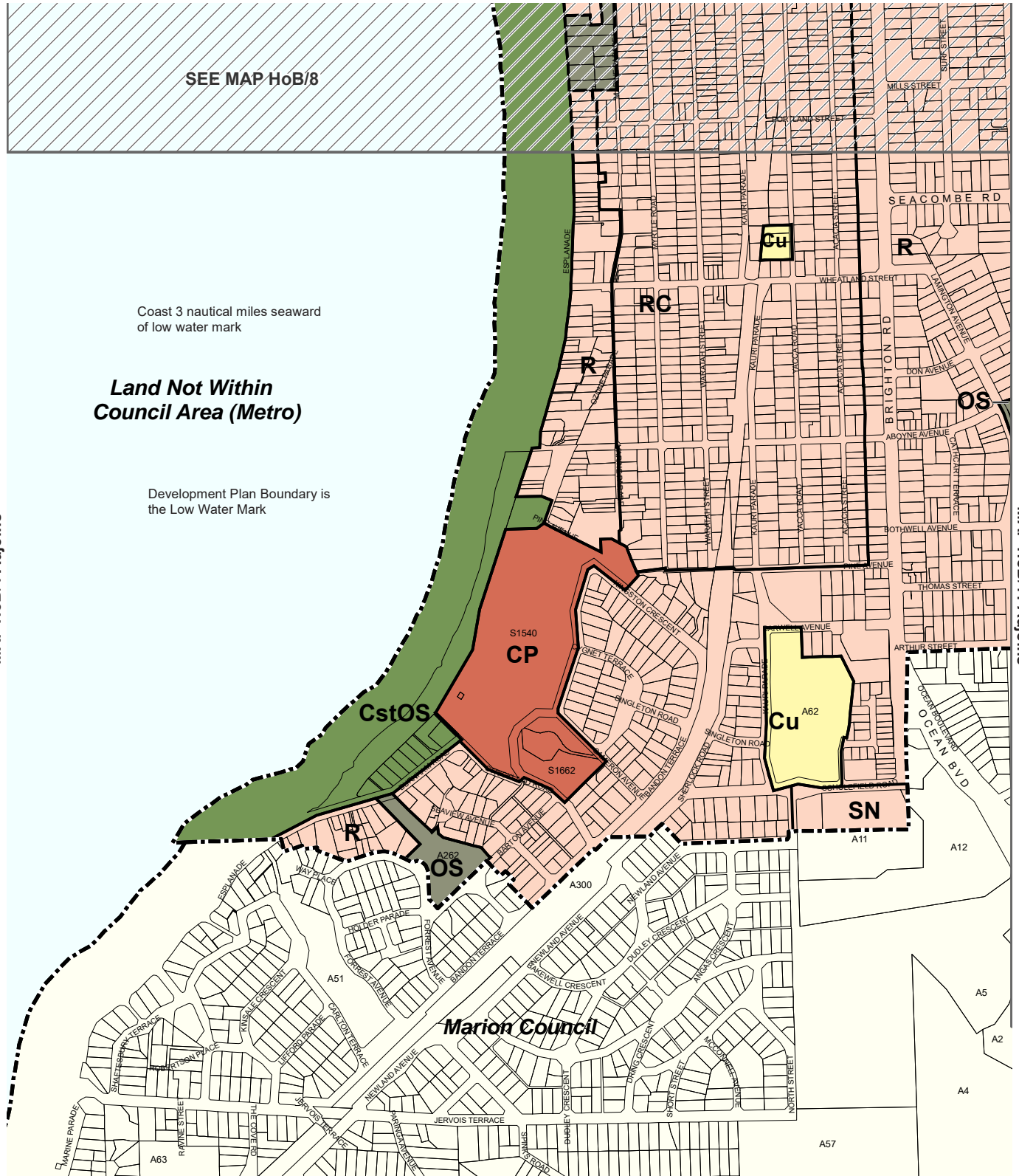
Coast 3 nautical miles seaward
of low water mark

**Land Not Within
Council Area (Metro)**

Development Plan Boundary is
the Low Water Mark

MAP HoB/1 Adjoins

MAP HoB/11 Adjoins



See enlargement map for accurate representation.
Lamberts Conformal Conic Projection, GDA94

Zones

- CP** Caravan and Tourist Park
- CstOS** Coastal Open Space
- Cu** Community
- OS** Open Space
- R** Residential
- RC** Residential Character
- SN** Suburban Neighbourhood
- Zone Boundary**
- Development Plan Boundary**

0



500m



Zone Map HoB/10

MAP HoB/8 Adjoins

SEE MAP HoB/8

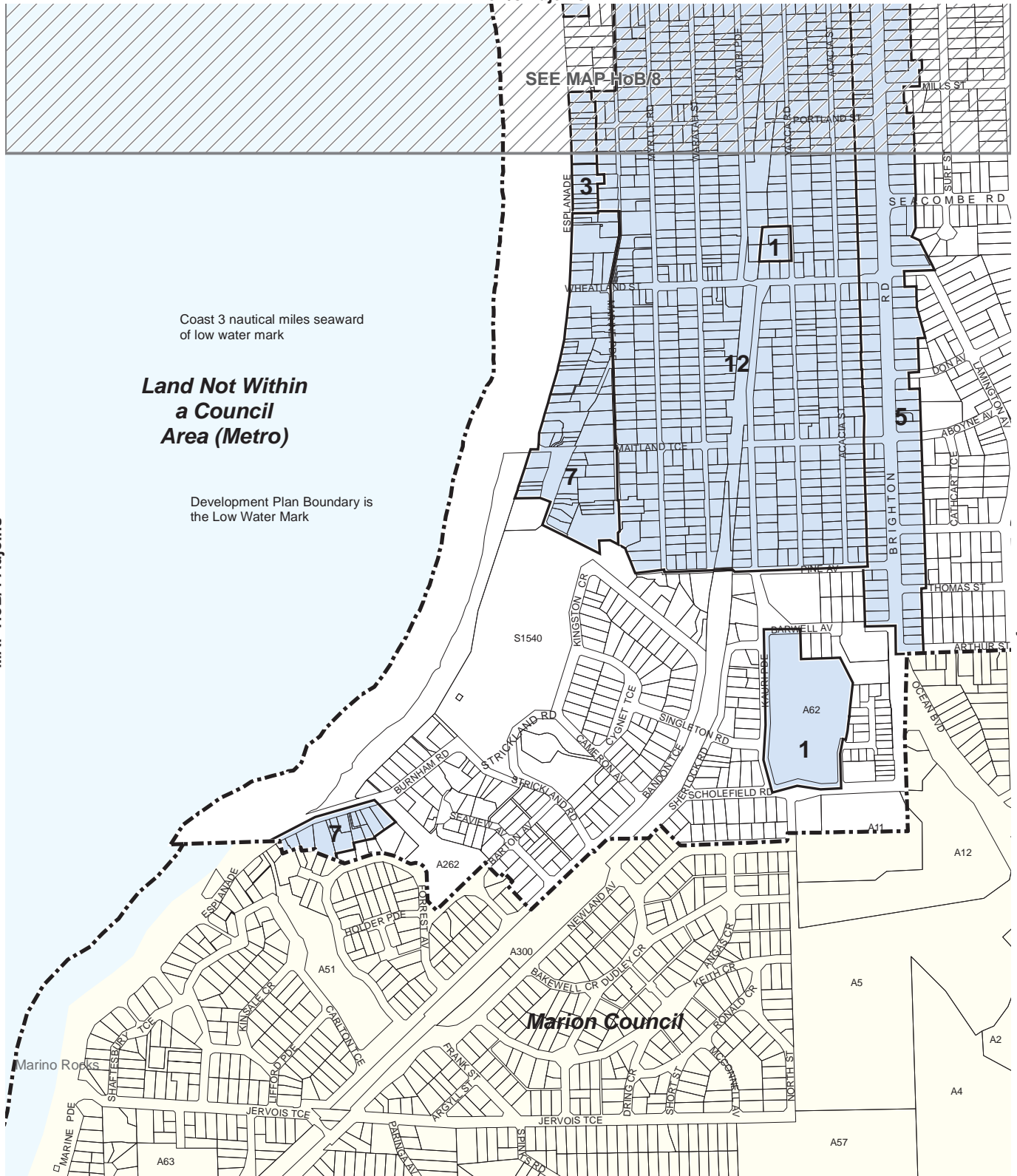
Coast 3 nautical miles seaward
of low water mark

**Land Not Within
a Council
Area (Metro)**

Development Plan Boundary is
the Low Water Mark

MAP HoB/1 Adjoins

MAP HoB/11 Adjoins



See enlargement map for accurate representation.
Lamberts Conformal Conic Projection, GDA94

Policy Area

- 1 Recreation
- 12 Seacliff
- 3 Central West
- 5 Medium Density
- 7 South West



MAP HoB/1 Adjoins

Policy Area Map HoB/10

- Policy Area Boundary
- Development Plan Boundary

MAP HoB/9 Adjoins



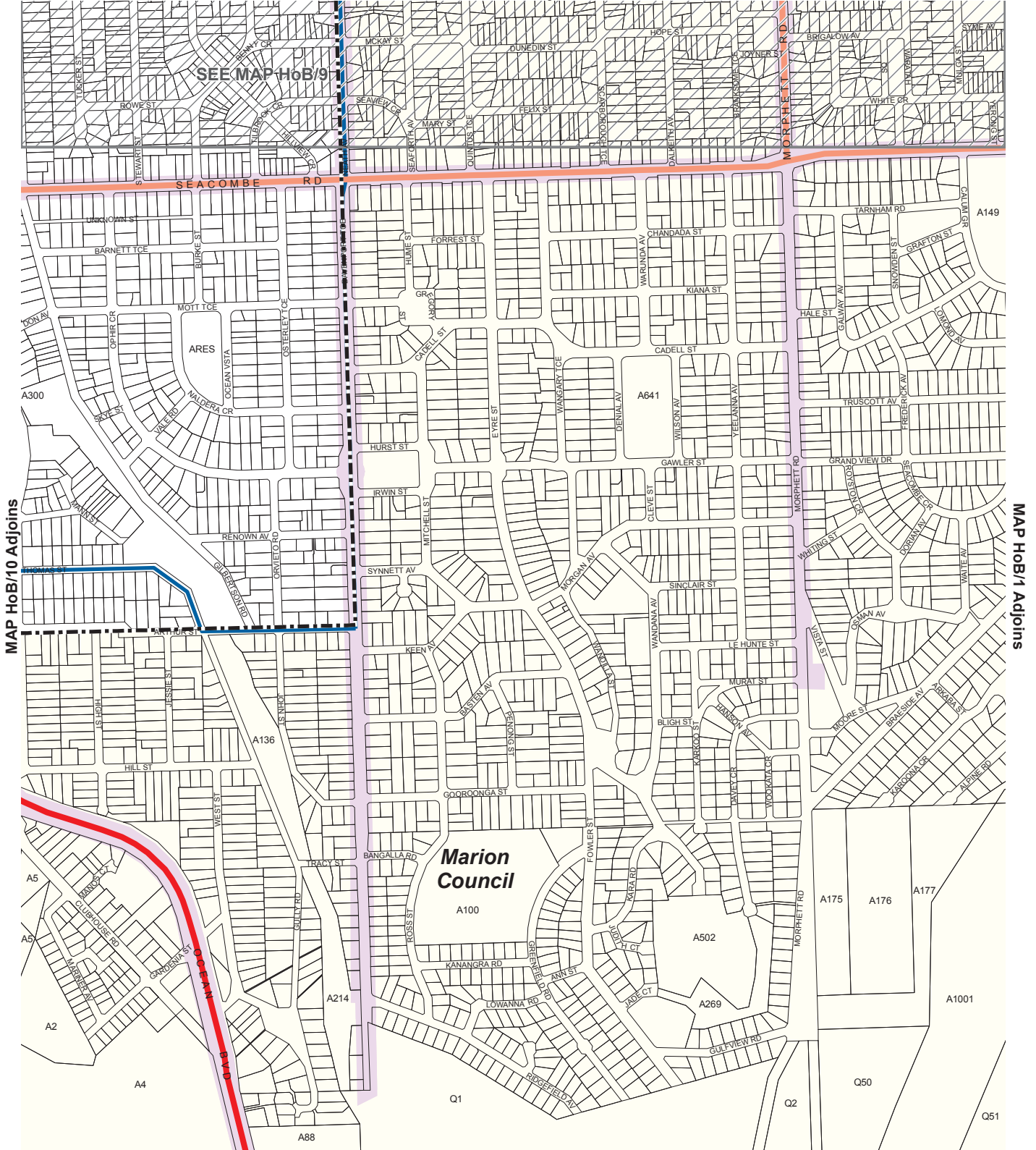
MAP HoB/1 Adjoins



- S** School
- P** Post Office
- Local Reserves
- Recreation Park
- Development Plan Boundary

Location Map HoB/11

MAP HoB/9 Adjoins



MAP HoB/1 Adjoins

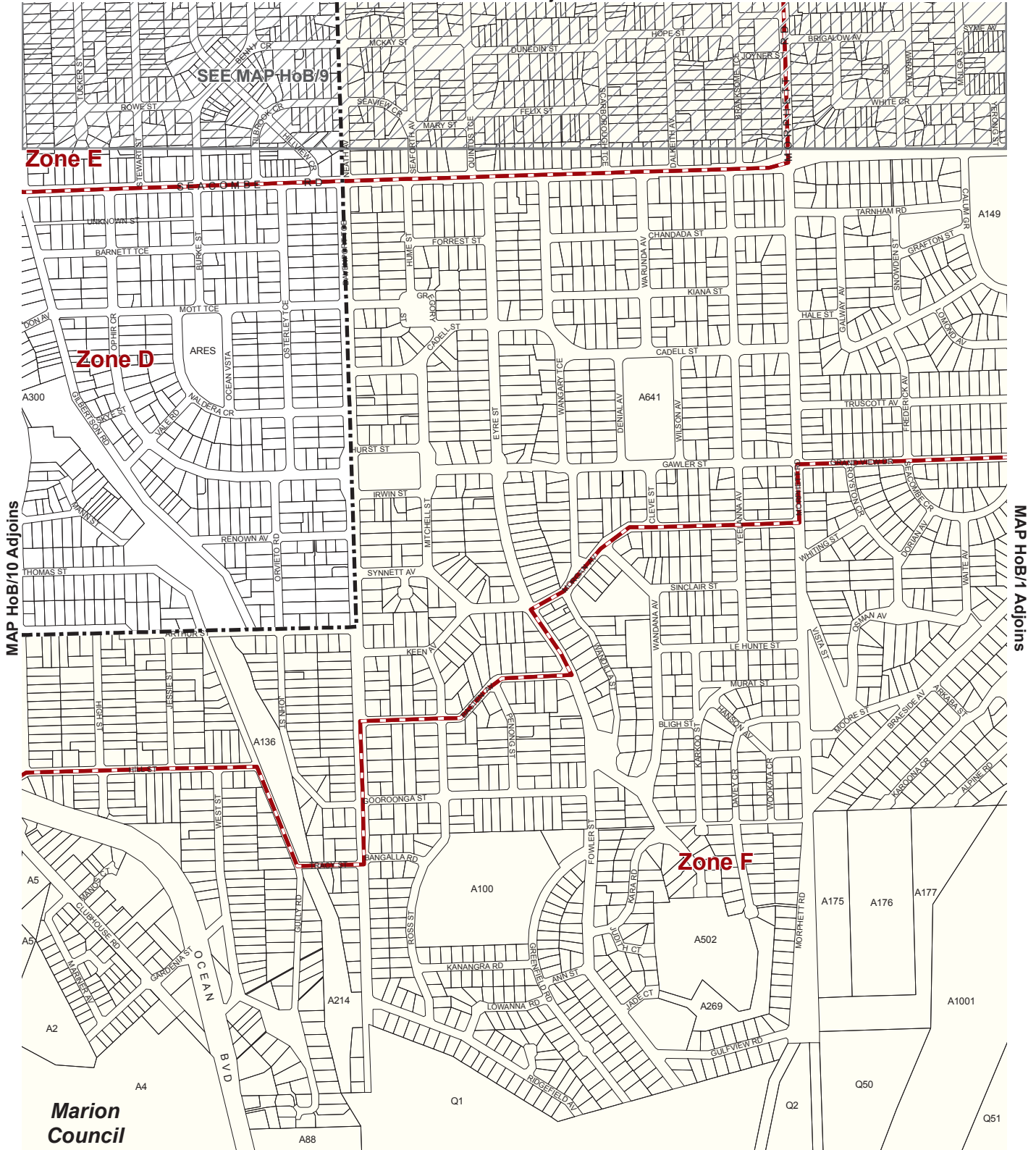


- Primary Arterial Roads
- Secondary Arterial Roads
- Bikedirect Network
- Main Road - Bike Lane
- Main Road
- Secondary Road
- Public Transport
- - - - - Development Plan Boundary

Overlay Map HoB/11

TRANSPORT

MAP HoB/9 Adjoins



MAP HoB/1 Adjoins

Airport Building Heights

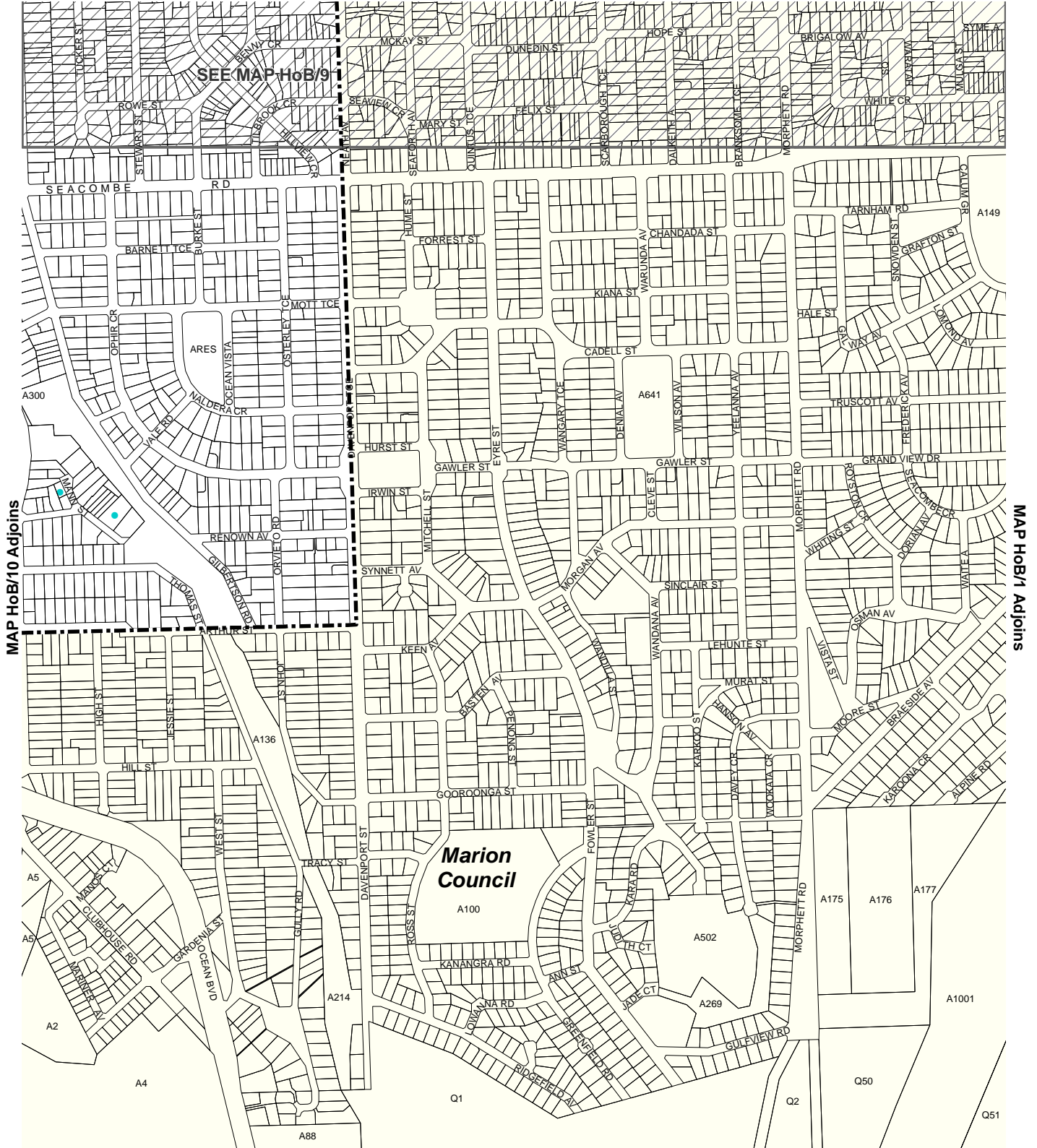
- Zone D** All Structures Exceeding 45 metres above existing ground level
- Zone E** All Structures Exceeding 100 metres above existing ground level
- Zone F** APPROVAL REQUIRED FOR ALL STRUCTURES OVER 30m



Overlay Map HoB/11 DEVELOPMENT CONSTRAINTS

- Airport Building Heights
- Development Plan Boundary

MAP HoB/9 Adjoins



MAP HoB/1 Adjoins

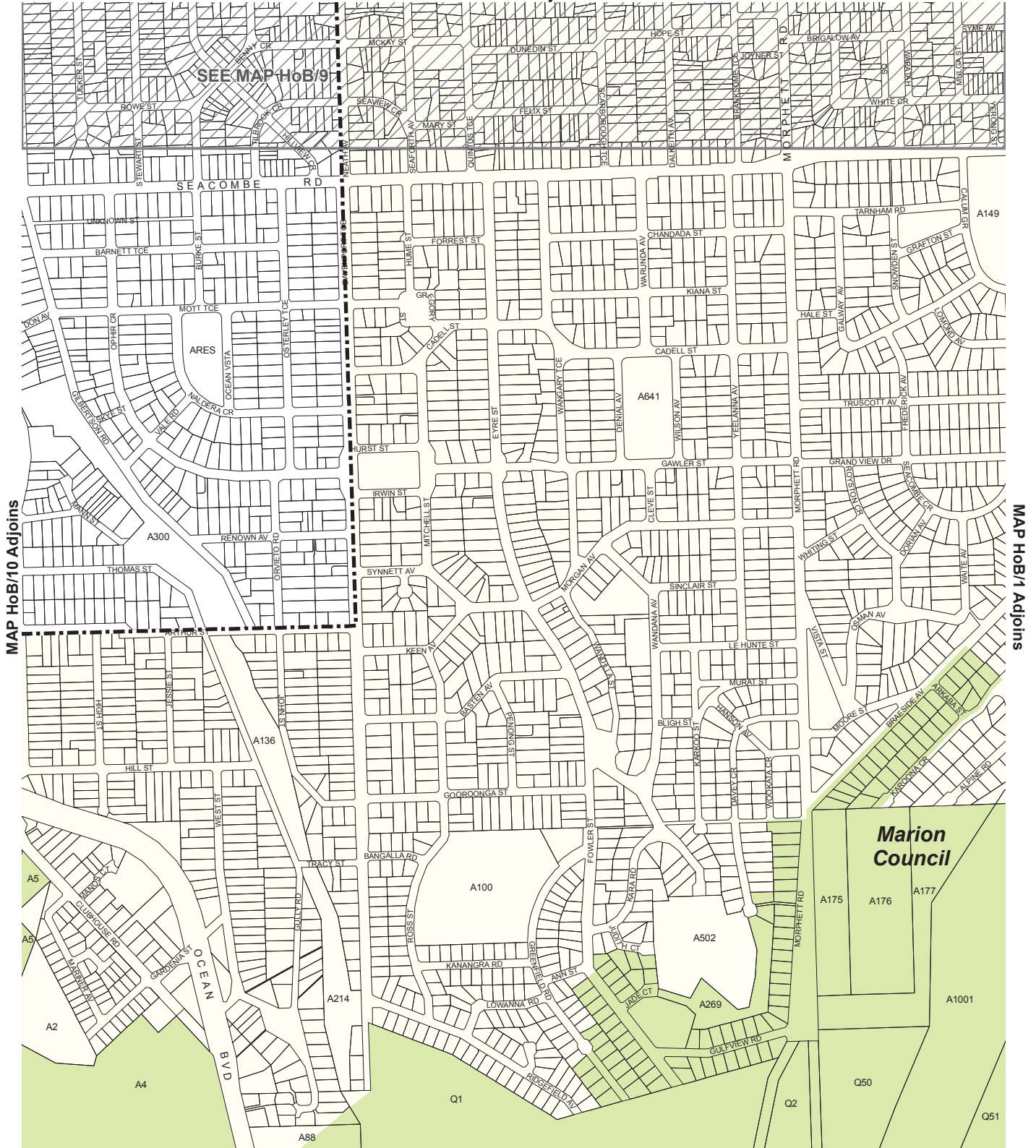
Heritage points are indicative only.
For further information on State and Local Heritage Places and Contributory
Items please refer to the relevant tables within this document.



Overlay Map HoB/11 HERITAGE

- Local heritage place
- Development Plan Boundary

MAP HoB/9 Adjoins



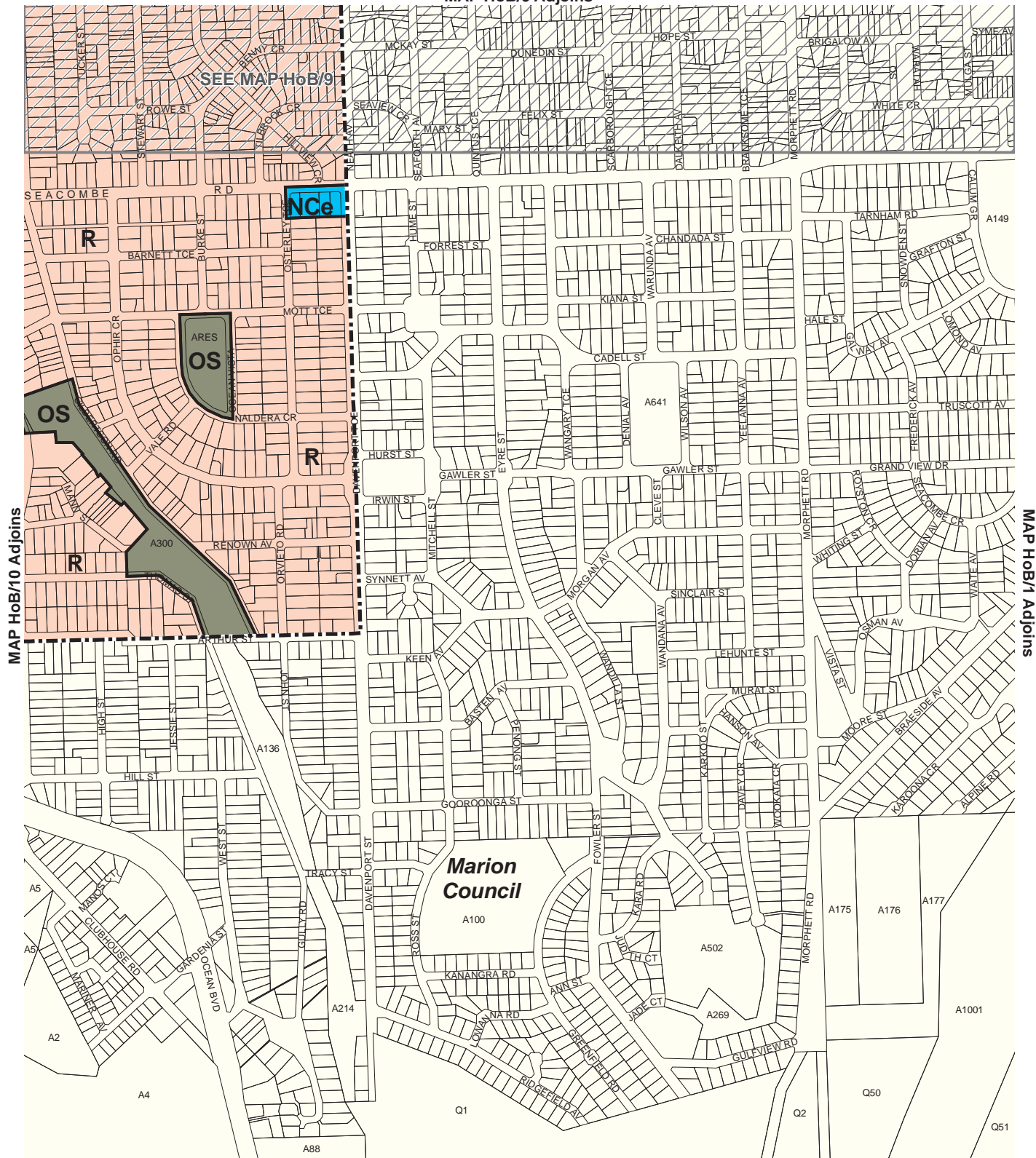
MAP HoB/1 Adjoins



Overlay Map HoB/11 NATURAL RESOURCES

- MOSS Study Area
- Development Plan Boundary

MAP HoB/9 Adjoins



MAP HoB/1 Adjoins

See enlargement map for accurate representation.
Lamberts Conformal Conic Projection, GDA94

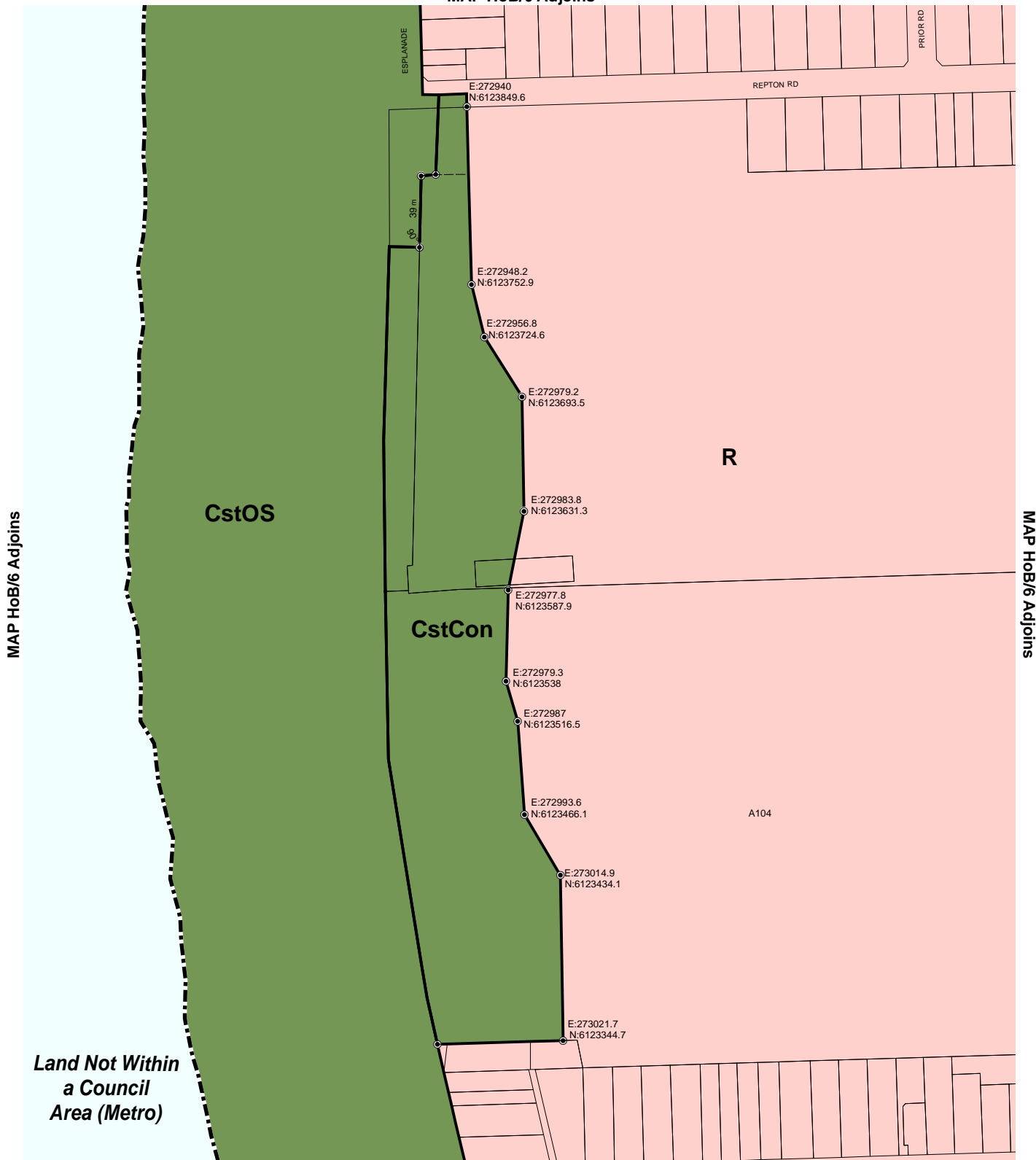


Zones

- NCe Neighbourhood Centre
- OS Open Space
- R Residential
- Zone Boundary
- Development Plan Boundary

Zone Map HoB/11

MAP HoB/6 Adjoins



See enlargement map for accurate representation.
MGA zone54, Transverse Mercator, GDA94

MAP HoB/6 Adjoins



Zones

- CstCon Coastal Conservation
- CstOS Coastal Open Space
- R Residential
- Zone Boundary
- Development Plan Boundary

Zone Map HoB/12 ENLARGEMENT



HOLDFAST BAY COUNCIL
Consolidated - 26 November 2020

MAP HoB/6 Adjoins

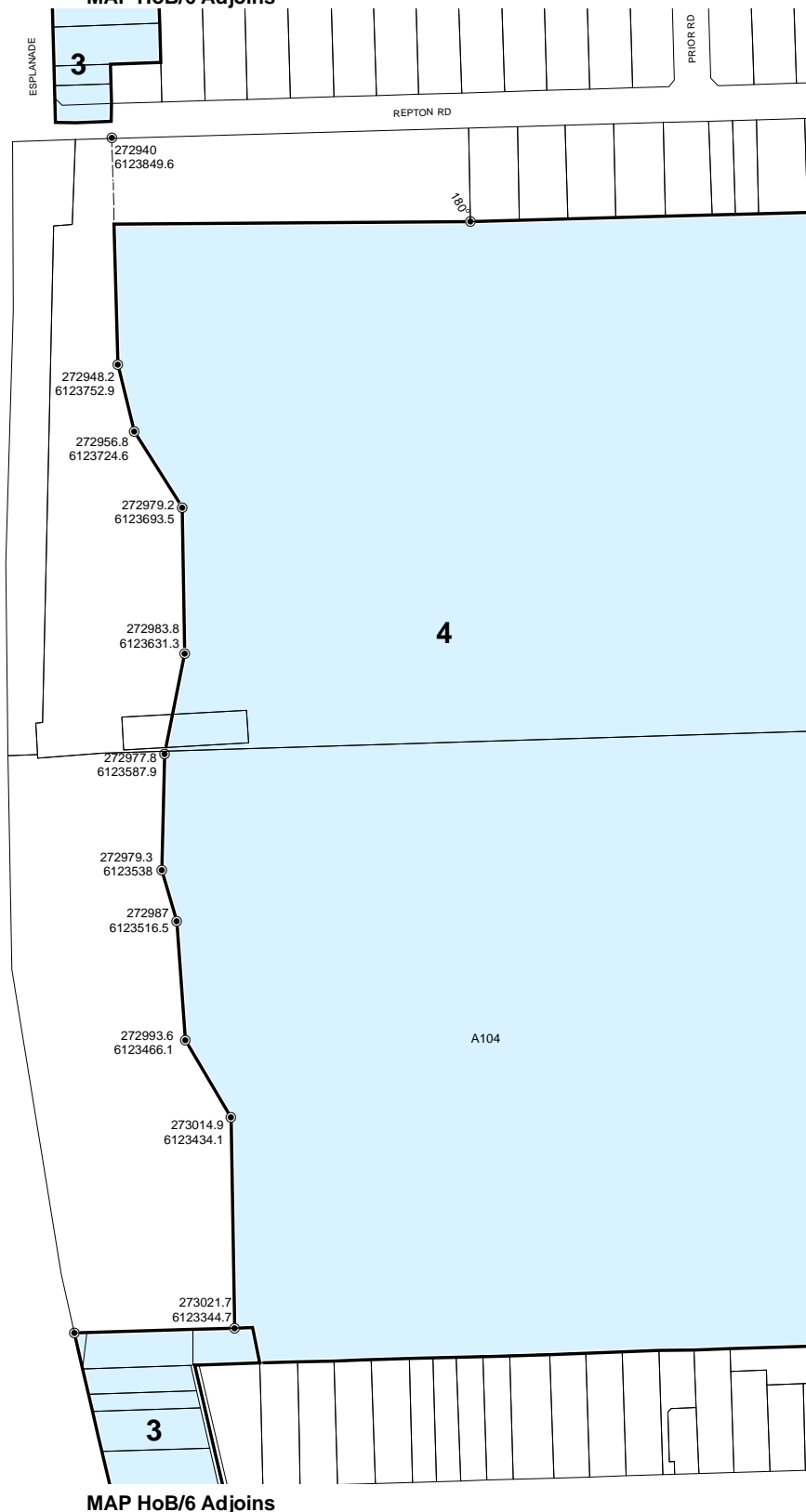
Land Not Within
a Council
Area (Metro)

See enlargement map for accurate representation.
MGA zone54, Transverse Mercator, GDA94

Policy Area
3 Central West
4 Institution

 Policy Area Boundary
 Development Plan Boundary

MAP HoB/6 Adjoins



MAP HoB/6 Adjoins

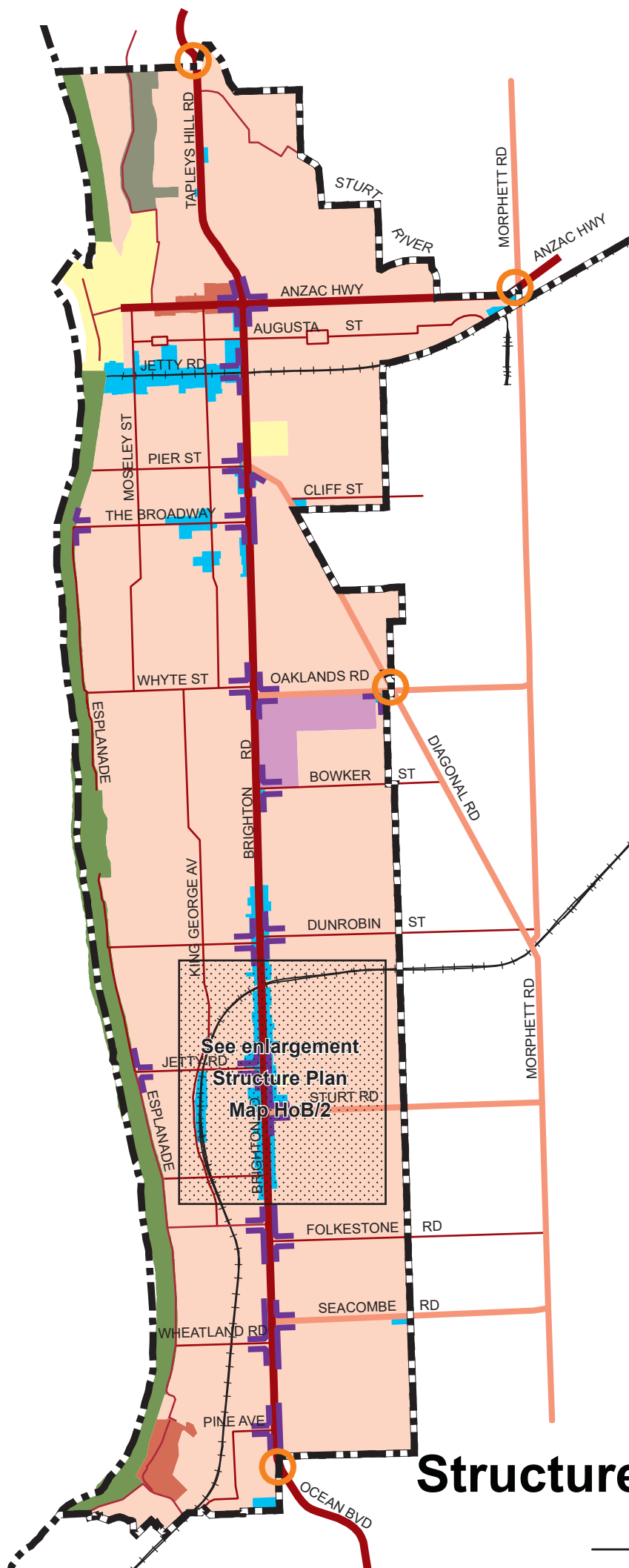
MAP HoB/6 Adjoins



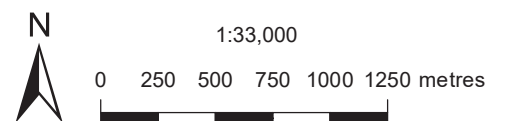
Policy Area Map HoB/12 ENLARGEMENT

HOLDFAST BAY COUNCIL
Consolidated - 26 November 2020

Structure Plan Maps

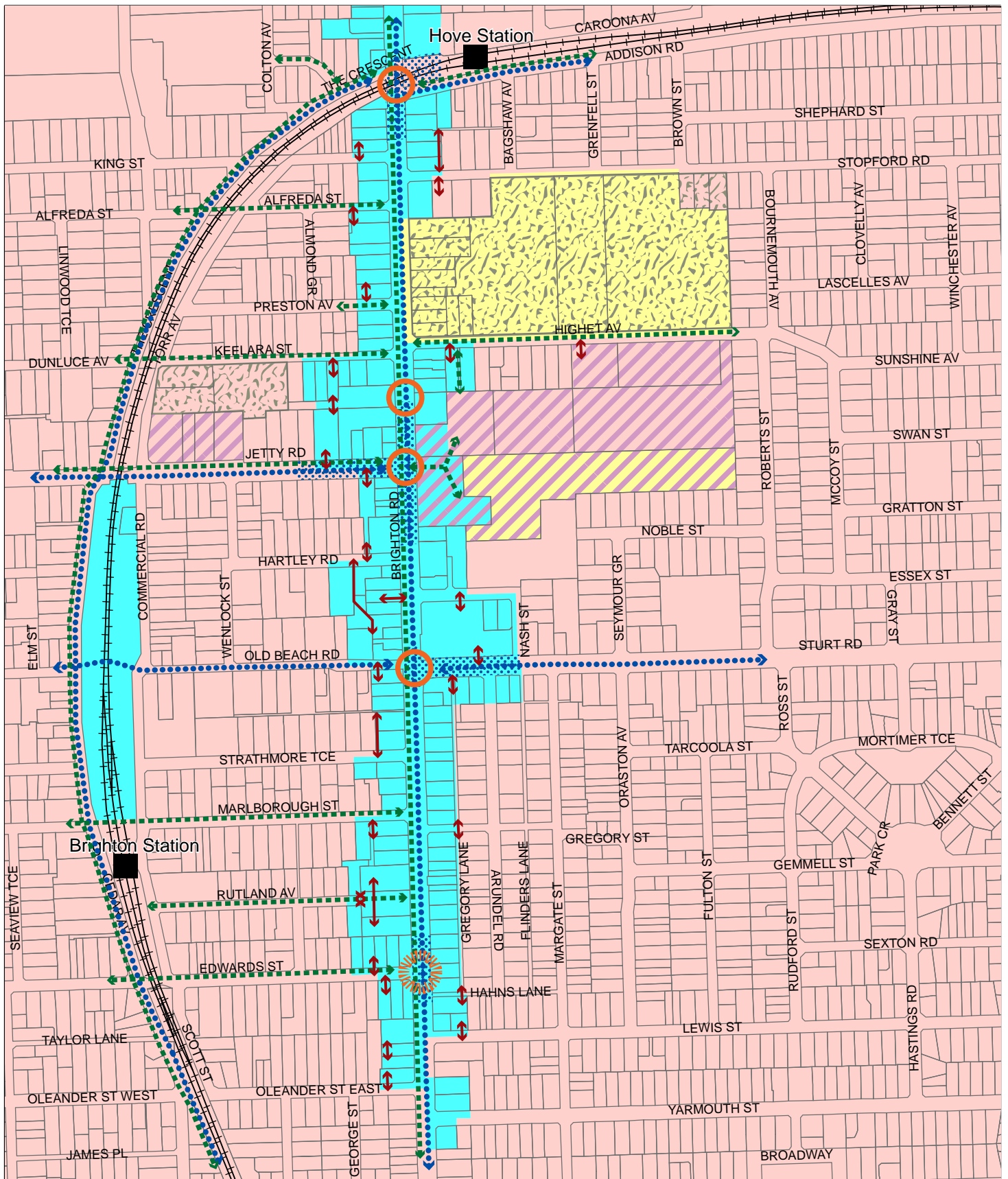


- Commercial
- Community Facilities
- Coastal
- Industrial
- Business/Mixed Use
- Open Space
- Residential
- Gateway
- Landmark Sites
- Primary Arterial Road
- Secondary Arterial Road
- Other Road
- Railways/Tramlines
- Development Plan Boundary

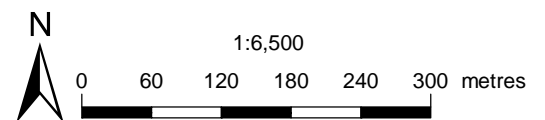


Structure Plan Map HoB/1

HOLDFAST BAY



- Commercial
- Community Facilities
- Residential
- Open Space
- Institutional Uses
- Cycling Path / Link
- Pedestrian Path / Link
- Gateway Public Realm Treatment
- No Vehicle access or egress between Rutland Avenue and shopping centre
- Preferred Vehicle Access Point
- Proposed Signalised Intersection
- Existing Signalised Intersection &/or Pedestrian Activated Crossing
- Railway Line
- Train Station

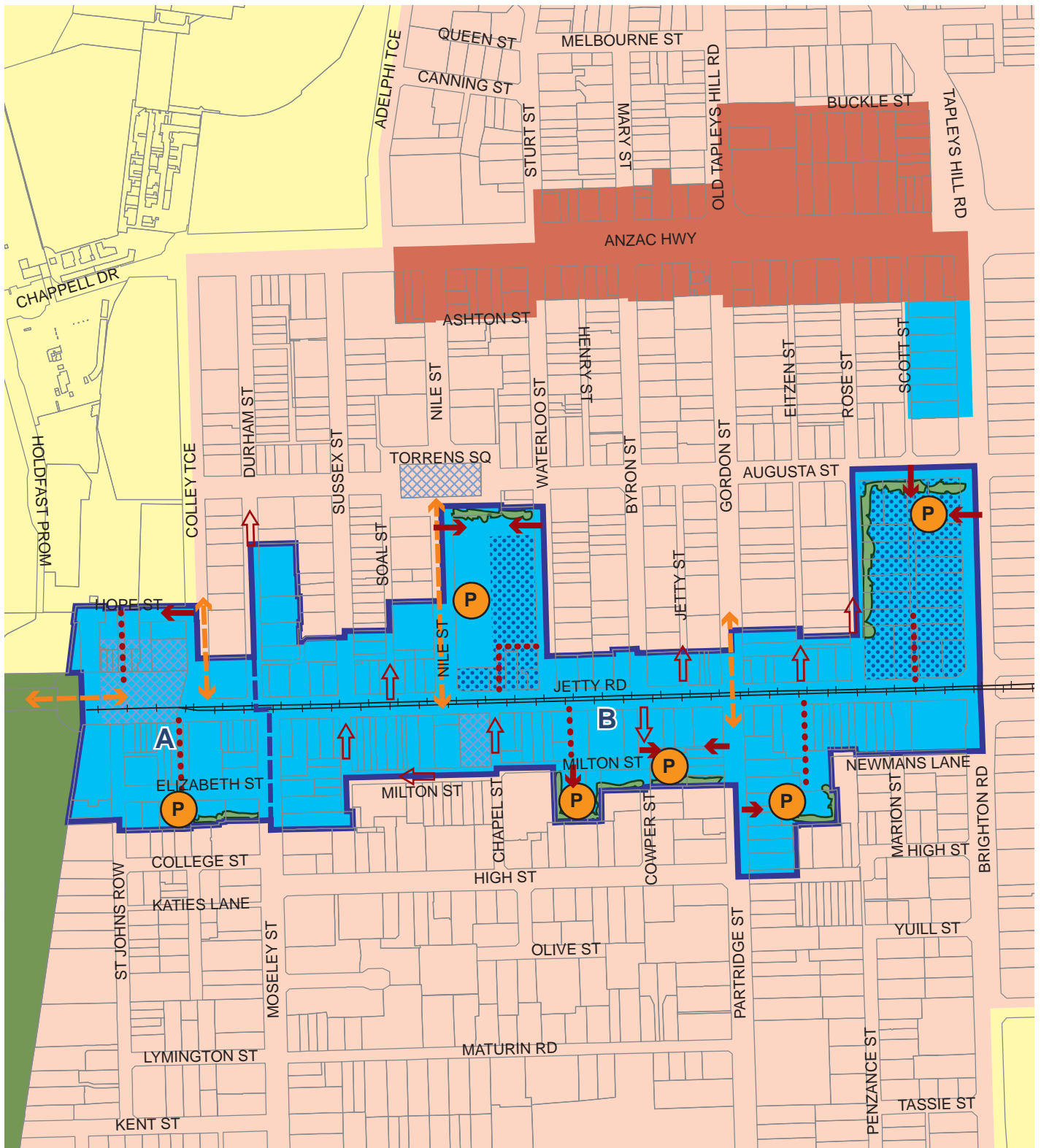


BRIGHTON AND HOVE

Structure Plan Map HoB/2

BRIGHTON AND HOVE DISTRICT CENTRE

Concept Plan Maps



- Commercial
- Community Facilities
- Coastal
- Residential
- Business
- Existing Major Shopping Focus
- Civic / Historic Focus

- Important Vista
- P Primary -Off Street Parking Area
- Landscaping/Acoustic Buffer
- Primary Vehicular Access
- One - Way Traffic
- Pedestrian

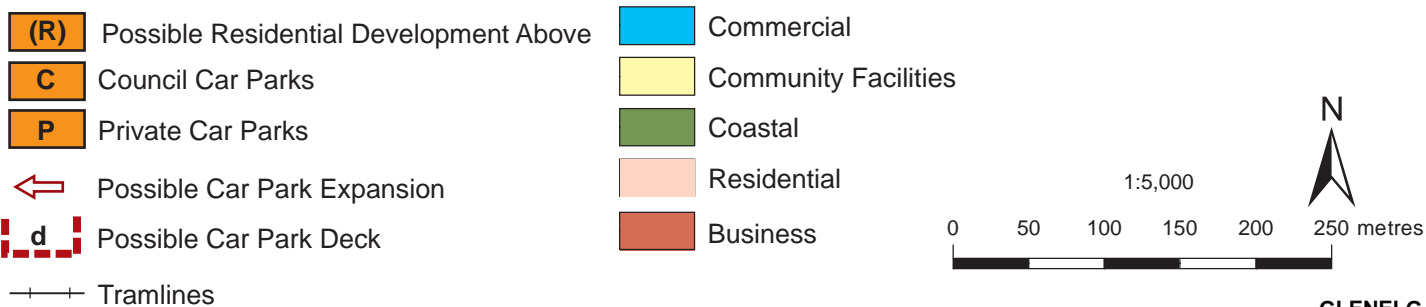
- Tramlines
 - Concept Boundary
- N
- 1:5,000
- 0 50 100 150 200 250 metres

GLENELG

- A Moseley Square Civic/Tourist Area
- B Jetty Road Core Shopping Area

Concept Plan Map HoB/1

JETTY ROAD AND MOSELEY SQUARE



Concept Plan Map HoB/2

CAR PARKING AREAS

Concept Areas

1. Interface with Jetty Road.

Key family entertainment area, restaurant, tourism facilities and associated development.

2. Tourism related accommodation with sea and park views.

Residential accommodation including medium rise hotel, serviced apartments, townhouse and associated facilities. Marina, pier, shops, food outlets. Main entrance to foreshore from Anzac Highway. Hotel, full range of residential accommodation of a medium and high density.

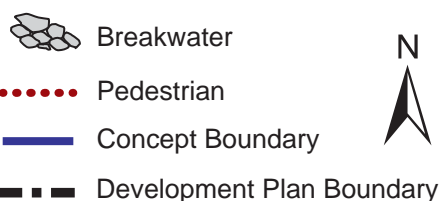
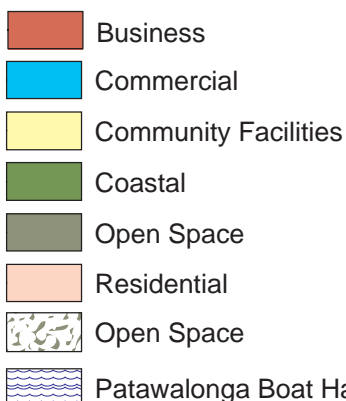
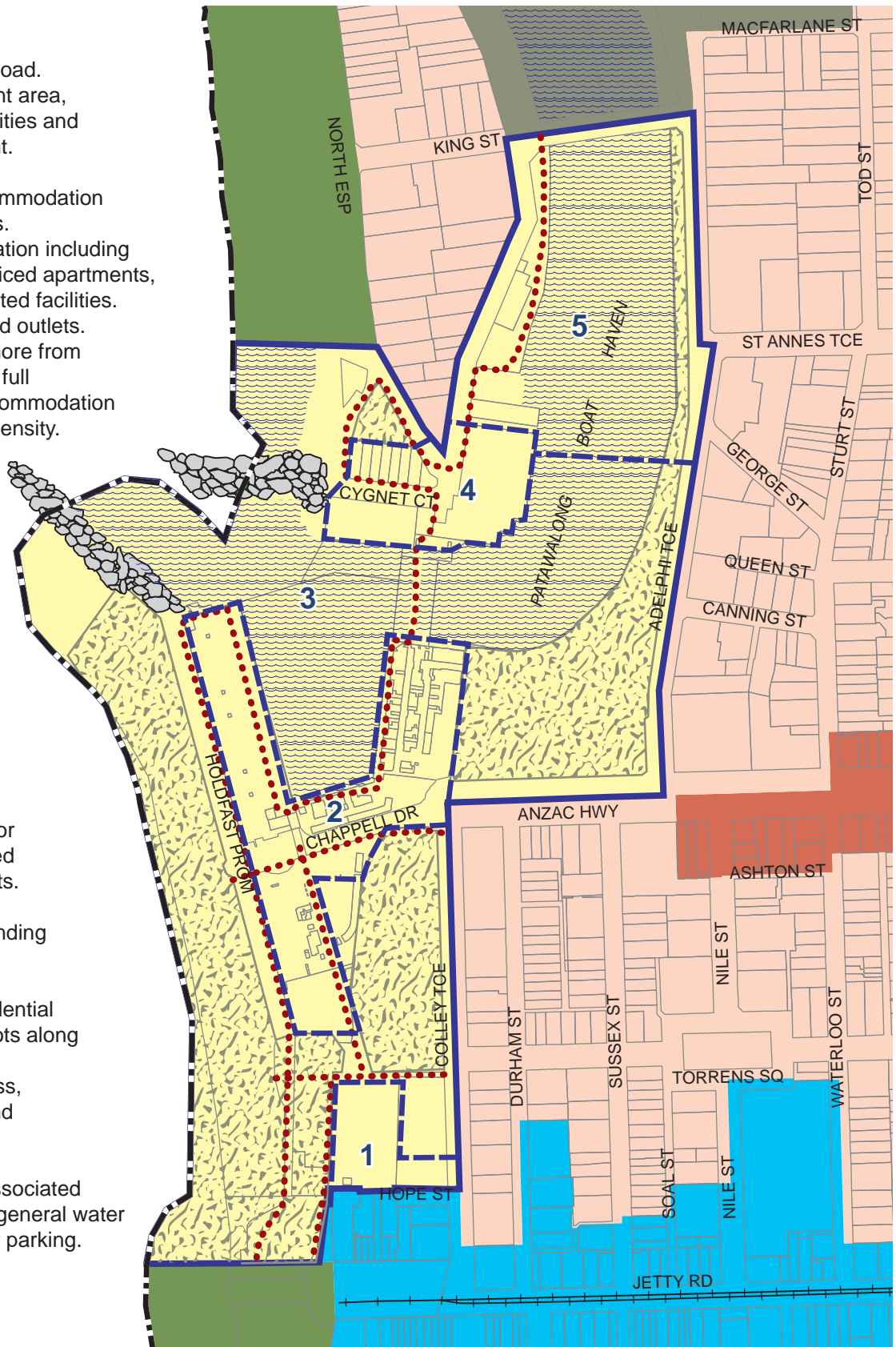
3. Patawalonga Basin for marina berths, integrated mixed use developments.

Focal point for tourists, water activities, ferry landing and charter craft.

4. Medium density residential housing lots.

Housing lots along Patawalonga frontage with private water access, private marinaberths and open space areas.

5. Marina berths and associated management facilities, general water based activities and car parking.

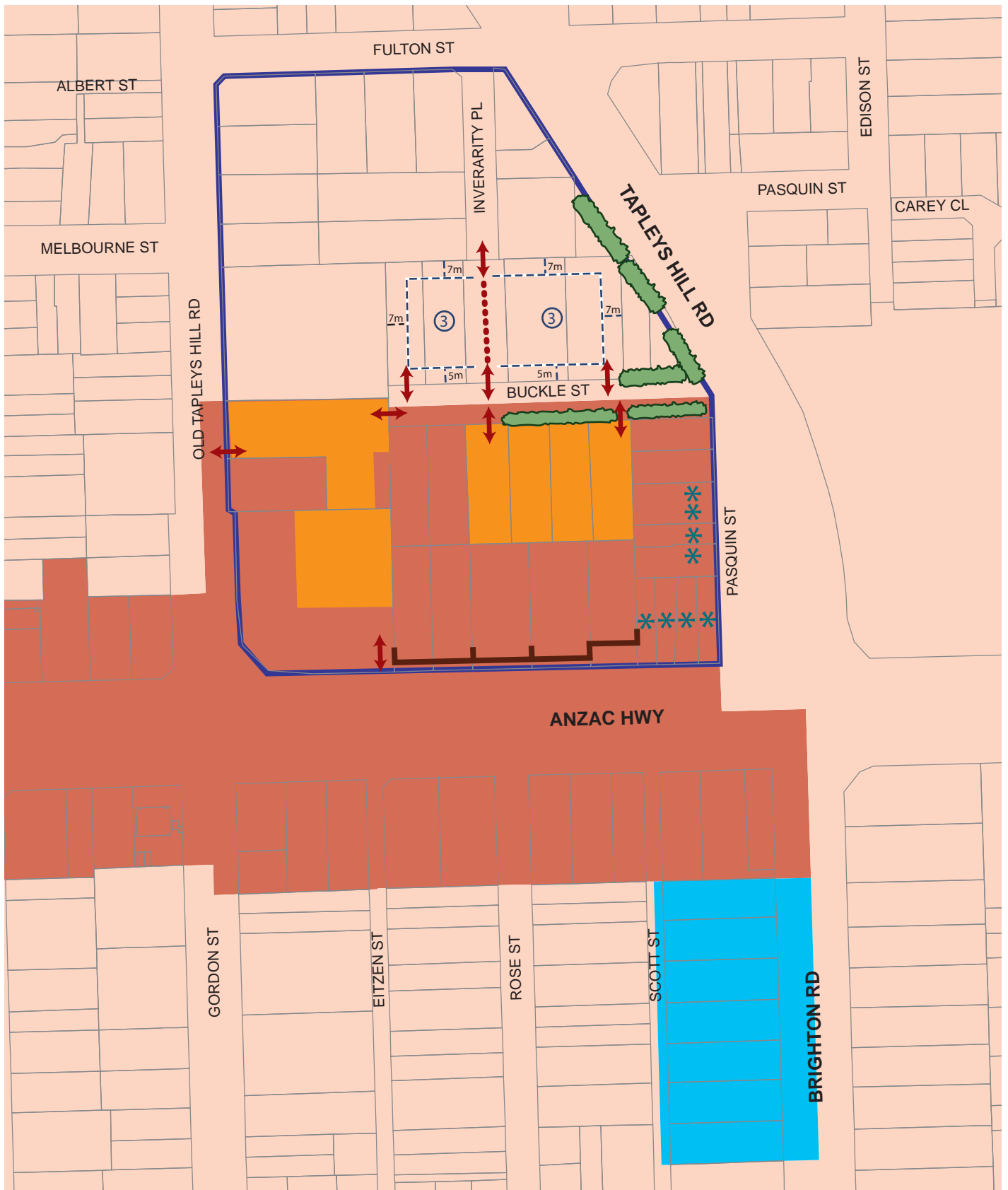


GLENELG

Concept Plan Map HoB/3

FORESHORE AND PATAWALONGA

Consolidated 20 November 2020



- Commercial
- Business
- Residential
- Existing/Future Car Parking
- Pedestrian and Stormwater Connection
- Built Form Edge
- Height in Storeys
- Storey Setback
- Landscape Buffer
- Vehicle Access
- Local Heritage Places
- Concept Boundary



1:2,000
0 20 40 60 80 100 metres

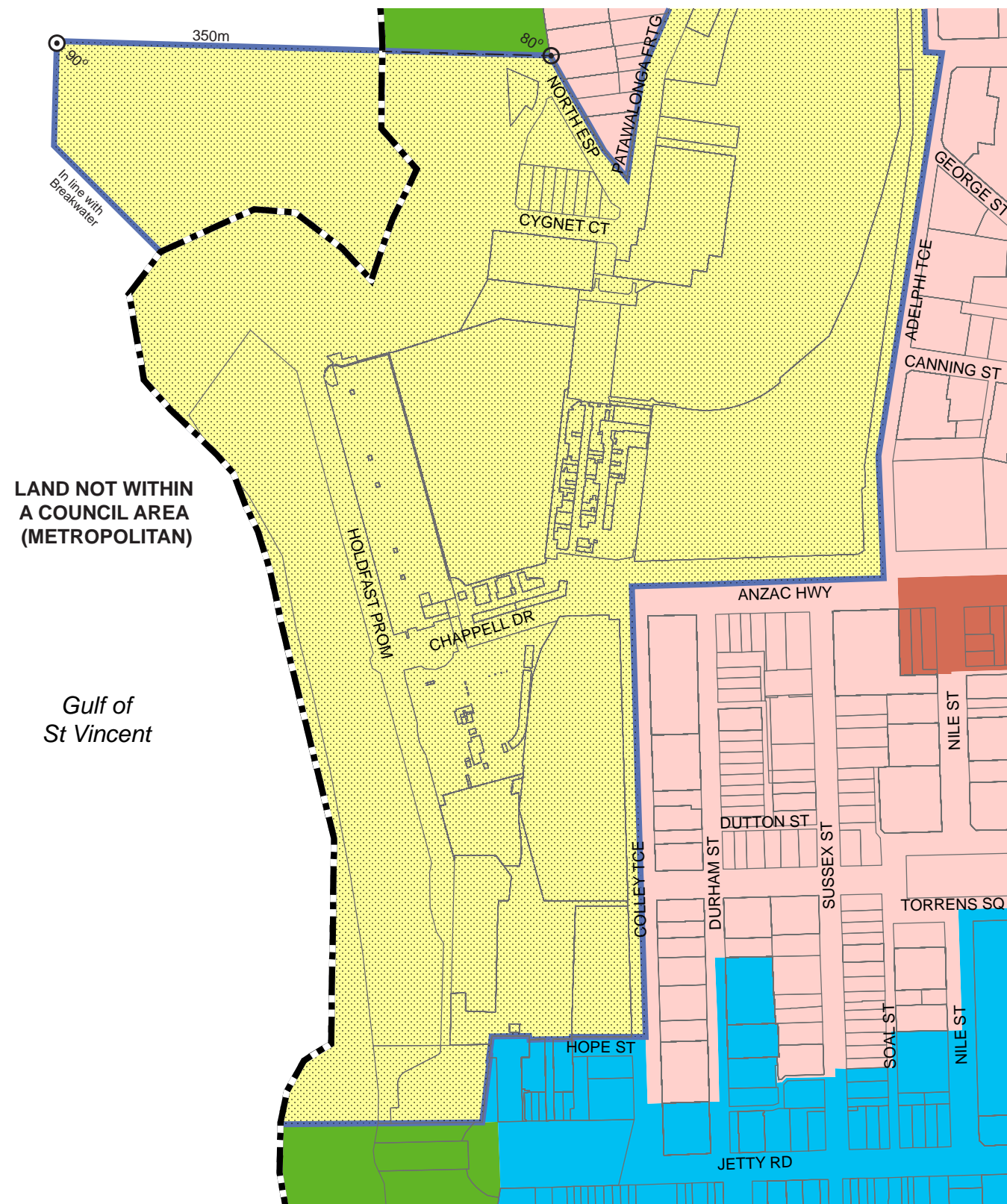
GLENELG NORTH

Concept Plan Map HoB/4

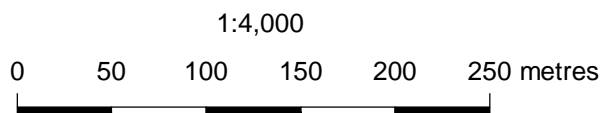
BUCKLE STREET

LAND NOT WITHIN
A COUNCIL AREA
(METROPOLITAN)

*Gulf of
St Vincent*



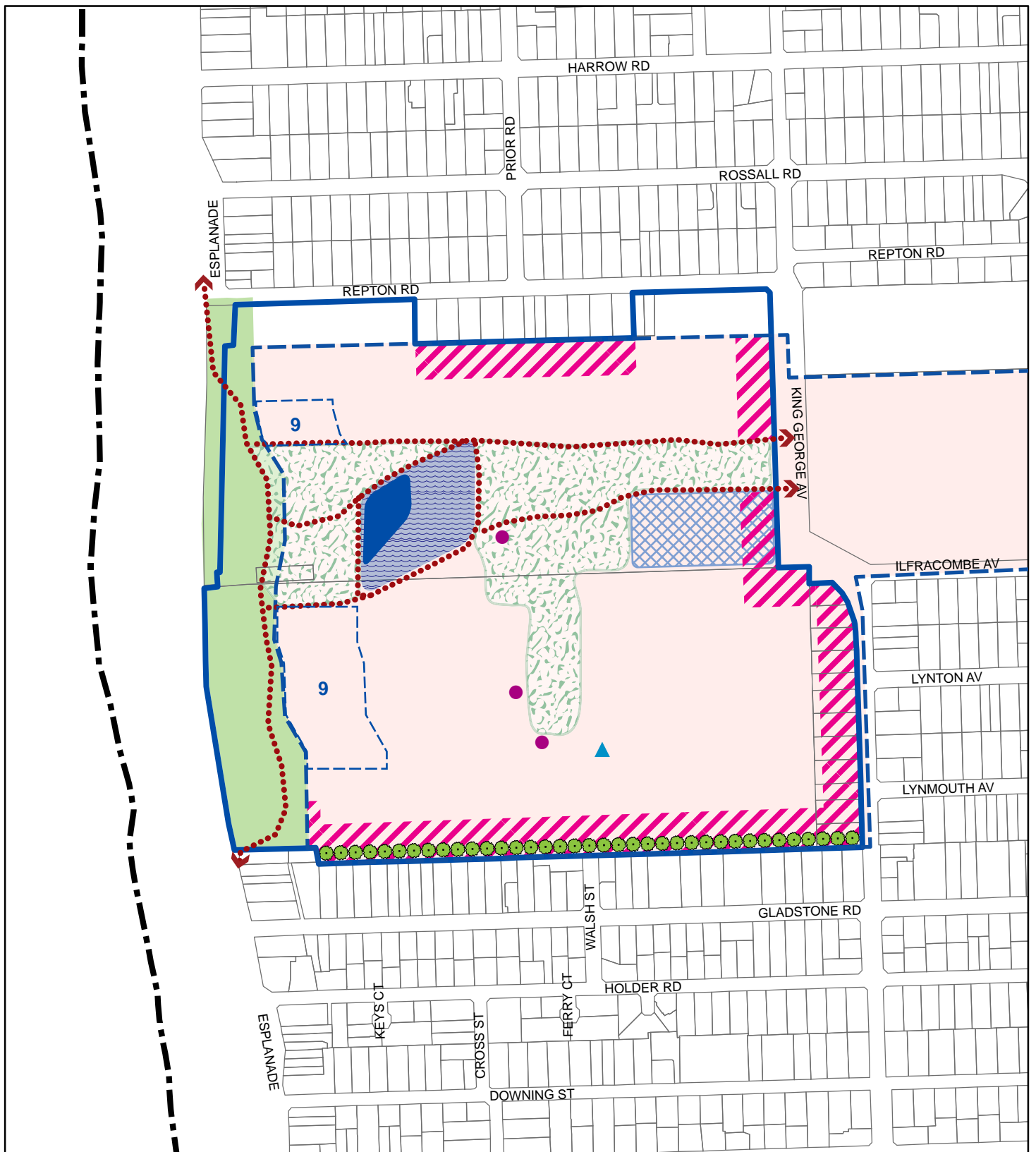
- Concept Boundary
- Glenelg Foreshore and Patawalonga Zone
- Commercial
- Coastal Open Space
- Residential
- Business
- Development Plan Boundary



GLENELG

Concept Plan Map HoB/5

EXTENT - GLENELG FORESHORE AND PATAWALONGA ZONE



— Minda Incorporated Brighton Campus Site

— Institution Policy Area

..... Coast Park Pedestrian/Cycle link

● State Heritage Place

▲ Local Heritage Place

9 — Foreshore Development Area - 9 Storey maximum building height

●●●● Existing Vegetation Buffer

Open Space/Public Access

Coastal Conservation Zone

Wetlands

Detention Basin

Interface Area

Commercial Area

1:5,000
0 50 100 150 200 250 metres

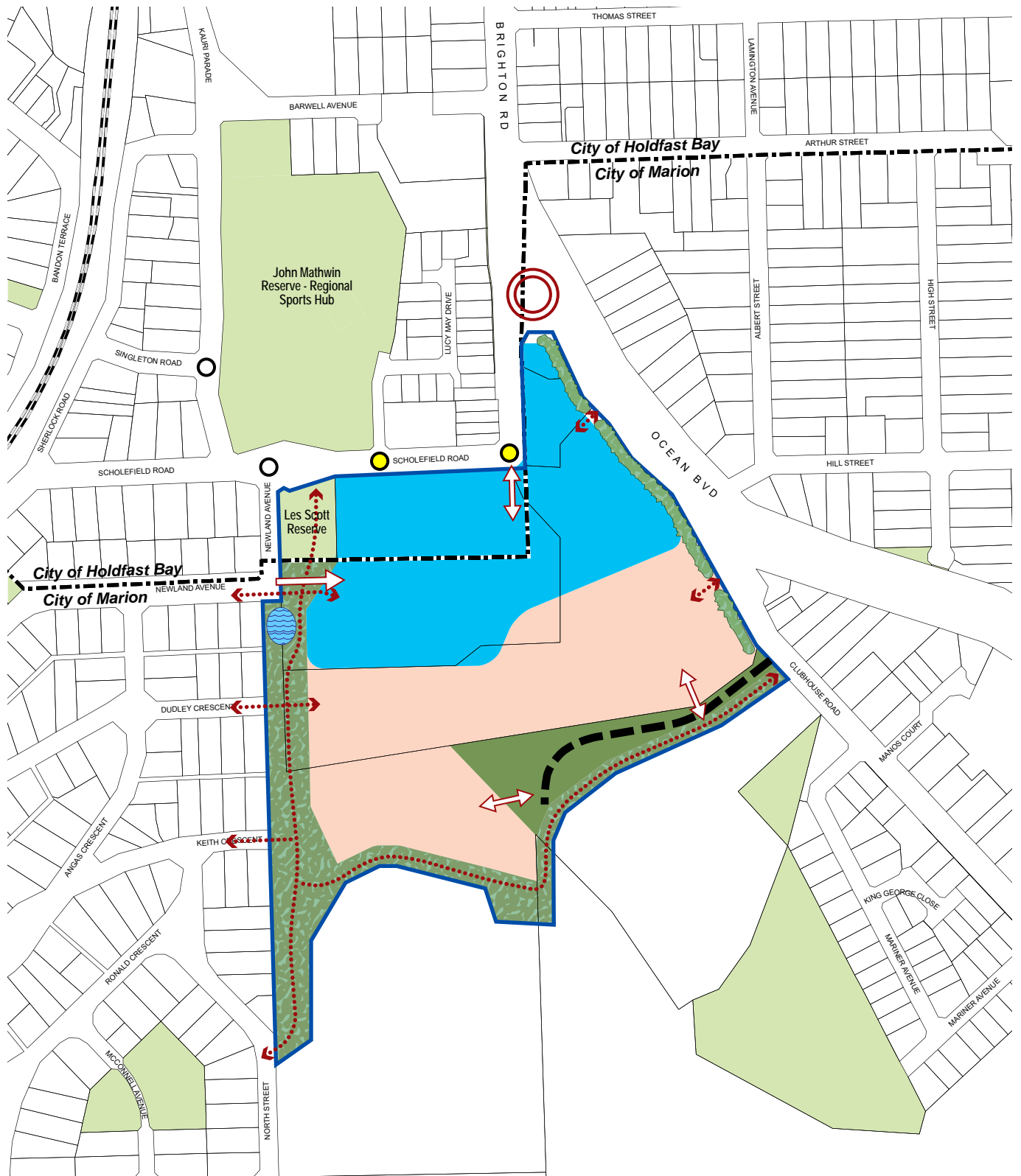


North Brighton

Concept Plan Map HoB/6

MINDA INCORPORATED BRIGHTON CAMPUS

Consolidated 26 November 2020
HOLDFAST BAY COUNCIL



- Hills Face Zone
- Residential
- Neighbourhood Activity Centre
- Existing Reserves
- Detention Basin / WSUD
- Open Space / Buffers
- Signalised Intersection
- New Traffic Management Device
- Existing Roundabouts
- Possible Access Point (two-way)
- Possible Access Point (one-way)
- Pedestrian / Cyclist Links
- Proposed Public Road
- Development Plan Boundary
- Concept Plan Boundary



SEACLIFF

Concept Plan Map HoB/7

SEACLIFF PARK