

# Development Plan

## West Torrens Council

Consolidated – 21 May 2020

Please refer to the West Torrens Council page at [www.sa.gov.au/developmentplans](http://www.sa.gov.au/developmentplans) to see any amendments not consolidated.



**Government of South Australia**  
Department of Planning,  
Transport and Infrastructure



**Government  
of South Australia**

Department of Planning,  
Transport and Infrastructure

## **Department of Planning, Transport and Infrastructure**

Level 5, 50 Flinders Street  
Adelaide

Postal Address  
GPO Box 1815  
Adelaide SA 5001

Phone 1800 752 664 (Option 4)

Email [DPTI.PDDPACoordinator@sa.gov.au](mailto:DPTI.PDDPACoordinator@sa.gov.au)  
Internet [www.saplanningportal.sa.gov.au](http://www.saplanningportal.sa.gov.au)



## **City of West Torrens**

165 Sir Donald Bradman Drive  
Hilton SA 5033

Telephone (08) 8416 6333  
Fax (08) 8443 5709

Email [csu@wtcc.sa.gov.au](mailto:csu@wtcc.sa.gov.au)  
Internet [www.wtcc.sa.gov.au](http://www.wtcc.sa.gov.au)

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# **Introduction**

# Section



## Amendment Record Table

The following table is a record of authorised amendments and their consolidation dates for the West Torrens Council Development Plan since its inception on 12 December 1996. Further information on authorised amendments prior to this date may be researched through the relevant Council, the Department of Planning, Transport and Infrastructure or by viewing Gazette records.

Consolidated	Amendment – [Gazetted date]
12 December 1996	Miscellaneous Amendments - Metropolitan Adelaide Development Plans PAR (Interim) ( <i>Ministerial</i> ) – [12 December 1996]
24 April 1997	Neighbourhood Centre (Hilton) Zone PAR ( <i>Ministerial</i> ) – [14 April 1997] Recreation (Mile End South) Zone PAR ( <i>Ministerial</i> ) – [17 April 1997]
26 June 1997	Miscellaneous Amendments - Metropolitan Adelaide Development Plans PAR ( <i>Ministerial</i> ) – [26 June 1997]
28 August 1997	Statewide Marine Aquaculture and Offshore Development PAR ( <i>Ministerial</i> ) – [5 June 1997]
25 September 1997	City of Glenelg, City of West Torrens and Land Not Within a Council Area (Metropolitan) - Glenelg-West Beach Foreshore and Environs PAR ( <i>Ministerial</i> ) – [4 September 1997]
18 December 1997	Section 29(2) Amendment – [6 November 1997]
5 February 1998	Fitzroy Avenue PAR – [5 February 1998]
2 July 1998	Richmond Road/Morley Street PAR – [2 July 1998]
26 August 1999	Section 29(2) Amendment – [12 March 1998] Section 29(2) Amendment – [2 July 1998] Waste Disposal (Landfill) PAR ( <i>Ministerial</i> ) – [19 August 1999]
16 December 1999	Industrial Development PAR – [16 December 1999] Section 29(2) Amendment – [16 December 1999]
13 January 2000	Metropolitan Adelaide - Industrial Land and Development PAR ( <i>Ministerial</i> ) – [25 November 1999]
Not Consolidated	Telecommunications Facilities State-wide Policy Framework PAR Interim ( <i>Ministerial</i> ) – [31 August 2000]
2 November 2000	Section 27(5) Amendment - Waste Disposal (Landfill) PAR ( <i>Ministerial</i> ) – [9 March 2000] City of West Torrens - Thebarton (CT) and West Torrens (City) - Consolidation PAR – [2 November 2000]
14 December 2000	Thebarton - Local Heritage PAR (Interim) – [14 December 2000]
24 May 2001	Metropolitan Adelaide Significant Tree Control PAR ( <i>Ministerial</i> ) – [21 December 2000] Adelaide Airport Zone PAR ( <i>Ministerial</i> ) – [24 May 2001] Section 29(2)(b) Amendment – [24 May 2001]
5 July 2001	Demolition Policy PAR (Interim) ( <i>Ministerial</i> ) – [29 June 2001]
6 September 2001	Telecommunications Facilities State-wide Policy Framework PAR ( <i>Ministerial</i> ) – [30 August 2001]
13 December 2001	Thebarton Local Heritage PAR – [13 December 2001]
27 June 2002	Demolition Policy PAR ( <i>Ministerial</i> ) – [27 June 2002]
16 January 2003	Stormwater in Urban Areas PAR ( <i>Ministerial</i> ) – [12 November 2002]
15 May 2003	Residential PAR – [15 May 2003]
12 June 2003	Flood Prone Areas PAR (Interim) – [12 June 2003] Section 29(2)(b)(i) Amendment – [12 June 2003]
9 October 2003	Wind Farms PAR ( <i>Ministerial</i> ) – [24 July 2003] Bulky Goods (Metropolitan) Zone PAR – [9 October 2003] Section 29(2)(b)(ii) Amendment – [9 October 2003]
6 November 2003	Section 29(2)(b)(ii) Amendment – [30 October 2003]

**West Torrens Council**  
**Introduction Section**  
**Amendment Record Table**

<b>Consolidated</b>	<b>Amendment – [Gazetted date]</b>
19 March 2004	Underdale Campus PAR (Interim) – [19 March 2004]
24 June 2004	Cessation of the 'Flood Prone Areas PAR' and its removal from the West Torrens (City) Development Plan – [17 June 2004] Brown Hill and Keswick Creeks Flood Plain PAR ( <i>Ministerial</i> ) (Interim) – [11 June 2004]
11 November 2004	Underdale Campus PAR – [11 November 2004]
24 February 2005	Termination of Interim Operation of the 'Brown Hill and Keswick Creeks Flood Plain PAR' (Ministerial) and its removal from the West Torrens (City) Development Plan – [17 February 2005]
31 March 2005	Flood Prone Areas PAR – [31 March 2005]
19 April 2007	General PAR – [19 April 2007]
9 August 2007	Underdale High School (River Torrens Linear Park Zone) PAR – [7 June 2007] Mile End South/Keswick PAR – [2 August 2007] Section 29(2)(b)(ii) Amendment – [9 August 2007]
11 October 2007	Local Heritage DPA (Interim) – [4 October 2007]
10 April 2008	Section 29(2)(b)(ii) Amendment – [10 April 2008]
2 October 2008	Local Heritage DPA – [2 October 2008]
13 May 2010	Brickworks Market Precinct DPA – [13 May 2010]
12 June 2010	Statewide Bulky Goods DPA ( <i>Ministerial</i> ) (Interim) – [12 June 2010]
3 February 2011	Statewide Bulky Goods DPA ( <i>Ministerial</i> ) – [13 January 2011]
18 August 2011	Better Development Plan (BDP) and General DPA – [18 August 2011]
13 October 2011	Section 29(2) Amendment – [13 October 2011]
2 February 2012	Regulated Trees DPA (Interim) ( <i>Ministerial</i> ) – [17 November 2011]
22 November 2012	Regulated Trees DPA ( <i>Ministerial</i> ) – [15 November 2012]
31 October 2013	Housing Diversity DPA (Part 1) – Port Road Corridor DPA – [29 October 2013]
25 September 2014	Adelaide Shores DPA ( <i>Ministerial</i> ) – [25 September 2014]
25 June 2015	Section 29(3)(a) Amendment – [19 March 2015] Housing Diversity DPA (Part 2) – [18 June 2015] Section 29(2)(b)(i) Amendment – [18 June 2015]
5 November 2015	Section 29(1) Amendment – [5 November 2015]
5 May 2016	Existing Activity Centres Policy Review DPA ( <i>Ministerial</i> ) – [21 April 2016]
30 May 2017	Inner and Middle Metropolitan Corridor (Design) DPA (Interim) ( <i>Ministerial</i> ) – [30 May 2017]
19 December 2017	Section 29(2)(b)(ii) Amendment – [4 July 2017] Inner and Middle Metropolitan Corridor (Design) DPA ( <i>Ministerial</i> ) – [19 December 2017] Inner and Middle Metropolitan Corridor (Sites) DPA ( <i>Ministerial</i> ) – [19 December 2017]
6 February 2018	Underdale and Torrensville Urban Renewal DPA – [6 February 2018]
12 July 2018	Section 29 (2)(b)(i), (2)(b)(ii) and (3)(c)(ii) Amendments – [12 July 2018]
7 May 2020	Morphettville Racecourse DPA ( <i>Ministerial</i> ) – [7 May 2020]
21 May 2020	Section 29(2)(b)(ii) Amendment – [21 May 2020]

Consolidated: The date of which an authorised amendment to a Development Plan was consolidated (incorporated into the published Development Plan) pursuant to section 31 of the *Development Act 1993*.

Gazetted: The date of which an authorised amendment was authorised through the publication of a notice in the Government Gazette pursuant to Part 3 of the *Development Act 1993*.

## Introduction to the Development Plan

Welcome to the Development Plan for the Council.

This introduction has been prepared by the Department of Planning, Transport and Infrastructure as a guide to assist you in understanding this Development Plan.

For full details about your rights and responsibilities, you are advised to refer to the *Development Act 1993* and the associated *Development Regulations 2008* and/or consult your council.

A number of guides and additional information regarding South Australia's Planning and Development Assessment System are available via the website [www.saplanningportal.sa.gov.au](http://www.saplanningportal.sa.gov.au) or by contacting the Department of Planning, Transport and Infrastructure at Level 5, 50 Flinders Street, Adelaide, SA 5000.

### Overview of the Planning System

South Australia has an integrated planning and development system, with three distinct but interrelated parts, these being:

- Legislation
- The Planning Strategy
- Development Plans.

The **legislative framework** establishing the planning and development system and setting out its statutory procedures is provided by the *Development Act 1993* and its associated *Development Regulations 2008*. The Development Act is the core legislation enacted by the South Australian Parliament to establish the planning and development system framework and many of the processes required to be followed within that framework (including processes for assessing development applications). The Regulations provide more details about the framework and are updated from time to time by the Governor (on the advice of the Minister for Planning).

The State Government's broad vision for sustainable land use and the built development of the state is outlined in the **Planning Strategy**. The relevant volume of the Planning Strategy for this Development Plan is The 30-Year Plan for Greater Adelaide (February 2010).

The Planning Strategy, which covers a full range of social, economic and environmental issues, informs and guides policies both across Government and in local area Development Plans. The Planning Strategy is required under section 22 of the *Development Act 1993* and is updated by the State Government every few years. Local councils also prepare strategic plans which guide the same matters but at a local level. These strategic plans are not, however, development assessment tools, that is the role of Development Plans.

**Development Plans** are the key on-the-ground development assessment documents in South Australia. They contain the rules that set out what can be done on any piece of land across the state, and the detailed criteria against which development applications will be assessed. Development Plans cover distinct and separate geographic areas of the state. There is a separate Development Plan for each one of the 68 local council areas, plus a handful of other Development Plans covering areas not situated within local government boundaries. Development Plans outline what sort of developments and land use are and are not envisaged for particular zones (eg residential, commercial, industrial), and various objectives, principles and policies further controlling and affecting the design and other aspects of proposed developments.

## What is Development?

'Development' is defined in Section 4 of the *Development Act 1993* as:

- a change in the use of land or buildings
- the creation of new allotments through land division (including Strata and Community Title division)
- building work (including construction, demolition, alteration and associated excavation/fill)
- cutting, damaging or felling of significant trees
- specific work in relation to State and Local heritage places
- prescribed mining operations
- other acts or activities in relation to land as declared by the Development Regulations.

No development can be undertaken without an appropriate **Development Approval** being obtained from the relevant authority after an application and assessment process.

## How does the Development Plan relate to other legislation?

The Development Plan is a self-contained policy document prepared under and given statutory recognition pursuant to *the Development Act 1993*.

It is generally independent of other legislation but is one of many mechanisms that control or manage the way that land and buildings are used.

The *Development Act, 1993* and *Development Regulations, 2008* contain a number of provisions to ensure that development applications are referred to other government agencies when appropriate.

## What doesn't a Development Plan do?

Development Plans are applicable only when new development is being designed or assessed. They do not affect existing development (see above for a description of what constitutes 'development').

Once a Development Approval is issued, the details contained within the application and any conditions attached to that approval are binding.

Development Plan policies guide the point in time assessment of a development application but do not generally seek to control the on-going management of land, which is the role of other legislation (eg the *Environment Protection Act 1993*, *Natural Resources Management Act 2004*, *Liquor Licensing Act 1997*).

## When do you use the Development Plan?

The Development Plan should be used during a development application process. This may include:

- when undertaking or proposing to undertake 'development' (eg building a house or factory or converting an office into a shop)
- when assessing or determining a development proposal (eg by council staff, elected members of council, a Development Assessment Panel, or the Development Assessment Commission)
- when you believe you could be affected by a proposed development and you are given an opportunity to comment on it as part of the assessment process.

## How to read the Development Plan

Development Plans are comprised of several sections as described below.

**All** sections and **all** relevant provisions within each section of the Development Plan must be considered in relation to a development proposal or application.

Development Plans use three text font colours:

- (a) Black text is used to identify all standard policy that forms the basis of all council Development Plans.

- (b) Green text is used to identify additional council-specific policy or variables that have been included in the Development Plan to reflect local circumstances.
- (c) Blue text illustrates hyperlinks to maps, overlays and tables in the Development Plan. These hyperlinks are operational only when viewing electronic versions of the Development Plan.

## Development Plan Structure Overview

Advisory Section	Function
<b>Table of Contents</b>	Navigational aid to reference sections within the Development Plan by name and page number.
<b>Amendment Record Table</b>	Tabled information recording previously-authorized Development Plan amendments and their consolidation dates.
<b>Introduction</b> Overview of the Planning System What is Development? How does the Development Plan relate to other legislation? What doesn't a Development Plan do? When do you use the Development Plan? How to read the Development Plan?	A general overview of the context, purpose and way a Development Plan is set out (this section is advisory only and not used for development assessment purposes).
<b>Strategic Setting</b> State Strategic Setting <i>(The 30-Year Plan for Greater Adelaide/Regional Planning Strategy)</i> Council Strategic Setting <i>(Council Strategy)</i>	To be developed, but intended to reflect the relevant Planning Strategy (as it relates to the council area) and council's own local strategic investigations.
<b>Council Preface Map</b>	Map of the entire Development Plan boundary and its spatial relationship to other Development Plans' boundaries.
Assessment Section	Function
<b>General Provisions</b> Objectives Principles of Development Control	These policies apply across the whole council area and relate to a range of social, environmental, and economic development issues such as: <ul style="list-style-type: none"> <li>▪ site and design criteria</li> <li>▪ access and vehicle parking requirements</li> <li>▪ heritage and conservation measures</li> <li>▪ environmental issues</li> <li>▪ hazards</li> <li>▪ infrastructure requirements</li> <li>▪ land use specific requirements.</li> </ul> They establish the development standards that apply to all forms of development and provide a yardstick against which the suitability of development proposals is measured.

Assessment Section	Function
<b>Zone Provisions</b>	<p>These policies give greater certainty and direction about where certain forms of developments should be located. Maps are referenced within zones that show where land uses are suitable to be located.</p> <p>Generally, envisaged forms of development within a zone are identified and encouraged through carefully worded policies.</p>
Desired Character Statements	These express a vision about how the zone should look and feel in the future. They may describe the valued elements of the neighbourhood or area to be retained and/or what level and nature of change is desired.
Objectives	These are the specific planning policies that determine what land uses are encouraged or discouraged in the zone. They often contain detailed provisions to further guide the scale and design of development.
Principles of Development Control	These also provide lists of complying and non-complying development and any public notification provisions that vary from those in the Development Regulations.
Policy Area	Policy areas apply to a portion of a zone and contain additional objectives, desired character statements and principles of development control for that portion.
Precincts	<p>Precincts are used to express policies for a small sub-area of a zone or a policy area.</p> <p>Precincts are used if additional site-specific principles of development control are needed to reflect particular circumstances associated with those sub-areas.</p>
Procedural Matters	<p>All zones have a procedural matters section that identifies and lists complying, non-complying and public notification categories for various forms of development.</p> <p>Policy areas and/or precincts, which are a sub-set of the zone, share this procedural matters section. Their respective lists can be modified to accommodate policy area and precinct variations.</p>
<b>Tables</b>	<p>These tables provide detailed data for the assessment of certain elements of development, for example, numeric values for setbacks from road boundaries and car parking rates for certain types of development.</p> <p>Conditions for complying development are grouped into their respective tables.</p>
<b>Mapping</b>	
Structure Plan Maps	Structure Plan maps will commonly show the general arrangement and broad distribution of land uses; key spatial elements; and movement patterns throughout the council area and major urban areas.
Council Index Maps	<p>This is the first point of reference when determining the appropriate map(s) applying to a specific property.</p> <p>An enlargement index map may be included where needed, eg for large townships.</p>



Assessment Section	Function
Extent Map Series Location Maps	<p>Individual overlay and spatial-based maps (based on the Council Index Maps) originate from a single Location Map and 'drill down' through relevant extent maps affecting that location.</p> <p><i>Note: the entire council area will always be represented as the first map in the extent map series and will commence as map 1.</i></p>
Overlay Maps	<p>Used to show issue areas or features that run across a number of zones, and are spatially defined to a cadastre, for example:</p> <ul style="list-style-type: none"> <li>▪ Transport</li> <li>▪ Development Constraints</li> <li>▪ Heritage</li> <li>▪ Natural Resources.</li> </ul> <p><i>Note: issues that are not spatially defined to a cadastre can appear in this section; however they will be presented as illustrative maps only.</i></p>
Zone Maps	Used to determine which zone applies to which land.
Policy Area Maps	Used to depict the presence and location of any applicable policy area.
Precinct Maps	Used to depict the presence and location of any applicable precincts.
Bushfire Maps <i>(where applicable)</i> Bushfire Protection Area BPA Maps - Bushfire Risk	Bushfire Protection Area - BPA Maps are used to determine the potential bushfire risk (high, medium or general), associated with an allotment located within an area prone to bushfires.
Concept Plan Maps	<p>Concept Plans are used to depict graphically key features and conceptual layouts of how specific areas should be developed.</p> <p>Concept Plans appear at the end of the extent map series as a separate section. Concept Plans are consecutively numbered, commencing with number 1.</p>

### Further info

Contact the the [City of West Torrens](#).

Visit the Department of Planning, Transport and Infrastructure website: [www.saplanningportal.sa.gov.au](http://www.saplanningportal.sa.gov.au).

Discuss your matter with your planning consultant.

## State Strategic Setting

### Background

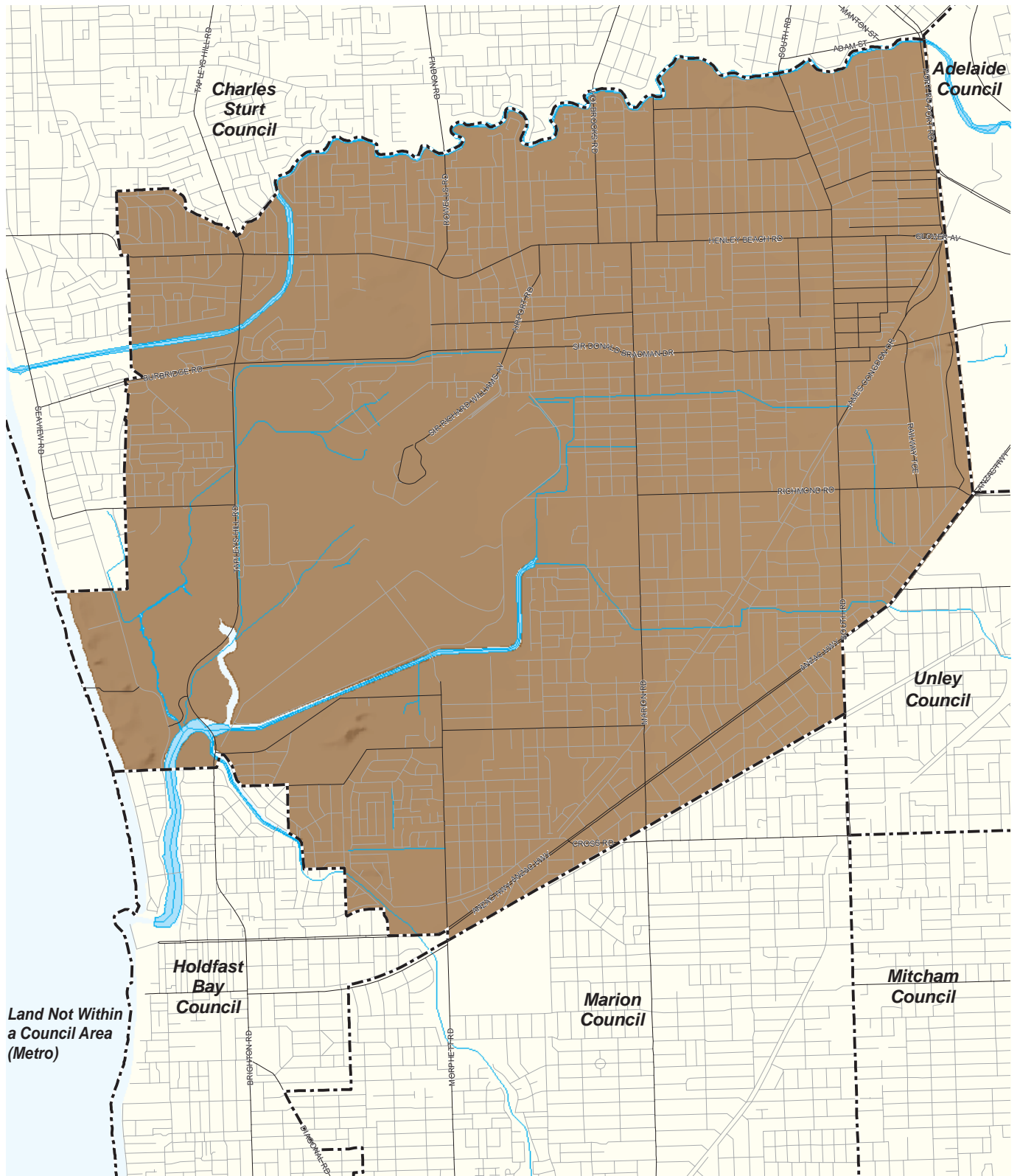
Metropolitan Adelaide is bounded on the west by Gulf St. Vincent and on the east by the South Mount Lofty Ranges which rise to over 700 metres. The city and suburbs lie between, built on the gently sloping Adelaide Plain, and backed by the steep and beautiful escarpment of the ranges.

Although the native vegetation has been largely cleared from the plains, there are still significant areas of bushland in the ranges within a short distance of the city.

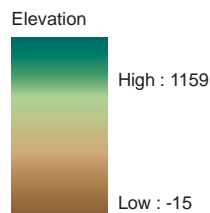
In the 1830s the South Australian Association was formed in England. An Act was passed and Colonel William Light was despatched to the new colony to lay out the first town. Late in 1836, Light selected a site astride the River Torrens 10 kilometres inland from the coast. His plan of Adelaide with its wide streets and surrounding belt of parklands is widely acclaimed.

Light was also instructed to lay out the surrounding district and his framework of roads has subsequently become the main road system of the metropolitan area. The early settlements on the plains and along the coast have now coalesced to form one built-up area, with the main business area centrally located in the original settlements near the River Torrens.

As the State capital, Adelaide is the centre of government administration; it is the financial, commercial and transport centre, and is the principal focus for shopping, entertainment and cultural activities.



To identify the precise location of the Development Plan boundary refer to Council Index Map then select the relevant map number.



# Council Preface Map



# **General Section**



## Advertisements

### OBJECTIVES

- 1 Urban and rural landscapes that are not disfigured by advertisements and/or advertising hoardings.
- 2 Advertisements and/or advertising hoardings that do not create a hazard.
- 3 Advertisements and/or advertising hoardings designed to enhance the appearance of the building and locality.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 The location, siting, design, materials, size, and shape of advertisements and/or advertising hoardings should be:
  - (a) consistent with the predominant character of the urban or rural landscape
  - (b) in harmony with any buildings or sites of historic significance or heritage value in the area
  - (c) co-ordinated with and complement the architectural form and design of the building they are to be located on.
- 2 The number of advertisements and/or advertising hoardings associated with a development should be minimised to avoid:
  - (a) clutter
  - (b) disorder
  - (c) untidiness of buildings and their surrounds
  - (d) driver distraction.
- 3 Buildings occupied by a number of tenants should exhibit co-ordinated and complementary advertisements and/or advertising hoardings to identify the tenants and their type of business.
- 4 The content of advertisements should be limited to information relating to the legitimate use of the associated land.
- 5 Advertisements and/or advertising hoardings should:
  - (a) be completely contained within the boundaries of the subject allotment
  - (b) be sited to avoid damage to, or pruning or lopping of, on-site landscaping or street trees
  - (c) not obscure views to vistas or objects of high amenity value.
- 6 Advertisements and/or advertising hoardings should not be erected on:
  - (a) a public footpath or veranda post
  - (b) a road, median strip or traffic island
  - (c) a vehicle adapted and exhibited primarily as an advertisement

**West Torrens Council**  
**General Section**  
**Advertisements**

- (d) residential land.
- 7 Advertisements and/or advertising hoardings attached to buildings should not be sited on the roof or higher than the walls of a building.
- 8 The total advertisement area on the fascias, parapets, gable ends, windows and other surfaces of buildings should not exceed:
  - (a) 20 per cent of the sides of the building
  - (b) in relation to the front wall of a building, 20 per cent of the area above 3.7 metres or above a canopy.
- 9 Advertisements and/or advertising hoardings erected on a veranda or that project from a building wall should:
  - (a) have a clearance over a footway to allow for safe and convenient pedestrian access
  - (b) where erected on the side of a veranda, not exceed the width of the veranda or project from the veranda
  - (c) where erected on the front of a veranda, not exceed the length of the veranda or project from the veranda
  - (d) where projecting from a wall, have the edge of the advertisement or advertising hoarding abutting the surface of the wall.
- 10 Advertisements should be designed to conceal their supporting advertising hoarding from view.
- 11 Advertisements should convey the owner/occupier and/or generic type of business, merchandise or services using simple, clear and concise language, symbols, print style and layout and a small number of colours.
- 12 Advertisements which perform a secondary role in identifying the business, goods or services should only be readable in the immediate vicinity of the site.
- 13 Outside of townships and country settlements advertisements other than traffic signs, tourist signs or advertisements on an existing tourist information bay display board, should not be erected in road reserves.

**Safety**

- 14 Advertisements and/or advertising hoardings should not create a hazard by:
  - (a) being so highly illuminated as to cause discomfort to an approaching driver, or to create difficulty in the driver's perception of the road or persons or objects on the road
  - (b) being liable to interpretation by drivers as an official traffic sign, or convey to drivers information that might be confused with instructions given by traffic signals or other control devices, or impair the conspicuous nature of traffic signs or signals
  - (c) distracting drivers from the primary driving task at a location especially where the demands on driver concentration are high
  - (d) obscuring a driver's view of other road or rail vehicles at/or approaching level crossings, or of pedestrians or of features of the road that are potentially hazardous (eg junctions, bends, changes in width, traffic control devices).



- 15 Any internally illuminated advertising signs and/or advertising hoardings which utilise LED, LCD or other similar technologies should be located a minimum of 80 metres from traffic signals, level crossings and other important traffic control devices.

### Free Standing Advertisements

- 16 Free standing advertisements and/or advertising hoardings should be:
- (a) limited to only one primary advertisement per site or complex
  - (b) of a scale and size in keeping with the desired character of the locality and compatible with the development on the site.
- 17 Except where otherwise specified in a particular zone, policy area or precinct, free standing advertisements should be designed within the following parameters:

Zone/Policy Area	Advertisement area (square metres)	Additional advertisement area per metre of site frontage with a public road or public thoroughfare (square metres)	Maximum height (metres)
District Centre Zone Neighbourhood Centre Zone	6	0.15	9
Arterial Roads Policy Area 1 Local Centre Zone	4	0.1	7
Other non-residential zones	2	0.1	6

- 18 Free standing advertisements and/or advertising hoardings for multiple-business tenancy complexes should:
- (a) incorporate the name or nature of each business or activity within the site or complex in a single advertisement
  - (b) be integrally designed and mounted below the more predominant main complex or site identity advertisement.
- 19 Portable, easel or A-frame advertisements should be displayed only where:
- (a) no other appropriate opportunity exists for an adequate co-ordinated and permanently erected advertisement and/or advertising hoarding
  - (b) they do not obstruct or compromise the safety of pedestrians or vehicle movement
  - (c) there is no unnecessary duplication or proliferation of advertising information
  - (d) there is no damage to, or removal of, any landscaping on the site.
- 20 Portable, easel or A-frame advertisements associated with a development should be displayed only during the hours the development is open for trading.

### Flags, Bunting and Streamers

- 21 Advertisements and/or advertising hoardings incorporating any flags, bunting, streamers, or suspended objects should:
- (a) be placed or arranged to complement and accord with the scale of the associated development

- (b) other than flags, not be positioned higher than the building they are attached or related to
- (c) not be displayed in residential areas.

### **Advertising along Arterial Roads**

- 22 Advertising and/or advertising hoardings should not be placed along arterial roads that have a speed limit of 80 km/h or more.

### **Advertising in Mixed Use, Urban Core and Corridor Zones**

- 23 Advertisements and/or advertising hoardings should be:
- (a) no higher than the height of the finished floor level of the second storey of the building to which it relates
  - (b) where located below canopy level, flush with the wall or projecting horizontally
  - (c) where located at canopy level, in the form of a fascia sign
  - (d) where located above the canopy, flush with the wall and within the height of the parapet.
- 24 Advertisements or advertising hoardings should not exceed 25 per cent of the ground floor wall area on the façade the sign is placed.

## Animal Keeping

### OBJECTIVES

- 1 Animals not kept at a density beyond the carrying capacity of the land or water.
- 2 Animal keeping development sited and designed to avoid adverse effects on surrounding development.
- 3 Intensive animal keeping protected from encroachment by incompatible development.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Animal keeping and associated activities should not create adverse impacts on the environment or the amenity of the locality.
- 2 Storage facilities for manure, used litter and other wastes should be designed and sited:
  - (a) to be vermin proof
  - (b) with an impervious base
  - (c) to ensure that all clean rainfall runoff is excluded from the storage area
  - (d) outside the 1-in-100 year average return interval flood event area.

### Horse Keeping

- 3 Stables, horse shelters or associated yards should be sited:
  - (a) at least 50 metres from a watercourse
  - (b) on land with a slope no greater than 1-in-10.
- 4 A concrete drainage apron should be provided along the front of stables directing water from wash-down areas onto a suitably vegetated area that can absorb all the water, or into a constructed drainage pit.
- 5 Stables, horse shelters or associated yards should be sited at least 30 metres from any dwelling on the site and from the nearest allotment boundary to avoid adverse impacts from dust, erosion and odour.
- 6 All areas accessible to horses should be separated from septic tank drainage areas.

### Intensive Animal Keeping

- 7 Intensive animal keeping operations and their associated components, including holding yards, temporary feeding areas, movement lanes and similar, should not be located on land within any of the following areas:
  - (a) 800 metres of a public water supply reservoir
  - (b) the 1-in-100 year average return interval flood event area of any watercourse
  - (c) 200 metres of a major watercourse (third order or higher stream)
  - (d) 100 metres of any other watercourse, bore or well used for domestic or stock water supplies

- (e) 2000 metres of a defined and zoned township, settlement or urban area (except for land based aquaculture)
  - (f) 500 metres of a dwelling (except for a dwelling directly associated with the intensive animal keeping facility).
- 8 Intensive animal keeping operations in uncovered situations should incorporate:
- (a) a controlled drainage system which:
    - (i) diverts runoff from external areas
    - (ii) directs surface runoff into an effluent management system that has sufficient capacity to hold runoff from the controlled drainage area
  - (b) pen floors which:
    - (i) ensure that effluent does not infiltrate and contaminate groundwater or soil
    - (ii) are graded to a consistent uniform slope of between 2 per cent and 6 per cent
  - (c) effluent drainage into an effluent lagoon(s) that has sufficient capacity to hold runoff from the controlled drainage area.
- 9 Intensive animal keeping facilities and associated wastewater lagoons and liquid/solid waste disposal areas should be sited, designed, constructed and managed to avoid adverse odour impacts on nearby sensitive land uses.

### **Kennels**

- 10 The floor of kennels should be constructed of concrete or similar impervious material and be designed to allow for adequate drainage when kennels are cleaned.
- 11 Kennels and exercise yards should be designed and sited to minimise noise nuisance to neighbours through:
- (a) orienting their openings away from sensitive land uses such as dwellings
  - (b) siting them as far as practicable from allotment boundaries.
- 12 Kennels should occur only where there is a permanently occupied dwelling on the land.

### **Land Based Aquaculture**

- 13 Land-based aquaculture and associated components should not be located on land within 500 metres of a defined and zoned township, settlement or urban area.
- 14 Land-based aquaculture ponds should be sited and designed to:
- (a) prevent surface flows from entering the ponds in a 1-in-100 year average return interval flood event
  - (b) prevent pond leakage that would pollute groundwater
  - (c) prevent any overflow that would enable the farmed species escaping and entering into any waters
  - (d) minimise the need for intake and discharge pipes to traverse sensitive environments.
- 15 Buildings associated with land-based aquaculture should provide enclosed storage areas to accommodate all equipment associated with aquaculture operations in a manner which is integrated with the use of the land.

- 16 Development should ensure that pipe inlet and outlets associated with land-based aquaculture are located to minimise the risk of disease transmission.

#### Marine Based Aquaculture

- 17 Marine aquaculture and other offshore development should be ecologically sustainable and be located, designed, constructed and managed to:
- (a) minimise adverse impacts on marine habitats and ecosystems, and public access to beaches, public watercourses or the foreshore
  - (b) take into account the requirements of traditional indigenous and commercial fishing grounds
  - (c) ensure satisfactory removal and disposal of litter, disused material, debris, detritus and dead animals from the development
  - (d) prevent the build up of waste.
- 18 In marine waters, marine aquaculture (other than inter tidal aquaculture) and other offshore development should be located a minimum of 100 metres seaward of the high-water mark.
- 19 Marine aquaculture development should not significantly obstruct or adversely affect any of the following:
- (a) areas of high public use
  - (b) areas established for recreational activities
  - (c) areas of outstanding visual, environmental, commercial or tourism value
  - (d) sites, including beaches, used for recreational activities such as swimming, fishing, skiing, sailing and other water sports.
- 20 Marine aquaculture should be sited, designed, constructed and managed to minimise interference and obstruction to the natural processes of the coastal and marine environment.
- 21 Marine aquaculture should be developed in areas where an adequate water current exists to disperse sediments and be sited a sufficient height above the sea floor to:
- (a) prevent the fouling of waters, publicly owned wetlands or the nearby coastline
  - (b) minimise seabed damage.
- 22 Racks, floats and other farm structures associated with marine aquaculture or other offshore development should where practicable be visually unobtrusive from the shoreline.
- 23 Marine aquaculture development should:
- (a) use feed hoppers that are painted in subdued colours and suspended as low as possible above the water
  - (b) position structures to protrude the minimum distance practicable above water
  - (c) avoid the use of shelters and structures above cages and platforms unless necessary to exclude predators and protected species from interacting with the farming structures and/or stock inside the cages, or for safety reasons.
- 24 Marine aquaculture should be developed to maintain existing rights of way within or adjacent to a site.

- 25 Marine aquaculture access, launching and maintenance facilities should:
- (a) where possible, use existing and established roads, tracks, ramps and paths to or from the sea
  - (b) be developed cooperatively and co-located.
- 26 Marine aquaculture and other offshore development should be located at least:
- (a) 550 metres from a proclaimed shipwreck
  - (b) 1000 metres seaward from the boundary of any reserve under the *National Parks and Wildlife Act 1972*, unless a lesser distance is agreed with the Minister responsible for that *Act*.
- 27 Marine aquaculture development should be located so as not to obstruct nor interfere with navigation channels, access channels, frequently used natural launching sites, safe anchorage areas, known diving areas, commercial shipping lanes or activities associated with existing jetties and wharves.
- 28 Marine aquaculture development should contribute to navigational safety by being:
- (a) suitably marked for navigational purposes
  - (b) sited to allow an adequate distance between farms for safe navigation
  - (c) located at least 250 metres from a commercial shipping lane
  - (d) comprised of structures that are secured and/or weighted to prevent drifting from the licensed site
  - (e) able to be rehabilitated when no longer operational.

## Building near Airfields

### OBJECTIVES

- 1 Development that ensures the long-term operational, safety, commercial and military aviation requirements of airfields (airports, airstrips and helicopter landing sites) continue to be met.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 The height and location of buildings and structures should not adversely affect the long-term operational, safety, commercial and military aviation requirements of airfields.
- 2 Buildings and structures that exceed the airport building heights as shown on the *Overlay Maps - Development Constraints* should not be developed unless a safety analysis determines that the building/structure does not pose a hazard to aircraft operations.
- 3 Development in the vicinity of airfields should not create a risk to public safety, in particular through any of the following:
  - (a) lighting glare
  - (b) smoke, dust and exhaust emissions
  - (c) air turbulence
  - (d) storage of flammable liquids
  - (e) attraction of birds
  - (f) reflective surfaces (eg roofs of buildings, large windows)
  - (g) materials that affect aircraft navigational aids.
- 4 Outdoor lighting within 6 kilometres of an airport should be designed so that it does not pose a hazard to aircraft operations.
- 5 Development that is likely to increase the attraction of birds should not be located within 3 kilometres of an airport used by commercial aircraft. If located closer than 3 kilometres the facility should incorporate bird control measures to minimise the risk of bird strikes to aircraft.
- 6 Development within areas affected by aircraft noise should be consistent with *Australian Standard AS2021 - Acoustics - Aircraft Noise Intrusion - Building Siting and Construction*.
- 7 Residential development on land within areas affected by aircraft noise as shown on [Overlay Map WeTo/8 - Development Constraints](#) should incorporate noise attenuation measures.

## **Bulk Handling and Storage Facilities**

### **OBJECTIVES**

- 1 Facilities for the bulk handling and storage of agricultural and other commodities sited and designed to minimise adverse impacts on the landscape and on and from surrounding land uses.

### **PRINCIPLES OF DEVELOPMENT CONTROL**

- 1 Facilities for the handling, storage and dispatch of commodities in bulk should be:
  - (a) located in bulk handling, industry or primary production type zones
  - (b) sited, designed and operated to minimise risks of contamination to the environment and adverse impacts on nearby sensitive land uses and from surrounding land uses.
- 2 Development of facilities for the handling, transportation and storage of bulk commodities should have:
  - (a) areas set aside on the site of the development for the marshalling and manoeuvring of vehicles attending the site
  - (b) roadways and parking areas surfaced in a manner sufficient to control dust emissions from the site
  - (c) vehicle circulation between activity areas contained within the site and without the need to use public roads
  - (d) landscaping, using locally indigenous plant species wherever practical, established within the site for the purpose of providing shade and shelter, and to assist with screening and dust filtration
  - (e) a buffer area for the establishment of dense landscaping adjacent road frontages
  - (f) security fencing around the perimeter of the site.
- 3 Temporary bunkers for storage should not compromise the efficient circulation and parking of vehicles within the site.
- 4 Access to and from the site should be designed to allow simultaneous movement of vehicles entering and exiting in a forward direction to minimise interference to other traffic using adjacent public roads.



## Centres and Retail Development

### OBJECTIVES

- 1 Shopping, administrative, cultural, community, entertainment, educational, religious and recreational facilities located in integrated centres and mixed use zones.
- 2 Centres that ensure rational, economic and convenient provision of goods and services and provide:
  - (a) a focus for community life
  - (b) safe, permeable, pleasant and accessible walking and cycling networks.
- 3 The provision of a safe pedestrian and cycling environments within centres which gives high priority to pedestrians, public and community transport.
- 4 Increased vitality and activity in centres through the introduction and integration of housing.
- 5 The central business district of the City of Adelaide providing the principal focus for the economic, social and political life of Greater Adelaide and the State.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development within centres should:
  - (a) integrate facilities within the zone
  - (b) allow for the multiple use of facilities and the sharing of utility spaces
  - (c) allow for the staging of development within the centre
  - (d) be integrated with public and community transport.
- 2 Development within centres should be designed to be compatible with adjoining areas. This should be promoted through landscaping, screen walls, centre orientation, location of access ways, buffer strips and transitional use areas.
- 3 Development within centres should provide:
  - (a) public spaces such as malls, plazas and courtyards
  - (b) street furniture, including lighting, signs, litter bins, seats and bollards, that is sited and designed to complement the desired character
  - (c) unobtrusive facilities for the storage and removal of waste materials
  - (d) public facilities including toilets, infant changing facilities for parents, telephones and community information boards
  - (e) access for public and community transport and sheltered waiting areas for passengers
  - (f) lighting for pedestrian paths, buildings and associated areas
  - (g) a single landscaping theme
  - (h) safe and secure bicycle parking.

### **Arterial Roads**

- 4 Centres should develop on one side of an arterial road or in one quadrant of an arterial road intersection.
- 5 Centre development straddling an arterial road should:
  - (a) concentrate on one side of the arterial road or one quadrant of the arterial road intersection
  - (b) minimise the need for pedestrian and vehicular movement from one part of the centre to another across the arterial road.

### **Retail Development**

- 6 Bulky goods outlets should only be located in centres and bulky goods zones.
- 7 Bulky goods outlets located within centres zones should:
  - (a) complement the overall provision of facilities
  - (b) be sited towards the periphery of those centres where the bulky goods outlet has a gross leaseable area of 500 square metres or more.

## Coastal Areas

### OBJECTIVES

- 1 The protection and enhancement of the natural coastal environment, including environmentally important features of coastal areas such as mangroves, wetlands, sand dunes, cliff-tops, native vegetation, wildlife habitat shore and estuarine areas.
- 2 Protection of the physical and economic resources of the coast from inappropriate development.
- 3 Preservation of areas of high landscape and amenity value including stands of vegetation, shores, exposed cliffs, headlands, islands and hill tops, and areas which form an attractive background to urban and tourist areas.
- 4 Development that maintains and/or enhances public access to coastal areas with minimal impact on the environment and amenity.
- 5 Development only undertaken on land which is not subject to or that can be protected from coastal hazards including inundation by storm tides or combined storm tides and stormwater, coastal erosion or sand drift, and probable sea level rise.
- 6 Development that can accommodate anticipated changes in sea level due to natural subsidence and probable climate change during the first 100 years of the development.
- 7 Development which will not require, now or in the future, public expenditure on protection of the development or the environment.
- 8 Management of development in coastal areas to sustain or enhance the remaining natural coastal environment.
- 9 Low intensity recreational uses located where environmental impacts on the coast will be minimal.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should be compatible with the coastal environment in terms of built-form, appearance and landscaping including the use of walls and low pitched roofs of non-reflective texture and natural earth colours.

#### Environmental Protection

- 2 The coast should be protected from development that would adversely affect the marine and onshore coastal environment, whether by pollution, erosion, damage or depletion of physical or biological resources, interference with natural coastal processes or any other means.
- 3 Development should not be located in delicate or environmentally-sensitive coastal features such as sand dunes, cliff-tops, wetlands or substantially intact strata of native vegetation.
- 4 Development should not be undertaken where it will create or aggravate coastal erosion, or where it will require coast protection works which cause or aggravate coastal erosion.
- 5 Development should be designed so that solid/fluid wastes and stormwater runoff is disposed of in a manner that will not cause pollution or other detrimental impacts on the marine and on-shore environment of coastal areas.
- 6 Effluent disposal systems incorporating soakage trenches or similar should prevent effluent migration onto the inter-tidal zone and be sited at least 100 metres from whichever of the following requires the greater distance:

- (a) the mean high-water mark at spring tide, adjusted for any subsidence for the first 50 years of development plus a sea level rise of 1 metre
  - (b) the nearest boundary of any erosion buffer determined in accordance with the relevant provisions in this Development Plan.
- 7 Development that proposes to include or create confined coastal waters, as well as water subject to the ebb and flow of the tide should be designed to ensure the quality of such waters is maintained at an acceptable level.
- 8 Development should be designed and sited so that it does not prevent natural landform and ecological adjustment to changing climatic conditions and sea levels and should allow for the following:
- (a) the unrestricted landward migration of coastal wetlands
  - (b) new areas to be colonised by mangroves, samphire and wetland species
  - (c) sand dune drift
  - (d) where appropriate, the removal of embankments that interfere with the abovementioned processes.

### **Maintenance of Public Access**

- 9 Development should maintain or enhance public access to and along the foreshore.
- 10 Development should provide for a public thoroughfare between the development and any coastal reserve.
- 11 Other than small-scale infill development in a predominantly urban zone, development adjacent to the coast should not be undertaken unless it incorporates an existing or proposed public reserve, not including a road or erosion buffer, of at least 50 metres width between the development and the landward toe of the frontal dune or the top edge of an escarpment.
- 12 If an existing reserve is less than 50 metres wide, the development should incorporate an appropriate width of reserve to achieve a total 50 metres wide reserve.
- 13 Except where otherwise specified in a particular zone or policy area, buildings on land abutting coastal reserves should be set back either a distance of 8 metres from any boundary with the reserve or in line with adjacent development, whichever is the greater distance.
- 14 Development that abuts or includes a coastal reserve should be sited and designed to be compatible with the purpose, management and amenity of the reserve, as well as to prevent inappropriate access to the reserve.
- 15 Development, including marinas and aquaculture, should be located and designed to ensure convenient public access along the waterfront to beaches and coastal reserves is maintained, and where possible enhanced through the provision of one or more of the following:
- (a) pedestrian pathways and recreation trails
  - (b) coastal reserves and lookouts
  - (c) recreational use of the water and waterfront
  - (d) safe public boating facilities at selected locations
  - (e) vehicular access to points near beaches and points of interest
  - (f) car parking.

- 16 Where a development such as a marina creates new areas of waterfront, provision should be made for public access to, and recreational use of, the waterfront and the water.
- 17 Public access through sensitive coastal landforms, particularly sand dunes, wetlands and cliff faces, should be restricted to defined pedestrian paths constructed to minimise adverse environmental impact.
- 18 Access roads to the coast and lookouts should preferably be spur roads rather than through routes, other than tourist routes where they:
  - (a) do not detract from the amenity or the environment
  - (b) are designed for slow moving traffic
  - (c) provide adequate car parking.

### **Hazard Risk Minimisation**

- 19 Development and its site should be protected against the standard sea-flood risk level which is defined as the 1-in-100 year average return interval flood extreme sea level (tide, stormwater and associated wave effects combined), plus an allowance to accommodate land subsidence until the year 2100.
- 20 Development including associated roads and parking areas, other than minor structures unlikely to be adversely affected by flooding, should be protected from sea level rise by ensuring all of the following apply:
  - (a) site levels are at least 0.3 metres above the standard sea-flood risk level
  - (b) building floor levels are at least 0.55 metres above the standard sea-flood risk level
  - (c) there are practical measures available to protect the development against an additional sea level rise of 0.7 metres, plus an allowance to accommodate land subsidence until the year 2100 at the site.
- 21 Buildings to be sited over tidal water or which are not capable of being raised or protected by flood protection measures in future, should have a floor level of at least 1.25 metres above the standard sea-flood risk level.
- 22 Development that requires protection measures against coastal erosion, sea or stormwater flooding, sand drift or the management of other coastal processes at the time of development, or in the future, should only be undertaken if all of the following apply:
  - (a) the measures themselves will not have an adverse effect on coastal ecology, processes, conservation, public access and amenity
  - (b) the measures do not nor will not require community resources, including land, to be committed
  - (c) the risk of failure of measures such as sand management, levee banks, flood gates, valves or stormwater pumping, is acceptable relative to the potential hazard resulting from their failure
  - (d) binding agreements are in place to cover future construction, operation, maintenance and management of the protection measures.
- 23 Development should not compromise the structural integrity of any sea wall or levee bank adjacent to the foreshore, or compromise its capacity to protect against coastal flooding and erosion.

### **Erosion Buffers**

- 24 Development should be set back a sufficient distance from the coast to provide an erosion buffer (in addition to a public reserve) which will allow for at least 100 years of coastal retreat for single buildings or small scale developments, or 200 years of coastal retreat for large scale developments (ie new townships) unless either of the following applies:

- (a) the development incorporates appropriate private coastal protection measures to protect the development and public reserve from the anticipated erosion.
  - (b) the council is committed to protecting the public reserve and development from the anticipated coastal erosion.
- 25 Where a coastal reserve exists or is to be provided it should be increased in width by the amount of any required erosion buffer. The width of an erosion buffer should be based on the following:
- (a) the susceptibility of the coast to erosion
  - (b) local coastal processes
  - (c) the effect of severe storm events
  - (d) the effect of a 0.3 metres sea level rise over the next 50 years on coastal processes and storms
  - (e) the availability of practical measures to protect the development from erosion caused by a further sea level rise of 0.7 metres per 50 years thereafter.
- 26 Development should not occur where essential services cannot be economically provided and maintained having regard to flood risk and sea level rise, or where emergency vehicle access would be prevented by a 1-in-100 year average return interval flood event, adjusted for 100 years of sea level rise.

### **Land Division**

- 27 Land in coastal areas should only be divided if:
- (a) it or the subsequent development and use of the land will not adversely affect the management of the land, adjoining land or the coast
  - (b) sand dunes, wetlands and substantially intact strata of native vegetation are maintained or consolidated within single allotments.
- 28 Land division in coastal areas outside of designated urban or settlement zones should not increase either of the following:
- (a) the number of allotments abutting the coast or a reserve
  - (b) the number of allotments, including community title allotments and those that incorporate rights of way, with direct access to the coast or a reserve.
- 29 Land should not be divided for commercial, industrial or residential purposes unless a layout can be achieved whereby roads, parking areas and development sites on each allotment are at least 0.3 metres above the standard sea-flood risk level, unless the land is, or can be provided with appropriate coastal protection measures.

### **Protection of Economic Resources**

- 30 Development should be sited, designed and managed so as not to conflict with or jeopardise the continuance of an existing aquaculture development.

### **Development in Appropriate Locations**

- 31 Development along the coast should be in the form of infill in existing developed areas or concentrated into appropriately chosen nodes and not be in a scattered or linear form.
- 32 Development of a kind or scale (eg commercial or large-scale retail) that does not require a coastal location and would not significantly contribute to the community's enjoyment of the coast should not be located in coastal areas.

## **Community Facilities**

### **OBJECTIVES**

- 1 Location of community facilities including social, health, welfare, education and recreation facilities where they are conveniently accessible to the population they serve.
- 2 The proper provision of public and community facilities including the reservation of suitable land in advance of need.

### **PRINCIPLES OF DEVELOPMENT CONTROL**

- 1 Community facilities should be sited and developed to be accessible by pedestrians, cyclists and public and community transport.
- 2 Community facilities should be integrated in their design to promote efficient land use.
- 3 Design of community facilities should encourage flexible and adaptable use of open space and facilities for a range of uses over time.

## Crime Prevention

### OBJECTIVES

- 1 A safe, secure, crime resistant environment where land uses are integrated and designed to facilitate community surveillance.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should be designed to maximise surveillance of public spaces through the incorporation of clear lines of sight, appropriate lighting and the use of visible permeable barriers wherever practicable.
- 2 Buildings should be designed to overlook public and communal streets and public open space to allow casual surveillance.
- 3 Development should provide a robust environment that is resistant to vandalism and graffiti.
- 4 Development should provide lighting in frequently used public spaces including those:
  - (a) along dedicated cyclist and pedestrian pathways, laneways and access routes
  - (b) around public facilities such as toilets, telephones, bus stops, seating, litter bins, automatic teller machines, taxi ranks and car parks.
- 5 Development, including car park facilities should incorporate signage and lighting that indicate the entrances and pathways to, from and within sites.
- 6 Landscaping should be used to assist in discouraging crime by:
  - (a) screen planting areas susceptible to vandalism
  - (b) planting trees or ground covers, rather than shrubs, alongside footpaths
  - (c) planting vegetation other than ground covers a minimum distance of two metres from footpaths to reduce concealment opportunities.
- 7 Site planning, buildings, fences, landscaping and other features should clearly differentiate public, communal and private areas.
- 8 Buildings should be designed to minimise and discourage access between roofs, balconies and windows of adjoining dwellings.
- 9 Public toilets should be located, sited and designed:
  - (a) to promote the visibility of people entering and exiting the facility (eg by avoiding recessed entrances and dense shrubbery that obstructs passive surveillance)
  - (b) near public and community transport links and pedestrian and cyclist networks to maximise visibility.
- 10 Development should avoid pedestrian entrapment spots and movement predictors (eg routes or paths that are predictable or unchangeable and offer no choice to pedestrians).



## Design and Appearance

### OBJECTIVES

- 1 Development of a high **design** standard and appearance that responds to and reinforces positive aspects of the local environment and built form.
- 2 Roads, open spaces, paths, buildings and land uses laid out and linked so that they are easy to understand and navigate.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Buildings should reflect the desired character of the locality while incorporating contemporary designs that have regard to the following:
  - (a) building height, mass and proportion
  - (b) external materials, patterns, colours and decorative elements
  - (c) roof form and pitch
  - (d) façade articulation and detailing
  - (e) verandas, eaves, parapets and window screens.
- 2 The external walls and roofs of buildings should not incorporate highly reflective materials which will result in glare to neighbouring properties or drivers.
- 3 Where a building is sited on or close to a side boundary, the side boundary wall should be sited and limited in length and height to minimise:
  - (a) the visual impact of the building as viewed from adjoining properties
  - (b) overshadowing of adjoining properties and allow adequate sun light to neighbouring buildings.
- 4 Structures located on the roofs of buildings to house plant and equipment should be screened from view and should form an integral part of the building design in relation to external finishes, shaping and colours.
- 5 Balconies should:
  - (a) be integrated with the overall form and detail of the building
  - (b) include balustrade detailing that enables line of sight to the street
  - (c) be recessed where wind would otherwise make the space unusable
  - (d) be self-draining and plumbed to minimise runoff.
- 6 Transportable buildings and buildings which are elevated on stumps, posts, piers, columns or the like, should have their suspended footings enclosed around the perimeter of the building, and the use of verandas, pergolas and other suitable architectural detailing to give the appearance of a permanent structure.

## Development Adjacent Heritage Places

- 7 The design of multi-storey buildings should not detract from the form and materials of adjacent State and local heritage places listed in [Table WeTo/5 – State Heritage Places](#) or in [Table WeTo/4 – Local Heritage Places](#).
- 8 Development on land adjacent to a State or local heritage place, as listed in [Table WeTo/5 – State Heritage Places](#) or in [Table WeTo/4 – Local Heritage Places](#), should be sited and designed to reinforce the historic character of the place and maintain its visual prominence.

## Overshadowing

- 9 The design and location of buildings should enable direct winter sunlight into adjacent dwellings and private open space and minimise the overshadowing of:
  - (a) windows of main internal living areas
  - (b) ground-level private open space
  - (c) upper-level private balconies that provide the primary open space area for a dwelling
  - (d) solar collectors (such as solar hot water systems and photovoltaic cells).

## Visual Privacy

- 10 Development should minimise direct overlooking of the habitable rooms and private open spaces of dwellings through measures such as:
  - (a) appropriate site layout and building orientation
  - (b) off-setting the location of balconies and windows of habitable rooms with those of other buildings so that views are oblique rather than direct to avoid direct line of sight
  - (c) building setbacks from boundaries (including building boundary to boundary where appropriate) that interrupt views or that provide a spatial separation between balconies or windows of habitable rooms
  - (d) screening devices (including fencing, obscure glazing, screens, external ventilation blinds, window hoods and shutters) that are integrated into the building design and have minimal negative effect on residents' or neighbours' amenity.
- 11 Permanently fixed external screening devices should be designed and coloured to complement the associated building's external materials and finishes.

## Relationship to the Street and Public Realm

- 12 Buildings (other than ancillary buildings, group dwellings or buildings on allotments with a battle axe configuration) should be designed so that the main façade faces the primary street frontage of the land on which they are situated.
- 13 Buildings, landscaping, paving and signage should have a co-ordinated appearance that maintains and enhances the visual attractiveness of the locality.
- 14 Buildings should be designed and sited to avoid extensive areas of uninterrupted walling facing areas exposed to public view.
- 15 Building design should emphasise pedestrian entry points to provide perceptible and direct access from public street frontages and vehicle parking areas.

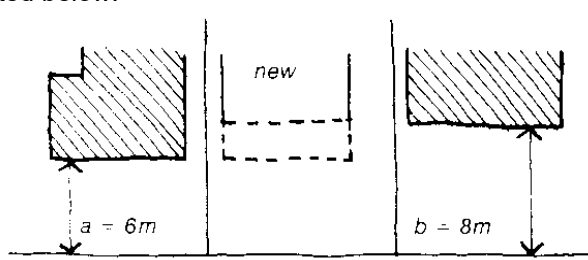
- 16 In mixed use and medium and high density residential areas, development facing the street should be designed to provide interesting and pedestrian friendly street frontages by:
- (a) including features such as frequent doors and display windows, retail shopfronts and/or outdoor eating or dining areas
  - (b) minimising the frontage for fire escapes, service doors, plant and equipment hatches
  - (c) avoiding undercroft, semi-basement or ground floor vehicle parking that is visible from the primary street frontage
  - (d) using colour, vertical and horizontal elements, roof overhangs and other design techniques to provide visual interest and reduce massing
  - (e) including awnings, eaves, verandahs or similar, to the street where setbacks and ground floor uses allow.
- 17 Where zero or minor setbacks are desirable, development should incorporate shelter over footpaths to enhance the quality of the pedestrian environment.
- 18 Multi level vehicle parking areas within buildings should be designed, sited and screened from public view by an appropriate combination of built form, landscaping and/or visual art while still allowing for natural ventilation within these structures.

### **Outdoor Storage and Service Areas**

- 19 Outdoor storage, loading and service areas should be:
- (a) screened from public view by a combination of built form, solid fencing and/or landscaping
  - (b) conveniently located and designed to enable the manoeuvring of service and delivery vehicles
  - (c) sited away from sensitive land uses.

### **Building Setbacks from Road Boundaries**

- 20 Except in areas where a new character is desired, the setback of buildings from public roads should:
- (a) be similar to, or compatible with, setbacks of buildings on adjoining land and other buildings in the locality
  - (b) contribute positively to the function, appearance and/or desired character of the locality.
- 21 Except where specified in a particular zone, policy area or precinct, the main face of a building should be set back from the primary road frontage in accordance with the following table:

Setback difference between buildings on adjacent allotments	Setback of new building
Up to 2 metres	<p>The same setback as one of the adjacent buildings, as illustrated below:</p>  <p style="text-align: center;"><i>When <math>b - a \leq 2</math>, setback of new dwelling = a or b</i></p>

Greater than 2 metres	At least the average setback of the adjacent buildings
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- 22 Except in areas where a new character is desired or where specified in a zone, policy area or precinct, the setback of development from a secondary street frontage should reflect the setbacks of the adjoining buildings and other buildings in the locality.
- 23 All setbacks from the road frontage should be additional to the road widening setback established under the *Metropolitan Adelaide Road Widening Plan Act 1972*.

**Building Setback from River Torrens**

- 24 Buildings and structures should be set back a minimum of 8 metres from the boundary of the **Open Space Zone** plus an additional 1 metre for every metre of vertical wall height above 5 metres.
- 25 Development should not impair, disfigure, interfere with or detrimentally affect the amenity, aesthetic appearance or scenic beauty of:
  - (a) the River Torrens
  - (b) land within 60 metres of either side of the River Torrens
  - (c) the landscape visible from the River Torrens.

## Energy Efficiency

### OBJECTIVES

- 1 Development designed and sited to conserve energy.
- 2 Development that provides for on-site power generation including photovoltaic cells and wind power.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should provide for efficient solar access to buildings and open space all year around.
- 2 Buildings should be sited and designed:
  - (a) to ensure adequate natural light and winter sunlight is available to the main activity areas of adjacent buildings
  - (b) so that open spaces associated with the main activity areas face north for exposure to winter sun.

### On-site Energy Generation

- 3 Development should facilitate the efficient use of photovoltaic cells and solar hot water systems by:
  - (a) taking into account overshadowing from neighbouring buildings
  - (b) designing roof orientation and pitches to maximise exposure to direct sunlight.
- 4 Public infrastructure and lighting, should be designed to generate and use renewable energy.

## Hazards

### OBJECTIVES

- 1 Maintenance of the natural environment and systems by limiting development in areas susceptible to natural hazard risk.
- 2 Development located away from areas that are vulnerable to, and cannot be adequately and effectively protected from the risk of natural hazards.
- 3 Critical community facilities such as hospitals, emergency control centres, major service infrastructure facilities, and emergency service facilities located where they are not exposed to natural hazard risks.
- 4 Development located and designed to minimise the risks to safety and property from flooding.
- 5 Development located to minimise the threat and impact of bushfires on life and property.
- 6 Expansion of existing non-rural uses directed away from areas of high bushfire risk.
- 7 The environmental values and ecological health of receiving waterways and marine environments protected from the release of acid water resulting from the disturbance of acid sulphate soils.
- 8 Protection of human health and the environment wherever site contamination has been identified or is suspected to have occurred.
- 9 Appropriate assessment and remediation of site contamination to ensure land is suitable for the proposed use and provides a safe and healthy living and working environment.
- 10 Minimisation of harm to life, property and the environment through appropriate location of development and appropriate storage, containment and handling of hazardous materials.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should be excluded from areas that are vulnerable to, and cannot be adequately and effectively protected from, the risk of hazards.
- 2 Development located on land subject to hazards as shown on the *Overlay Maps - Development Constraints* should not occur unless it is sited, designed and undertaken with appropriate precautions being taken against the relevant hazards.
- 3 There should not be any significant interference with natural processes in order to reduce the exposure of development to the risk of natural hazards.

### Flooding

- 4 Development should not occur on land where the risk of flooding is likely to be harmful to safety or damage property.
- 5 Development should not be undertaken in areas liable to inundation by tidal, drainage or flood waters unless the development can achieve all of the following:
  - (a) it is developed with a public stormwater system capable of catering for a 1-in-100 year average return interval flood event
  - (b) buildings are designed and constructed to prevent the entry of floodwaters in a 1-in-100 year average return interval flood event.

- 6 Development, including earthworks associated with development, should not do any of the following:
  - (a) impede the flow of floodwaters through the land or other surrounding land
  - (b) increase the potential hazard risk to public safety of persons during a flood event
  - (c) aggravate the potential for erosion or siltation or lead to the destruction of vegetation during a flood
  - (d) cause any adverse effect on the floodway function
  - (e) increase the risk of flooding of other land
  - (f) obstruct a watercourse.
- 7 Ground floor levels of all development on land subject to a 1-in-100 year average return interval flood event as shown on *Overlay Maps - Development Constraints* should be located above a design flood level which:
  - (a) provides an acceptable level of risk to persons and property
  - (b) minimises the impact of floodwaters onto adjoining properties
  - (c) ensures development will not adversely affect the level of floodwaters on adjoining properties.

### **Salinity**

- 8 Development should not increase the potential for, or result in an increase in, soil and water salinity.
- 9 Preservation, maintenance and restoration of locally indigenous plant species should be encouraged in areas affected by dry land salinity.
- 10 Irrigated horticulture and pasture should not increase groundwater-induced salinity.

### **Acid Sulfate Soils**

- 11 Development and activities, including excavation and filling of land, that may lead to the disturbance of potential or actual acid sulfate soils should be avoided unless such disturbances are managed in a way that effectively avoids the potential for harm or damage to any of the following:
  - (a) the marine and estuarine environment
  - (b) natural water bodies and wetlands
  - (c) agricultural or aquaculture activities
  - (d) buildings, structures and infrastructure
  - (e) public health.
- 12 Development, including primary production, aquaculture activities and infrastructure, should not proceed unless it can be demonstrated that the risk of releasing acid water resulting from the disturbance of acid sulfate soils is minimal.

### **Site Contamination**

- 13 Development, including land division, should not occur where site contamination has occurred unless the site has been assessed and remediated as necessary to ensure that it is suitable and safe for the proposed use.

### **Containment of Chemical and Hazardous Materials**

- 14 Hazardous materials should be stored and contained in a manner that minimises the risk to public health and safety and the potential for water, land or air contamination.
- 15 Development that involves the storage and handling of hazardous materials should ensure that these are contained in designated areas that are secure, readily accessible to emergency vehicles, impervious, protected from rain and stormwater intrusion and other measures necessary to prevent:
  - (a) discharge of polluted water from the site
  - (b) contamination of land
  - (c) airborne migration of pollutants
  - (d) potential interface impacts with sensitive land uses.



## Heritage Places

### OBJECTIVES

- 1 The conservation of State and local heritage places.
- 2 The continued use, or adaptive reuse of State and local heritage places that supports the conservation of their cultural significance.
- 3 Conservation of the setting of State and local heritage places.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 A heritage place shown on *Overlay Maps - Heritage* and more specifically identified in [Table WeTo/4 - Local Heritage Places](#) or in [Table WeTo/5 - State Heritage Places](#) should not be demolished, destroyed or removed, in total or in part, unless any of the following apply:
  - (a) that portion of the place to be demolished, destroyed or removed is excluded from the extent of the places identified in the *Table(s)*
  - (b) in the case of a State heritage place, the structural condition of the place is so seriously unsound as to be unsafe and irredeemable
  - (c) in the case of a local heritage place, the structural condition of the place is seriously unsound and cannot reasonably be rehabilitated.
- 2 Development of a State or local heritage place should retain those elements contributing to its heritage value, which may include (but not be limited to):
  - (a) principal elevations
  - (b) important vistas and views to and from the place
  - (c) setting and setbacks
  - (d) building materials
  - (e) outbuildings and walls
  - (f) trees and other landscaping elements
  - (g) access conditions (driveway form/width/material)
  - (h) architectural treatments
  - (i) the use of the place.
- 3 Development of a State or local heritage place should be compatible with the heritage value of the place.
- 4 Original unpainted plaster, brickwork, stonework, or other masonry of existing State or local heritage places should be preserved, unpainted.
- 5 New buildings should not be placed or erected between the front street boundary and the façade of existing State or local heritage places.

- 6 Development that materially affects the context within which the heritage place is situated should be compatible with the heritage place. It is not necessary to replicate historic detailing, however design elements that should be compatible include, but are not limited to:
  - (a) scale and bulk
  - (b) width of frontage
  - (c) boundary setback patterns
  - (d) proportion and composition of design elements such as roof lines, openings, fencing and landscaping
  - (e) colour and texture of external materials.
- 7 Multi-storey additions to a State or local heritage place should be compatible with the heritage value of the place through a range of design solutions such as:
  - (a) extending into the existing roof space or to the rear of the building
  - (b) retaining the elements that contribute to the building's heritage value
  - (c) distinguishing between the existing and new portion of the building
  - (d) stepping in parts of the building that are taller than the front facade.
- 8 The introduction of advertisements and signage to a State or local heritage place should:
  - (a) be placed on discrete elements of its architecture such as parapets and wall panels, below the canopy, or within fascias and infill end panels and windows
  - (b) not conceal or obstruct historical detailing of the heritage place
  - (c) not project beyond the silhouette or skyline of the heritage place
  - (d) not form a dominant element of the place.
- 9 The division of land adjacent to or containing a State or local heritage place should occur only where it will:
  - (a) create an allotment pattern that maintains or reinforces the integrity of the heritage place and the character of the surrounding area
  - (b) create an allotment or allotments of a size and dimension that can accommodate new development that will reinforce and complement the heritage place and the zone or policy area generally
  - (c) be of a size and dimension that will enable the siting and setback of new buildings from allotment boundaries so that they do not overshadow, dominate, encroach on or otherwise impact on the setting of the heritage place
  - (d) provide an area for landscaping of a size and dimension that complements the landscape setting of the heritage place and the landscape character of the locality
  - (e) enable the State or local heritage place to have a curtilage of a size sufficient to protect its setting.
- 10 A tree identified as a heritage place should only be pruned, lopped or removed if the tree satisfies any of the following:
  - (a) it poses an unacceptable risk to public or private safety

- (b) it is significantly diseased and its life expectancy is short
- (c) in the case of pruning or lopping, is the cause of serious nuisance to adjoining land by obstructing solar access or compromising power supply infrastructure
- (d) is the cause of serious obstruction of the only means of safe pedestrian or vehicle access
- (e) is shown to be causing, or threatening to cause, substantial damage to a heritage place or development of economic value.

## Historic Conservation Area

Refer to the [Map Reference Tables](#) for a list of the *Overlay Maps - Heritage* that relate to this Historic Conservation Area.

### OBJECTIVES

- 1 The conservation of areas of historical significance.
- 2 Development that promotes, conserves and enhances the cultural significance and historic character of identified places and areas.
- 3 Development that complements the historic significance of the area.
- 4 The retention and conservation of places such as land, buildings, structures and landscape elements that contribute positively to the historic character of the area.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should not be undertaken unless it is consistent with the desired character for the Historic Conservation Area.
- 2 Places such as land, buildings, structures and landscape elements that contribute to the historic character of the area identified on the *Overlay Maps - Heritage* and more specifically identified in the respective [Table WeTo/3 - Contributory Items](#), should be retained and conserved.
- 3 Development of a contributory item should:
  - (a) not compromise its value to the historic significance of the area
  - (b) retain its present integrity or restore its original design features
  - (c) maintain or enhance the prominence of the original street facade
  - (d) ensure additions are screened by, and/or located to the rear of the building
  - (e) ensure original unpainted plaster, brickwork, stonework, or other masonry is preserved, unpainted.
- 4 New buildings should not be placed or erected between the front street boundary and the facade of contributory items.
- 5 Buildings and structures should not be demolished in whole or in part, unless they are:
  - (a) structurally unsafe and/or unsound and cannot reasonably be rehabilitated
  - (b) inconsistent with the desired character for the Historic Conservation Area
  - (c) associated with a proposed development that supports the desired character for the Historic Conservation Area.
- 6 Development should take design cues from the existing historic built forms. In doing this, it is not necessary to replicate historic detailing; however design elements for consideration should be compatible with building and streetscape character and should include but not be limited to:

- (a) scale and bulk
  - (b) width of frontage
  - (c) boundary setback patterns
  - (d) proportion and composition of design elements such as roof lines, pitches, openings, verandas, fencing and landscaping
  - (e) colour and texture of external materials
  - (f) visual interest.
- 7 New residential development should include landscaped front garden areas that complement the desired character.
- 8 Second storey additions to single storey dwellings should achieve one or more of the following:
- (a) a sympathetic two-storey addition that uses existing roof space or incorporates minor extensions to the roof space at the rear of the dwelling
  - (b) a second storey within the roof space, where the overall building height, scale and form is compatible with existing single-storey development in the policy area
  - (c) second storey 'dormer' windows having a total length less than 30 per cent of the total roof length along each elevation.
- 9 Front fences and gates should:
- (a) reflect and conserve the traditional period, style and form of the associated building
  - (b) be no more than 1.2 metres in height.
- 10 Rear and side boundary fences located behind the front dwelling alignment should be no more than 1.8 metres in height.
- 11 Advertisements and/or advertising hoardings associated with places and areas of heritage significance should:
- (a) be of a size, colour, shape and materials that enhances the character of the locality
  - (b) not dominate or detract from the prominence of any place and/or area of historic significance.
- 12 Development should respect the existing topography and the relationship of sites to street levels and to adjoining land and not involve substantial cut and/or fill or sites.
- 13 The division of land should occur only where it will maintain the traditional pattern and scale of allotments.

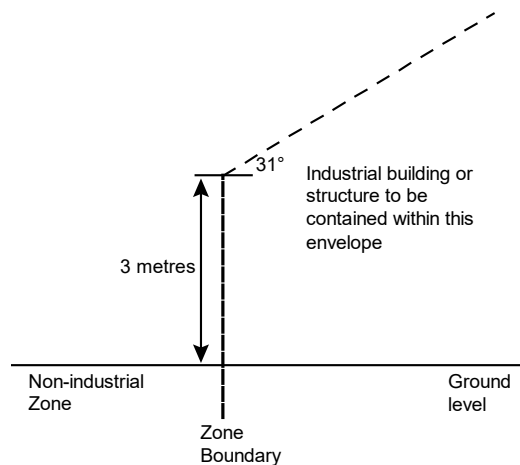
## Industrial Development

### OBJECTIVES

- 1 Industrial, warehouse, storage and transport distribution development on appropriately located land, integrated with transport networks and designed to minimise potential impact on these networks.
- 2 Industrially zoned allotments and uses protected from encroachment by adjoining uses that would reduce industrial development or expansion.
- 3 Industrial development occurring without adverse effects on the health and amenity of occupiers of land in adjoining zones.
- 4 Compatibility between industrial uses within industrial zones.
- 5 The improved amenity of industrial areas.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Offices and showrooms associated with industrial, warehouse, storage and transport development should be sited at the front of the building with direct and convenient pedestrian access from the main visitor parking area.
- 2 Any building or structure on, or abutting the boundary of, a non-industrial zone should be restricted to a height of 3 metres above ground level at the boundary and a plane projected at 31 degrees above the horizontal into the development site from that 3 metre height, as shown in the following diagram:



- 3 Industrial development should enable all vehicles to enter and exit the site in a forward direction.
- 4 Industrial development abutting an arterial road, a non-industrial zone boundary, or significant open space should be developed in a manner that does not create adverse visual impacts on the locality.
- 5 Building facades facing a non-industrial zone, public road, or public open space should:
  - (c) use a variety of building finishes
  - (d) not consist solely of metal cladding
  - (e) contain materials of low reflectivity

- (f) incorporate design elements to add visual interest
  - (g) avoid large expanses of blank walls.
- 6 Industrial development should minimise significant adverse impact on adjoining uses due to hours of operation, traffic, noise, fumes, smell, dust, paint or other chemical over-spray, vibration, glare or light spill, electronic interference, ash or other harmful or nuisance-creating impacts.
- 7 Industrial development within 50 metres of a residential zone boundary should:
- (a) use masonry or equivalent building construction with openings located so as to minimise transmission of noise to residential properties
  - (b) not include road transport oriented uses likely to cause significant nuisance to residents due to traffic movement, on-street parking or noise
  - (c) ensure that noise generating activities which have the potential to cause annoyance to residents are conducted as far as possible from the boundary of the residential zone and shielded from the residential zone by the building or major parts thereof.
- 8 Landscaping should be incorporated as an integral element of industrial development along non-industrial zone boundaries.
- 9 Fencing (including colour-coated wire mesh fencing) adjacent to public roads should be set back in one of the following ways:
- (a) in line with the building facade
  - (b) behind the building line
  - (c) behind a landscaped area that softens its visual impact.
- 10 Marine aquaculture onshore storage, cooling and processing facilities should not impair the coastline and its visual amenity and should:
- (a) be sited, designed, landscaped and developed at a scale and using external materials that minimise any adverse visual impact on the coastal landscape
  - (b) be sited and designed with appropriate vehicular access arrangement
  - (c) include appropriate waste treatment and disposal.
- 11 Retail development should primarily serve the businesses or employees working within the industrial zones and should:
- (a) only comprise of a delicatessen, restaurant, café, or a small shop retailing components of industry, storage or transport related uses
  - (b) not hinder the development, function and/or viability of any centre zone.
- 12 Residential development should only occur if it:
- (a) is ancillary to and in association with industrial development
  - (b) is located on the same allotment
  - (c) comprises a maximum floor area of 100 square metres
  - (d) is sited and designed to ensure occupant amenity and to maintain a safe and liveable environment.

## Infrastructure

### OBJECTIVES

- 1 Infrastructure provided in an economical and environmentally sensitive manner.
- 2 The visual impact of infrastructure facilities minimised.
- 3 The efficient and cost-effective use of existing infrastructure.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should only occur where it has access to adequate utilities and services, including:
  - (a) electricity supply
  - (b) water supply
  - (c) drainage and stormwater systems
  - (d) garbage disposal and recycling collection
  - (e) effluent disposal systems
  - (f) formed all-weather public roads
  - (g) telecommunications services
  - (h) gas services.
- 2 Development should incorporate any relevant and appropriate social infrastructure, community services and facilities.
- 3 Development should only occur where it provides, or has access to, relevant easements for the supply of infrastructure.
- 4 Development should incorporate provision for the supply of infrastructure services to be located within common service trenches where practicable.
- 5 Development should not occur until adequate and coordinated drainage of the land is provided.
- 6 Development in urban areas should not occur without provision of an adequate reticulated domestic quality mains water supply and an appropriate waste treatment system.
- 7 In areas where no reticulated water supply is available, buildings whose usage is reliant on a water supply should be equipped with an adequate and reliable on-site water storage system.
- 8 Urban development should have a direct water supply.
- 9 Electricity infrastructure should be designed and located to minimise visual and environmental impacts.
- 10 Utilities and services, including access roads and tracks, should be sited on areas already cleared of native vegetation. If this is not possible, their siting should cause minimal interference or disturbance to existing native vegetation and biodiversity.



- 11 Utility buildings and structures should be grouped with non-residential development where possible.
- 12 Development in proximity to infrastructure facilities should be sited and be of a scale to ensure adequate separation to protect people and property.
- 13 Incompatible uses should not encroach upon the easements of infrastructure corridors for existing and proposed transmission lines.
- 14 In urban areas, electricity supply (except transmission lines) serving new development should be installed underground, excluding lines having a capacity greater than or equal to 33kV.
- 15 Provision should be made for new transmission and distribution substations and overhead major electricity line corridors (having a capacity greater than or equal to 33kV) in areas which have the required buffer distance to protect people and allow for adequate access.
- 16 Land division for the purpose of residential and other sensitive land uses should not occur within electricity line corridors or existing electricity easements unless the resultant allotments are large enough to enable accommodation of safe clearances and expected structures.

## Interface between Land Uses

### OBJECTIVES

- 1 Development located and designed to minimise adverse impact and conflict between land uses.
- 2 Protect community health and amenity from adverse impacts of development.
- 3 **Protect desired land uses from the encroachment of incompatible development.**

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should not detrimentally affect the amenity of the locality or cause unreasonable interference through any of the following:
  - (a) the emission of effluent, odour, smoke, fumes, dust or other airborne pollutants
  - (b) noise
  - (c) vibration
  - (d) electrical interference
  - (e) light spill
  - (f) glare
  - (g) hours of operation
  - (h) traffic impacts.
- 2 Development should be sited and designed to minimise negative impacts on existing and potential future land uses desired in the locality.
- 3 Development adjacent to a **Residential Zone** should be designed to minimise overlooking and overshadowing of nearby residential properties.
- 4 Residential development adjacent to non-residential zones and land uses should be located, designed and/or sited to protect residents from potential adverse impacts from non-residential activities.
- 5 Sensitive uses likely to conflict with the continuation of lawfully existing developments and land uses desired for the zone should be designed to minimise negative impacts.
- 6 Non-residential development on land abutting a residential zone should be designed to minimise noise impacts to achieve adequate levels of compatibility between existing and proposed uses.
- 7 **Non-residential buildings and structures in zones other than the Urban Corridor Zone should be set back from boundaries of the site where its abuts the Residential Zone:**
  - (a) a minimum of 5 metres from the front property boundary
  - (b) a minimum of 3 metres from side or rear property boundaries where the vertical wall height (from natural ground level) is 4 metres or less, plus an additional 0.6 metres for every metre that the vertical wall height (from natural ground level) exceeds 4 metres.

## Noise Generating Activities

- 8 Development that emits noise (other than music noise) should include noise attenuation measures that achieve the relevant *Environment Protection (Noise) Policy* criteria when assessed at the nearest existing noise sensitive premises.
- 9 Development with the potential to emit significant noise (e.g. industry) should incorporate noise attenuation measures that prevent noise from causing unreasonable interference with the amenity of noise sensitive premises.
- 10 Outdoor areas (such as beer gardens or dining areas) associated with licensed premises should be designed or sited to minimise adverse noise impacts on adjacent existing or future noise sensitive development.
- 11 Development proposing music should include noise attenuation measures that achieve the following desired noise levels:

Noise level assessment location	Desired noise level
Adjacent existing <i>noise sensitive development</i> property boundary	Less than 8 dB above the level of background noise ( $L_{90,15min}$ ) in any octave band of the sound spectrum  and  Less than 5 dB(A) above the level of background noise ( $LA_{90,15min}$ ) for the overall (sum of all octave bands) A-weighted level
Adjacent <i>land</i> property boundary	Less than 65dB(Lin) at 63Hz and 70dB(Lin) in all other octave bands of the sound spectrum  or  Less than 8 dB above the level of background noise ( $L_{90,15min}$ ) in any octave band of the sound spectrum and 5 dB(A) overall (sum of all octave bands) A-weighted level

## Air Quality

- 12 Development with the potential to emit harmful or nuisance-generating air pollution (including gaseous odours) should incorporate air pollution control measures to prevent harm to human health or unreasonable interference with the amenity of sensitive uses within the locality.
- 13 Chimneys or exhaust flues associated with commercial development (including cafes, restaurants and fast food outlets) should be designed to ensure they do not cause a nuisance or health concerns to nearby sensitive land uses by:
  - (a) incorporating appropriate treatment technology before exhaust emissions are released to the atmosphere
  - (b) ensuring that the location and design of chimneys or exhaust flues maximises dispersion and takes into account the location of nearby sensitive uses.

## Land Division

### OBJECTIVES

- 1 Land division that occurs in an orderly sequence allowing efficient provision of new infrastructure and facilities and making optimum use of existing under utilised infrastructure and facilities.
- 2 Land division that creates allotments appropriate for the intended use.
- 3 Land division layout that is optimal for energy efficient building orientation.
- 4 Land division that is integrated with site features, including landscape and environmental features, adjacent land uses, the existing transport network and the availability of infrastructure.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 When land is divided:
  - (a) stormwater should be capable of being drained safely and efficiently from each proposed allotment and disposed of from the land in an environmentally sensitive manner
  - (b) a sufficient water supply should be made available for each allotment
  - (c) provision should be made for the disposal of wastewater, sewage and other effluent from each allotment without risk to health
  - (d) proposed roads should be graded, or be capable of being graded to connect safely and conveniently with an existing road or thoroughfare.
- 2 Land should not be divided if any of the following apply:
  - (a) the size, shape, location, slope or nature of the land makes any of the allotments unsuitable for the intended use
  - (b) any allotment will not have a frontage to one of the following:
    - (i) an existing road
    - (ii) a proposed public road
    - (iii) access to a public road via an internal roadway in a plan of community division
  - (c) the intended use of the land is likely to require excessive cut and/or fill
  - (d) it is likely to lead to undue erosion of the subject land or land within the locality
  - (e) the wastewater treatment plant to which subsequent development will be connected does not have sufficient capacity to handle the additional wastewater volumes and pollutant loads generated by such development
  - (f) the area is unsewered and cannot accommodate an appropriate onsite wastewater disposal system within the allotment that complies with (or can comply with) the relevant public and environmental health legislation applying to the intended use(s)
  - (g) any allotments will straddle more than one zone, policy area or precinct
  - (h) the allotments unreasonably restrict access to publicly owned land such as recreation areas.

## Design and Layout

- 3 Land divisions should be designed to ensure that areas of native vegetation and wetlands:
  - (a) are not fragmented or reduced in size
  - (b) do not need to be cleared as a consequence of subsequent development.
- 4 The design of a land division should incorporate:
  - (a) roads, thoroughfares and open space that result in safe and convenient linkages with the surrounding environment, including public and community transport facilities, and which, where necessary, facilitate the satisfactory future division of land and the inter-communication with neighbouring localities
  - (b) new road and allotment access points providing appropriate separation distances from existing road junctions or level crossings
  - (c) safe and convenient access from each allotment to an existing or proposed road or thoroughfare
  - (d) areas to provide appropriate separation distances between potentially conflicting land uses and/or zones
  - (e) suitable land set aside for useable local open space
  - (f) public utility services within road reserves and where necessary within dedicated easements
  - (g) the preservation of significant natural, cultural or landscape features including State and local heritage places
  - (h) protection for existing vegetation and drainage lines
  - (i) where appropriate, the amalgamation of smaller allotments to ensure co-ordinated and efficient site development
  - (j) the preservation of significant trees.
- 5 Land division should result in allotments of a size suitable for their intended use.
- 6 Land division should facilitate optimum solar access for energy efficiency.
- 7 Allotments in the form of a battleaxe configuration should:
  - (a) have an area of at least the minimum site area specified by the zone, policy area or precinct (excluding the area of the 'handle' of such an allotment)
  - (b) provide for an access onto a public road, with the driveway 'handle' being not less than:
    - (i) 4 metres in width to facilitate landscape planting along the driveway, and
    - (ii) 5.5 metres for at least the first 5 metres of the driveway for an allotment accommodating two or more dwellings to allow vehicles to pass safely
  - (c) contain sufficient area on the allotment for a vehicle to turn around to enable it to egress the allotment in a forward direction
  - (d) not be created where it would lead to multiple access points onto a road which would dominate or adversely affect the amenity of the streetscape (for example through the loss of mature street trees, on-street parking or pedestrian safety)
  - (e) be avoided where their creation would be incompatible with the prevailing pattern of development.

- 8 Allotments should have an orientation, size and configuration to encourage development that:
  - (a) minimises the need for earthworks and retaining walls
  - (b) maintains natural drainage systems
  - (c) faces abutting streets and open spaces
  - (d) does not require the removal of native vegetation to facilitate that development
  - (e) will not overshadow, dominate, encroach on or otherwise detrimentally affect the setting of the surrounding locality.
- 9 Where the land to be divided borders a river, lake, wetland or creek, the land adjoining the bank should become public open space and linked with an existing or proposed pedestrian or transport network.
- 10 Land division should make provision for a reserve or an area of open space that is at least 30 metres wide from the top of the bank of a watercourse and that incorporates land within the 1-in-100 year average return interval flood event area.
- 11 The arrangement of roads, allotments, reserves and open space should enable the provision of a stormwater management drainage system that:
  - (a) contains and retains all watercourses, drainage lines and native vegetation
  - (b) enhances amenity
  - (c) integrates with the open space system and surrounding area.
- 12 On-street vehicle parking should be provided at a ratio of one car parking space for every two allotments.

### **Roads and Access**

- 13 Road reserves should be of a width and alignment that can:
  - (a) provide for safe and convenient movement and parking of projected volumes of vehicles and other users
  - (b) provide for footpaths, cycle lanes and shared-use paths for the safety and convenience of residents and visitors
  - (c) allow vehicles to enter or reverse from an allotment or site in a single movement allowing for a car parked on the opposite side of the street
  - (d) accommodate street tree planting, landscaping and street furniture
  - (e) accommodate the location, construction and maintenance of stormwater drainage and public utilities
  - (f) provide unobstructed, safe and efficient vehicular access to individual allotments and sites
  - (g) allow for the efficient movement of service and emergency vehicles.
- 14 The design of the land division should facilitate the most direct route to local facilities for pedestrians and cyclists and enable footpaths, cycle lanes and shared-use paths to be provided of a safe and suitable width and reasonable longitudinal gradient.

- 15 The layout of land divisions should result in roads designed and constructed to ensure:
  - (a) that traffic speeds and volumes are restricted where appropriate by limiting street length and/or the distance between bends and slow points
  - (b) there are adequate sight distances for motorists at intersections, junctions, pedestrian and cyclist crossings, and crossovers to allotments to ensure the safety of all road users and pedestrians
  - (c) that existing dedicated cycling and walking routes are not compromised.
- 16 The design of the land division should provide space sufficient for on-street visitor car parking for the number and size of allotments, taking account of:
  - (a) the size of proposed allotments and sites and opportunities for on-site parking
  - (b) the availability and frequency of public and community transport
  - (c) on-street parking demand likely to be generated by nearby uses.
- 17 The layout of land divisions should incorporate street patterns designed to enhance the efficient movement of traffic and minimise trip lengths.

## Landscaping, Fences and Walls

### OBJECTIVES

- 1 The amenity of land and development enhanced with appropriate planting and other landscaping works, using locally indigenous plant species where possible.
- 2 Functional fences and walls that enhance the attractiveness of development.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should incorporate open space and landscaping and minimise hard paved surfaces in order to:
  - (a) complement built form and reduce the visual impact of larger buildings (eg taller and broader plantings against taller and bulkier building components)
  - (b) enhance the appearance of road frontages
  - (c) screen service yards, loading areas and outdoor storage areas
  - (d) minimise maintenance and watering requirements
  - (e) enhance and define outdoor spaces, including car parking areas
  - (f) maximise shade and shelter
  - (g) assist in climate control within and around buildings
  - (h) minimise heat absorption and reflection
  - (i) maintain privacy
  - (j) maximise stormwater reuse
  - (k) complement existing vegetation, including native vegetation
  - (l) contribute to the viability of ecosystems and species
  - (m) promote water and biodiversity conservation.
- 2 Landscaping should:
  - (a) include the planting of **drought tolerant species, including** locally indigenous species where appropriate
  - (b) be oriented towards the street frontage
  - (c) result in the appropriate clearance from powerlines and other infrastructure being maintained.
- 3 Landscaping should not:
  - (a) unreasonably restrict solar access to adjoining development
  - (b) cause damage to buildings, paths and other landscaping from root invasion, soil disturbance or plant overcrowding



- (c) introduce pest plants
  - (d) increase the risk of bushfire
  - (e) remove opportunities for passive surveillance
  - (f) increase leaf fall in watercourses
  - (g) increase the risk of weed invasion
  - (h) obscure driver sight lines
  - (i) create a hazard for train or tram drivers by obscuring sight lines at crossovers.
- 4 A minimum of 10 per cent of a development site should be landscaped. The development site refers to the land which incorporates a development and all the features and facilities associated with that development, such as outbuildings, driveways, parking areas, landscaped areas, service yards and fences. Where a number of buildings or dwellings have shared use of such features and facilities, the development site incorporates all such buildings or dwellings and their shared features and facilities.
- 5 A landscape area of at least 3 metres in width should be provided between non-residential development and the boundary of a residential zone.
- 6 Fences and walls, including retaining walls, should:
- (a) not result in damage to neighbouring trees
  - (b) be compatible with the associated development and with existing predominant, attractive fences and walls in the locality
  - (c) enable some visibility of buildings from and to the street to enhance safety and allow casual surveillance
  - (d) incorporate articulation or other detailing where there is a large expanse of wall facing the street
  - (e) assist in highlighting building entrances
  - (f) be sited and limited in height, to ensure adequate sight lines for motorists and pedestrians especially on corner sites
  - (g) in the case of side and rear boundaries, be of sufficient height to maintain privacy and/or security without adversely affecting the visual amenity or access to sunlight of adjoining land
  - (h) be constructed of non-flammable materials.

## Marinas and Maritime Structures

### OBJECTIVE

- 1 The provision, in appropriate locations, of marinas, pontoons, jetties, piers, wharves and boat moorings that cater for vessels and:
  - (a) maintain public access to the waterfront
  - (b) do not compromise public safety
  - (c) preserve the structural integrity of the marine infrastructure
  - (d) minimise adverse impacts on the natural environment.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Marina development should include one or more of the following:
  - (a) wet and dry berthing of boats
  - (b) launching and retrieval of recreational boats and associated trailer and car parking areas
  - (c) access ramps, landings, storage and other structures associated with a marina
  - (d) clubrooms for maritime organisations.
- 2 The design of marinas, berths, channels, fairways, gangways and floating structures should comply with:
  - (a) *Australian Standard AS 3962: Guidelines for Design of Marinas*
  - (b) *Australian Standard AS 4997: Guidelines for the Design of Maritime Structures.*
- 3 Development should not obstruct or impair:
  - (a) navigation and access channels
  - (b) maintenance activities of marine infrastructure including revetment walls
  - (c) the operation of wharves.
- 4 Safe public access should be provided or maintained to:
  - (a) the waterfront
  - (b) known diving areas
  - (c) jetties, wharves and associated activities.
- 5 Marinas should be designed to:
  - (a) facilitate water circulation and exchange
  - (b) maximise the penetration of sunlight into the water.

## Medium and High Rise Development (3 or More Storeys)

### OBJECTIVES

- 1 Medium and high rise development that provides housing choice and employment opportunities.
- 2 Residential development that provides a high standard of amenity and adaptability for a variety of accommodation and living needs.
- 3 Development that is contextual and responds to its surroundings, having regard to adjacent built form and character of the locality and the Desired Character for the Zone and Policy Area.
- 4 Development that integrates built form within high quality landscapes to optimize amenity, security and personal safety for occupants and visitors.
- 5 Development that enhances the public environment, provides activity and interest at street level and a high quality experience for residents, workers and visitors by:
  - (a) enlivening building edges
  - (b) creating attractive, welcoming, safe and vibrant spaces
  - (c) improving public safety through passive surveillance
  - (d) creating interesting and lively pedestrian environments
  - (e) integrating public art into the development where it fronts the street and public spaces
  - (f) incorporating generous areas of high quality fit for purpose landscaping, green walls and roofs.
- 6 Commercial, office and retail development that is designed to create a strong visual connection to the public realm and that contributes to the vitality of the locality.
- 7 Buildings designed and sited to be energy and water efficient.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Design and Appearance

*Note: Some of the following Principles of Development Control (PDC) prescribe a measurable design solution as one way of achieving the intent of the PDC. Where this solution is met, it should be taken as meeting the intent of the principle. Alternative design solutions may also achieve the intent of the PDC and, when proposed should be assessed on their merits.*

#### Design and Appearance

- 1 Buildings should be designed to respond to key features of the prevailing local context within the same zone as the development. This may be achieved through design features such as vertical rhythm, proportions, composition, material use, parapet or balcony height, and use of solid and glass.
- 2 In repetitive building types, such as row housing, the appearance of building facades should provide some variation, but maintain an overall coherent expression such as by using a family of materials, repeated patterns, facade spacings and the like.
- 3 Windows and doors, awnings, eaves, verandas or other similar elements should be used to provide variation of light and shadow and contribute to a sense of depth in the building façade.

- 4 Buildings should:
  - (a) achieve a comfortable human scale at ground level through the use of elements such as variation in materials and form, building projections and elements that provide shelter (for example awnings, verandas, and tree canopies)
  - (b) be designed to reduce visual mass by breaking up the building façade into distinct elements
  - (c) ensure walls on the boundary that are visible from public land include visually interesting treatments to break up large blank facades.
- 5 Buildings should reinforce corners through changes in setback, materials or colour, roof form or height.
- 6 Materials and finishes should be selected to be durable and age well to minimise ongoing maintenance requirements. This may be achieved through the use of materials such as masonry, natural stone and prefinished materials that minimise staining, discolouring or deterioration.
- 7 Balconies should be integrated into the overall architectural form and detail of the development and should:
  - (a) utilise sun screens, pergolas, louvres, green facades and openable walls to control sunlight and wind
  - (b) be designed and positioned to respond to daylight, wind, and acoustic conditions to maximise comfort and provide visual privacy
  - (c) allow views and casual surveillance of the street while providing for safety and visual privacy of nearby living spaces and private outdoor areas
  - (d) be of sufficient size, particularly depth, to accommodate outdoor seating.

### **Street Interface**

- 8 Development facing the street should be designed to provide attractive, high quality and pedestrian friendly street frontage(s) by:
  - (a) incorporating active uses such as shops or offices, prominent entry areas for multi-storey buildings (where it is a common entry), habitable rooms of dwellings, and areas of communal public realm with public art or the like where consistent with the Zone and/or Policy Area provisions;
  - (b) providing a well landscaped area that contains a deep soil zone space for a medium to large tree in front of the building (except in a High Street Policy Area or other similar location where a continuous ground floor façade aligned with the front property boundary is desired).

One way of achieving this is to provide a 4 metre x 4 metre deep soil zone area in front of the building
  - (c) designing building façades that are well articulated by creating contrasts between solid elements (such as walls) and voids (for example windows, doors and balcony openings);
  - (d) positioning services, plant and mechanical equipment (such as substations, transformers, pumphouses and hydrant boosters, car park ventilation) in discreet locations, screened or integrated with the façade;
  - (e) ensuring ground, undercroft, semi-basement and above ground parking does not detract from the streetscape;

- (f) minimising the number and width of driveways and entrances to car parking areas to reduce the visual dominance of vehicle access points and impacts on street trees and pedestrian areas.
- 9 Common areas and entry points of the ground floor level of buildings should be designed to enable surveillance from public land to the inside of the building at night.
- 10 Entrances to multi-storey buildings should:
- (a) be oriented towards the street
  - (b) be visible and clearly identifiable from the street, and in instances where there are no active or occupied ground floor uses, be designed as a prominent, accentuated and welcoming feature
  - (c) provide shelter, a sense of personal address and transitional space around the entry
  - (d) provide separate access for residential and non-residential land uses
  - (e) be located as close as practicable to the lift and/or lobby access
  - (f) avoid the creation of potential areas of entrapment.
- 11 To contribute to direct pedestrian access and street level activation, the finished ground level of buildings should be no more than 1.2 metres above the level of the footpath, except for common entrances to apartment buildings which should be at ground level or universally accessible.
- 12 Dwellings located on the ground floor with street frontage should have individual direct pedestrian street access.
- 13 The visual privacy of ground floor dwellings within multi-storey buildings should be protected through the use of design features such as orientation, elevation of ground floors above street level, setbacks from street and the location of verandas, windows, porticos or the like.

One way of achieving this is for ground floor level dwellings in multi-storey developments to be raised by up to 1.2 metres (provided access is not compromised where relevant).

### **Building Separation and Outlook**

- 14 Residential buildings (or the residential floors of mixed use buildings) should have habitable rooms, windows and balconies designed and positioned with adequate separation and screening from one another to provide visual and acoustic privacy and allow for natural ventilation and the infiltration of daylight into interior and outdoor spaces.

One way of achieving this is to ensure any habitable room windows and/or balconies are separated by at least 6 metres from one another where there is a direct 'line of sight' between them and be at least 3 metres from a side or rear property boundary. Where a lesser separation is proposed, alternative design solutions may be applied (such as changes to orientation, staggering of windows or the provision of screens or blade walls, or locating facing balconies on alternating floors as part of double floor apartments), provided a similar level of occupant visual and acoustic privacy, as well as light access, can be demonstrated.

- 15 Living rooms should have a satisfactory short range visual outlook to public, communal or private open space.

### **Dwelling Configuration**

- 16 Buildings comprising more than 10 dwellings should provide a variety of dwelling sizes and a range in the number of bedrooms per dwelling.
- 17 Dwellings located on the ground floor with street frontage should have habitable rooms with windows overlooking the street or public realm.

- 18 Dwellings with 3 or more bedrooms, should, where possible, have the windows of habitable rooms overlooking internal courtyard space or other public space.

### Adaptability

- 19 Multi-storey buildings should include a variety of internal designs that will facilitate adaptive reuse, including the conversion of ground floor residential to future commercial use (i.e. by including floor to ceiling heights suitable for commercial use).

### Environmental

- 20 Multi-storey buildings should:
- (a) minimise detrimental micro-climatic and solar access impacts on adjacent land or buildings, including effects of patterns of wind, temperature, daylight, sunlight, glare and shadow
  - (b) incorporate roof designs that enable the provision of photovoltaic cells and other features that enhance sustainability (including landscaping).
- 21 Green roofs (which can be a substitute for private or communal open space provided they can be accessed by occupants of the building) are encouraged for all new residential commercial or mixed use buildings.
- 22 Development of 5 or more storeys, or 21 metres or more in building height (excluding the rooftop location of mechanical plant and equipment), should be designed to minimise the risk of wind tunnelling effects on adjacent streets by adopting one or more of the following:
- (a) a podium at the base of a tall tower and aligned with the street to deflect wind away from the street
  - (b) substantial verandas around a building to deflect downward travelling wind flows over pedestrian areas
  - (c) the placement of buildings and use of setbacks to deflect the wind at ground level.
- 23 Deep soil zones should be provided to retain existing vegetation or provide areas that can accommodate new deep root vegetation, including tall trees with large canopies.

One way of achieving this is in accordance with the following table:

Site area	Minimum deep soil area	Minimum dimension	Tree/ deep soil zones
<300m <sup>2</sup>	10m <sup>2</sup>	1.5 metres	1 small tree / 10m <sup>2</sup> deep soil
300-1500m <sup>2</sup>	7% site area	3 metres	1 medium tree / 30m <sup>2</sup> deep soil
>1500m <sup>2</sup>	7% site area	6 metres	1 large or medium tree / 60m <sup>2</sup> deep soil
Tree size and site area definitions			
Small tree	< 6 metres mature height and < less than 4 metres canopy spread		
Medium tree	6-12 metres mature height and 4-8 metres canopy spread		
Large tree	12 metres mature height and > 8 metres canopy spread		
Site area	The total area for development site, not average area per dwelling		

- 24 Deep soil zones should be provided with access to natural light to assist in maintaining vegetation health.

### **Site Facilities and Storage**

- 25 Dwellings should provide a covered storage area of not less than 8 cubic metres in one or more of the following areas:
- (a) in the dwelling (but not including a habitable room)
  - (b) in a garage, carport, outbuilding or an on-site communal facility and be conveniently located and screened from view from streets and neighbouring properties.
- 26 Development should provide a dedicated area for the on-site collection and sorting of recyclable materials and refuse, green organic waste and wash-bay facilities for the ongoing maintenance of bins. This area should be screened from view from public areas so as to not detract from the visual appearance of the ground floor.
- 27 Where the number of bins to be collected kerbside is 10 or more at any one time, provision should be made for on-site collection.
- 28 The size of lifts, lobbies and corridors should be sufficient to accommodate movement of bicycles, strollers, mobility aids and visitor waiting areas.

### **Zone Interface**

- 29 Unless separated by a public road or reserve, development site(s) adjacent to any zone that has a primary purpose of accommodating low rise (1-2 storey) residential activity should incorporate deep soil zones along the common boundary to enable medium to large trees to be retained or established to assist in screening new buildings of 3 or more storeys in height.

One way of achieving this is for development comprising building elements of three or more storeys in height to be setback at least 6 metres from a zone boundary, and incorporate a deep soil zone area capable of accommodating medium to large trees with a canopy spread of not more than 8 metres when fully mature.

## Metropolitan Open Space System

### OBJECTIVES

- 1 A Metropolitan Open Space System (MOSS), in and around metropolitan Adelaide, that:
  - (a) is comprised of public and private land
  - (b) is clearly defined and linked
  - (c) has an open or natural character
  - (d) provides a visual and scenic contrast to the built urban environment
  - (e) separates different parts of the metropolitan area.
- 2 Conservation and restoration of existing and modified habitats.
- 3 Conservation of sites of scientific or heritage interest.
- 4 The provision of corridors for the movement of wildlife.
- 5 A range of recreation and leisure areas including a network of recreation trails integrating MOSS and adjoining land uses.
- 6 A range of active recreation and sporting facilities of regional or State significance, including facilities that can be used for national and international events.
- 7 Stormwater management in association with recreation, aquifer recharge and water quality management.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should preserve and enhance the natural and open character and amenity of land located within the MOSS.
- 2 Development within the MOSS should contribute to the rehabilitation and restoration of aquatic and terrestrial ecosystems and water catchments.
- 3 Landscaping within MOSS locations should:
  - (a) incorporate remnant vegetation
  - (b) use locally indigenous plant species wherever possible
  - (c) provide shade and windbreaks along cyclist and pedestrian routes and around picnic and barbecue areas, seating and car parking areas
  - (d) maximise opportunities for passive surveillance along the park.
- 4 Development on public land within the MOSS should incorporate:
  - (a) easily identified access points
  - (b) pedestrian and cycle linkages within open spaces and between other open space networks
  - (c) facilities to provide and support a range of recreation and leisure activities.



## Mineral Extraction

### OBJECTIVES

- 1 Development of mining activities in a way that contributes to the sustainable growth of the industry.
- 2 Protection of mineral deposits against intrusion by inappropriate forms of development.
- 3 Areas with scenic or conservation significance protected from undue damage arising from mining operations.
- 4 Mining operations undertaken with minimal adverse impacts on the environment and on the health and amenity of adjacent land uses.
- 5 Minimisation of the impacts from mining activities upon the existing groundwater level and the quality of groundwater resources.
- 6 Mining operations that make adequate provision for site rehabilitation.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Known reserves of economically-viable mineral deposits should be kept free of development that may inhibit their future exploitation.
- 2 Development in proximity to mining operations should not be undertaken where it may be exposed to adverse impacts resulting from mining activities.
- 3 Mining in scenic and native vegetation areas should only be undertaken if:
  - (a) the proposed location is the best site in regard to minimising loss of amenity, degradation of the landscape and loss of native vegetation
  - (b) there are a limited number of known reserves of the minerals in the area or elsewhere in the State
  - (c) the extraction and transportation of materials from alternative sites to principal centres of consumption carry significantly higher costs
  - (d) the site is capable of restoration with locally indigenous plant species to counter the long-term impact on the landscape and biodiversity.
- 4 Stormwater and/or wastewater from land used for mining should be diverted into a silt retention structure so that it can be reused on-site for purposes such as truck wash-down, dust control, washing of equipment and landscape irrigation or for disposal off-site in an environmentally responsible manner.
- 5 Access to land used for mining should be sited and designed to accommodate heavy-vehicle traffic and ensure the safety of all road users.
- 6 Mining operations should:
  - (a) ensure that minimal damage is caused to the landscape
  - (b) minimise the area required for operations, and provide for the progressive reclamation of disturbed areas
  - (c) minimise disturbance to natural hydrological systems.

### **Separation Treatments, Buffers and Landscaping**

- 7 Mining development should be sited, designed and sequenced to protect the amenity of surrounding land uses from environmental nuisance such as dust or vibration emanating from mining operations.
- 8 Mining operations that are likely to impact upon the amenity of the locality should incorporate a separation distance and/or mounding/vegetation between the mining operations (including stockpiles) and adjoining allotments to help minimise exposure to those potential impacts.
- 9 Quarry faces should be orientated away from public view.
- 10 Screening of mining areas should occur in advance of extraction commencing.
- 11 An area of densely vegetated and/or mounded land should be established around the perimeter of mining sites in order to screen excavated land and mineral processing facilities from all of the following:
  - (a) residential areas
  - (b) tourist areas
  - (c) tourist routes
  - (d) scenic routes.
- 12 Screen planting around mining operations should incorporate a mixture of trees and shrubs that:
  - (a) contribute to an attractive landscape
  - (b) suit local soil and climatic conditions
  - (c) are fast growing and/or have a long life expectancy
  - (d) are locally indigenous species.
- 13 Borrow pits for road making materials should be sited so as to cause the minimum effect on their surroundings.

## Natural Resources

### OBJECTIVES

- 1 Retention, protection and restoration of the natural resources and environment.
- 2 Protection of the quality and quantity of South Australia's surface waters, including inland, marine and estuarine and undergroundwaters.
- 3 The ecologically sustainable use of natural resources including water resources, including marine waters groundwater, surface water and watercourses.
- 4 Natural hydrological systems and environmental flows reinstated, and maintained and enhanced.
- 5 Development consistent with the principles of water sensitive design.
- 6 Development sited and designed to:
  - (a) protect natural ecological systems
  - (b) achieve the sustainable use of water
  - (c) protect water quality, including receiving waters
  - (d) reduce runoff and peak flows and prevent the risk of downstream flooding
  - (e) minimise demand on reticulated water supplies
  - (f) maximise the harvest and use of stormwater
  - (g) protect stormwater from pollution sources.
- 7 Storage and use of stormwater which avoids adverse impact on public health and safety.
- 8 Native flora, fauna and ecosystems protected, retained, conserved and restored.
- 9 Restoration, expansion and linking of existing native vegetation to facilitate habitat corridors for ease of movement of fauna.
- 10 Minimal disturbance and modification of the natural landform.
- 11 Protection of the physical, chemical and biological quality of soil resources.
- 12 Protection of areas prone to erosion or other land degradation processes from inappropriate development.
- 13 Protection of the scenic qualities of natural and rural landscapes.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should be undertaken with minimum impact on the natural environment, including air and water quality, land, soil, biodiversity, and scenically attractive areas.
- 2 Development should ensure that South Australia's natural assets, such as biodiversity, water and soil, are protected and enhanced.

- 3 Development should not significantly obstruct or adversely affect sensitive ecological areas such as creeks, wetlands, estuaries and significant seagrass and mangrove communities.
- 4 Development should be appropriate to land capability and the protection and conservation of water resources and biodiversity.

### **Water Sensitive Design**

- 5 Development should be designed to maximise conservation, minimise consumption and encourage re-use of water resources.
- 6 Development should not take place if it results in unsustainable use of surface or groundwater resources.
- 7 Development should be sited and designed to:
  - (a) capture and re-use stormwater, where practical
  - (b) minimise surface water runoff
  - (c) prevent soil erosion and water pollution
  - (d) protect and enhance natural water flows
  - (e) protect water quality by providing adequate separation distances from watercourses and other water bodies
  - (f) not contribute to an increase in salinity levels
  - (g) avoid the water logging of soil or the release of toxic elements
  - (h) maintain natural hydrological systems and not adversely affect:
    - (i) the quantity and quality of groundwater
    - (ii) the depth and directional flow of groundwater
    - (iii) the quality and function of natural springs.
- 8 Water discharged from a development site should:
  - (a) be of a physical, chemical and biological condition equivalent to or better than its pre-developed state
  - (b) not exceed the rate of discharge from the site as it existed in pre-development conditions.
- 9 Development should have adequate provision to control any stormwater over-flow runoff from the site and should be sited and designed to improve the quality of stormwater and minimise pollutant transfer to receiving waters.
- 10 Development should include stormwater management systems to mitigate peak flows and manage the rate and duration of stormwater discharges from the site to ensure the carrying capacities of downstream systems are not overloaded.
- 11 Development should include stormwater management systems to minimise the discharge of sediment, suspended solids, organic matter, nutrients, bacteria, litter and other contaminants to the stormwater system.
- 12 Stormwater management systems should preserve natural drainage systems, including the associated environmental flows.

- 13 Stormwater management systems should:
- (a) maximise the potential for stormwater harvesting and reuse, either on-site or as close as practicable to the source
  - (b) utilise, but not be limited to, one or more of the following harvesting methods:
    - (i) the collection of roof water in tanks
    - (ii) the discharge to open space, landscaping or garden areas, including strips adjacent to car parks
    - (iii) the incorporation of detention and retention facilities
    - (iv) aquifer recharge.
- 14 Where it is not practicable to detain or dispose of stormwater on site, only clean stormwater runoff should enter the public stormwater drainage system.
- 15 Artificial wetland systems, including detention and retention basins, should be sited and designed to:
- (a) ensure public health and safety is protected
  - (b) minimise potential public health risks arising from the breeding of mosquitoes.
- 16 On site detention and retention of stormwater should be sited away from areas where site contamination has occurred.

### **Water Catchment Areas**

- 17 Development should ensure watercourses and their beds, banks, wetlands and floodplains are not damaged or modified and are retained in their natural state, except where modification is required for essential access or maintenance purposes.
- 18 Development adjacent to Patawalonga Creek should enhance the character and appearance of the area and retain the creek as a water resource and stormwater channel.
- 19 No development should occur where its proximity to a swamp or wetland will damage or interfere with the hydrology or water regime of the swamp or wetland.
- 20 A wetland or low-lying area providing habitat for native flora and fauna should not be drained, except temporarily for essential management purposes to enhance environmental values.
- 21 Along watercourses, areas of remnant native vegetation, or areas prone to erosion, that are capable of natural regeneration should be fenced off to limit stock access.
- 22 Development such as cropping, intensive animal keeping, residential, tourism, industry and horticulture, that increases the amount of surface runoff should include a strip of land at least 20 metres wide measured from the top of existing banks on each side of a watercourse that is:
- (a) fenced to exclude livestock
  - (b) kept free of development, including structures, formal roadways or access ways for machinery or any other activity causing soil compaction or significant modification of the natural surface of the land
  - (c) revegetated with locally indigenous vegetation comprising trees, shrubs and other groundcover plants to filter runoff so as to reduce the impacts on native aquatic ecosystems and to minimise soil loss eroding into the watercourse.

- 23 Development resulting in the depositing of an object or solid material in a watercourse or floodplain or the removal of bank and bed material should not:
- (a) adversely affect the migration of aquatic biota
  - (b) adversely affect the natural flow regime
  - (c) cause or contribute to water pollution
  - (d) result in watercourse or bank erosion
  - (e) adversely affect native vegetation upstream or downstream that is growing in or adjacent to a watercourse.
- 24 The location and construction of dams, water tanks and diversion drains should:
- (a) occur off watercourse
  - (b) not take place in ecologically sensitive areas or on erosion-prone sites
  - (c) provide for low flow by-pass mechanisms to allow for migration of aquatic biota
  - (d) not negatively affect downstream users
  - (e) minimise in-stream or riparian vegetation loss
  - (f) incorporate features to improve water quality (eg wetlands and floodplain ecological communities)
  - (g) protect ecosystems dependent on water resources.
- 25 Irrigated horticulture and pasture should not increase groundwater-induced salinity.
- 26 Development should comply with the current *Environment Protection (Water Quality) Policy*.

### **Biodiversity and Native Vegetation**

- 27 Development should retain existing areas of native vegetation and where possible contribute to revegetation using locally indigenous plant species.
- 28 Development should be designed and sited to minimise the loss and disturbance of native flora and fauna, including marine animals and plants, and their breeding grounds and habitats.
- 29 Native vegetation should be conserved and its conservation value and function not compromised by development if the native vegetation does any of the following:
- (a) provides an important habitat for wildlife or shade and shelter for livestock
  - (b) has a high plant species diversity or includes rare, vulnerable or endangered plant species or plant associations and communities
  - (c) provides an important seed bank for locally indigenous vegetation
  - (d) has high amenity value and/or significantly contributes to the landscape quality of an area, including the screening of buildings and unsightly views
  - (e) has high value as a remnant of vegetation associations characteristic of a district or region prior to extensive clearance for agriculture
  - (f) is growing in, or is characteristically associated with a wetland environment.

- 30 Native vegetation should not be cleared if such clearing is likely to lead to, cause or exacerbate any of the following:
- (a) erosion or sediment within water catchments
  - (b) decreased soil stability
  - (c) soil or land slip
  - (d) deterioration in the quality of water in a watercourse or surface water runoff
  - (e) a local or regional salinity problem
  - (f) the occurrence or intensity of local or regional flooding.
- 31 Development that proposes the clearance of native vegetation should address or consider the implications that removing the native vegetation will have on the following:
- (a) provision for linkages and wildlife corridors between significant areas of native vegetation
  - (b) erosion along watercourses and the filtering of suspended solids and nutrients from runoff
  - (c) the amenity of the locality
  - (d) bushfire safety
  - (e) the net loss of native vegetation and other biodiversity.
- 32 Where native vegetation is to be removed, it should be replaced in a suitable location on the site with locally indigenous vegetation to ensure that there is not a net loss of native vegetation and biodiversity.
- 33 Development should be located and occur in a manner which:
- (a) does not increase the potential for, or result in, the spread of pest plants, or the spread of any non-indigenous plants into areas of native vegetation or a conservation zone
  - (b) avoids the degradation of remnant native vegetation by any other means including as a result of spray drift, compaction of soil, modification of surface water flows, pollution to groundwater or surface water or change to groundwater levels
  - (c) incorporates a separation distance and/or buffer area to protect wildlife habitats and other features of nature conservation significance.
- 34 Development should promote the long-term conservation of vegetation by:
- (a) avoiding substantial structures, excavations, and filling of land in close proximity to the trunk of trees and beneath their canopies
  - (b) minimising impervious surfaces beneath the canopies of trees
  - (c) taking other effective and reasonable precautions to protect both vegetation and the integrity of structures and essential services.
- 35 Horticulture involving the growing of olives should be located at least:
- (a) 500 metres from:
    - (i) a national park
    - (ii) a conservation park

- (iii) a wilderness protection area
  - (iv) the edge of a substantially intact stratum of native vegetation greater than 5 hectares in area
  - (b) 50 metres from the edge of stands of native vegetation 5 hectares or less in area.
- 36 Horticulture involving the growing of olives should have at least one locally indigenous tree that will grow to a height of at least 7 metres sited at least every 100 metres around the perimeter of the orchard.

### **Soil Conservation**

- 37 Development should not have an adverse impact on the natural, physical, chemical or biological quality and characteristics of soil resources.
- 38 Development should be designed and sited to prevent erosion.
- 39 Development should take place in a manner that will minimise alteration to the existing landform.
- 40 Development should minimise the loss of soil from a site through soil erosion or siltation during the construction phase of any development and following the commencement of an activity.



## Open Space and Recreation

### OBJECTIVES

- 1 The creation of a network of linked parks, reserves, recreational trails and recreation areas at regional and local levels.
- 2 Pleasant, functional and accessible open spaces providing a range of physical environments.
- 3 A wide range of settings for active and passive recreational opportunities.
- 4 The provision of open space in the following hierarchy:
  - State
  - Regional
  - District
  - Neighbourhood
  - Local.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Urban development should include public open space and recreation areas.
- 2 Public open space and recreation areas should be of a size, dimension and location that:
  - (a) facilitate a range of formal and informal recreation activities
  - (b) provide for the movement of pedestrians and cyclists
  - (c) incorporate existing vegetation and natural features, watercourses, wildlife habitat and other sites of natural or cultural value
  - (d) link habitats, wildlife corridors, public open spaces and existing recreation facilities
  - (e) enable effective stormwater management
  - (f) provides for the planting and retention of large trees and vegetation.
- 3 Open space should be designed to incorporate:
  - (a) pedestrian, cycle linkages to other open spaces, centres, schools and public transport nodes
  - (b) park furniture, shaded areas and resting places to enhance pedestrian comfort
  - (c) safe crossing points where pedestrian routes intersect the road network
  - (d) easily identified access points
  - (e) frontage to abutting public roads to optimise pedestrian access and visibility
  - (f) reuse of stormwater for irrigation purposes
  - (g) recreational trails where appropriate.
- 4 Where practical, access points to regional parks should be located close to public transport.

**West Torrens Council**  
**General Section**  
***Open Space and Recreation***

- 5 District level parks should be at least 3 hectares in size, and provided within 2 kilometres of all households that they serve.
- 6 Neighbourhood parks should be at least 0.5 hectares and generally closer to 1 hectare in size, and provided within 500 metres of households that they serve.
- 7 Local parks should be:
  - (a) a minimum of 0.2 hectares in size
  - (b) centrally located within a residential area, close to schools, shops and generally within 300 metres of households that they serve.
- 8 No more than 20 per cent of land allocated as public open space should:
  - (a) have a slope in excess of 1-in-4
  - (b) comprise creeks or other drainage areas.
- 9 Signage should be provided at entrances to and within public open space to provide clear orientation to major points of interest such as the location of public toilets, telephones, safe routes and park activities.
- 10 Buildings in open space, including structures and associated car parking areas, should be designed, located and of a scale that is unobtrusive and does not detract from the desired open space character.
- 11 Development in open space should:
  - (a) be clustered where practical to ensure that the majority of the site remains open
  - (b) where practical, be developed for multi-purpose use
  - (c) be constructed to minimise the extent of hard paved areas.
- 12 Open spaces and recreation areas should be located and designed to maximise safety and security by:
  - (a) ensuring that within urban areas, their edges are overlooked by housing, commercial or other development that can provide effective informal surveillance
  - (b) ensuring fenced parks and playgrounds have more than one entrance or exit when fenced
  - (c) locating play equipment where it can be informally observed by nearby residents and users during times of use
  - (d) clearly defining the perimeters of play areas
  - (e) providing lighting around facilities such as toilets, telephones, seating, litter bins, bike storage and car parks
  - (f) focusing pedestrian and bicycle movement after dark along clearly defined, adequately lit routes with observable entries and exits.
- 13 Landscaping associated with open space and recreation areas should:
  - (a) not compromise the drainage function of any drainage channel
  - (b) provide shade and windbreaks along cyclist and pedestrian routes, around picnic and barbecue areas and seating, and in car parking areas
  - (c) maximise opportunities for informal surveillance throughout the park

- (d) enhance the visual amenity of the area and complement existing buildings
  - (e) be designed and selected to minimise maintenance costs
  - (f) provide habitat for local fauna.
- 14 Development of recreational activities in areas not zoned for that purpose should be compatible with surrounding activities.
- 15 Recreation facilities development should be sited and designed to minimise negative impacts on the amenity of the locality.

## Orderly and Sustainable Development

### OBJECTIVES

- 1 Orderly and economical development that creates a safe, convenient and pleasant environment in which to live.
- 2 Development occurring in an orderly sequence and in a compact form to enable the efficient provision of public services and facilities.
- 3 Development that does not jeopardise the continuance of adjoining authorised land uses.
- 4 Development that does not prejudice the achievement of the provisions of the Development Plan.
- 5 Urban development located only in zones designated for such development.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should not prejudice the development of a zone for its intended purpose.
- 2 Land outside of townships and settlements should primarily be used for primary production and conservation purposes.
- 3 Urban development should form a compact extension to an existing built-up area.
- 4 Ribbon development should not occur along the coast, water frontages or arterial roads shown in *Overlay Maps - Transport*.
- 5 Development should be located and staged to achieve the economical provision of public services and infrastructure, and to maximise the use of existing services and infrastructure.
- 6 Where development is expected to impact upon the existing infrastructure network (including the transport network), development should demonstrate how the undue effect will be addressed.
- 7 Vacant or under utilised land should be developed in an efficient and co-ordinated manner to not prejudice the orderly development of adjacent land.
- 8 Development should be undertaken in accordance with the following concept plan maps:
  - (a) [Concept Plan Map WeTo/1 - Mile End South Bulky Goods Zone](#)
  - (b) [Concept Plan Map WeTo/2 - Prominent Development Sites](#)
  - (c) [Concept Plan Map WeTo/3a - Sir Donald Bradman Drive & Marion Road Intersection Commercial](#)
  - (d) [Concept Plan Map WeTo/3b - Marion Road & Richmond Road Intersection Commercial](#)
  - (e) [Concept Plan Map WeTo/3c - Richmond Road & South Road Intersection Commercial](#)
  - (f) [Concept Plan Map WeTo/4a - Richmond Road \(Marleston\) Commercial](#)
  - (g) [Concept Plan Map WeTo/4b - Richmond Road \(Richmond\) Commercial](#)
  - (h) [Concept Plan Map WeTo/4c - Richmond Road \(Keswick/Mile End South\) Commercial](#)

- (i) [Concept Plan Map WeTo/5 - Sir Donald Bradman Drive \(Mile End\) Commercial](#)
- (j) [Concept Plan Map WeTo/6 - South Road \(Keswick\) Commercial](#)
- (k) [Concept Plan Map WeTo/7 - South Road \(Mile End\) Commercial](#)
- (l) [Concept Plan Map WeTo/8 - South Road \(Mile End South\) Commercial](#)
- (m) [Concept Plan Map WeTo/9 - Office Park Commercial](#)
- (n) [Concept Plan Map WeTo/10 - Mile End Recreation](#)
- (o) [Concept Plan Map WeTo/11 - Mile End South Recreation](#)
- (p) [Concept Plan Map WeTo/12 - Adelaide Shores](#)
- (q) [Concept Plan Map WeTo/13 - Brickworks District Centre](#)
- (r) [Concept Plan Map WeTo/14 - Kurralta Park District Centre](#)
- (s) [Concept Plan Map WeTo/15 - Former Apollo Stadium Industry](#)
- (t) [Concept Plan Map WeTo/16 - North Plympton/Camden Park Industry](#)
- (u) [Concept Plan Map WeTo/17 - Novar Gardens Industry](#)
- (v) [Concept Plan Map WeTo/18 - Netley Industry](#)
- (w) [Concept Plan Map WeTo/19 - Hilton Neighbourhood Centre](#)
- (x) [Concept Plan Map WeTo/20 - Marleston Neighbourhood Centre](#)
- (y) [Concept Plan Map WeTo/21 - Novar Gardens Neighbourhood Centre](#)
- (z) [Concept Plan Map WeTo/22 - Richmond Neighbourhood Centre](#)
- (aa) [Concept Plan Map WeTo/23 - Tennyson Street Neighbourhood Centre](#)
- (bb) [Concept Plan Map WeTo/24 - Residential Promenade](#)
- (cc) [Concept Plan Map WeTo/25 - Underdale Urban Renewal](#)
- (dd) [Concept Plan Map WeTo/26 - Morphettville Racecourse](#)

## **Regulated Trees**

### **OBJECTIVES**

- 1 The conservation of regulated trees that provide important aesthetic and/or environmental benefit.
- 2 Development in balance with preserving regulated trees that demonstrate one or more of the following attributes:
  - (a) significantly contributes to the character or visual amenity of the locality
  - (b) indigenous to the locality
  - (c) a rare or endangered species
  - (d) an important habitat for native fauna.

### **PRINCIPLES OF DEVELOPMENT CONTROL**

- 1 Development should have minimum adverse effects on regulated trees.
- 2 A regulated tree should not be removed or damaged other than where it can be demonstrated that one or more of the following apply:
  - (a) the tree is diseased and its life expectancy is short
  - (b) the tree represents a material risk to public or private safety
  - (c) the tree is causing damage to a building
  - (d) development that is reasonable and expected would not otherwise be possible
  - (e) the work is required for the removal of dead wood, treatment of disease, or is in the general interests of the health of the tree.
- 3 Tree damaging activity other than removal should seek to maintain the health, aesthetic appearance and structural integrity of the tree.

## **Renewable Energy Facilities**

### **OBJECTIVES**

- 1 The development of renewable energy facilities, such as wind and biomass energy facilities, in appropriate locations.
- 2 Location, siting, design and operation of renewable energy facilities to avoid or minimise adverse impacts and maximise positive impacts on the environment, the local community and the State.

### **PRINCIPLES OF DEVELOPMENT CONTROL**

- 1 Renewable energy facilities, including wind farms and ancillary developments, should be located in areas that maximise efficient generation and supply of electricity.
- 2 Wind farms and ancillary development such as substations, maintenance sheds, access roads and connecting powerlines, should be sited, designed and operated in a manner that:
  - (a) avoids or minimises negative impacts on the character, landscape quality, visual significance or amenity of the area
  - (b) uses elements of the landscape and appropriate materials and finishes to minimise visual impact
  - (c) avoids or minimises the potential for adverse impact on areas of native vegetation, conservation, environmental, geological, tourism or built or natural heritage significance
  - (d) does not impact on the safety of water or air transport and the operation of ports, airfields and designated landing strips
  - (e) avoids or minimises nuisance or hazard to nearby property owners and/or occupiers, road users and wildlife by not:
    - (i) causing shadowing, flickering, reflection or blade glint impacts
    - (ii) creating excessive noise
    - (iii) interfering with television and radio signals
    - (iv) modifying vegetation, soils and habitats
    - (v) striking birds or bats.

## Residential Development

### OBJECTIVES

- 1 Safe, convenient, pleasant and healthy-living environments that meet the full range of needs and preferences of the community.
- 2 An increased mix in the range and number of dwelling types available within urban boundaries to cater for changing demographics, particularly smaller household sizes and supported accommodation.
- 3 Medium and high density residential development in areas close to activity centres, public and community transport and public open spaces.
- 4 The revitalisation of residential areas to support the viability of community services and infrastructure.
- 5 Affordable housing, student housing and housing for aged persons provided in appropriate locations.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Residential allotments and sites should maximise solar orientation and have the area and dimensions to accommodate:
  - (a) the siting and construction of a dwelling and associated ancillary outbuildings
  - (b) the provision of landscaping and private open space
  - (c) convenient and safe vehicle, pedestrian and cycling access and parking
  - (d) water sensitive design systems that enable the storage, treatment and reuse of stormwater.
- 2 Buildings on battleaxe allotments that do not have frontage to a public road should be single storey and be designed to maintain the privacy of adjoining properties.
- 3 Residential allotments should be of varying sizes to encourage housing diversity.

### Design and Appearance

- 4 Building appearance should be compatible with the desired character statement of the relevant zone, policy area or precinct, in terms of built form elements such as:
  - (a) building height
  - (b) building mass and proportion
  - (c) external materials, patterns, textures, colours and decorative elements
  - (d) ground floor height above natural ground level
  - (e) roof form and pitch
  - (f) facade articulation and detailing and window and door proportions
  - (g) verandas, eaves and parapets
  - (h) driveway crossovers, fence style and alignment.



- 5 Residential development should avoid undue repetition of style and external appearance.
- 6 Dwellings and accommodation at ground floor level should contribute to the character of the locality and create active, safe streets by incorporating one or more of the following:
  - (a) front landscaping or terraces that contribute to the spatial and visual structure of the street while maintaining adequate privacy for occupants
  - (b) individual entries for ground floor accommodation
  - (c) opportunities to overlook adjacent public space.
- 7 Residential development should be designed to ensure living rooms have an external outlook.
- 8 Entries to dwellings or foyer areas should be clearly visible from the street, or access ways that they face to enable visitors to easily identify individual dwellings.
- 9 Residential development should provide a high quality living environment by ensuring the following minimum internal floor areas (including internal storage areas but not including balconies and car parking):
  - (a) studio (where there is no separate bedroom): 37 square metres
  - (b) 1 bedroom dwelling/apartment: 50 square metres
  - (c) 2 bedroom dwelling/apartment: 75 square metres
  - (d) 3+ bedroom dwelling/apartment: 100 square metres.

### **Overshadowing**

- 10 The design and location of buildings should ensure that direct winter sunlight is available to adjacent dwellings, with particular consideration given to:
  - (a) windows of habitable rooms, particularly living areas
  - (b) ground-level private open space
  - (c) upper-level private balconies that provide the primary open space area for any dwelling
  - (d) access to solar energy.
- 11 Development should ensure that north-facing windows to habitable rooms of existing dwelling(s) on the same allotment, and on adjacent allotments, receive at least 3 hours of direct sunlight over a portion of their surface between 9.00 am and 5.00 pm on the 21 June.
- 12 Development should ensure that ground-level open space of existing buildings receives direct sunlight for a minimum of two hours between 9.00 am and 3.00 pm on 21 June to at least the smaller of the following:
  - (a) half of the existing ground-level open space
  - (b) 35 square metres of the existing ground-level open space (with at least one of the area's dimensions measuring 2.5 metres).
- 13 Development should not increase the overshadowed area by more than 20 per cent in cases where overshadowing already exceeds these requirements.

## Garages, Carports and Outbuildings

- 14 Garages, carports and residential outbuildings should have a roof form and pitch, building materials and detailing that complements the associated dwelling.
- 15 Residential outbuildings, including garages and sheds, should not be constructed unless in association with an existing dwelling.
- 16 Garages, carports and residential outbuildings should not dominate the streetscape and not adversely impact on the safety of road users and pedestrians, and be designed within the following parameters:

Parameter	Value
Maximum floor area	60 square metres
Maximum wall height	3 metres
Maximum building height	5 metres
Minimum setback from a primary road frontage	Garages and carports sited no closer to the primary road frontage than any part of its associated dwelling Outbuildings should not protrude forward of any part of its associated dwelling
Minimum setback from a secondary road frontage	0.9 metres or in line with the existing dwelling
Maximum length along the boundary	8 metres or 50 per cent of the length along that boundary (whichever is the lesser)
Maximum frontage width of garage or carport with an opening facing a rear access lane	No maximum
Maximum frontage width of garage or carport opening facing the street	6 metres or 50 per cent of the allotment frontage, whichever is less

- 17 Carports and garages should be set back not less than 1 metre from any rear lane boundary to facilitate vehicle manoeuvring.

## Private Open Space

- 18 Private open space (available for exclusive use by residents of each dwelling) should be provided for each dwelling and should be sited and designed:
- to be accessed directly from the internal living areas of the dwelling
  - to be generally at ground level (other than for residential flat buildings) and to the side or rear of a dwelling and screened for privacy
  - to take advantage of, but not adversely affect, natural features of the site
  - to minimise overlooking from adjacent buildings
  - to achieve separation from bedroom windows on adjoining sites
  - to have a northerly aspect to provide for comfortable year round use
  - not to be significantly shaded during winter by the associated dwelling or adjacent development
  - to be partly shaded in summer

- (i) to minimise noise or air quality impacts that may arise from traffic, industry or other business activities within the locality
- (j) to have sufficient area and shape to be functional, taking into consideration the location of the dwelling, and the dimension and gradient of the site.

19 Dwellings at ground level should provide private open space in accordance with the following table:

<b>Site area per dwelling (square metres)</b>	<b>Minimum area excluding any area at ground level at the front of the dwelling (square metres)</b>	<b>Minimum dimension (metres)</b>	<b>Minimum area provided at the rear or side of the dwelling, directly accessible from a habitable room (square metres)</b>
<300	24, of which 8 may comprise balconies, roof patios and the like, provided they have a minimum dimension of 2 metres	3 (excluding balconies)	16
300-500	60, of which 10 may comprise balconies, roof patios and the like, provided they have a minimum dimension of 2 metres	4	16
>500	80, of which 10 may comprise balconies, roof patios and the like, provided they have a minimum dimension of 2 metres	4	24

- 20 Private open space should not include driveways, effluent drainage areas, rubbish bin storage areas, sites for rainwater tanks and other utility areas, sites for outbuildings, and common areas such as parking areas and communal open space.
- 21 Private open space at ground level should be designed to provide a consolidated area of deep soil (an area of natural ground which excludes areas where there is a structure underneath, pools and non-permeable paved areas) to:
- (a) assist with ease of drainage
  - (b) allow for effective deep planting
  - (c) reduce urban heat loading and improve micro-climatic conditions around sites and buildings.

22 Dwellings located above ground level should provide private open space in accordance with the following table:

<b>Dwelling type</b>	<b>Minimum area of private open space</b>
Studio (where there is no separate bedroom)	No minimum requirement
1 bedroom dwelling	8 square metres
2 bedroom dwelling	11 square metres
3+ bedroom dwelling	15 square metres

23 Private open space located above ground level should have a minimum dimension of 2 metres and be directly accessible from a habitable room.

- 24 Private open space may be substituted for the equivalent area of communal open space where:
- (a) at least 50 per cent of the communal open space is visually screened from public areas of the development
  - (b) ground floor communal space is overlooked by habitable rooms to facilitate passive surveillance
  - (c) it contains landscaping and facilities that are functional, attractive and encourage recreational use.

### **Communal Open Space**

- 25 Communal open space should be shared by more than one dwelling, not be publicly accessible and exclude:
- (a) private open space
  - (b) public rights of way
  - (c) private streets
  - (d) parking areas and driveways
  - (e) service and storage areas
  - (f) narrow or inaccessible strips of land.
- 26 Communal open space should only be located on elevated gardens or roof tops where the area and overall design is useful for the recreation and amenity needs of residents and where it is designed to:
- (a) address acoustic, safety, security and wind effects
  - (b) minimise overlooking into habitable room windows or onto the useable private open space of other dwellings
  - (c) facilitate landscaping and food production
  - (d) be integrated into the overall façade and composition of buildings.

### **Visual Privacy**

- 27 Except for buildings of 3 or more storeys, upper level windows, balconies, terraces and decks that overlook habitable room windows or private open space of dwellings should maximise visual privacy through the use of measures such as sill heights of not less than 1.7 metres or permanent screens having a height of 1.7 metres above finished floor level.

### **Noise**

- 28 Noise generated by fixed noise sources such as air conditioning units and pool pumps should be located, designed and attenuated to avoid nuisance to adjoining landowners and occupiers.
- 29 External noise and artificial light intrusion into bedrooms should be minimised by separating or shielding these rooms from:
- (a) active communal recreation areas, parking areas and vehicle access ways
  - (b) service equipment areas and fixed noise sources on the same or adjacent sites.

### Site Facilities and Storage

- 30 Site facilities for group dwellings, multiple dwellings and residential flat buildings should include:
- (a) mail box facilities sited close to the major pedestrian entrance to the site
  - (b) bicycle parking for residents and visitors (for developments containing more than 6 dwellings)
  - (c) household waste and recyclable material storage areas away from dwellings.
- 31 A dwelling should incorporate a minimum storage area of 8 cubic metres for goods and chattels, other than food and clothing, within at least one of the following:
- (a) a non habitable room of the dwelling
  - (b) a garage, carport or outbuilding
  - (c) an on-site communal facility.

### Affordable Housing

- 32 Affordable housing should be well integrated and complementary in design and appearance to other dwellings within the development.
- 33 Affordable housing should be located to optimise access to shops, social services and facilities, or public transport.

### Dependent Accommodation

- 34 Dependent accommodation (i.e. accommodation where the living unit is connected to the same services of the main dwelling) should be developed on the same allotment as the existing dwelling only where:
- (a) the dependent accommodation site is of adequate size and configuration and the minimum total site is 450 square metres
  - (b) the accommodation has a small floor area relative to the associated main dwelling(s)
  - (c) adequate outdoor space of a minimum of 20 square metres is provided for the use of all occupants
  - (d) adequate on-site car parking is provided by one additional car parking space being provided
  - (e) the building is designed to, and comprises colours and materials that will, complement the associated dwelling.

### Swimming Pools and Outdoor Spas

- 35 Swimming pools, outdoor spas and associated ancillary equipment and structures should be sited to protect the amenity of adjoining residential land.

## Significant Trees

### OBJECTIVES

- 1 The conservation of significant trees, in Metropolitan Adelaide, that provide important aesthetic and environmental benefit.
- 2 The conservation of significant trees in balance with achieving appropriate development.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should preserve the following attributes where a significant tree demonstrates at least one of the following attributes:
  - (a) makes an important contribution to the character or amenity of the local area; or
  - (b) is indigenous to the local area and its species is listed under the *National Parks and Wildlife Act 1972* as a rare or endangered native species
  - (c) represents an important habitat for native fauna
  - (d) is part of a wildlife corridor of a remnant area of native vegetation
  - (e) is important to the maintenance of biodiversity in the local environment
  - (f) forms a notable visual element to the landscape of the local area.
- 2 Development should be undertaken so that it has a minimum adverse effect on the health of a significant tree.
- 3 Significant trees should be preserved, and tree-damaging activity should not be undertaken, unless:
  - (a) in the case of tree removal, where at least one of the following apply:
    - (i) the tree is diseased and its life expectancy is short
    - (ii) the tree represents an unacceptable risk to public or private safety
    - (iii) the tree is within 20 metres of a residential, tourist accommodation or habitable building and is a bushfire hazard within a Bushfire Prone Area
  - (b) the tree is shown to be causing or threatening to cause substantial damage to a substantial building or structure of value
  - (c) all other reasonable remedial treatments and measures have been determined to be ineffective
  - (d) it is demonstrated that all reasonable alternative development options and design solutions have been considered to prevent substantial tree-damaging activity occurring
  - (e) in any other case, any of the following circumstances apply:
    - (i) the work is required for the removal of dead wood, treatment of disease, or is in the general interests of the health of the tree
    - (ii) the work is required due to unacceptable risk to public or private safety

- (iii) the tree is shown to be causing or threatening to cause damage to a substantial building or structure of value
  - (iv) the aesthetic appearance and structural integrity of the tree is maintained
  - (v) it is demonstrated that all reasonable alternative development options and design solutions have been considered to prevent substantial tree-damaging activity occurring.
- 4 Development involving ground work activities such as excavation, filling, and sealing of surrounding surfaces (whether such work takes place on the site of a significant tree or otherwise) should only be undertaken where the aesthetic appearance, health and integrity of a significant tree, including its root system, will not be adversely affected.
- 5 Land should not be divided or developed where the division or development would be likely to result in a substantial tree-damaging activity occurring to a significant tree.

## Siting and Visibility

### OBJECTIVES

- 1 Protection of scenically attractive areas, particularly natural coastal landscapes.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should be sited and designed to minimise its visual impact on:
  - (a) the natural, rural or heritage character of the area
  - (b) areas of high visual or scenic value, particularly and coastal areas
  - (c) views from the coast, near-shore waters, public reserves, tourist routes and walking trails
  - (d) the amenity of public beaches.
- 2 Buildings should be sited in unobtrusive locations and, in particular, should:
  - (a) be grouped together
  - (b) where possible be located in such a way as to be screened by existing vegetation when viewed from public roads.
- 3 Buildings outside of urban areas and in undulating landscapes should be sited in unobtrusive locations and in particular should be:
  - (a) sited below the ridgeline
  - (b) sited within valleys or behind spurs
  - (c) sited in such a way as to not be visible against the skyline when viewed from public roads
  - (d) set well back from public roads, particularly when the allotment is on the high side of the road.
- 4 Buildings and structures should be designed to minimise their visual impact in the landscape, in particular:
  - (a) the profile of buildings should be low and the roof lines should complement the natural form of the land
  - (b) the mass of buildings should be minimised by variations in wall and roof lines and by floor plans which complement the contours of the land
  - (c) large eaves, verandas and pergolas should be incorporated into designs so as to create shadowed areas that reduce the bulky appearance of buildings.
- 5 The nature of external surface materials of buildings should not detract from the visual character and amenity of the landscape.
- 6 The number of buildings and structures on land outside of urban areas should be limited to that necessary for the efficient management of the land.



- 7 Driveways and access tracks should be designed and surfaced to blend sympathetically with the landscape and to minimise interference with natural vegetation and landforms.
- 8 Development should be screened through the establishment of landscaping using locally indigenous plant species:
  - (a) around buildings and earthworks to provide a visual screen as well as shade in summer, and protection from prevailing winds
  - (b) along allotment boundaries to provide permanent screening of buildings and structures when viewed from adjoining properties and public roads
  - (c) along the verges of new roads and access tracks to provide screening and minimise erosion.

## Sloping Land

### OBJECTIVES

- 1 Development on sloping land designed to minimise environmental and visual impacts and protect soil stability and water quality.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development and associated driveways and access tracks should be sited and designed to integrate with the natural topography of the land and minimise the need for earthworks.
- 2 Development and associated driveways and access tracks, including related earthworks, should be sited, designed and undertaken in a manner that:
  - (a) minimises their visual impact
  - (b) reduces the bulk of the buildings and structures
  - (c) minimises the extent of cut and/or fill
  - (d) minimises the need for, and the height of, retaining walls
  - (e) does not cause or contribute to instability of any embankment or cutting
  - (f) avoids the silting of watercourses
  - (g) protects development and its surrounds from erosion caused by water runoff.
- 3 Driveways and access tracks across sloping land should be accessible and have a safe, all-weather trafficable surface.
- 4 Development sites should not be at risk of landslide.
- 5 Development on steep land should include site drainage systems to minimise erosion and avoid adverse impacts on slope stability.
- 6 Steep sloping sites in unsewered areas should not be developed unless the physical characteristics of the allotments enable the proper siting and operation of an effluent drainage field suitable for the development intended.
- 7 The cutting and/or filling of land outside townships and urban areas should:
  - (a) be kept to a minimum and be limited to a maximum depth or height no greater than 1.5 metres so as to preserve the natural form of the land and the native vegetation
  - (b) only be undertaken in order to reduce the visual impact of buildings, including structures, or in order to construct water storage facilities for use on the allotment
  - (c) only be undertaken if the resultant slope can be stabilised to prevent erosion
  - (d) result in stable scree slopes which are covered with top soil and landscaped so as to preserve and enhance the natural character or assist in the re-establishment of the natural character of the area.

# **Supported Accommodation, Housing for Aged Persons and People with Disabilities**

## **OBJECTIVES**

- 1 Provision of well designed supported accommodation for community groups with special needs in appropriate locations.

## **PRINCIPLES OF DEVELOPMENT CONTROL**

- 1 Supported accommodation (including nursing homes, hostels, retirement homes, retirement villages, residential care facilities and special accommodation houses) and housing for aged persons and people with disabilities should be:
  - (a) located within walking distance of essential facilities such as convenience shops, health and community services and public and community transport
  - (b) located where on-site movement of residents is not unduly restricted by the slope of the land
  - (c) sited and designed to promote interaction with other sections of the community, without compromising privacy
  - (d) of a scale and appearance that reflects the residential style and character of the locality
  - (e) provided with public and private open space and landscaping.
- 2 Supported accommodation and housing for aged persons and people with disabilities should be designed to provide safe, secure, attractive, convenient and comfortable living conditions for residents that include:
  - (a) internal communal areas and private spaces
  - (b) useable recreation areas for residents and visitors, including visiting children
  - (c) spaces to accommodate social needs and activities, including social gatherings, internet use, gardening, keeping pets, preparing meals and doing personal laundry
  - (d) storage areas for items such as boats, trailers, caravans and specialised equipment
  - (e) mail boxes and waste disposal areas within easy walking distance of all units.
- 3 Access roads within supported accommodation and development incorporating housing for aged persons and people with disabilities should:
  - (a) not have steep gradients
  - (b) provide convenient access for emergency vehicles, visitors and residents
  - (c) provide space for manoeuvring cars and community buses
  - (d) include kerb ramps at pedestrian crossing points
  - (e) have level-surface passenger loading areas.

- 4 Car parking associated with supported accommodation and housing for aged persons and people with disabilities should:
  - (a) be conveniently located on site within easy walking distance of resident units
  - (b) be adequate for residents, service providers and visitors
  - (c) include covered and secure parking for residents' vehicles
  - (d) have slip-resistant surfaces with gradients not steeper than 1-in-40
  - (e) allow ease of vehicle manoeuvrability
  - (f) be designed to allow the full opening of all vehicle doors
  - (g) minimise the impact of car parking on adjacent residences owing to visual intrusion and noise
  - (h) be appropriately lit to enable safe and easy movement to and from vehicles.
- 5 Supported accommodation should include:
  - (a) ground-level access or lifted access to all units
  - (b) an interesting and attractive outlook from units and communal areas for all residents including those in wheelchairs
  - (c) adequate living space allowing for the use of wheelchairs with an attendant
  - (d) storage for items such as small electric powered vehicles and other personal items, including facilities for recharging small electric powered vehicles.
- 6 Car parking associated with supported accommodation should:
  - (a) have adequate identifiable provisions for staff
  - (b) include private parking spaces for independent living units
  - (c) include separate and appropriately marked places for people with disabilities and spaces for small electrically powered vehicles.

## Telecommunications Facilities

### OBJECTIVES

- 1 Telecommunications facilities provided to deliver communication services to the community.
- 2 Telecommunications facilities sited and designed to minimise visual impact on the amenity of the local environment.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Telecommunications facilities should:
  - (a) be located in a co-ordinated manner to deliver communication services efficiently
  - (b) use materials and finishes that minimise visual impact
  - (c) have antennae located as close as practical to the support structure
  - (d) be located primarily in industrial, commercial, business, office, centre and rural zones
  - (e) where technically feasible, be co-located with other telecommunications facilities
  - (f) incorporate landscaping to screen the development, particularly equipment shelters and huts
  - (g) be designed and sited to minimise the visual impact on the character and amenity of the local environment, in particular visually prominent areas, main focal points and significant vistas.
- 2 Telecommunications facilities in areas of high visitation and community use should use innovative design techniques (eg sculpture and other artworks) where possible and where the resulting design would positively contribute to the character of the area.
- 3 Telecommunications facilities should be located in residential zones only if sited and designed to minimise visual impact by:
  - (a) using existing buildings and vegetation for screening
  - (b) incorporating the facility within an existing structure that may serve another purpose
  - (c) taking into account the size, scale, context and characteristics of existing structures, landforms and vegetation so as to complement the local environment.
- 4 Telecommunications facilities should not have a direct or significant effect on the amenity, character and settings of Historic Conservation Areas, local heritage places, State heritage places or State Heritage Areas.

## Tourism Development

### OBJECTIVES

- 1 Environmentally sustainable and innovative tourism development.
- 2 Tourism development that assists in the conservation, interpretation and public appreciation of significant natural and cultural features including State or local heritage places.
- 3 Tourism development that sustains or enhances the local character, visual amenity and appeal of the area.
- 4 Tourism development that protects areas of exceptional natural value, allows for appropriate levels of visitation, and demonstrates an environmental analysis and design response which enhances environmental values.
- 5 Ensure new development, together with associated bushfire management minimise the threat and impact of bushfires on life and property while protecting the environment.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Tourism development should have a functional or locational link with its natural, cultural or historical setting.
- 2 Tourism development and any associated activities should not damage or degrade any significant natural and cultural features.
- 3 Tourism development should ensure that its scale, form and location will not overwhelm, over commercialise or detract from the intrinsic natural values of the land on which it is sited or the character of its locality.
- 4 Natural features, signs and walkways should be used to manage and minimise potential risks of visitors damaging areas of cultural or natural significance, fragile areas, and areas of highest environmental value.

### Tourism Development in Association with Dwelling(s)

- 5 Tourist facilities developed on the site of a dwelling should not detrimentally affect residential amenity.
- 6 Car parking for tourist accommodation associated with a dwelling should be provided at the rate of one space for each guest room or suite of rooms, and ensure that:
  - (a) parking areas are attractively developed and landscaped, or screen fenced, and do not dominate the street frontage
  - (b) the bedrooms of residential neighbours are suitably shielded from noise and headlight glare associated with guest vehicle movements
  - (c) a domestic character is retained through the scale and appearance of landscaping and paving materials that provide a suitable all-weather surface.

### Residential Parks and Caravan and Tourist Parks

- 7 Residential parks which are principally designed for residents should be located in areas with access to employment, shops, schools, public transport and community and recreation facilities.

- 8 Residential parks and Caravan and Tourist parks should be designed to:
  - (a) minimise potential conflicts between long-term residents and short-term tourists
  - (b) protect the privacy and amenity of occupants through landscaping and fencing
  - (c) minimise traffic speeds and provide a safe environment for pedestrians
  - (d) include centrally located recreation areas
  - (e) include extensive landscaping that enhances the appearance of the locality, with a landscape buffer around the perimeter of the site
  - (f) ensure that adequate amenity blocks (showers, toilets, laundry and kitchen facilities) and service facilities (eg public telephones, kiosks and restaurants) are provided to serve the population to be accommodated by the facility.
- 9 Visitor car parking should be provided at the rate of:
  - (a) one space per 10 sites to be used for accommodation for parks with less than 100 sites
  - (b) one space per 15 sites to be used for accommodation for parks with greater than 100 sites.
- 10 On-site visitor parking in Caravan and Tourist parks should:
  - (a) be designed and located to be accessible to visitors at all times
  - (b) not dominate the internal site layout
  - (c) be clearly defined as visitor spaces and not specifically associated with any particular accommodation site.
- 11 Long-term occupation of Caravan and Tourist parks should not lead to the displacement of existing tourist accommodation, particularly in important tourist destinations, such as in coastal or riverside locations.
- 12 A minimum of 12.5 per cent of a park should comprise communal open space, landscaped areas and recreation areas.
- 13 Landscaping should comprise locally indigenous species that are appropriate to the development and the subject land, and facilitate amenity and environmental sustainability.

## Transportation and Access

### OBJECTIVES

- 1 A comprehensive, integrated, affordable and efficient air, rail, sea, road, cycle and pedestrian transport system that will:
  - (a) provide equitable access to a range of public, community and private transport services for all people
  - (b) ensure a high level of safety
  - (c) effectively support the economic development of the State
  - (d) have minimal negative environmental and social impacts
  - (e) maintain options for the introduction of suitable new transport technologies.
- 2 Development that:
  - (a) provides safe and efficient movement for all transport modes
  - (b) ensures access for vehicles including emergency services, public infrastructure maintenance and commercial vehicles
  - (c) provides off-street parking
  - (d) is appropriately located so that it supports and makes best use of existing transport facilities and networks
  - (e) provides convenient and safe access to public transport stops.
- 3 A road hierarchy that promotes safe and efficient transportation in an integrated manner throughout the State.
- 4 Provision of safe, pleasant, accessible, integrated and permeable pedestrian and cycling networks that are connected to the public transport network.
- 5 Safe and convenient freight movement throughout the State.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 Land uses arranged to support the efficient provision of sustainable transport networks and encourage their use.

#### Movement Systems

- 2 Development should be integrated with existing transport networks, particularly major rail, road and public transport corridors as shown on *Location Maps* and *Overlay Maps – Transport*, and designed to minimise its potential impact on the functional performance of the transport network.
- 3 Transport corridors should be sited and designed so as to not unreasonably interfere with the health and amenity of adjacent sensitive land uses.



- 4 Roads should be sited and designed to blend with the landscape and be in sympathy with the terrain.
- 5 Land uses that generate large numbers of visitors such as shopping centres, places of employment, schools, hospitals and medium to high density residential uses should be located so that they can be serviced by the public transport network and encourage walking and cycling.
- 6 Development generating high levels of traffic, such as schools, shopping centres and other retail areas, entertainment and sporting facilities, should incorporate passenger pick-up and set down areas. The design of such areas should ensure interference to existing traffic is minimised and give priority to pedestrians, cyclists and public and community transport users.
- 7 The location and design of public and community transport set-down and pick-up points should maximise safety and minimise the isolation and vulnerability of users.
- 8 Development should provide safe and convenient access for all anticipated modes of transport.
- 9 Development at intersections, pedestrian and cycle crossings, and crossovers to allotments should maintain or enhance sightlines for motorists, cyclists and pedestrians to ensure safety for all road users and pedestrians.
- 10 Driveway crossovers affecting pedestrian footpaths should maintain the level and surface colour of the footpath.
- 11 Driveway crossovers should be separated and the number minimised to optimise the provision of on-street visitor parking (where on-street parking is appropriate).
- 12 Development should be designed to discourage commercial and industrial vehicle movements through residential streets and adjacent other sensitive land uses.
- 13 Industrial/commercial vehicle movements should be separated from passenger vehicle car parking areas.
- 14 Development should provide for the on-site loading, unloading and turning of all traffic, including any waste collection vehicles, likely to be generated.
- 15 The road network in new residential areas should be designed to allow for bus routes with convenient connection to adjoining public transport routes (including future routes) which allow for ease of movement of buses between areas and facilitate linkages between activity centres within and external to the new areas.

### **Cycling and Walking**

- 16 Development should ensure that a permeable street and path network is established that encourages walking and cycling through the provision of safe, convenient and attractive routes with connections to adjoining streets, paths, open spaces, schools, pedestrian crossing points on arterial roads, public and community transport stops and activity centres.
- 17 Development should provide access, and accommodate multiple route options, for pedestrians and cyclists by enhancing and integrating with:
  - (a) open space networks, recreational trails, parks, reserves, and sport and recreation areas
  - (b) Adelaide's principal cycling network (Bikedirect), which includes arterial roads, local roads and off-road paths as depicted in *Overlay Maps – Transport*.
- 18 New developments should give priority to and not compromise existing designated bicycle routes.
- 19 Where development coincides with, intersects or divides a proposed bicycle route or corridor, development should incorporate through-access for cyclists.

- 20 Development should encourage and facilitate cycling as a mode of transport by incorporating end-of-journey facilities including:
- (a) showers, changing facilities and secure lockers
  - (b) signage indicating the location of bicycle facilities
  - (c) bicycle parking facilities provided at the rate set out in [Table WeTo/7 - Off-street Bicycle Parking Requirements for Urban Corridor Zone](#).
- 21 On-site secure bicycle parking facilities should be:
- (a) located in a prominent place
  - (b) located at ground floor level
  - (c) located undercover
  - (d) located where surveillance is possible
  - (e) well lit and well signed
  - (f) close to well used entrances
  - (g) accessible by cycling along a safe, well lit route.
- 22 Pedestrian and cycling facilities and networks should be designed and provided in accordance with relevant provisions of the *Australian Standards and Austroads Guides*.

### **Access**

- 23 Development should have direct access from an all weather public road.
- 24 Development should be provided with safe and convenient access which:
- (a) avoids unreasonable interference with the flow of traffic on adjoining roads
  - (b) provides appropriate separation distances from existing roads or level crossings
  - (c) accommodates the type and volume of traffic likely to be generated by the development or land use and minimises induced traffic through over-provision
  - (d) is sited and designed to minimise any adverse impacts on the occupants of and visitors to neighbouring properties.
- 25 Development should not restrict access to publicly owned land such as recreation areas.
- 26 The number of vehicle access points onto arterial roads shown on *Overlay Maps – Transport* should be minimised and, where possible, access points should be:
- (a) limited to local roads (including rear lane access)
  - (b) shared between developments.
- 27 Development with access from roads with existing or projected traffic volumes exceeding 6000 vehicles per day should be sited to avoid the need for vehicles to reverse onto or from the road.
- 28 Development with access from arterial roads or roads as shown on *Overlay Maps - Transport* should be sited to avoid the need for vehicles to reverse on to or from the road.

- 29 Structures such as canopies and balconies that encroach onto the footpath of an arterial road should not cause visual or physical obstruction to:
- (a) signalised intersections
  - (b) heavy vehicles
  - (c) street lighting
  - (d) overhead electricity lines
  - (e) street trees
  - (f) bus stops.
- 30 Driveways, access tracks and parking areas should be designed and constructed to:
- (a) follow the natural contours of the land
  - (b) minimise excavation and/or fill
  - (c) minimise the potential for erosion from runoff
  - (d) avoid the removal of existing vegetation
  - (e) be consistent with *Australian Standard AS 2890 Parking facilities*.
- 31 Gates across a driveway should be set back at least 5.5 metres from the boundary with an arterial road.

### **Access for People with Disabilities**

- 32 Development should be sited and designed to provide convenient access for people with a disability.
- 33 Where appropriate and practical, development should provide for safe and convenient access to the coast and beaches for disabled persons.

### **Vehicle Parking**

- 34 Development should provide off-street vehicle parking and specifically marked disabled car parking places to meet anticipated demand in accordance with [Table WeTo/2 - Off Street Vehicle Parking Requirements](#).
- 35 Development should be consistent with *Australian Standard AS 2890 Parking facilities*.
- 36 Vehicle parking areas should be sited and designed to:
- (a) facilitate safe and convenient pedestrian linkages to the development and areas of significant activity or interest in the vicinity of the development
  - (b) include safe pedestrian and bicycle linkages that complement the overall pedestrian and cycling network
  - (c) facilitate safe and convenient traffic circulation
  - (d) result in minimal conflict between customer and service vehicles
  - (e) avoid the necessity to use public roads when moving from one part of a parking area to another
  - (f) minimise the number of vehicle access points onto public roads

- (g) avoid the need for vehicles to reverse onto public roads
  - (h) where practical, provide the opportunity for shared use of car parking and integration of car parking areas with adjoining development to reduce the total extent of vehicle parking areas and the requirement for multiple access points
  - (i) not dominate the character and appearance of a site when viewed from public roads and spaces
  - (j) provide landscaping that will shade and enhance the appearance of the vehicle parking areas
  - (k) include infrastructure such as underground cabling and connections to power infrastructure that will enable the recharging of electric vehicles.
- 37 Vehicle parking areas should be designed to reduce opportunities for crime by:
- (a) maximising the potential for passive surveillance by ensuring they can be overlooked from nearby buildings and roads
  - (b) incorporating walls and landscaping that do not obscure vehicles or provide potential hiding places
  - (c) being appropriately lit
  - (d) having clearly visible walkways.
- 38 Where parking areas are not obviously visible or navigated, signs indicating the location and availability of vehicle parking spaces associated with businesses should be displayed at locations readily visible to customers.
- 39 Vehicle parking areas that are likely to be used during non-daylight hours should provide floodlit entry and exit points and site lighting directed and shaded in a manner that will not cause nuisance to adjacent properties or users of the parking area.
- 40 Vehicle parking areas should be sealed or paved to minimise dust and mud nuisance.
- 41 To assist with stormwater detention and reduce heat loads in summer, outdoor vehicle parking areas should include landscaping.
- 42 Vehicle parking areas should be line-marked to delineate parking bays, movement aisles and direction of traffic flow.
- 43 On-site visitor parking spaces should be sited and designed to:
- (a) not dominate internal site layout
  - (b) be clearly defined as visitor spaces not specifically associated with any particular dwelling
  - (c) be accessible to visitors at all times.

### **Vehicle Parking for Residential Development**

- 44 On-site vehicle parking should be provided having regard to:
- (a) the number, nature and size of proposed dwellings
  - (b) proximity to centre facilities, public and community transport within walking distance of the dwellings
  - (c) the anticipated mobility and transport requirements of the likely occupants, particularly groups such as aged persons.

- 45 Vehicle parking areas servicing more than one dwelling should be of a size and location to:
- (a) serve users, including pedestrians, cyclists and motorists, efficiently, conveniently and safely
  - (b) provide adequate space for vehicles, including emergency service vehicles, to manoeuvre between the street and the parking area
  - (c) reinforce or contribute to attractive streetscapes.

### **Undercroft and Below Ground Garaging and Parking of Vehicles**

- 46 Undercroft and below ground garaging of vehicles should only occur where envisaged in the relevant zone or policy area or precinct and ensure:
- (a) the overall height and bulk of the undercroft structure does not adversely impact on streetscape character of the locality or the amenity of adjacent properties
  - (b) vehicles can safely enter and exit from the site without compromising pedestrian or cyclist safety or causing conflict with other vehicles
  - (c) driveway gradients provide for safe and functional entry and exit
  - (d) driveways and adjacent walls, fencing and landscaping are designed to provide adequate sightlines from vehicles to pedestrians using the adjacent footpath
  - (e) openings to undercroft areas are integrated with the main building so as to minimise visual impact
  - (f) landscaping, mounding and/or fencing is incorporated to improve its presentation to the street and to adjacent properties
  - (g) the overall streetscape character of the locality is not adversely impaired (e.g. visual impact, building bulk, front setbacks relative to adjacent development)
  - (h) the height of the car park ceiling does not exceed 1 metre above the finished ground level.
- 47 In the case of undercroft and below ground car parks where cars are visible from public areas, adequate screening and landscaping should be provided.

## Waste

### OBJECTIVES

- 1 Development that, in order of priority, avoids the production of waste, minimises the production of waste, reuses waste, recycles waste for reuse, treats waste and disposes of waste in an environmentally sound manner.
- 2 Development that includes the treatment and management of solid and liquid waste to prevent undesired impacts on the environment including, soil, plant and animal biodiversity, human health and the amenity of the locality.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should be sited and designed to prevent or minimise the generation of waste (including wastewater) by applying the following waste management hierarchy in the order of priority as shown below:
  - (a) avoiding the production of waste
  - (b) minimising waste production
  - (c) reusing waste
  - (d) recycling waste
  - (e) recovering part of the waste for re-use
  - (f) treating waste to reduce the potentially degrading impacts
  - (g) disposing of waste in an environmentally sound manner.
- 2 The storage, treatment and disposal of waste materials from any development should be achieved without risk to health or impairment of the environment.
- 3 Development should avoid as far as practical, the discharge or deposit of waste (including wastewater) onto land or into any waters (including processes such as seepage, infiltration or carriage by wind, rain, sea spray, stormwater or by the rising of the water table).
- 4 Untreated waste should not be discharged to the environment, and in particular to any water body.
- 5 Development should include appropriately sized area to facilitate the storage of receptacles that will enable the efficient recycling of waste.
- 6 Development that involves the production and/or collection of waste and/or recyclable material should include designated collection and storage area(s) that are:
  - (a) screened and separated from adjoining areas
  - (b) located to avoid impacting on adjoining sensitive environments or land uses
  - (c) designed to ensure that wastes do not contaminate stormwater or enter the stormwater collection system
  - (d) located on an impervious sealed area graded to a collection point in order to minimise the movement of any solids or contamination of water

- (e) protected from wind and stormwater and sealed to prevent leakage and minimise the emission of odours
- (f) stored in such a manner that ensures that all waste is contained within the boundaries of the site until disposed of in an appropriate manner.

## **Wastewater**

- 7 The disposal of wastewater to land should only occur where methods of wastewater reduction and re-use are unable to remove the need for its disposal, and where its application to the land is environmentally sustainable.
- 8 Wastewater lagoons should not be sited in any of the following areas:
  - (a) within land subject to a 1-in-100 year average return interval flood event
  - (b) within 50 metres of the top of the bank of a watercourse
  - (c) within 500 metres of the coastal high water mark
  - (d) where the base of the lagoon would be below any seasonal water table.
- 9 Artificial wetland systems for the storage of treated wastewater, such as wastewater lagoons, should be:
  - (a) sufficiently separated from adjoining sensitive uses to minimise potential adverse odour impacts
  - (b) sited and designed to minimise potential public health risks arising from the breeding of mosquitoes.

## **Waste Treatment Systems**

- 10 Development that produces any sewage or effluent should be connected to a waste treatment system that complies with (or can comply with) the relevant public and environmental health legislation applying to that type of system.
- 11 The methods for, and siting of, effluent and waste storage, treatment and disposal systems should minimise the potential for environmental harm and adverse impacts on:
  - (a) the quality of surface and groundwater resources
  - (b) public health
  - (c) the amenity of a locality
  - (d) sensitive land uses.
- 12 Waste treatment should only occur where the capacity of the treatment facility is sufficient to accommodate likely maximum daily demands including a contingency for unexpected high flows and breakdowns.
- 13 Any on-site wastewater treatment system/ re-use system or effluent drainage field should be located within the allotment of the development that it will service.
- 14 A dedicated on-site effluent disposal area should not include any areas to be used for, or could be reasonably foreseen to be used for, private outdoor open space, driveways, car parking or outbuildings.
- 15 The spreading or discharging of treated liquid or solid waste onto the ground should only occur where the disposal area consists of soil and vegetation that has the capacity to store and use the waste without contaminating soil or surface or groundwater resources or damaging crops.

- 16 Stock slaughter works, poultry processors, saleyards, piggeries, cattle feedlots, milking sheds, milk processing works, fish processing works, wineries, distilleries, tanneries and fellmongeries, composting works, waste or recycling depots and concrete batching works should have a wastewater management system that is designed so as not to discharge wastes generated by the premises:
- (a) into any waters
  - (b) onto land in a place where it is reasonably likely to enter any waters by processes such as:
    - (i) seepage
    - (ii) infiltration
    - (iii) carriage by wind, rain, sea spray, or stormwater
    - (iv) the rising of the watertable.



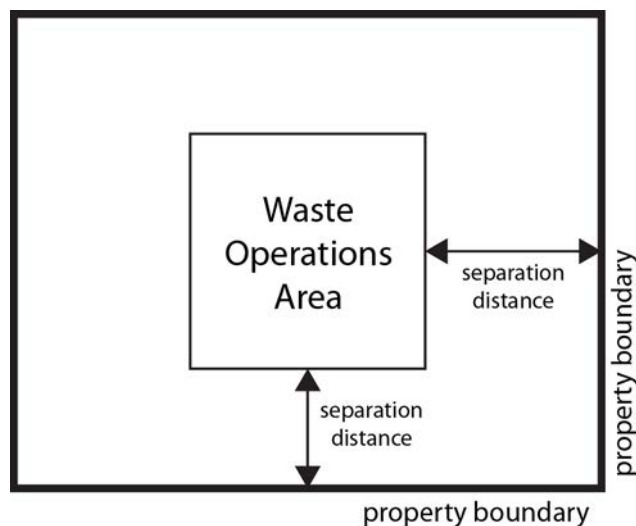
## Waste Management Facilities

### OBJECTIVES

- 1 The orderly and economic development of waste management facilities in appropriate locations.
- 2 Minimisation of human and environmental health impacts from the location and operation of waste management facilities.
- 3 Protection of waste management facilities from incompatible development.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Waste management facilities should be located and designed to minimise adverse impacts on both the site and surrounding areas from the generation of surface water and groundwater pollution, traffic, noise, odours, dust, vermin, weeds, litter, gas and visual impact.
- 2 Waste management facilities in the form of land fill and organic processing facilities should not be located in existing or future township, living, residential, centre, office, business, institutional or environmental protection, conservation, landscape, water protection and open space areas.
- 3 Waste management facilities should not be located where access to the facility requires, or is likely to involve, the use of non-arterial roads in adjacent residential areas.
- 4 Waste management facilities should:
  - (a) be appropriately separated from sensitive land uses and environmentally-sensitive areas
  - (b) incorporate the separation distance between the waste operations area (including all closed, operating and future cells) and sensitive uses within the development site as illustrated in the figure below:



- (c) not incorporate other land uses and activities within the separation distance unless they are compatible with both a waste management facility and any adjacent land uses.
- 5 Separation and/or noise attenuation should be used to ensure noise generation associated with the waste management operation does not unreasonably interfere with the amenity of sensitive land uses.

- 6 Sufficient area should be provided within the waste operations area for the:
  - (a) maximum expected volume of material on the site at any one time
  - (b) containment of potential groundwater and surface water contaminants
  - (c) diversion of clean stormwater away from the waste and potentially-contaminated areas.
- 7 Processing facilities and operational areas should be screened from public view.
- 8 Waste management sites should be accessed by appropriately constructed and maintained roads.
- 9 Traffic circulation movements within any waste management site should:
  - (a) be of a dimension and constructed to support all vehicles transporting waste
  - (b) enable all vehicles to enter and exit the site in a forward direction.
- 10 Suitable access for emergency vehicles should be provided to and within waste management sites.
- 11 Chain wire mesh or pre-coated painted metal fencing to a minimum height of 2 metres should be erected on the perimeter of a waste management facility site to prevent access other than at entry points.
- 12 Plant, equipment or activities that could cause a potential hazard to the public should be enclosed by a security fence.
- 13 Litter control measures that minimise the incidence of wind blown litter should be provided.
- 14 The waste operations area of a landfill or organic waste processing facility should be sited at least:
  - (a) 3 kilometres from an airfield used by commercial aircraft to minimise the risk of bird strikes to aircraft
  - (b) 500 metres from:
    - (i) the boundaries of the allotment
    - (ii) the nearest dwelling, shop, office, public institution or other building designed primarily for human occupation in the case of an organic waste processing facility for the composting of waste
  - (c) 250 metres from a public open space reserve, forest reserve, national park, conservation zone or policy area
  - (d) 100 metres from:
    - (i) the nearest surface water (whether permanent or intermittent)
    - (ii) a 1-in-100 year average return interval flood event area.
- 15 The waste operations area of a landfill should not be located on land:
  - (a) that is subject to land slipping
  - (b) with ground slopes greater than 10 per cent, except where the site incorporates a disused quarry.
- 16 The waste operations area of an organic waste processing facility should not be located on land:
  - (a) that is subject to land slipping

- (b) with ground slopes greater than 6 per cent
  - (c) where the interface of the engineered landfill liner and natural soils would be within any of the following:
    - (i) 15 metres of unconfined aquifers bearing groundwater with less than 3000 mg/L total dissolved salts
    - (ii) 5 metres of groundwater with a water quality of 3000 to 12 000 mg/L total dissolved salts
    - (iii) 2 metres of groundwater with a water quality of greater than 12 000 mg/L total dissolved salts.
- 17 Where required, a leachate barrier should be provided between the operational areas and underlying soil and groundwater.
- 18 Landfill activities that have a total storage capacity exceeding 230 000 cubic metres should make sustainable use of landfill gas emissions. For smaller landfill activities, if the sustainable use of the landfill gas emissions is not practical or feasible, flaring should be used to avoid gases being vented directly to the air.



# Overlay Section



## Affordable Housing Overlay

Refer to the [Map Reference Tables](#) for a list of maps that relate to this overlay. The following policies apply to the 'designated area' marked on the relevant Overlay Map.

### **INTERPRETATION**

Where the Objectives and/or Principles of Development Control that apply in relation to this overlay are in conflict with the relevant General Objectives and/or Principles of Development Control in the Development Plan, the overlay will prevail.

### **OBJECTIVES**

- 1 Affordable housing that is integrated into residential and mixed use development.
- 2 Development that comprises a range of affordable dwelling types that caters for a variety of household structures.

### **PRINCIPLES OF DEVELOPMENT CONTROL**

- 1 Development comprising 20 or more dwellings should include a minimum of 15 per cent affordable housing (as defined by the *South Australian Housing Trust Regulations as amended*).

## **Noise and Air Emissions Overlay**

Refer to the [Map Reference Tables](#) for a list of maps that relate to this overlay. The following policies apply to the 'designated area' marked on the relevant Overlay Map.

### **INTERPRETATION**

Where the Objectives and/or Principles of Development Control that apply in relation to this overlay are in conflict with the relevant General Objectives and/or Principles of Development Control in the Development Plan, the overlay will prevail.

### **OBJECTIVES**

- 1 Protect community health and amenity from adverse impacts of noise and air emissions.

### **PRINCIPLES OF DEVELOPMENT CONTROL**

- 1 Noise and air quality sensitive development located adjacent to high noise and/or air pollution sources should:
  - (a) shield sensitive uses and areas through one or more of the following measures:
    - (i) placing buildings containing less sensitive uses between the emission source and sensitive land uses and areas
    - (ii) within individual buildings, place rooms more sensitive to air quality and noise impacts (e.g. bedrooms) further away from the emission source
    - (iii) erecting noise attenuation barriers provided the requirements for safety, urban design and access can be met
  - (b) use building design elements such as varying building heights, widths, articulation, setbacks and shapes to increase wind turbulence and the dispersion of air pollutants provided wind impacts on pedestrian amenity are acceptable
  - (c) locate ground level private open space, communal open space and outdoor play areas within educational establishments (including childcare centres) away from the emission source.



## **Strategic Transport Routes Overlay**

Refer to the [Map Reference Tables](#) for a list of maps that relate to this overlay. The following policies apply to the 'designated area' marked on the relevant Overlay Map.

### **INTERPRETATION**

Where the Objectives and/or Principles of Development Control that apply in relation to this overlay are in conflict with the relevant General Objectives and/or Principles of Development Control in the Development Plan, the overlay will prevail.

### **OBJECTIVES**

- 1 Development that recognises the importance of strategic transport routes and does not impede traffic flow or create hazardous conditions for pedestrians, cyclists or drivers of vehicles, including emergency services vehicles.

### **PRINCIPLES OF DEVELOPMENT CONTROL**

- 1 Development adjacent to a strategic transport route should:
  - (a) avoid the provision of parking on the main carriageway
  - (b) not impede the potential for overhead cabling and associated infrastructure to be established in an existing or proposed tram corridor.
- 2 Vehicular site access should not be provided along the main street frontage where an alternative access is available.
- 3 Development adjacent kerbside bus stops should be set back to provide sufficient space for indented bus bays with associated hard stand area, shelter and a 1.2 metre wide continuous accessible path behind the bus shelter.



# Zone Section



## Adelaide Shores Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

### OBJECTIVES

- 1 A zone accommodating a range of short-term tourist accommodation and associated facilities, sporting, entertainment, cultural and recreational activities including associated spectator and administrative facilities.
- 2 Development of commercial and retail uses that are associated with and in support of recreation and tourist development.
- 3 Short-term tourist accommodation that is predominantly in the form of caravan and camping sites, cabins and serviced apartments surrounded by open landscaped areas.
- 4 Development of integrated recreational areas and facilities that accommodate a range of activities accessible to the community.
- 5 Buildings and facilities designed to high architectural standards and established within a landscaped setting, including tree plantings around buildings and car parking areas.
- 6 Development including car parking and facilities that are located and designed to blend in with existing vegetation and landscaping and designed to enhance the natural features of the local environment, including visual amenity, landforms, fauna and flora.
- 7 Development that contributes to the desired character of the zone.

### DESIRED CHARACTER

The zone includes land in the care and control of the West Beach Trust, which will be developed and maintained as a distinct and vibrant recreation, sporting and tourism destination of State significance, in accordance with [Concept Plan Map WeTo/12 - Adelaide Shores](#). The zone will include a range of tourist activities and tourist development, including tourist accommodation, such as camping sites, caravans and cabins. Land within the zone also comprises a range of recreation and sporting uses including public reserves, golf courses, driving ranges, tennis courts, sporting clubs and grounds, indoor sporting facilities, skate and BMX facilities. It is desirable for recreation uses to be integrated further with tourism related land uses in the area, aiming to generate year-round activities. The zone presents as a key location for tourist accommodation opportunities to be established adjacent the West Beach dune, beaches and coastal areas and the **Coastal Marina Zone**, which includes opportunity for boating activities.

Commercial and retail activities in the zone will be associated with recreation, sporting or tourism uses. Development should be of a size and scale required to primarily serve the needs of tourists, visitors to the area and users of nearby recreational facilities. Development in the order of 500 to 1500 square metres of total retail floor space, that may include smaller tenancies that cater for a mix of uses, is envisaged. Development should be consistent with the open landscape character of the site through the application of appropriate setbacks and landscaping, building scale and materials that are compatible with this character.

Land with frontage to Tapleys Hill Road may accommodate commercial development, where designed to complement other forms or retail and commercial development within the immediate locality. Tapleys Hill Road, however, is classified as a major traffic and freight route that caters for the safe and efficient movement of all types of road users. As this road provides optimal travel efficiency and reliability of travel times throughout the day for heavy vehicles, particularly when freight and commuter peaks coincide, it is envisaged that new development within the vicinity of Tapleys Hill Road will be designed and sited to minimise any impact or interference on the flow of traffic along Tapleys Hill Road. In achieving this, no additional access points are to be created to Tapleys Hill Road.

SARDI (SA Research and Development Institute-a research division of Primary Industries and Resources SA) is a purpose built marine and freshwater research facility located off Hamra Road. There may be an opportunity to provide improved linkages to the facility or incorporate an educational component, such as an interpretive centre.

Development including buildings and facilities will be sited so as not to cause an unreasonable nuisance or disturbance to occupants of adjacent residential buildings or tourist accommodation within the locality. Furthermore, development will have regard to possible noise impacts from the operation of the adjacent Adelaide Airport and must ensure that the safe operation of the airport is not compromised. Development will have also regard to potential odour impacts of the adjacent Glenelg Wastewater Treatment Plant and must ensure that the future operation of the plant is not compromised.

There may be areas within the zone that are contaminated because of previous activities. Further investigations to determine the extent of site contamination and necessary remediation measures may therefore be required to ensure that sites are suitable and safe for their intended use.

Public realm improvements along Military Road will enhance the appearance and street presentation, while promoting low speed vehicle movements and providing for a safer environment for pedestrians and cyclists. Convenient and safe pedestrian access along with pathways for cycling and movement throughout the zone are encouraged, including improved pedestrian and cycling connections beyond the zone.

The Patawalonga Creek which traverses the zone may in the long term accommodate part of a proposed airport runway extension. The Patawalonga Creek and surrounding land will continue to be protected and enhanced as a location of environment and habitat value.

The zone will be developed as a major tourism and sporting hub including the improvement to existing sporting and recreational facilities as well as the establishment of new sporting and recreational facilities. It is envisaged that tourism uses will be primarily located to the west of Military Road with recreation and sporting uses, occurring mostly to the east of Military Rd.

## **PRINCIPLES OF DEVELOPMENT CONTROL**

### **Land Use**

1 The following forms of development are envisaged in the zone:

- amenity block, including shower, toilet and laundry facilities
- amusement machine centre
- cabin
- car parking
- caravan permanently fixed to land
- clubroom or meeting room in association with a recreation, community or tourist development
- community centre
- consulting room in association with a recreation, community or tourist development
- dwelling in association with and ancillary to tourist accommodation (ie manager's residence) or a dwelling and/or accommodation that provide accommodation for participants and/or staff of sports related activities
- educational establishments where they relate to marine, sport, recreation or tourism activities
- emergency services facility
- entertainment, cultural or exhibition facility in association with a recreation, community or tourist development
- fencing in association with recreation, community or tourist development
- golf course or golf driving range
- hall or theatre
- hotel or licensed premises in association with a recreation, community or tourist development
- indoor and outdoor recreation facility
- lighting for night use of facilities
- marine-related research activities
- office in association with recreation, community or tourist development

- outbuilding or store in association with recreation, community or tourist development
  - personal service establishment
  - recreation area/sports ground and associated facilities including a tennis court, basketball court or playground
  - restaurant
  - shops or groups of shops in association with recreation, community or tourist development
  - showground
  - special event
  - spectator and administrative facilities ancillary to recreation, community or tourist development
  - swimming pool/spa
  - tourist park and other forms of tourist accommodation including a motel, backpacker's hostel, caravan park and/or camping ground.
- 2 Development listed as non-complying is generally inappropriate.
  - 3 Tourist accommodation and other uses associated with tourist development including laundries and ablution facilities, shops, restaurants, entertainment, cultural and recreational facilities should be designed and sited to serve holiday makers visiting the area, users of nearby recreational facilities and tourists.
  - 4 Development within the zone should be sited and designed in an integrated and co-ordinated manner to facilitate connection with tourist, recreation, community and boating land uses and activities within the zone and adjacent coastal areas.
  - 5 A community, sport and recreation hub comprising indoor and outdoor sport, community, entertainment and function facilities should be developed on the southern side of Hamra Avenue and be integrated and coordinated with the range of land uses and activities within the locality, including the adjacent marine-related research activities, where possible.
  - 6 The community, sport and recreation hub should be designed to include a hotel or tavern that is able to provide dining facilities for at least twice the number of patrons as bar facilities, to primarily serve the needs of tourists, visitors, local residents and people associated with recreation activities in the immediate locality.
  - 7 Intensive tourist and golfing related facilities including function and conference facilities, restaurants, entertainment and recreation facilities should be located primarily on allotments that front onto Military Road.
  - 8 Retail development is to be associated with recreation, sporting or tourism uses and should be of a size and scale required to serve the needs of tourists, visitors to the area and users of nearby recreational facilities.
  - 9 Administrative, service, storage and maintenance facilities should be unobtrusively sited and screened.

### **Form and Character**

- 10 Development should not be undertaken unless it is consistent with the desired character for the zone.
- 11 Buildings, caravans fixed to land, annexes and recreational facilities within a caravan park or tourist development should be setback a minimum of:
  - (a) 1 metre from an internal road
  - (b) 6 metres from a public road
  - (c) 2 metres from the boundary of the caravan park or camping ground.
- 12 Recreation facilities associated with tourist accommodation should be of a size and scale that will maintain the open natural character of the area and be ancillary to the primary role and function of the caravan park or tourist development.

- 13 Strong thematic landscaping incorporating locally indigenous plant species should be established to improve the landscape, provide shade and shelter, create interest and provide attractive landscaped civic spaces that are well connected with other activities and encourage public interaction.
- 14 Landscaping should form an integral part of the design and be used to define spaces and activity areas, reinforce internal networks, screen utility areas and enhance the visual amenity of the area.
- 15 Development adjacent to the Patawalonga Creek should enhance the character and appearance of the area and retain the creek as a water resource and stormwater channel.
- 16 Internal roads should be surfaced to prevent dust becoming a nuisance.
- 17 Safe and convenient pedestrian and bicycle pathways should be developed to encourage access between land uses and activities within the area, such as tourist accommodation, recreation and tourist facilities, boating and marine facilities and adjacent coastal areas.
- 18 Priority should be given to pedestrian movement within the area and provision for vehicular movement should be limited to that required for access to car parking areas, accommodation units and servicing requirements.
- 19 Every caravan, cabin or dwelling site within a caravan park or tourist development should provide parking for at least one vehicle, either located on the site or grouped within the park.
- 20 Car parking areas should be consolidated and co-ordinated where possible, shaded and screened with vegetation and setback to both improve the amenity of the zone and minimise the impact on adjacent residential areas
- 21 The shared use of car parking facilities between different activities and venues should be encouraged.
- 22 In areas prone to flooding or other natural hazards, buildings and structures (including annexes attached to caravans or caravans fixed to land) should be designed and constructed so that they can be removed in the event of a hazard.
- 23 Development adjacent to the facilities within the Glenelg Wastewater Treatment Plant should satisfy (a) and (b):
  - (a) not prejudice the plant's current or future operations
  - (b) be located, designed and developed having regard to the potential environmental impacts associated with odour from the operation of the facility and the potential expanded operation of the facility.
- 24 Advertisements and/or advertising hoardings should be associated with recreation, sporting and tourism activities within Adelaide Shores and measure no greater than 9 metres in height, and up to 18 square metres in sign area.

### **Traffic and Access**

- 25 There should be no additional access points to West Beach Road unless it can be demonstrated that there are movement and safety benefits that reduce impact on adjacent residential areas. Intersection treatments should also be designed to minimise impact on adjacent land uses.

### **Land Division**

- 26 Other than a lease plan, land division can only occur where a rearrangement of existing allotment boundaries takes place as part of a co-ordinated development scheme or as a rationalisation of land holdings, that is designed to allow more efficient and economic use of land consistent with the objectives for the zone.
- 27 No additional allotments should be created within the zone.



## PROCEDURAL MATTERS

### Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

### Non-complying Development

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

Form of development	Exceptions
Dwelling	Except for any of the following: (a) a manager's residence in association with and ancillary to tourist accommodation (b) a dwelling and/or accommodation for participants and/or staff of sports related activities.
Fuel depot	
Horse keeping	
Industry	
Land division	Except where no additional allotments are created either partly or wholly within the zone or the division is the subject of a lease plan in association with and ancillary to sporting, tourist or recreational uses.
Motor repair station	
Nursing home	
Service trade premises	
Supported accommodation	
Warehouse	
Waste reception, storage, treatment or disposal	

### Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development (except where the development is non-complying) are designated:

Category 1	Category 2
All forms of development which are envisaged in the zone and located 60 metres or more from a <b>Residential Zone</b> .	All forms of development which are envisaged in the zone, excluding development of a minor nature, where they are located less than 60 metres from a <b>Residential Zone</b> .

## Airfield Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

### OBJECTIVES

- 1 A zone primarily accommodating aircraft operations, passenger terminals, airport and aviation-related light industrial, service industrial, warehouse and storage purposes
- 2 Development that contributes to the desired character of the zone.

### DESIRED CHARACTER

This zone accommodates a range of services and facilities necessary for the safe, convenient and efficient operation of aviation activities at the Adelaide Airport. This zone will continue to accommodate a range of airport and export related industrial and commercial uses, and recreational activities of an open character on land reserved for long term aviation needs. Development will be compatible with the principle aviation function of the airport and maintain the long term operational and safety needs of the airport.

Development will promote the economic improvement of the State and the City of West Torrens by maintaining the airport as the international, national and regional gateway to South Australia, enhancing the airport as a major element of public infrastructure and facilitating the movement of time sensitive freight and passengers by infrastructure improvements.

Adequate separation distances between non-aviation and aviation development on airport land, and between development on airport land and off airport uses will be adopted.

The Adelaide Airport has also been committed as one of the sites for stormwater harvesting. The Adelaide Airport stormwater scheme will source water from the Brownhill/Keswick Creek System. A storage basin will capture flows, which will then be treated using biofiltration and media filtration. It is anticipated to initially harvest about 400 megalitres a year.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development are envisaged in the zone:
  - air passenger or air freight terminal
  - aircraft related facility
  - airport
  - fuel depot
  - general industry in the area of Morphett Street
  - light industry ancillary to and in association with aviation activities
  - recreation area
  - service industry ancillary to and in association with aviation activities
  - shop within the terminal building
  - store ancillary to and in association with aviation activities
  - warehouse ancillary to and in association with aviation activities.
- 2 Development listed as non-complying is generally inappropriate.
- 3 Development that would be adversely affected by noise and other hazards caused by airport activities should not be undertaken in the zone.

- 4 Development should not impede the use of the zone for aviation purposes.
- 5 Development west of Tapleys Hill Road should primarily be for recreational activities of an open character until such time as the area is required for aviation purposes.
- 6 The area north of Sir Donald Bradman Drive should only be developed for recreational land use.

### Form and Character

- 7 Development should not be undertaken unless it is consistent with the desired character for the zone.
- 8 Commercial and light industrial development located on the airport site should:
  - (a) facilitate the more efficient operation of the airport
  - (b) be sited in defined clusters
  - (c) not adversely affect the amenity of surrounding land uses.

## PROCEDURAL MATTERS

### Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

### Non-complying Development

Development (including combinations thereof, or more than one of a particular kind, alterations, extensions and/or additions to existing buildings and structures, building work, a change in the use of land, or division of an allotment) for the following is non-complying:

Form of development	Exceptions				
Advertisement and/or advertising hoarding	Except where: <ol style="list-style-type: none"> <li>(a) it measures 6 metres or less in height</li> <li>(b) the advertisement area satisfies the following criteria:               <table border="1" data-bbox="689 1377 1489 1550"> <thead> <tr> <th>Advertisement area (square metres)</th> <th>Additional area per metre of site frontage with a public road or public thoroughfare (square metres)</th> </tr> </thead> <tbody> <tr> <td>2</td> <td>0.1</td> </tr> </tbody> </table> </li> </ol>	Advertisement area (square metres)	Additional area per metre of site frontage with a public road or public thoroughfare (square metres)	2	0.1
Advertisement area (square metres)	Additional area per metre of site frontage with a public road or public thoroughfare (square metres)				
2	0.1				
Community centre					
Consulting room					
Dwelling					
Educational establishment	Except where ancillary to and in association with aviation activities.				
Hospital					
Horticulture					
Hotel					
Intensive animal keeping					
Motel					

Form of development	Exceptions
Nursing home	
Place of worship	
Pre-school	
Residential flat building	
Shop	Except where located within the terminal building.
Tourist accommodation	
Waste reception, storage, treatment or disposal	

### **Public Notification**

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

## Bulky Goods Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

### OBJECTIVES

- 1 A zone accommodating a range of buildings used for bulky goods outlets and service trade premises.
- 2 Development that contributes to the desired character of the zone.

### DESIRED CHARACTER

This zone will accommodate a range of bulky good tenancies. Development will occur in a co-ordinated, integrated and holistic manner. Development will be on amalgamated sites or with significant integrated features, in order to achieve an efficient layout, minimise access points and the length of driveways and to maximise pedestrian accessibility. The zone will also provide convenience retail activity at a local centre level in order to satisfy the needs of staff and visitors.

The major bulky goods tenancies will be at least 5000 square metres in floor area. At least half the total floor space in the zone will be taken up by major tenants. It is expected that the development of the bulky goods zone will occur in stages. Adequate parking and access points will be provided for each stage.

Due to the size of the bulky goods tenancies, the size of the buildings is likely to be large with relatively low building heights. The provision of interesting, articulated and varied facades, through the use of texture, pattern, graphics and colour to the buildings, is important in order to make the scale of the development more human. Buildings facing Railway Terrace, Sir Donald Bradman Drive, Scotland Road, London Road and James Congdon Drive will present an attractive façade.

Buildings will be constructed of durable, attractive materials that weather well and have strong colour schemes that are complementary to other bulky goods buildings in the zone. Buildings will incorporate glazing to all public frontages to increase the void to solid ratio of external surfaces.

High quality, structured landscaping will also be required to mitigate large scale building facades, provide visual amenity and shade. Landscaping will be provided at vehicular entry points to the zone, along the Sir Donald Bradman Drive frontage, James Congdon Drive frontage and other road verges, in the vehicle parking areas, and in the zone.

Decorative lighting will be incorporated to enhance the night time experience of building facades or landscaping along Railway Terrace, Sir Donald Bradman Drive and James Congdon Drive.

A clear hierarchy of streets, vehicular and pedestrian movement patterns, and car parks will be established across the site. Individual premises will be accessed via internal service roads. Vehicle parking, access and service areas will be shared to achieve efficiency in land use. Generously dimensioned designated pedestrian routes will be developed between car parks and buildings to provide safe, convenient and pleasant pedestrian movement. They will be clearly defined by landscaping, pavement treatment, lighting and street furniture. At least one north-south and east-west major outdoor sheltered pedestrian route will be established linking the various tenancies. Service bays and loading docks will be located away from public areas.

The development of the former Perry Engineering site will accommodate the potential for an internal vehicular link to the western side of the existing Bunnings site.

## **PRINCIPLES OF DEVELOPMENT CONTROL**

### **Land Use**

- 1 The following forms of development are envisaged in the zone:
  - bulky goods outlet
  - service trade premises that comprise only indoor displays or are primarily for the sale, rental or display of building materials, landscaping materials, sheds, domestic garages or outbuildings.
- 2 Development listed as non-complying is generally inappropriate.
- 3 Bulky goods outlets and service trade premises should have a gross leasable area of 500 square metres or more for each individual tenancy.
- 4 The total floorspace for bulky goods development within the zone should be limited to 50 000 square metres.
- 5 At least 50 per cent of the total bulky goods development floorspace in the zone should be provided by tenancies of at least 5000 square metres.
- 6 Bulky goods development should not occur on a site of less than 1.5 hectares unless it is integrated with an adjoining bulky goods development in order to create a site of greater than 1.5 hectares.
- 7 Development of premises primarily for the sale by retail of foodstuffs and/or personal effects should not exceed an aggregate total leasable area of such premises within the zone of 600 square metres or more with no single tenancy being greater than 150 square metres in total leaseable area.
- 8 Restaurants and cafes should only be located in bulky goods outlets or service trade premises that are larger than 2000 square metres, and should have a gross leaseable area of 150 square metres or less.
- 9 Warehouses and stores should only be developed if they are ancillary to and in association with a bulky goods outlet or service trade premises.
- 10 A childcare facility should not be developed unless it is ancillary to and in association with a bulky goods outlet or service trade premises.

### **Form and Character**

- 11 Development should not be undertaken unless it is consistent with the desired character for the zone.
- 12 Development should be undertaken in accordance with [Concept Plan Map WeTo/1 - Mile End South Bulky Goods](#).
- 13 Pick-up areas should be provided to avoid the necessity for customers to carry large items to vehicles and to accommodate vehicles with trailers.
- 14 Primary freight access should only occur from any of the following:
  - (a) Railway Terrace
  - (b) Scotland Road
  - (c) London Road.
- 15 Buildings (except for entry statements and architectural/sculptural elements) should not exceed 9 metres in height from finished floor level.
- 16 Buildings should be set back from public roads for a distance equal to or greater than the height of the building measured from the finished floor level of that building.

- 17 Sheltered bicycle racks should be provided at the rate of:
- (a) one for every 750 square metres of sales floor area for employees, which are to be provided in a secure location
  - (b) one for every 1000 square metres of sales floor area for customers, which are to be provided in a clearly identified location, which has high passive surveillance.
- 18 At least 10 per cent of the site should be landscaped.
- 19 Landscaping should be at least:
- (a) 10 metres wide along Sir Donald Bradman Drive and the James Congdon Drive frontages
  - (b) 5 metres wide along other road frontages, except where existing buildings are to be retained and are located on street boundaries.
- 20 A total of one tree per 8 car parking bays should be provided within the off street vehicle parking areas.
- 21 Advertisements and/or advertising hoardings should:
- (a) only be provided at the rate of one free standing advertisement per the major road frontages of Sir Donald Bradman Drive, Railway Terrace, London Road, Scotland Road or James Congdon Drive
  - (b) measure a maximum of 10 metres in height
  - (c) be located in close proximity to the major entry points.
- 22 Advertisements attached to buildings should:
- (a) not cover more than 15 per cent of a single wall face
  - (b) in the case where the building contains more than one tenancy, not consist of more than one wall mounted advertisement per tenancy.
- 23 Portable, easel or A-frame advertisements should not be displayed within the zone.

## Land Division

- 24 Land division should create allotments that are of a size and shape suitable for the intended use.

## PROCEDURAL MATTERS

### Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

### Non-complying Development

Development (including combinations thereof, or more than one of a particular kind, alterations, extensions and/or additions to existing buildings or structures, building work, a change in the use of land, or division of an allotment) for the following is non-complying:

Form of development	Exceptions				
Advertisements and/or advertising hoardings	Except where: <ul style="list-style-type: none"> <li>(a) it measures 6 metres or less in height</li> <li>(b) the advertisement area satisfies the following criteria:</li> </ul> <table border="1" style="margin-left: 40px;"> <thead> <tr> <th>Advertisement area (square metres)</th> <th>Additional area per metre of site frontage with a public road or public thoroughfare (square metres)</th> </tr> </thead> <tbody> <tr> <td>2</td> <td>0.1</td> </tr> </tbody> </table>	Advertisement area (square metres)	Additional area per metre of site frontage with a public road or public thoroughfare (square metres)	2	0.1
Advertisement area (square metres)	Additional area per metre of site frontage with a public road or public thoroughfare (square metres)				
2	0.1				
Amusement machine centre					
Caravan park					
Cemetery					
Child care centre	Except a short term crèche established as an adjunct to a bulky goods outlet for short term use by patrons.				
Community centre					
Consulting room					
Crematorium					
Demolition of all or part of a building identified as a State heritage place					
Dwelling					
Educational establishment					
Fuel depot					
General industry					
Horse keeping					
Horticulture					
Hospital					
Hotel					
Indoor recreation centre					
Industry					
Intensive animal keeping					
Major public service depot					
Motel					
Motor repair station					
Nursing home					
Petrol filling station					
Place of worship					
Residential flat building					



Form of development	Exceptions
Road transport terminal	
Shop or group of shops	<p>Except where it achieves one of the following:</p> <ul style="list-style-type: none"> <li>(a) the shop is a bulky goods outlet with a gross leasable area of 500 square metres or more</li> <li>(b) the shop is a restaurant (including café) and:               <ul style="list-style-type: none"> <li>(i) measures 150 square metres or less in gross leasable area</li> <li>(ii) is part of a bulky goods tenancy which measures 2000 square metres or more in gross leasable area</li> <li>(iii) the aggregate total gross leasable area of such premises within the zone does not exceed 600 square metres</li> </ul> </li> <li>(c) the shop is primarily used for the sale of foodstuffs, and/or is a restaurants and/or café and:               <ul style="list-style-type: none"> <li>(i) measures 150 square metres or less in gross leasable area</li> <li>(ii) the aggregate total gross leasable area of such premises within the zone does not exceed 600 square metres.</li> </ul> </li> </ul>
Special industry	
Stock slaughter works	
Tourist accommodation	
Waste reception, storage, treatment or disposal	
Wrecking yard	

### Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development (except where the development is non-complying) are designated:

Category 1	Category 2
<p>Restaurant (including café) which is located within a bulky goods outlet.</p> <p>Shop primarily for the sale of foodstuffs.</p> <p>Service trade premises which only comprise indoor displays or are used primarily for the sale, rental or display of building materials, landscaping materials, sheds, domestic garages or outbuildings.</p>	

## Coastal Marina Zone

Refer to the [Map Reference Table](#) for a list of the maps that relate to this zone.

### OBJECTIVES

- 1 A zone that provides for a marina and maritime development.
- 2 A zone accommodating a range of marine activities associated with and in support of recreation and tourist development in the locality, including retail and boat storage.
- 3 A range of boating activities and facilities that are integrated with the adjacent recreation and tourist related activities in the locality.
- 4 Buildings and facilities designed to high architectural standards and established within a landscaped setting, including tree plantings around buildings and car parking areas.
- 5 Development that contributes to the desired character of the zone.

### DESIRED CHARACTER

The zone which is located on land in the care and control of the West Beach Trust and currently accommodates the South Australian Sea Rescue Squadron and the Adelaide Sailing Club, provides an opportunity for development of a boat launching and return facility and other associated marine activities. Potential exists for the water and sea related clubs to be co-located in updated facilities and the substantial redevelopment of the area for boat launching, car parking and associated activities including retail and boat storage. It is desirable for marine uses to be integrated with recreation and tourist land uses in the area to generate year-round activities.

Commercial and retail activities in the zone will be associated with boating and marine uses. Development should be of a size and scale required to primarily serve the needs of tourists, visitors to the area and users of nearby recreational facilities. Development in the order of 500 to 1500 square metres of total retail floor space, that may include smaller tenancies that cater for a mix of uses, is envisaged. Development should be consistent with the open landscape character of the site through the application of appropriate setbacks and landscaping, building scale and materials that are compatible with this character.

Development including buildings and facilities will be sited so as not to cause an unreasonable nuisance or disturbance to occupants of residential buildings or tourist accommodation in the locality. Furthermore, development will have regard to possible noise impacts from the operation of the adjacent Adelaide airport and must ensure that the safe operation of the airport is not compromised.

Development will have regard to potential odour impacts of the adjacent Glenelg wastewater treatment plant and must ensure that the future operation of the plant is not compromised.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development are envisaged in the zone:
  - amenity block associated with marine or boating activities
  - boat berth, jetty, pier and pontoons
  - boat construction, maintenance, repair or sale
  - boat servicing facility (including fuel supply, power, water, effluent pump, toilets and showers)
  - car parking

- coastal protection work
  - clubroom in association with a marina or a water and sea related club
  - educational establishment associated with marine or boating activities
  - emergency services facility
  - entertainment, cultural and exhibition facility
  - fencing associated with marine or boating activities
  - hall
  - hotel
  - licensed premises
  - lighting for night use of facilities
  - loading and unloading facility
  - marina
  - maritime structures
  - office associated with marine or boating facilities
  - outbuilding or store in association with marine or boating activities
  - parking for vehicles and boats
  - restaurant
  - service trade premises in association with the sale, rental or display of boats
  - shop or groups of shops in association with marine or boating facilities
  - small scale tourist development
  - storage
  - wastewater collection, storage and transfer facility.
- 2 Development listed as non-complying is generally inappropriate.
- 3 Development within the zone should incorporate boat launching and return facilities with parking and public access for provision for associated development such as a chandlery, boat servicing and storage, boat sales, fuelling facilities, water-based recreation club(s) and an off-shore boat launching facility.
- 4 Other uses associated with boating and tourist activities including ablutions and store rooms, shops and restaurants can be developed within the zone.
- 5 Administrative, service, storage and maintenance facilities should be unobtrusively sited and screened.

### **Form and Character**

- 6 Development should not be undertaken unless it is consistent with the desired character for the zone.
- 7 Strong thematic landscaping incorporate locally indigenous plant species should be to improve the landscape, provide shade and shelter, create interest and provide attractive landscaped civic spaces that are well connected with other activities and encourage public interaction.
- 8 Landscaping should form an integral part of the design and be used to define spaces, reinforce internal networks, screen utility areas and enhance the visual amenity of the area.
- 9 Car parking areas should be consolidated and co-ordinated where possible, and shaded and screened with vegetation to improve the amenity of the zone.
- 10 The shared use of car parking facilities between different land uses and activities should be encouraged.
- 11 Development should maintain pedestrian and bicycle access to and along the coast with provision for vehicular movement limited to that required to allow access to servicing requirements, boat sales, the off-shore and shore based boat launching facilities and associated clubs and facilities.
- 12 Pedestrian and bicycle pathways should link tourist accommodation with other recreation and tourist facilities within the area.
- 13 Development including associated roads and parking areas, other than minor structures unlikely to be adversely affected by flooding, should be protected from sea level rise by ensuring all of the following apply:

- (a) site levels are at least 3.30 metres Australian Height Datum
  - (b) building floor levels are at least 3.55 metres Australian Height Datum
  - (c) there are practical measures available to protect the development against an additional sea level rise of 0.7 metres, plus an allowance to accommodate land subsidence until the year 2100 at the site.
- 14 The sea wall and sand pipeline adjacent to the foreshore shall be maintained so as to provide adequate protection from coastal erosion.
- 15 Over water development other than boat berthing and servicing facilities, ancillary walkways and channel markers should have a floor area of at least 4.25 metres AHD to provide protection for coastal flooding to the year 2100.
- 16 Development adjacent to the facilities within the Glenelg wastewater treatment plant should satisfy (a) and (b):
- (a) not prejudice the plant's current or future operations
  - (b) be located, designed and developed having regard to the potential environmental impacts associated with odour from the operation of the facility and the potential expanded operation of the facility.
- 17 Advertisements and/or advertising hoarding should be associated with marine or boating activities within Adelaide Shores and measure no greater than 9 metres in height, and up to 18 square metres in sign area.

## Land Division

- 18 Other than a lease plan, land division can only occur where a rearrangement of existing allotment boundaries takes place as part of a co-ordinated development scheme or as a rationalisation of land holdings, that is designed to allow more efficient and economic use of land consistent with the objectives for the zone.
- 19 No additional allotments should be created within the zone.

## PROCEDURAL MATTERS

### Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

In addition, the following forms of development are designated as complying subject to the conditions contained in [Table We/To1 – Conditions for Complying Development](#):

### Non-complying Development

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

Form of Development	Exceptions
Agriculture	
Auction room	
Bus depot	

Form of Development	Exceptions
Dwelling	
Crematorium	
Fuel Depot	Except in association with marine uses.
Hospital	
Horticulture	
Industry	Except boat repairs or marine associated activities.
Intensive animal keeping	
Land division	Except where no additional allotments are created either partly or wholly within the zone or the division is the subject of a lease plan in association with and ancillary to marine or boating uses.
Motor repair station	
Petrol filling station	Except in association with marine uses.
Place of worship	
Plant nursery	
Prescribed mining operations	
Residential Flat Building	
Primary school	
Road transport terminal	
Special industry	
Stock sales yard	
Warehouse	
Waste reception, storage, treatment or disposal	
Wrecking yard	

### Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development (except where the development is non-complying) are designated:

Category 1	Category 2
All forms of development which are envisaged in the zone	

## Coastal Open Space Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

### OBJECTIVES

- 1 Coastal land protected from development other than that necessary for conservation, recreational activity and public facilities.
- 2 Preservation and upgrading of the scenic character of the coastal landscape and foreshore areas fronting urban areas, townships or settlements.
- 3 Development of foreshore areas for a range of passive and active outdoor recreation activities and open space development, conservation and revegetation, in a parkland setting.
- 4 Land subject to inundation or susceptible to erosion kept free of development.
- 5 Maintained as a buffer to coastal processes for adjoining urban development.
- 6 Maintenance of the open space character and amenity of the coast.
- 7 Maintenance of open space links with the various local and regional open space networks and corridors to ensure passive recreational linkages.
- 8 Development that contributes to the desired character of the zone.

### DESIRED CHARACTER

This zone is a scenic area which is used for a range of passive and active outdoor recreation activities. The natural features of the coast will be conserved, rehabilitated and improved where required.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development are envisaged in the zone:
  - coastal protection works
  - conservation works
  - recreation area.
- 2 Development listed as non-complying is generally inappropriate.
- 3 The provision of facilities should be related to the demand for such facilities so as to prevent oversupply and inappropriate siting.
- 4 Development should be for public purposes and use.

#### Form and Character

- 5 Development should not be undertaken unless it is consistent with the desired character for the zone.
- 6 Development should not diminish the ability of the public to use and enjoy the coast or to gain access to the foreshore.

- 7 Development should be of a high standard of co-ordinated design with an emphasis on the creation of pedestrian areas.
- 8 Community facilities including shelters, boat ramps, public conveniences and kiosks, should be sited in convenient and accessible locations linked to the surrounding vehicular and pedestrian movement networks.
- 9 Development should be designed and sited to be compatible with conservation and enhancement of the coastal environment and scenic beauty of the zone.

### Land Division

- 10 Land division should not be undertaken except where:
  - (a) it will facilitate an envisaged use within the zone
  - (b) no additional allotments are created
  - (c) there is no increase in the number of allotments with frontage or direct access to the coast.

## PROCEDURAL MATTERS

### Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

### Non-complying Development

Development (including combinations thereof, or more than one of a particular kind, alterations, extensions and/or additions to existing buildings or structures, building work, a change in the use of land, or division of an allotment) for the following is non-complying:

Form of Development	Exceptions				
Advertisement and/or advertising hoarding	Except where: <ol style="list-style-type: none"> <li>(a) it measures 6 metres or less in height</li> <li>(b) the advertisement area satisfies the following criteria:</li> </ol> <table border="1" style="margin-left: 40px;"> <thead> <tr> <th style="text-align: left;">Advertisement area (square metres)</th> <th style="text-align: left;">Additional area per metre of site frontage with a public road or public thoroughfare (square metres)</th> </tr> </thead> <tbody> <tr> <td style="text-align: center;">2</td> <td style="text-align: center;">0.1</td> </tr> </tbody> </table>	Advertisement area (square metres)	Additional area per metre of site frontage with a public road or public thoroughfare (square metres)	2	0.1
Advertisement area (square metres)	Additional area per metre of site frontage with a public road or public thoroughfare (square metres)				
2	0.1				
Animal keeping					
Auction room					
Bus depot					
Bus station					
Caravan park					
Cemetery					
Commercial forestry					
Community centre					
Consulting room					

Form of Development	Exceptions
Crematorium	
Dairy	
Dam	
Demolition of all or part of a building identified as a State heritage place	
Dwelling	
Educational establishment	
Farming	
Fuel depot	
Hall	
Horse keeping	
Horticulture	
Hospital	
Hotel	
Indoor recreation centre	
Industry	
Intensive animal keeping	
Land division	<p>Except where both of the following apply:</p> <ul style="list-style-type: none"> <li>(a) no additional allotments are created wholly or partly within the zone</li> <li>(b) there is no increase in the number of allotments with frontage or direct access to the coast.</li> </ul>
Major public service depot	
Motel	
Motor racing or testing venue	
Motor repair station	
Nursing home	
Office	
Petrol filling station	
Place of worship	
Plant nursery	
Pre-school	
Prescribed mining operations	
Public service depot	
Residential flat building	



Form of Development	Exceptions
Road transport terminal	
Service trade premises	
Shop or group of shops	
Stadium	
Stock sales yard	
Stock slaughter works	
Store	
Tourist accommodation	
Warehouse	
Waste reception, storage, treatment or disposal	
Water tank	
Wrecking yard	

### **Public Notification**

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

## Commercial Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

### OBJECTIVES

- 1 A zone accommodating a range of commercial and business land uses.
- 2 Development that minimises any adverse impacts upon the amenity of the locality within the zone.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development are envisaged in the zone:
  - bulky goods outlet
  - consulting room
  - light industry
  - motor vehicle related business other than wrecking yard
  - office
  - petrol filling station
  - service trade premises
  - shop with a gross leasable area of 250 square metres or less
  - store
  - warehouse.
- 2 Development listed as non-complying is generally inappropriate.
- 3 Retail development in the zone should not hinder the development or function of any centre zone.
- 4 Shops, other than a bulky goods outlet, should have a gross leasable area of 250 square metres or less.

#### Land Division

- 5 Land division should create allotments that vary in size and are suitable for a variety of commercial activities.

## Arterial Roads Policy Area 1

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 Development that contributes to the desired character of the policy area.

### DESIRED CHARACTER

This policy area will accommodate a wide range of commercial and light industrial uses.

It is envisaged that the appearance of commercial development within the policy area will be improved through the redevelopment and upgrading of existing development sites.

Development site refers to the land which incorporates a development and all the features and facilities associated with that development, such as outbuildings, driveways, parking areas, landscaped areas, service yards and fences. Where a number of buildings or dwellings have shared use of such features and facilities, the development site incorporates all such buildings or dwellings and their shared features and facilities.

#### Precinct 1 Intersection

This precinct will accommodate bulky goods outlets and small scale offices.

A major integrated mixed use development will be developed in the north eastern quadrant of Richmond Road and South Road.

Functions unique to the Royal Automobile Association including office, workshop, motor vehicle repair and testing, retailing and minor storage are envisaged in this section of the precinct. The diversity of this range of activities and the significant area required for testing render the use unsuited to any existing centre zone. Accordingly, specific allowance has been made by depiction of an appropriately sized site.

Development will predominantly be two to three storeys in height. Buildings on prominent development sites as identified on [Concept Plan Map WeTo/2 - Prominent Development Sites](#) will be designed to define the intersection.

#### Precinct 2 Richmond Road

This precinct will accommodate light industry, service industry and warehouses and the office components of these uses.

Development facing Richmond Road will be of high quality and well landscaped. Large scale development up to three storeys is envisaged on the northern side of Richmond Road, east of Railway Terrace intersection, with development of reduced height further west of the precinct.

Front setbacks on the north side of Richmond Road east of the point opposite Surrey Road will be extensively landscaped.

#### Precinct 3 Sir Donald Bradman Drive (Mile End)

This precinct will accommodate bulky goods outlets, light industry, service industry and warehouses. Residential developments in the form of two and three storey residential flat buildings or dwellings above office and consulting room developments are envisaged in the area west of the South Road intersection.

**West Torrens Council**  
**Zone Section**  
**Commercial Zone**  
**Arterial Road Policy Area 1**

Development facing Sir Donald Bradman Drive and South Road will be of high quality and well landscaped. Large scale development up to three storeys is envisaged east of the South Road intersection, reducing to smaller scale development west of the intersection.

That part of the precinct between the Hilton Bridge and South Road will accommodate high quality bulky goods outlets development and upper level office space.

Extensive landscaping will be undertaken in the setback areas near the eastern boundary of the precinct incorporating substantial trees which will grow to form prominent features in the eastern part of the precinct, particularly as viewed from the Hilton Bridge.

### **Precinct 4 South Road (Keswick)**

This precinct will accommodate light industry, service industry, warehousing, service trade premises and a range of recreational and community uses.

Development will be one and two storeys in height and designed to complement the residential character of adjoining suburbs.

Facades of buildings facing South Road will be finished in stone, brick or rendered finishes, or finishes which complement the predominant brick and rendering building finishes in the area and the locality, with smooth, highly reflective finishes being avoided.

Development will be typified by a prominent and characteristic style of street landscaping. Development facing South Road will be well landscaped.

### **Precinct 5 South Road (Mile End)**

This precinct will primarily accommodate small scale offices and consulting rooms.

Development will be one and two storeys in height, and establish in existing buildings or in buildings which complement the character of the Edwardian buildings in the area. In particular, development will exhibit architectural features which complement the predominant style in the area particularly in terms of the style and pitch of roofs, the style and proportions of windows visible from the site, the style of verandas and carports and roof and external wall materials.

### **Precinct 6 South Road (Mile End South)**

This precinct will be distinguished by a variety of building forms and site layouts within which the unifying element is a prominent and characteristic style of landscaping both within the road reserve and on development sites. Development site refers to the land which incorporates a development and all the features and facilities associated with that development, such as outbuildings, driveways, parking areas, landscaped areas, service yards and fences. Where a number of buildings or dwellings have shared use of such features and facilities, the development site incorporates all such buildings or dwellings and their shared features and facilities.

Trees will be substantial in height, adding to the tree cover in the area.

## **PRINCIPLES OF DEVELOPMENT CONTROL**

### **Land Use**

1 The following forms of development are envisaged specifically in the policy area:

- community facility
- consulting room
- light industry
- office
- residential flat building
- row dwelling

- service industry
- service trade premises
- shop
- warehouse.

### Form and Character

- 2 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 3 A consulting room or office should not exceed 250 square metres gross leasable area other than where it satisfies (a) or (b) below:
  - (a) it is ancillary to a commercial or industrial activity and does not exceed 30 per cent of the gross leasable area of the development
  - (b) it occurs on a site specifically prescribed as suitable for a larger provision of consulting room or office space by a precinct.
- 4 A shop should only be in the form of any of the following:
  - (a) bulky goods outlet where it is envisaged in a precinct
  - (b) restaurant, café or delicatessen
  - (c) shop measuring a maximum gross leasable area of 250 square metres and which is in association with and ancillary to office, light industry, service industry or warehouses.
- 5 Residential development should predominantly take the form of either (a), (b) or (c) below:
  - (a) residential flat building up to three storeys in height
  - (b) row dwelling up to three storeys in height
  - (c) dwelling above a community facility, consulting room, office or bulky goods outlet.
- 6 A dwelling and residential flat building should be constructed on sites of not less than that shown in the following table:

Dwelling type	Minimum site area (square metres)
Detached	200
Semi-detached	200
Residential flat building	150
Row dwelling	180

- 7 Industrial development should only occur if the associated office and/or showroom component of the development presents to the arterial road.
- 8 Except for the following intrusions, buildings should be set back 3 metres from all road boundaries after road widening requirements are met:
  - (a) porticos defining main entrances to buildings which can be located up to the road boundary
  - (b) sun hoods and balconies which can be located up to 2 metres from the road boundary.

- 9 Any building located on a **'Prominent Development Site'** as identified by [Concept Plan Map WeTo/2 - Prominent Development Sites](#) should define the corners of the arterial road intersections by:
- having a primary orientation towards the arterial road
  - being located close to the arterial road boundaries taking into account road widening requirements
  - following the alignment of the arterial road boundaries around the corner.

## PRECINCT SPECIFIC PROVISIONS

Refer to the [Map Reference Tables](#) for a list of the maps that relate to the following precincts.

### Precinct 1 Intersection

- 10 The following forms of development are envisaged specifically in the precinct:
- bulky goods outlet
  - small scale office.
- 11 Mixed use development within that area identified as **'Major Integrated Mixed Use Development'** on [Concept Plan Map WeTo/3c – Richmond Road & South Road Intersection Commercial](#) should accommodate the following land uses:
- vehicle inspection, repair and servicing areas (including stores, warehouses and loading areas) of approximately 3800 square metres
  - mapping, control and communications, technical advice, museum and retail outlet including staff facilities of approximately 1500 square metres
  - office and office related uses, including executive and boardroom areas, data processing, training, technical support and administration, insurance and phone answering, of approximately 5710 square metres in gross leasable area.

### Form and Character

- 12 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 13 Development should be undertaken in accordance with the following concept plan maps:
- [Concept Plan Map WeTo/3a - Sir Donald Bradman Drive/Marion Road Intersection Commercial Precinct](#)
  - [Concept Plan Map WeTo/3b - Marion Road/Richmond Road Intersection Commercial Precinct](#)
  - [Concept Plan Map WeTo/3c - Richmond Road/South Road Intersection Commercial Precinct.](#)
- 14 Except for those sites identified as **'Major Integrated Mixed Use Development'** on [Concept Plan Map WeTo/3c Richmond Road/South Road Intersection Commercial Precinct](#), an office should not exceed 30 per cent of the gross leasable area in a mixed use development.
- 15 Development should measure between two storeys (8.5 metres) and three storeys (12.5 metres) in height where the site fronts the primary arterial road intersection.
- 16 Except where located in any of the following areas and the building is setback less than 16 metres from any arterial road, off street vehicle parking areas should not be located between buildings and the arterial roads:

- (a) at the intersection of Marion Road and Sir Donald Bradman Drive
  - (b) at the intersection of Marion Road and Richmond Road
  - (c) on sites east of the northern prolongation of Surrey Road.
- 17 A landscaped area with a minimum width of 3 metres should be established along the front property boundary of the site where adjoining an arterial road.

### Precinct 2 Richmond Road

- 18 The following forms of development are envisaged specifically in the precinct:
- light industry
  - office ancillary to an envisaged land use
  - service industry
  - warehouse.
- 19 An office should not exceed 30 per cent of the gross leasable area in a mixed use development.

### Form and Character

- 20 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 21 Development should be undertaken in accordance with the following concept plan maps:
- (a) [Concept Plan Map WeTo/4a - Richmond Road \(Marleston\) Commercial](#)
  - (b) [Concept Plan Map WeTo/4b - Richmond Road \(Richmond\) Commercial](#)
  - (c) [Concept Plan Map WeTo/4c - Richmond Road \(Keswick/Mile End South\) Commercial](#).
- 22 Development should be set back in accordance with the following parameters:

Location	Minimum value in addition to setback required for road widening (metres)
North side of Richmond Road between Railway Terrace and Keswick Creek	3
South side of Richmond Road between Eton Road and Surrey Road	3
South side of Richmond Road between Sarah Street and Bruce Avenue	3
Brooker Terrace	5 metres for non-residential development.
North side of Richmond Road between Main Terrace and the western boundary of the precinct	3
South side of Richmond Road between Grove Avenue and the western boundary of the precinct	3

**West Torrens Council**  
**Zone Section**  
**Commercial Zone**  
**Arterial Road Policy Area 1**

- 23 Development should not exceed three storeys (12.5 metres) in height except in areas where (a) or (b) applies:
- (a) development on the north side of Richmond Road between Railway Terrace and the prolongation of Farnham Road should measure between two storeys (8.5 metres) and three storeys (12.5 metres) in height
  - (b) non-residential development should not exceed two storeys (8.5 metres) in height within 30 metres of the boundary of a residential zone.
- 24 Off street vehicle parking areas should not be located between buildings and an arterial road.
- 25 A landscaped area with a minimum width of 3 metres should be established along the front property boundary of the site where adjoining an arterial road.

**Precinct 3 Sir Donald Bradman Drive (Mile End)**

- 26 The following forms of development are envisaged specifically in the precinct:
- bulky goods outlet
  - consulting room
  - light industry
  - office ancillary to an envisaged land use
  - service industry
  - warehouse.
- 27 Consulting rooms and offices should not exceed 30 per cent of the gross leasable area in a mixed use development.
- 28 Residential development in the area west of the South Road intersection should be in the form of either a two to three storey residential flat building or a dwelling above an office or consulting room.

**Form and Character**

- 29 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 30 Development should be undertaken in accordance with [Concept Plan Map WeTo/5 - Sir Donald Bradman Drive \(Mile End\) Commercial](#).
- 31 Development should be set back 3 metres after road widening from the following arterial roads:
- (a) South Road
  - (b) Sir Donald Bradman Drive (east and west of the intersection with South Road).
- 32 Except where non-residential development adjoins a residential zone boundary, development should not exceed three storeys (12.5 metres) in height.
- 33 Non-residential development adjoining a residential zone boundary should not exceed two storeys (8.5 metres) in height.
- 34 Off street vehicle parking areas should not be located between buildings and an arterial road.

**Precinct 4 South Road (Keswick)**

- 35 The following forms of development are envisaged specifically in the precinct:
- community centre
  - educational establishment
  - indoor recreation centre



- light industry
- office ancillary to light industry, service industry, warehouse and service trade premises
- place of worship
- service industry
- service trade premises
- warehouse.

36 An office should not exceed 30 per cent of the gross leasable area of a mixed use development.

### **Form and Character**

37 Development should not be undertaken unless it is consistent with the desired character for the precinct.

38 Development should be undertaken in accordance with [Concept Plan Map WeTo/6 - South Road \(Keswick\) Commercial](#).

39 Development should be set back at least 3 metres after road widening from the South Road property boundary.

40 Development should not exceed two storeys (8.5 metres) in height.

41 Off street vehicle parking areas should not be located between buildings and an arterial road.

42 A landscaped area with a minimum width of 3 metres should be established along the front property boundary of the site where it abuts South Road.

### **Precinct 5 South Road (Mile End)**

43 The following forms of development are envisaged specifically in the precinct:

- consulting room
- small scale office.

44 Consulting rooms and offices should not exceed 30 per cent of the gross leasable area of a mixed use development.

### **Form and Character**

45 Development should not be undertaken unless it is consistent with the desired character for the precinct.

46 Development should be undertaken in accordance with [Concept Plan Map WeTo/7 - South Road \(Mile End\) Commercial](#).

47 Development should be set back at least 3 metres after road widening from the South Road property boundary.

48 Development should not exceed two storeys (8.5 metres) in height.

49 Off street vehicle parking areas should not be located between buildings and an arterial road.

50 A landscaped area with a minimum width of 3 metres should be established along the front property boundary of the site where it abuts South Road.

### **Precinct 6 South Road (Mile End South)**

51 The following forms of development are envisaged specifically in the precinct:

- builder's yard on the east side of South Road
- general industry on the east side of South Road

**West Torrens Council**  
**Zone Section**  
**Commercial Zone**  
**Arterial Road Policy Area 1**

- light industry
- motor repair station on the east side of South Road
- office where ancillary to an envisaged land use
- service industry
- service trade premises
- warehouse.

**Form and Character**

- 52 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 53 Development should be undertaken in accordance with [Concept Plan Map WeTo/8 - South Road \(Mile End South\) Commercial](#).
- 54 Development should be set back at least 3 metres after road widening from the South Road property boundary.
- 55 Development should not exceed three storeys (12.5 metres) in height.
- 56 Off street vehicle parking areas should not be located between buildings and an arterial road.
- 57 A landscaped area with a minimum width of 3 metres should be established along the front property boundary of the site where it abuts South Road.
- 58 A development site should include trees which will grow to a height of at least 10 metres to enhance the tree cover in the area.

## District Commercial Policy Area 2

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 A policy area accommodating major servicing, storage, wholesaling and associated activities including road transport terminals.
- 2 Development that contributes to the desired character of the policy area.

### DESIRED CHARACTER

Development within the part of the policy area between Taylors Lane and George Street will be designed to avoid or reduce conflict with the adjacent residential zone and be of a scale, form and finish compatible with adjoining residential development. Landscaping will be provided to screen undesirable views, reduce visual impact of buildings, and contribute to the attractiveness of the streetscape. Planting will be provided along the rear of allotments to diminish the visual impact of buildings. Entrance and exit points will be located so that the use of nearby residential roads by non-residential vehicles visiting the development is avoided or minimised.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development are envisaged specifically in the policy area:
  - office
  - road transport terminal
  - service industry
  - store
  - wholesale
  - warehouse.

#### Form and Character

- 2 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 3 Development between Taylors Lane and George Street should incorporate a landscape strip measuring a minimum of 3 metres in width along the front property boundary.

## Local Commercial Policy Area 3

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### **OBJECTIVES**

- 1 A policy area accommodating local service activities which are compatible with the amenity of the locality.

### **PRINCIPLES OF DEVELOPMENT CONTROL**

#### **Land Use**

- 1 Development should be, primarily, local service activities which are compatible with the amenity of the locality.

## Office Park Policy Area 4

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 A mix of office and related commercial uses that complement and mutually reinforce each other, with an emphasis towards those activities that require buildings with larger floor areas and tenancy sizes.
- 2 Development that contributes to the desired character of the policy area.

### DESIRED CHARACTER

This policy area will achieve a physical and business environment for office and related activities, that is not presently provided for elsewhere within metropolitan Adelaide, in a co-ordinated and comprehensive manner. The policy area is described as an 'office park' and will best respond to the needs and preferences of corporations that require office accommodation with larger floor areas in a 'campus' style or high quality landscaped setting.

Development will achieve a high degree of integration between buildings and sites, together with user efficiency and amenity.

Development will seek to maximise the locational advantages of the policy area together with the form and arrangement of buildings for the conduct of business based on physical accessibility together with the utilisation of contemporary information technology and telecommunications infrastructure and systems. Given the various opportunities and constraints presented by land in this policy area, a total floor area of 50 000 square metres of office accommodation is envisaged.

Development will occur in a staged manner according to demonstrated need to ensure that buildings are used efficiently and economically, acknowledging the need to provide office accommodation in anticipation of and to facilitate identified need. Car parking, landscaping of open areas and other supporting infrastructure will be undertaken in a co-ordinated manner and to the extent required to serve the needs of each stage of development

Given the previous uses of land within the policy area, potential does exist for site contamination.

Development within this policy area will result in a significant employment node adjacent the Adelaide City Centre, and will assist in concentrating office development that may otherwise occur in a dispersed manner throughout the metropolitan area. By focusing such development to occur in a single location,

a range of economies and efficiencies will be achieved, together with a 'critical mass' for the provision of services such as public transport.

New development will be encouraged to provide the necessary infrastructure and physical improvements on land within the policy area and on that adjacent to promote the use of public transport systems. In particular, development will be comprehensively connected by advanced communications and information systems, so as to maximise the benefits of this technology in terms of minimising the need for physical travel in order to conduct business.

To the extent practicable, development within the policy area will promote and facilitate the use of bicycles and walking as a legitimate and sustainable manner in which to make the daily journey to and from work. Access networks will integrate in an efficient manner with existing and proposed cycle networks and walking trails provided within adjoining areas. In particular cyclist and pedestrian access will be improved across arterial roads and key intersections.

**West Torrens Council**  
**Zone Section**  
**Commercial Zone**  
**Office Park Policy Area 4**

The policy area seeks to regenerate land by providing an opportunity that maximizes the advantages to be derived from this fringe location in relation to the Adelaide City Centre, existing office development along Greenhill Road, Adelaide Airport, and access to arterial roads. The use of this land for office and related commercial development represents the highest and best use of this land.

In so doing, the relationship and interface with existing land uses within the locality that have potential for off site impacts such as noise, dust or odour, will be acknowledged and responded to by new development with appropriate siting, design and construction of buildings, landscaping and fencing.

Development within the policy area is to occur to a high standard and display design excellence and achieve a high level of user amenity. Development will achieve architectural excellence in the appearance and function of buildings, structures and landscaping. Except for the corner site to Richmond Road and Railway Terrace, building facades will display a colour palate of lighter natural tones that would assist in blending with the surrounding locality.

Buildings should achieve a human scale at ground level through the use of elements such as canopies, verandas or projections that assist in minimising the apparent bulk and scale of buildings when viewed from ground level and facilitate a comfortable pedestrian environment.

Building layouts will be configured such that larger tenancies will be achieved on upper levels, with the ability to provide for smaller tenancies on the ground floor.

Landscaping which includes locally indigenous species will be undertaken so as to achieve an open spacious character of high visual and user amenity, while still allowing for visual surveillance of open spaces. Where appropriate, landscaping features including public art will be used in order to punctuate and identify spaces that will assist in establishing a sense of place within the policy area for the enjoyment and benefit of users.

Limited retail shopping is anticipated within the policy area being limited to that associated with satisfying the daily needs of office workers and generally limited to the purchase of foodstuffs, limited comparison goods and personal services. Restaurants and/or cafes are desirable uses, preferably as a ground

floor component of office and commercial buildings, as are recreation uses such as a gymnasium or health club. A child care facility is a highly desirable use and will be encouraged where feasible.

Redevelopment of the site adjacent the intersection of Richmond Road and Railway Terrace will address and assist in the improvement of the function and safety of this intersection. Should this require localized road widening, such will be addressed as part of the redevelopment of this site.

The redevelopment of this site also presents an opportunity to improve the pedestrian environment in this locality and enhance the streetscape presentation of this site to the public realm that assists in enhancing the visual amenity of this locality, achieving a 'gateway' to the City.

## **PRINCIPLES OF DEVELOPMENT CONTROL**

### **Land Use**

1 The following forms of development are envisaged specifically in the policy area:

- bulky goods outlet that includes warehouse distribution.
- call centre
- child care facility
- gymnasium and recreational facility
- high technology industry that does not generate off site impacts
- office
- shop
- research and development facility
- restaurant/café.

- 2 A shop should be integrated within an office building and:
  - (a) should be limited to goods and services required on a daily basis by workers in the locality
  - (b) the shop component should not exceed 300 square metres gross leasable area of the total floor area of the office building.
- 3 An office development should have a gross leasable area greater than 1200 square metres per floor.

### **Form and Character**

- 4 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 5 Development should be undertaken in accordance with [Concept Plan Map WeTo/9 - Office Park Commercial](#).
- 6 Development should make provision for improved linkages that would facilitate the movement of people between the policy area and the rail transport corridor to the east.
- 7 Buildings should be provided with verandas, canopies or similar projections that assist in providing all weather access between buildings, with walkways designed and arranged to enable convenient and safe access for all persons.
- 8 Development on the corner of Richmond Road and Railway Terrace should provide off street vehicle parking areas to the rear of the built form.
- 9 Buildings should include provision for off street vehicle parking areas in either:
  - (a) an undercroft or basement arrangement
  - (b) a multi deck structure that does not exceed five building levels above ground.
- 10 Simultaneous two way access should be provided from Richmond Road and Railway Terrace with the number of access points being limited to two points from either arterial road.
- 11 Fencing should be limited to the northern and eastern perimeter of the policy area to maintain an open character when viewed from Richmond Road and Railway Terrace.
- 12 Advertisements and/or advertising hoardings should not flash or move.

## PROCEDURAL MATTERS

### Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

In addition, the following forms of development where located within the **District Commercial Policy Area 2** are designated as complying subject to the conditions contained in [Table WeTo/1 - Conditions for Complying Development](#):

- consulting room
- office
- recreation area.

### Non-complying Development

Development (including combinations thereof, or more than one of a particular kind, alterations, extensions and/or additions to existing buildings or structures, building work, a change in the use of land, or division of an allotment) for the following is non-complying:

Form of development	Exceptions								
Advertisement and/or advertising hoarding	Except where (a) or (b) applies: <ul style="list-style-type: none"> <li>(a) where located outside of the <b>Arterial Roads Policy Area 1</b>:               <ul style="list-style-type: none"> <li>(i) it measures 6 metres or less in height</li> <li>(ii) the advertisement area satisfies the following criteria:</li> </ul> <table border="1"> <thead> <tr> <th>Advertisement area (square metres)</th> <th>Additional area per metre of site frontage with a public road or public thoroughfare (square metres)</th> </tr> </thead> <tbody> <tr> <td>2</td> <td>0.1</td> </tr> </tbody> </table> </li> <li>(b) where located within the <b>Arterial Roads Policy Area 1</b>:               <ul style="list-style-type: none"> <li>(i) it measures 7 metres or less in height</li> <li>(ii) the advertisement area satisfies the following criteria:</li> </ul> <table border="1"> <thead> <tr> <th>Advertisement area (square metres)</th> <th>Additional area per metre of site frontage with a public road or public thoroughfare (square metres)</th> </tr> </thead> <tbody> <tr> <td>4</td> <td>0.1</td> </tr> </tbody> </table> </li> </ul>	Advertisement area (square metres)	Additional area per metre of site frontage with a public road or public thoroughfare (square metres)	2	0.1	Advertisement area (square metres)	Additional area per metre of site frontage with a public road or public thoroughfare (square metres)	4	0.1
Advertisement area (square metres)	Additional area per metre of site frontage with a public road or public thoroughfare (square metres)								
2	0.1								
Advertisement area (square metres)	Additional area per metre of site frontage with a public road or public thoroughfare (square metres)								
4	0.1								
Animal keeping where located within <b>Office Park Policy Area 4</b>									
Dairy									
Caravan park where located within <b>Office Park Policy Area 4</b> and/or <b>Local Commercial Policy Area 3</b>									
Cemetery where located within <b>Office Park Policy Area 4</b> and/or <b>Local Commercial Policy Area 3</b>									
Crematorium where located within <b>Office Park Policy Area 4</b> and/or <b>Local Commercial Policy Area 3</b>									
Demolition of all or part of a building comprising a State heritage place									



Form of development	Exceptions
Dwelling where located within the <b>District Commercial Policy Area 2</b> , and/or the <b>Office Park Policy Area 4</b> and/or the <b>Local Commercial Policy Area 3</b>	Except where located above non-residential development within the <b>District Commercial Policy Area 2</b> and/or <b>Local Commercial Policy Area 3</b> .
Farm building	
Farming	
Fuel depot	
General industry	Except within <b>Precinct 6 South Road (Mile End South)</b> .
Horticulture	
Intensive animal keeping	
Major public service depot where located within the <b>District Commercial Policy Area 2</b> , and/or the <b>Office Park Policy Area 4</b> and/or the <b>Local Commercial Policy Area 3</b>	
Minor public service depot where located within <b>Office Park Policy Area 4</b>	
Motor repair station where located within the <b>Arterial Roads Policy Area 1</b>	Except where located within either (a) or (b): (a) <b>Precinct 1 Intersection</b> (b) <b>Precinct 6 South Road (Mile End South)</b> .
Nursing home	
Office measuring 250 square metres or more in gross leasable area	Except where (a), (b), (c), (d) or (e) applies, the office: (a) forms part of a mixed use development (b) is within the <b>Local Commercial Policy Area 3</b> (c) is within the <b>District Commercial Policy Area 2</b> (d) is within the <b>Office Park Policy Area 4</b> .
Petrol filling station where located within <b>Office Park Policy Area 4</b>	
Place of worship	
Pre-school	
Prescribed mining operations	
Road transport terminal	
Service trade premises where located within the <b>Arterial Roads Policy Area 1</b> and/or the <b>Office Park Policy Area 4</b>	Except where located within <b>Precinct 6 South Road (Mile End South)</b> .
Shop or group of shops within the <b>Arterial Roads Policy Area 1</b> and/or the <b>District Commercial Policy Area 2</b> and/or the <b>Local Commercial Policy Area 3</b>	Except where it achieves one of the following: (a) the gross leasable area measures 250 square metres or less (b) the shop is a bulky goods outlet within the <b>Arterial Roads Policy Area 1</b> (c) it is a restaurant or café within the <b>Arterial Roads Policy Area 1</b> .

Form of development	Exceptions
Special industry	
Stadium	
Stock sales yard	
Stock slaughter works	
Telecommunications facility above 30 metres in height within the <b>Local Commercial Policy Area 3</b>	
Waste reception, storage, treatment, or disposal	
Waste water treatment plant where located within the <b>Office Park Policy Area 4</b>	
Winery	
Wrecking yard	

### Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development (except where the development is classified as non-complying) are designated:

Category 1	Category 2
<p>Within the <b>District Commercial Policy Area 2</b>, except where the site of the development is adjacent land to land in a zone which is different to the zone that applies to the site of the development:</p> <ul style="list-style-type: none"> <li>▪ service industry</li> <li>▪ store</li> <li>▪ timber yard</li> <li>▪ warehouse.</li> </ul> <p>Within the <b>Office Park Policy Area 4</b>:</p> <ul style="list-style-type: none"> <li>▪ bulky goods outlet</li> <li>▪ call centre</li> <li>▪ child care centre</li> <li>▪ consulting room</li> <li>▪ gymnasium and recreation facility</li> <li>▪ light industry</li> <li>▪ office</li> <li>▪ shop, where:               <ol style="list-style-type: none"> <li>(a) it is integrated within an office building</li> <li>(b) the shop component does not exceed 300 square metres gross leasable area of the total floor area of the office building.</li> </ol> </li> </ul>	<p>Within the <b>District Commercial Policy Area 2</b>: Demolition of a building listed in any of the following tables:</p> <ol style="list-style-type: none"> <li>(a) <a href="#">Table WeTo/3 - Contributory Items</a></li> <li>(b) <a href="#">Table WeTo/4 - Local Heritage Places</a>.</li> </ol> <p>Within the <b>District Commercial Policy Area 2</b> where the site of the development is adjacent land to land in a zone which is different to the zone that applies to the site of the development:</p> <ul style="list-style-type: none"> <li>▪ service industry</li> <li>▪ store</li> <li>▪ timber yard</li> <li>▪ warehouse.</li> </ul>

## Community Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

### OBJECTIVES

- 1 A zone accommodating community, educational, recreational and health care facilities for the general public's benefit.
- 2 Development that is integrated in function and provides a co-ordinated base to promote efficient service delivery.
- 3 Development that contributes to the desired character of the zone.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development are envisaged in the zone:
  - cemetery
  - community centre
  - consulting room
  - educational establishment
  - emergency services facility
  - hall
  - health facility
  - hospital
  - library
  - office associated with community service
  - place of worship
  - public administration office
  - recreation centre
  - theatre
  - welfare institution.
- 2 Development listed as non-complying is generally inappropriate.
- 3 Development should not be undertaken if it would inhibit or prejudice the integrated development of land within the zone for further community and institutional uses.

## Recreation Policy Area 5

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 A policy area accommodating sporting, entertainment, cultural and recreational activities and associated spectator and administrative facilities.
- 2 Development of integrated recreational areas and facilities that accommodate a range of activities accessible to the community.
- 3 Buildings, facilities and car parks located and designed to blend in with existing or additional trees, vegetation and landscaping.
- 4 Development that contributes to the desired character of the policy area.

### DESIRED CHARACTER

This policy area will accommodate a variety of sporting, entertainment, cultural and recreational activities and associated spectator and administrative facilities.

#### Precinct 7 Mile End

The section of the precinct north of Hilton Bridge, Sir Donald Bradman Drive is located in a highly visible position. Development will achieve a visual integration of all elements and uses in this section, in keeping with the city edge location. This part of the precinct accommodates an athletics complex which consists of club rooms, administrative facilities, training rooms, gym facilities, kiosk, officials rooms, lighting and terraced viewing areas. A range of other active recreation and ancillary uses are also appropriate and opportunity for passive recreation, integrated pedestrian access, bikeways and public transport is available. Areas for passive recreation will be available for shared use, being accessible to the public when not being used for athletics.

The design of development will be of a high quality. The form, scale and materials of the built form will visually integrate the uses for the site with existing features and the nearby park lands. The design will also enable future improvement of the athletics stadium and facilitate safe pedestrian and cycle links to the athletics stadium, and adjacent areas.

Ancillary commercial development and shops associated with the operation of the stadium will be incorporated into the overall design theme for the precinct.

That part of the policy area south of Hilton Bridge, Sir Donald Bradman Drive, will accommodate sporting and recreation facilities focused on netball and associated activities. Development will rehabilitate this area and achieve visual integration of all elements and uses on the site in keeping with the city edge location. A range of other active recreation and ancillary uses are also appropriate and opportunity for passive recreation, integrated pedestrian access, bikeways and public transport is available.

Lighting should be focused as far as possible on the stadium and should minimise light spill to adjoining residential areas.

Landscaping should be provided along the western perimeter of the site to form a buffer between the recreation area and the new arterial road.

Car parking and access areas should be co-ordinated with development within the section located to the north of the site to optimise layout and opportunities for shared use.

## Precinct 8 Open Space

This precinct will accommodate private recreation land and open space for a range of active and passive recreational land uses. This precinct envisages the continued development of the existing golf courses for active recreation as international and national championship private courses with associated ancillary facilities. Horses often access the site.

Development will be of a high architectural standard, designed and landscaped to enhance the amenity of the locality.

Internal roadways, driveways and paths, other than for private golf course pathways, should be sealed in bitumen, concrete or similar impervious materials.

## PRINCIPLES OF DEVELOPMENT CONTROL

### Land Use

- 1 The following forms of development are envisaged in the policy area:
  - car parking
  - clubroom associated with a sports facility
  - community centre
  - community hall
  - entertainment, cultural and exhibition facility
  - golf course
  - indoor and outdoor recreation facility
  - lighting for night use of facilities
  - meeting hall
  - office associated with community or recreation facility
  - playground
  - shops or groups of shops ancillary to recreation development
  - showground
  - sports ground and associated facility
  - special event
  - spectator and administrative facilities ancillary to recreation development
  - swimming pool.
- 2 A shop or group of shops should only be developed where:
  - (a) it is ancillary to recreation and sport development
  - (b) the total gross leasable area is 250 square metres or less.

### Form and Character

- 3 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 4 Strong thematic landscaping should be planted and maintained on individual sites to improve the amenity of the area, provide shade and shelter, create interest, provide habitat, retain existing native vegetation, use locally indigenous plant species in plantings where possible and define different activity areas.
- 5 All car parking areas should be shaded and screened with vegetation to improve the amenity of the zone.

### Land Division

- 6 Land division or the rearrangement of existing allotment boundaries should take place as part of a co-ordinated development scheme, or as a rationalisation of land holdings that is designed to allow more efficient and economic use of land consistent with the objectives of the zone.

## PRECINCT SPECIFIC PROVISIONS

Refer to the [Map Reference Tables](#) for a list of the maps that relate to the following precincts.

### Precinct 7 Mile End

7 The following forms of development are envisaged specifically within the precinct:

- sports facility
- small scale commercial activity associated with the sports facility
- small scale retail activity associated with the sports facility.

### Form and Character

- 8 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 9 Development should occur in accordance with [Concept Plan Map WeTo/10 - Mile End Recreation](#) and in particular:
- (a) should be developed with recreation facilities focused on athletics, with areas for passive recreation available for shared use, being accessible to the public when not being used for athletics
  - (b) the athletics complex should consist of club rooms, administration facilities, training rooms, gym facilities, kiosk, officials rooms, lighting and terraced viewing areas
  - (c) should provide integrated pedestrian access and bikeways to the athletics stadium, public transport and adjacent areas
  - (d) should provide off street parking for visitor, employee and services vehicles at the rate of one car parking space per 2.7 persons
  - (e) should incorporate commercial and retail development associated with the operation of the stadium
  - (f) the combined total floor area for retail development should not exceed 250 square metres.
- 10 Development should occur in accordance with [Concept Plan Map WeTo/11 - Mile End South Recreation](#) and in particular should:
- (a) be developed for indoor and outdoor netball facilities and associated sporting and recreation uses
  - (b) provide sufficient car parking for visitor, employee and service vehicles at the rate of one car parking space per 3.4 persons
  - (c) incorporate landscaping on the western perimeter of the site.
- 11 Development should be of a form, scale and constructed of materials to visually integrate development with existing features and the nearby Adelaide Park Lands.
- 12 The athletics stadium to the north of Hilton Bridge, and the netball facility to the south of Hilton Bridge should form high quality cohesive design elements at the entry to the City of Adelaide.
- 13 Stormwater drainage swales and detention areas should be designed to facilitate multiple use.

### Precinct 8 Open Space

14 The following forms of development are envisaged specifically in the precinct:

- sports facility
- small scale commercial activity associated with the sports facility

- car park
- club room associated with a golf club
- golf course
- shop or group of shops ancillary to a golf club.

**Form and Character**

- 15 Development should not be undertaken unless it is consistent with the desired character for the precinct.

## PROCEDURAL MATTERS

### Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

In addition, the following forms of development are designated as complying subject to the conditions contained in [Table WeTo/1 - Conditions for Complying Development](#):

- recreation area within **Precinct 8 Open Space**.

### Non-complying Development

Development (including combinations thereof, or more than one of a particular kind, alterations, extensions and/or additions to existing buildings or structures building work, a change in the use of land, or division of an allotment) for the following is non-complying:

Form of development	Exceptions				
Advertisement and/or advertising hoarding	Except where: <ol style="list-style-type: none"> <li>it measures 6 metres or less in height</li> <li>the advertisement area satisfies the following criteria:</li> </ol> <table border="1"> <thead> <tr> <th>Advertisement area (square metres)</th> <th>Additional area per metre of site frontage with a public road or public thoroughfare (square metres)</th> </tr> </thead> <tbody> <tr> <td>2</td> <td>0.1</td> </tr> </tbody> </table>	Advertisement area (square metres)	Additional area per metre of site frontage with a public road or public thoroughfare (square metres)	2	0.1
Advertisement area (square metres)	Additional area per metre of site frontage with a public road or public thoroughfare (square metres)				
2	0.1				
Auction room within <b>Precinct 7 Mile End</b> .					
Bus depot					
Bus station within <b>Precinct 8 Open Space</b>					
Caravan park within <b>Precinct 7 Mile End</b>					
Community centre within <b>Precinct 8 Open Space</b>					
Consulting room where located within the <b>Recreation Policy Area 5</b>					
Crematorium where located within the <b>Recreation Policy Area 5</b>					
Demolition of all or part of a building identified as a State heritage place within the <b>Recreation Policy Area 5</b>					
Dwelling where located within the <b>Recreation Policy Area 5</b>					
Emergency services facility within <b>Precinct 8 Open Space</b>					
Fuel depot					
Hall within <b>Precinct 7 Mile End</b> and/or within <b>Precinct 8 Open Space</b>					



Form of development	Exceptions
Hospital where located within the <b>Recreation Policy Area 5</b>	
Horticulture	
<b>Hotel within Precinct 8 Open Space</b>	
Industry	
<b>Indoor recreation centre within Precinct 8 Open Space</b>	
Intensive animal keeping	
Land division where located within the <b>Recreation Policy Area 5</b>	Except where no additional allotments are created partly or wholly within the policy area.
<b>Library within Precinct 8 Open Space.</b>	
Motel where located within the <b>Recreation Policy Area 5</b>	
Major public service depot	
Motor repair station	
Nursing home where located in the <b>Recreation Policy Area 5</b>	
<b>Office where located within Precinct 8 Open Space</b>	Except where associated with community or recreation facilities.
Petrol filling station	
Place of worship	
<b>Plant nursery within Precinct 7 Mile End</b>	
Prescribed mining operations	Except within <b>Precinct 8 Open Space.</b>
<b>Primary school within Precinct 8 Open Space.</b>	
<b>Public service depot within the Recreation Policy Area 5.</b>	
<b>Radio studio within Precinct 8 Open Space</b>	
Residential flat building where located within the <b>Recreation Policy Area 5</b>	
Road transport terminal	
Service trade premises	
Shop or group of shops	Except where located within <b>Precinct 7 Mile End</b> and either (a) or (b) is satisfied: (a) the premises will be used as a restaurant (b) the gross leasable area of a shop (except a restaurant) is 250 square metres or less.
Stock sales yard	
Stock slaughter works	

Form of development	Exceptions
Store	
Supported accommodation	
Television studio within <b>Precinct 8 Open Space</b>	
Warehouse	
Waste reception, storage, treatment or disposal	
Wrecking yard	

### Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development (except where the development is non-complying) are designated:

Category 1	Category 2
<p>Netball facility within <b>Precinct 7 Mile End</b>.            Stadium within <b>Precinct 7 Mile End</b>.</p> <p>Within the <b>Precinct 7 Mile End</b>, the following forms of development where ancillary to a stadium or netball facility:</p> <ul style="list-style-type: none"> <li>(a) club room</li> <li>(b) consulting room</li> <li>(c) gymnasium</li> <li>(d) office</li> <li>(e) recreation area</li> <li>(f) shop measuring 250 square metres or less in gross leasable floor area</li> <li>(g) training and seminar room</li> <li>(h) stadium for sport and spectator event.</li> </ul>	<p>Within the <b>Precinct 7 Mile End</b>:</p> <ul style="list-style-type: none"> <li>(a) community centre</li> <li>(b) exhibition centre</li> <li>(c) meeting hall.</li> </ul>

## District Centre Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

### OBJECTIVES

- 1 A centre that accommodates a range of retail facilities, offices, consulting rooms, and cultural, community, public administration, entertainment, educational, religious and residential facilities to serve the community and visitors within the surrounding district.
- 2 Development of a visually and functionally cohesive and integrated district centre.
- 3 A centre accommodating medium to high-density residential development in conjunction with non-residential development.
- 4 Development that contributes to the desired character of the zone.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development are envisaged in the zone:
  - bulky goods outlet
  - affordable housing
  - bank
  - child care centre
  - civic centre
  - consulting room
  - cultural and community facility
  - discount department store
  - dwelling in conjunction with non-residential development
  - educational establishment
  - emergency services facility
  - entertainment facility
  - health facility
  - hospital
  - hotel
  - indoor games centre
  - library
  - light industry within the **Brickworks Policy Area 6**
  - motor repair station
  - office
  - place of worship
  - playing field
  - pre-school
  - primary school
  - public transport terminal
  - residential flat building in conjunction with non-residential development
  - restaurant
  - serviced apartment within the **Brickworks Policy Area 6**
  - shop
  - supermarket
  - swimming pool.

- 2 Development listed as non-complying is generally inappropriate.
- 3 Development comprising a variety of medium density residential (including affordable housing) and non-residential uses should be developed only if it does not prejudice the operation of existing or future non-residential activity within the zone.

### **Form and Character**

- 4 Development should not be undertaken unless it is consistent with the desired character for the zone.
- 5 Development should be sited and designed to promote linkages between the various developments within a centre and adjoining main roads.
- 6 Facilities within the centre should be sited and designed with a view to promoting after-hours use to reinforce the centre as the focus of social activity in the district.
- 7 Dwellings should only be located above non-residential uses on the same allotment.

### **Vehicle Parking**

- 8 Vehicle parking should be provided in accordance with the rates set out in [Table WeTo/2 – Off Street Vehicle Parking Requirements](#) or [Table WeTo/6 - Off Street Vehicle Parking Requirements for Designated Areas](#) (whichever applies).

### **Land Division**

- 9 Land division in the zone is appropriate provided new allotments are of a size and configuration to ensure the objectives of the zone can be achieved.

## Brickworks Policy Area 6

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 Development that contributes to the desired character of the policy area.

### DESIRED CHARACTER

The policy area is strategically significant represented by its area, location at the gateway of the Council area, location on a transit corridor and location relative to key economic areas, in particular the 'Thebarton BioScience Precinct'.

Given the strategic and iconic significance of the Brickworks Markets site, it is important that development be of a high architectural standard, recognising the policy area's location as a gateway to the Council area. Development within the policy area will retain, protect and adaptively re-use the heritage listed chimney and brick kiln, provide a prominent built form edge along South Road and Ashwin Parade, with well designed, medium scale development establishing a consistent scale and quality of built form along the two road frontages. Development will demonstrate principles of best practise in terms of innovative, sustainable and integrated development that responds to the opportunities and constraints within and adjacent the policy area.

Non original structures attached to or immediately abutting the kiln or chimney will be removed to improve the integrity and prominence of the State Heritage Place. New development will provide a 15 metre curtilage around the State Heritage Place, preferably in the form of an open plaza. Such a plaza will accommodate temporary structures erected and removed on market trading days.

The design and layout of access roads, car parking, entry points onto public roads, landscaping and direction of traffic movements will be in accordance with [Concept Plan Map WeTo/13 - Brickworks District Centre](#).

The policy area will be a vibrant, active area incorporating a mix of uses including restaurants, shops and commercial activity operating beyond normal business hours, attracting both the local population and visitors from a wider catchment. Facilities within the centre will be sited and designed with a view to promoting after-hours use to reinforce the centre as the focus of social activity in the district. Strong physical and visual connections will be established with nearby facilities such as the River Torrens/Karrawirra Parri Linear Park and Thebarton Oval precinct.

The policy area will accommodate a wide range of uses which may include a reconfigured and revitalised market area centred around the heritage listed chimney and kiln, including both indoor and temporary outdoor stalls. In the event that an alternate use to the existing market is proposed, the area surrounding the heritage listed chimney and kiln will be developed for a range of mixed uses. Restaurants and cafes and entertainment related facilities are specifically encouraged in order to create vibrancy and activity at the interface with the River Torrens/Karrawirra Parri.

Development will create a vibrant environment through the incorporation of a public plaza. The plaza will have active surrounding edges, providing shade and shelter for pedestrians and cyclists. Feature lighting will be integrated into the plaza to provide a safe pedestrian space at night.

Development in the policy area will address and embrace the adjacent River Torrens/Karrawirra Parri Linear Park. The policy area will accommodate 'district level' retailing in the form of a discount department store, supermarket and specialty shops, combined with activities such as banks, offices, consulting rooms, post office and health centre, educational and community facilities such as child care centre, civic uses and educational establishments, expansion of the existing 'Thebarton BioScience Precinct' including research and development facilities, low impact laboratories and associated support facilities and high density residential such as apartments as part of an integrated mixed use development where interface, potential land use conflict and amenity impacts can be appropriately managed and addressed.

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**Zone Section**  
**District Centre Zone**  
**Brickworks Policy Area 6**

Development will emphasise visual and physical connectivity with the surrounding area and facilities. Appropriate and cohesive pedestrian and bicycle movement corridors and visual links will be established between new development and adjoining areas including public recreation areas, public transport stops and community facilities.

The subdivision layout and subsequent development of the policy area will create attractive, well landscaped, legible and liveable environments, convey a sense of place to workers, residents and visitors, establish movement networks for vehicular, pedestrian and bicycle traffic, adopt passive energy and water conservation principles, establish an appropriate curtilage around the heritage listed brick kiln and establish a maximum total gross leasable floor space in the order of 25 000 square metres, with a minimum gross leasable floor space in the order of 20 000 square metres of that dedicated to retailing (including a redeveloped market).

Development within the policy area will contribute to a cohesive built form and style and be of high architectural standard, incorporating simple forms yet with a high degree of articulation. The built form character will be established through appropriate design, including interesting roof forms, use of light colours and materials and appropriate landscaping.

Development within the policy area will take into consideration the possible future development of the nearby Thebarton Oval and Kings Reserve by providing opportunities for integration, shared use of facilities, movement and open space corridors.

Areas adjacent to the policy area include non-residential uses such as industry. In order to ensure that the long term viability of those uses, residential development will be most appropriately located towards the centre of the site, preferably as part of a 'main street', integrated mixed use development.

All medium to high density residential or serviced apartment development will be designed to ensure the living rooms have a satisfactory external outlook. Living rooms that do not have an outlook or the only source of outlook is through high level windows or skylights are not considered to provide an appropriate level of amenity for the occupiers.

It is envisaged that high density housing be established as part of an integrated development scheme whereby the appropriate relationship and distribution between uses is considered in order to provide an aesthetically pleasant setting for those dwellings, ensure residents without ground level private open space areas have easy access to public reserves, facilitate access to services and facilities for a greater number of people, provide a sense of place for residents, improve opportunities for passive energy conservation in buildings, facilitate passive surveillance of public open space, walkways and communal areas in the vicinity and minimises potential amenity impacts arising from the siting, form and operation of adjacent uses and external site influences.

All forms of residential development will have regard to the mix of activities and lively ambience contemplated for the policy area.

Development will provide a fully integrated design from streetscapes, public plazas and public promenade through to private residential spaces. Landscaping will provide shade, vertical form, complement new development, frame views and integrate ecological issues into its design.

Multi level and undercroft car parks will be integrated with a principal building and designed to provide active street frontages and land uses such as commercial, retail or other non car park uses, along ground floor street frontages to maintain pedestrian interest and activity at street level.

## **PRINCIPLES OF DEVELOPMENT CONTROL**

### **Land Use**

- 1 Medium to high density residential development, including a minimum of 15 per cent affordable housing, and development comprising a variety of residential and non-residential uses should only be undertaken where such development does not prejudice the primary retail focus within the zone.

- 2 Residential development should be in the form of apartments and shop top housing.

### **Form and Character**

- 3 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 4 Development should be sited and designed to promote linkages between the various developments within the policy area and adjoining main roads and where applicable, State Heritage Places, River Torrens/Karrawirra Parri Linear Park and Kings Reserve.
- 5 Development should be undertaken in accordance with [Concept Plan Map WeTo/13 - Brickworks District Centre](#), and in particular:
- (a) the area for markets should be established around the State Heritage listed kiln and chimney
  - (b) create a vibrant environment through the incorporation of a public plaza and designed to provide a direct relationship with the River Torrens/Karrawirra Parri to the north and provide greater integration and use of this public space
  - (c) outdoor cafes (operated either in association with or independently of the markets) should occur within the plaza
  - (d) except for temporary structures associated with market trading days that can be readily removed, no new buildings or structures should be located within a 15 metre curtilage around the State Heritage listed kiln and chimney
  - (e) the provision of a continuous public promenade for pedestrians and cyclists forming a link between Kings Reserve (Ashwin Parade) and the River Torrens/Karrawirra Parri, that:
    - (i) is located along any major vehicle access roads from Ashwin Parade
    - (ii) provides a visual link between Ashwin Parade and the brick kiln and chimney
    - (iii) incorporates a road pavement measuring a minimum of 8 metres wide
    - (iv) accommodates pedestrian and cyclist paths varying in width
    - (v) incorporates landscaping, street furniture and lighting that creates a boulevard effect
  - (f) retail and commercial activities located on the ground floor to support residential landuses above developed immediately adjacent a public promenades
  - (g) buildings should be built to the boundary of internal streets to:
    - (i) create a continuity of frontage
    - (ii) provide definition and enclosure of the public realm
    - (iii) contribute to the interest, vitality and security of the pedestrian environment
    - (iv) provide emphasis to the street corners
  - (h) the north eastern portion of the policy area should be kept free of buildings and structures in order to facilitate and maximise views of the State Heritage listed kiln and chimney
  - (i) undercroft and/or multi level car parking should be established in the south-eastern corner of the site where the existing ground level is below that of the road reserve of South Road.

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**Zone Section**  
**District Centre Zone**  
**Brickworks Policy Area 6**

- 6 Buildings should be primarily one to three storeys in height, except buildings including residential land uses and landmark structures which should be to higher heights of five to six storeys, with all buildings grouped or positioned in accord with the [Concept Plan Map WeTo/13 - Brickworks District Centre](#).
- 7 Buildings should be designed and sited to avoid creating extensive areas of uninterrupted walling facing areas exposed to public view, with the length of uninterrupted walling being no more than 30 metres.
- 8 Medium to high density residential or serviced apartment development should incorporate a mix of dwelling sizes to provide a high quality living environment by satisfying the following parameters:

Dwelling type	No. of bedrooms	Minimum internal floor area (square metres)	Minimum area of private open space (square metres)	Minimum cubic area of storage facilities for the occupants (cubic metres)
Studio	No separate bedroom	37	8	6
Dwelling/apartment	2	50	8	8
Dwelling/apartment	2	75	11	8
Dwelling/apartment	3	100	15	8

- 9 Private open space should only be reduced by 10 per cent where communal open space is accessible to all occupants within a residential allotment.
- 10 Private open space should have a minimum dimension of 2 metres for upper level balconies and be well proportioned to be functional and promote indoor/outdoor living.
- 11 The distribution of storage facilities should be provided as follows:
  - (a) no less than 50 per cent of the storage area within the dwelling/apartment
  - (b) the remainder of the storage area within the basement or other communal areas.
- 12 Medium to high density residential or serviced apartment development should be designed to maximise opportunities to facilitate natural ventilation and capitalise on natural daylight and minimise the need for artificial lighting during daylight hours.
- 13 The finished floor level resulting from earthworks in the south-eastern corner of the policy area should not exceed the finished floor level of the immediately adjacent road reserve of either South Road or Ashwin Parade.
- 14 Attached or abutting dwellings/residential flat buildings/apartments should be designed to minimise the transmission of sound between dwellings and, in particular, to protect bedrooms from noise intrusions.
- 15 Residential development should be located, orientated and designed in accordance with *Australian Standard/New Zealand Standard 2107:2000 Acoustics - Recommended design sound levels and reverberation times for building interiors*.
- 16 Major vehicle access should be provided from Ashwin Parade rather than South Road.
- 17 Multi level and undercroft car parks should be integrated with a principal building and where:
  - (a) on a corner site with two major street frontages, should be set back from the major street frontages, with commercial or other non-car park floor space in front of and screening the car parking building



- (b) on a site with only one major street frontage, incorporate a façade treatment on the major street frontage sufficiently enclosed and detailed to complement neighbouring buildings in a manner consistent with desired character of the policy area.
- 18 Existing vegetation in the north eastern corner of the site and along the northern boundary adjoining the River Torrens/Karrawirra Parri Linear Park should be retained.
- 19 Chimneys or exhaust flues associated with commercial development should terminate at a height at least 3 metres higher than the highest building structure within a 30 metre radius.

## Kurralta Park Policy Area 7

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 Development that contributes to the desired character of the policy area.

### DESIRED CHARACTER

It is envisaged that this policy area will accommodate a range of retail and non-retail facilities such as office, commercial, entertainment and community facilities. The retail floor space will incorporate specialty shops and sub-major tenancies offering both convenience and comparison goods.

Development will be co-ordinated and new and existing facilities will be integrated to achieve shared car parking areas, the provision of strategically placed pedestrian linkages and complementary and visually compatible landscaping and built-form.

Car parking spaces will be located as close as possible to the facilities they serve and large expansive parking areas will be avoided where possible through the provision of roof-top, undercroft and multi-level car parks and the use of landscaping.

Re-development of the district centre will ensure that potential impacts on the residential areas and streets are minimised through means such as the provision of landscaped buffer areas, orientation of buildings and plant equipment, minimising patron and service vehicle intrusion into residential streets and the careful placement and design of ingress/egress points and internal traffic movement patterns to avoid congestion within surrounding roads.

Landscaping within the front setback area along Anzac Highway will complement the existing style of street tree planting on Anzac Highway, as modified by any landscaping proposals for the highway by public authorities.

Due to the length of the site, advertisements directing customers to the specific retail outlets is envisaged, providing the number of advertisements do not dominate the landscaping envisaged in the front setback and the physical landscape of the centre.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Form and Character

- 1 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 2 Development should be undertaken in accordance with [Concept Plan Map WeTo/14 - Kurralta Park District Centre](#).
- 3 Development should be primarily oriented towards Anzac Highway.
- 4 Development should be set back no less than 10 metres from Anzac Highway.
- 5 Development should be set back no less than 5 metres from any of the following:
  - (a) Mortimer Street
  - (b) Warwick Avenue.

- 6 Development should not exceed three storeys (12.5 metres) in height.
- 7 Non-residential development adjacent to Mortimer Street and Warwick Avenue should not exceed two storeys (8.5 metres) in height.
- 8 Vehicle access to non-residential development from Warwick Avenue should be limited to one access point only.
- 9 Service vehicle access to non-residential development should be from a limited number of access points on Mortimer Street.
- 10 Extensive landscaping should be incorporated within the 5 metre setback area adjacent to Mortimer Street and Warwick Avenue.
- 11 There should be no more than two advertisements and/or advertising hoardings serving as primary identifiers of the businesses and uses and such structures should only be located near the Anzac Highway frontage.
- 12 An advertisement and/or advertising hoarding facing Mortimer Street and Warwick Avenue should:
  - (a) perform a secondary role in identifying a business
  - (b) not incorporate a moving display.

## PROCEDURAL MATTERS

### Complying Development

Complying developments are prescribed in schedule 4 of the *Development Regulations 2008*.

In addition, the following forms of development (except where the development is non-complying) are complying:

A change of use to a shop, office, consulting room or any combination of these uses where all of the following are achieved:

- (a) the area to be occupied by the proposed development is located in an existing building and is currently used as a shop, office, consulting room or any combination of these uses
- (b) the building is not a State heritage place
- (c) it will not involve any alterations or additions to the external appearance of a local heritage place as viewed from a public road or public space
- (d) if the proposed change of use is for a shop that primarily involves the handling and sale of foodstuffs, it achieves either (i) or (ii):
  - (i) all of the following:
    - (A) areas used for the storage and collection of refuse are sited at least 10 metres from any Residential Zone boundary or a dwelling (other than a dwelling directly associated with the proposed shop)
    - (B) if the shop involves the heating and cooking of foodstuffs in a commercial kitchen and is within 30 metres of any Residential Zone boundary or a dwelling (other than a dwelling directly associated with the proposed shop), an exhaust duct and stack (chimney) exists or is capable of being installed for discharging exhaust emissions
  - (ii) the development is the same or substantially the same as a development, which has previously been granted development approval under the *Development Act 1993* or any subsequent Act and Regulations, and the development is to be undertaken and operated in accordance with the conditions attached to the previously approved development
- (e) if the change in use is for a shop with a gross leasable floor area greater than 250 square metres and has direct frontage to an arterial road, it achieves either (i) or (ii):
  - (i) the primary vehicle access (being the access where the majority of vehicles access / egress the site of the proposed development) is from a road that is not an arterial road
  - (ii) the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared
- (f) off-street vehicular parking is provided in accordance with the rate(s) specified in [Table WeTo/2 - Off Street Vehicle Parking Requirements](#) or the desired minimum in rate in [Table WeTo/6 - Off Street Vehicle Parking Requirements for Designated Areas](#) (whichever table applies) to the nearest whole number, except in any one or more of the following circumstances:
  - (i) the building is a local heritage place

- (ii) the development is the same or substantially the same as a development, which has previously been granted development approval under the *Development Act 1993* or any subsequent Act and Regulations, and the number and location of parking spaces is the same or substantially the same as that which was previously approved
- (iii) the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared.

## Non-complying Development

Development (including combinations thereof, or more than one of a particular kind, alterations, extensions and/or additions to existing buildings or structures, building work, a change in the use of land, or division of an allotment) for the following is non-complying:

Form of development	Exceptions				
Advertisement and/or advertising hoarding	<p>Except where:</p> <ul style="list-style-type: none"> <li>(a) it measures 9 metres or less in height</li> <li>(b) the advertisement area satisfies the following criteria:</li> </ul> <table border="1"> <thead> <tr> <th>Advertisement area (square metres)</th> <th>Additional area per metre of site frontage with a public road or public thoroughfare (square metres)</th> </tr> </thead> <tbody> <tr> <td>6</td> <td>0.15</td> </tr> </tbody> </table>	Advertisement area (square metres)	Additional area per metre of site frontage with a public road or public thoroughfare (square metres)	6	0.15
Advertisement area (square metres)	Additional area per metre of site frontage with a public road or public thoroughfare (square metres)				
6	0.15				
Demolition of all or part of a building comprising a State heritage place within the <b>Kurralta Park Policy Area 7</b>					
Dwelling	<p>Except where located within:</p> <ul style="list-style-type: none"> <li>(a) the <b>Kurralta Park Policy Area 7</b>, and located above non-residential development</li> <li>(b) the <b>Brickworks Policy Area 6</b> and where built in conjunction with a non-residential development and located on the same allotment.</li> </ul>				
Fuel depot					
Horse keeping within the <b>Brickworks Policy Area 6</b>					
Horticulture					
Industry	<p>Except where any of the following is satisfied:</p> <ul style="list-style-type: none"> <li>(a) light industry located within the <b>Brickworks Policy Area 6</b></li> <li>(b) service industry within the <b>Brickworks Policy Area 6</b>.</li> </ul>				
Intensive animal keeping within the <b>Brickworks Policy Area 6</b>					
Major public service depot					
Nursing home within the <b>Kurralta Park Policy Area 7</b>					
Plant nursery within the <b>Kurralta Park Policy Area 7</b>					

Form of development	Exceptions
Residential flat building	Except: (a) where located within <b>Kurralta Park Policy Area 7</b> , two and three storey residential flat buildings where located above non-residential development (b) where located within the <b>Brickworks Policy Area 6</b> and where built in conjunction with a non-residential development and located on the same allotment.
Road transport terminal	
Service trade premises within the <b>Kurralta Park Policy Area 7</b>	
Showground	
Stock sales yard	
Stock slaughter works	
Warehouse within the <b>Kurralta Park Policy Area 7</b>	
Waste reception, storage, treatment or disposal	
Wrecking yard	

### Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development (except where the development is non-complying) are designated:

Category 1	Category 2
Any of the following within the <b>Brickworks Policy Area 6</b> : (a) dwelling that is built in conjunction with a non-residential development and located on the same allotment (b) shop (c) market or market related activity (d) office (e) child care centre.	

## Industry Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

### OBJECTIVES

- 1 A zone primarily accommodating a wide range of industrial, warehouse, storage and transport land uses.
- 2 Manufacturing activities, within North Plympton and Camden Park, well removed from adjacent residential areas.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development are envisaged in the zone:
  - industry except special industry
  - office
  - petrol filling station
  - public service depot
  - service trade premises
  - shop of 250 square metres or less in gross leasable area
  - store
  - road transport terminal
  - warehouse.
- 2 Development listed as non-complying is generally inappropriate.
- 3 An office or group of offices should only occur where (a), (b) or (c) is satisfied:
  - (a) it is ancillary to an industrial, storage or related activity
  - (b) it does not exceed 250 square metres in gross leasable area if not ancillary to an industrial, storage or related activity
  - (c) it is located within the **Mixed Use Policy Area 11**.

#### Form and Character

- 4 Development should be undertaken in accordance with the following Concept Plan Maps:
  - (a) [Concept Plan Map WeTo/15 - Former Apollo Stadium Industry](#)
  - (b) [Concept Plan Map WeTo/16 - North Plympton/Camden Park Industry](#)
  - (c) [Concept Plan Map WeTo/17 - Novar Gardens Industry](#).
- 5 Building set back should be at least 3 metres from the street boundary after road widening, except where a greater street setback distance is otherwise indicated on any of following concept plan maps:
  - (a) [Concept Plan Map WeTo/15 - Former Apollo Stadium Industry](#)
  - (b) [Concept Plan Map WeTo/16 - North Plympton/Camden Park Industry](#)

(c) [Concept Plan Map WeTo/17 - Novar Gardens Industry.](#)

- 6 Buildings should be set back not less than 3 metres from at least one side boundary, except non-residential development near a residential zone should be set back a minimum distance of 3 metres and an additional 0.6 metres for every metre of vertical height of the building or structure above 4 metres.
- 7 No building should exceed 12 metres in overall height.
- 8 Building facades facing land zoned for residential purposes should not contain openings or entrance ways that would result in the transmission of noise that would adversely affect the residential amenity.
- 9 Any plant or equipment with potential to cause an environmental nuisance (including a chimney stack or air-conditioning plant) should be sited as far as possible from adjoining non-industrially zoned allotments, and should be designed to minimise its effect on the amenity of the locality.
- 10 A minimum of 10 percent of the site should be landscaped.
- 11 A landscaped buffer strip of at least 3 metres in width should be provided along all road frontages and along at least one side boundary, except where the development is adjacent to the boundary of a residential zone, in which case, an intensively landscaped area of at least 3 metres should be provided between the non-residential development and the boundary of the residential zone, with such area containing trees which have the capacity to grow to a height which screens development as viewed from the residential zone.
- 12 Advertisements and advertising hoardings should not include any of the following:
  - (a) flashing or animated signs
  - (b) bunting, streamers, flags, or wind vanes
  - (c) roof-mounted advertisements projected above the roof line
  - (d) parapet mounted advertisements projecting above the top of the parapet.

**Land Division**

- 13 Land division should create allotments that are of a size and shape suitable for the intended use.



## Infrastructure Policy Area 8

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 Primarily, a policy area for the provision of infrastructure.
- 2 Infrastructure facilities and land required for infrastructure facilities preserved from the encroachment of incompatible land uses.
- 3 Development that contributes to the desired character of the policy area.

### DESIRED CHARACTER

This policy area accommodates the wastewater treatment works. This area will be heavily landscaped to screen infrastructure.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development are envisaged **specifically** in the policy area:
  - drainage system, including stormwater retention basin
  - electricity substation
  - public service depot
  - recreational facility
  - sewerage infrastructure.
- 2 A recreation facility should not be developed where they are likely to impair the operation of the Glenelg Sewage Treatment Works.

#### Form and Character

- 3 Development should not be undertaken unless it is consistent with the desired character for the policy area.

## Netley Policy Area 9

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 An area for retention, improvement and expansion of existing industrial, warehouse, storage and transport related activities with new development primarily for aviation and airport related industrial, warehousing and storage purposes.
- 2 Development that contributes to the desired character of the policy area.

### DESIRED CHARACTER

This policy area will primarily accommodate industry, warehousing or storage land uses which are airport and aviation related.

Existing industrial, warehouse, storage and transport related activities will be retained, improved and expanded.

All development will be compatible with existing or planned aviation-related activities or facilities within Adelaide International Airport (such as aircraft operations, aircraft maintenance, fuel storage and communications within the airport) and will ensure that the operational integrity of the airport is maintained and enhanced.

Development facing Richmond Road, Marion Road and Watson Avenue will be well landscaped.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development, where related to airport and aviation activities, are envisaged specifically in the policy area:
  - industry
  - store
  - warehouse.
- 2 Non aviation related land uses and expansion thereof, should only occur where development:
  - (a) is well landscaped on sites facing Richmond Road, Marion Road and Watson Avenue
  - (b) of a size and type that will not hinder the development or function of the airport.

#### Form and Character

- 3 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 4 Buildings should not exceed 10 metres in height.
- 5 Vehicle access should be provided from Richmond Road, Marion Road and Transport Avenue.
- 6 Development should be undertaken in accordance with [Concept Plan Map WeTo/18 - Netley Industry](#), in terms of vehicle access, building setbacks and landscape buffers.

## Mixed Use Policy Area 10

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 Development that contributes to the desired character of the policy area.

### DESIRED CHARACTER

This policy area will accommodate a mix of small-scale commercial, home industry and low impact industrial activities utilising existing small allotments.

Development will avoid or minimise adverse impacts on the amenity of adjoining remnant residential land use, or adjacent residential zones through appropriate design, building setbacks, bulk and scale.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development are envisaged specifically in the policy area:
  - home based industry
  - light industry
  - office
  - small scale commercial activity.

#### Form and Character

- 2 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 3 A home based industry, being an industry carried out in association with and on the same site as a dwelling, should only occur where:
  - (a) it is on a site containing an existing dwelling, in residential occupation and which will be occupied by the proprietor of the home industry on the site
  - (b) where the residential use component accords with the relevant provisions for residential development
  - (c) no more than two persons, other than persons living on the site, are employed within the industry.

## PROCEDURAL MATTERS

### Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

In addition, the following forms of development are designated as complying subject to the conditions contained in [Table WeTo/1 - Conditions for Complying Development](#):

- store
- warehouse.

### Non-complying Development

Development (including combinations thereof, or more than one of a particular kind, alterations, extensions and/or additions to existing buildings or structures, building work, a change in the use of land, or division of an allotment) for the following is non-complying:

Form of development	Exceptions				
Advertisement and/or Advertising hoarding where it is located within the <b>Infrastructure Policy Area 8</b>	Except where: <ol style="list-style-type: none"> <li>(a) it measures 6 metres or less in height</li> <li>(b) the advertisement area satisfies the following criteria:</li> </ol> <table border="1" style="margin-left: 40px;"> <thead> <tr> <th>Advertisement area (square metres)</th> <th>Additional area per metre of site frontage with a public road or public thoroughfare (square metres)</th> </tr> </thead> <tbody> <tr> <td>2</td> <td>0.1</td> </tr> </tbody> </table>	Advertisement area (square metres)	Additional area per metre of site frontage with a public road or public thoroughfare (square metres)	2	0.1
Advertisement area (square metres)	Additional area per metre of site frontage with a public road or public thoroughfare (square metres)				
2	0.1				
Amusement machine centre					
Caravan park where it is located within the <b>Infrastructure Policy Area 8</b>					
Community centre					
Consulting room					
Dairy where it is located within the <b>Infrastructure Policy Area 8</b>					
Demolition of a building identified as a State heritage place					
Dwelling					
Educational establishment	Except where: <ol style="list-style-type: none"> <li>(a) located outside the Infrastructure policy area</li> <li>(b) ancillary to and in association with industrial development</li> <li>(c) located on the same allotment.</li> </ol>				
Hall where it is located within the <b>Infrastructure Policy Area 8</b>					
Horticulture					
Hospital					
Hotel					

Form of development	Exceptions
Indoor recreation centre where it is located within the <b>Infrastructure Policy Area 8</b>	
Intensive animal keeping	
Landfill that constitutes solid waste disposal required to be licensed as a waste depot under the <i>Environment Protection Act 1993</i>	
Land division where it is located within the <b>Infrastructure Policy Area 8</b>	Except where no additional allotments are created partly or wholly within the zone.
Motel	
Motor race track where it is located within the <b>Infrastructure Policy Area 8</b>	
Motor repair station where it is located within the <b>Infrastructure Policy Area 8</b>	
Nursing home	
Office where it exceeds 250 square metres in gross leasable area	Except where it is ancillary to and in association with an industry, warehouse, store, transport distribution or service trade premises, located on the same site.
Petrol filling station where it is located within the <b>Infrastructure Policy Area 8</b>	
Pre-school	
Place of worship	
Residential flat building	
Service trade premises where it is located within the <b>Infrastructure Policy Area 8</b>	
Shop or group of shops	Except where: (a) located outside the <b>Infrastructure Policy Area 8</b> (b) the gross leasable area is 250 square metres or less.
Showground where it is located within the <b>Infrastructure Policy Area 8</b>	
Special industry where it is located within the <b>Infrastructure Policy Area 8</b>	
Stadium where it is located within the <b>Infrastructure Policy Area 8</b>	
Stock sales yard where it is located within the <b>Infrastructure Policy Area 8</b>	
Stock slaughter works where it is located within the <b>Infrastructure Policy Area 8</b>	
Supported accommodation within the <b>Infrastructure Policy Area 8</b>	

Form of development	Exceptions
Tourist accommodation	
Warehouse where it is located within the <b>Infrastructure Policy Area 8</b>	
Wrecking yard where it is located within the <b>Infrastructure Policy Area 8</b>	

### **Public Notification**

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

## Local Centre Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

### OBJECTIVES

- 1 A centre accommodating small-scale convenience shopping, office, medical and community facilities to serve the local community.
- 2 A centre characterised by traditional corner stores or small groups of shops located within easy walking distance of the population they serve.
- 3 A centre accommodating residential development **above** non-residential development.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development are envisaged in the zone:
  - advertisement
  - community facility
  - consulting room
  - office
  - office and dwelling
  - shop **with a maximum gross leasable floor area in the order of 450 square metres**
  - shop and dwelling.
- 2 Development listed as non-complying is generally inappropriate.
- 3 Development comprising a variety of residential and non-residential uses should only be undertaken if such development does not prejudice the operation of existing or future non-residential activity within the zone.
- 4 **Each centre should contain a convenience shop such as a super delicatessen or mini-mart, selling a range of convenience goods and operating extended hours.**

#### Form and Character

- 5 **Dwellings should be located above non-residential uses on the same allotment.**
- 6 **Retail development (including buildings, access, public spaces, car parking, service areas and landscaping) should not occupy more than 90 per cent of the zone.**
- 7 **Development should not exceed two storeys (8.5 metres) in height.**

#### Vehicle Parking

- 8 Vehicle parking should be provided in accordance with the rates set out in [Table WeTo/2 - Off Street Vehicle Parking Requirements](#) or [Table WeTo/6 - Off Street Vehicle Parking Requirements for Designated Areas](#) (whichever applies).

#### Land Division

- 9 Land division in the zone is appropriate provided new allotments are of a size and configuration to ensure the objectives of the zone can be achieved.

## PROCEDURAL MATTERS

### Complying Development

Complying developments are prescribed in schedule 4 of the *Development Regulations 2008*.

In addition, the following forms of development (except where the development is non-complying) are complying:

A change of use to a shop, office, consulting room or any combination of these uses where all of the following are achieved:

- (a) the area to be occupied by the proposed development is located in an existing building and is currently used as a shop, office, consulting room or any combination of these uses
- (b) the building is not a State heritage place
- (c) it will not involve any alterations or additions to the external appearance of a local heritage place as viewed from a public road or public space
- (d) if the proposed change of use is for a shop that primarily involves the handling and sale of foodstuffs, it achieves either (i) or (ii):
  - (i) all of the following:
    - (A) areas used for the storage and collection of refuse are sited at least 10 metres from any Residential Zone boundary or a dwelling (other than a dwelling directly associated with the proposed shop)
    - (B) if the shop involves the heating and cooking of foodstuffs in a commercial kitchen and is within 30 metres of any Residential Zone boundary or a dwelling (other than a dwelling directly associated with the proposed shop), an exhaust duct and stack (chimney) exists or is capable of being installed for discharging exhaust emissions
  - (ii) the development is the same or substantially the same as a development, which has previously been granted development approval under the *Development Act 1993* or any subsequent Act and Regulations, and the development is to be undertaken and operated in accordance with the conditions attached to the previously approved development
- (e) if the change in use is for a shop with a gross leasable floor area greater than 250 square metres and has direct frontage to an arterial road, it achieves either (i) or (ii):
  - (i) the primary vehicle access (being the access where the majority of vehicles access / egress the site of the proposed development) is from a road that is not an arterial road
  - (ii) the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared
- (f) off-street vehicular parking is provided in accordance with the rate(s) specified in [Table WeTo/2 - Off Street Vehicle Parking Requirements](#) or the desired minimum in rate in [Table WeTo/6 - Off Street Vehicle Parking Requirements for Designated Areas](#) (whichever table applies) to the nearest whole number, except in any one or more of the following circumstances:
  - (i) the building is a local heritage place



- (ii) the development is the same or substantially the same as a development, which has previously been granted development approval under the *Development Act 1993* or any subsequent Act and Regulations, and the number and location of parking spaces is the same or substantially the same as that which was previously approved
- (iii) the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared.

## Non-complying Development

Development (including combinations thereof, or more than one of a particular kind, alterations, extensions and/or additions to existing buildings or structures, building work, a change in the use of land, or division of an allotment) for the following is non-complying:

Form of development	Exceptions				
Advertisement and/or advertising hoarding	Except where: <ul style="list-style-type: none"> <li>(a) it measures 7 metres or less in height</li> <li>(b) the advertisement area satisfies the following criteria:</li> </ul> <table border="1" style="margin-left: 40px;"> <thead> <tr> <th>Advertisement area (square metres)</th> <th>Additional area per metre of site frontage with a public road or public thoroughfare (square metres)</th> </tr> </thead> <tbody> <tr> <td>4</td> <td>0.1</td> </tr> </tbody> </table>	Advertisement area (square metres)	Additional area per metre of site frontage with a public road or public thoroughfare (square metres)	4	0.1
Advertisement area (square metres)	Additional area per metre of site frontage with a public road or public thoroughfare (square metres)				
4	0.1				
Amusement machine centre					
Bus depot					
Bus station					
Caravan park					
Cemetery					
Crematorium					
Demolition of all or part of a building comprising a State heritage place					
Dwelling	Except where located above a non-residential development.				
Entertainment venue					
Fuel depot					
Hall					
Horticulture					
Indoor recreation centre					
Industry					
Motor repair station					
Petrol filling station					

Form of development	Exceptions
Plant nursery	
Public service depot	
Residential flat building	Except where in conjunction with a non-residential development.
Road transport terminal	
Service trade premises	
Showground	
Stadium	
Store	
Telecommunications facility above 30 metres in height	
Warehouse	
Waste reception, storage, treatment or disposal	
Wrecking yard	

### Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development (except where the development is non-complying) are designated:

Category 1	Category 2
	Demolition of a building or structure listed in any of the following tables: (a) <a href="#">Table WeTo/3 - Contributory Items</a> (b) <a href="#">Table WeTo/4 - Local Heritage Places</a> .

## Neighbourhood Centre Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

### OBJECTIVES

- 1 A centre providing a range of shopping, community, business, and recreational facilities for the surrounding neighbourhood.
- 2 A centre that provides the main focus of business and community life outside a district centre, and provides for the more frequent and regularly recurring needs of a community.
- 3 A centre accommodating residential development in conjunction with non-residential development.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development are envisaged in the zone:
  - bank
  - child care facility
  - consulting room
  - dwelling in conjunction with non-residential land use
  - library
  - health centre
  - office
  - petrol filling station
  - place of worship
  - playing field
  - pre-school
  - primary school
  - recreation area
  - restaurant
  - shop
  - supermarket.
- 2 Development listed as non-complying is generally inappropriate.
- 3 Development comprising a variety of residential and non-residential uses should only be undertaken if such development does not prejudice the operation of existing or future non-residential activity within the zone.

#### Form and Character

- 4 Dwellings should be located only behind or above non-residential uses on the same allotment.

#### Vehicle Parking

- 5 Vehicle parking should be provided in accordance with the rates set out in [Table WeTo/2 - Off Street Vehicle Parking Requirements](#) or [Table WeTo/6 - Off Street Vehicle Parking Requirements for Designated Areas](#) (whichever applies).

#### Land Division

- 6 Land division in the zone is appropriate provided new allotments are of a size and configuration to ensure the objectives of the zone can be achieved.

## Hilton Policy Area 11

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 Development that contributes to the desired character of the policy area.

### DESIRED CHARACTER

The centre at Hilton is intended to function as a neighbourhood centre facility with an emphasis on civic, office and community facilities and shops to serve the daily and weekly needs of the surrounding population.

The area to the north of Sir Donald Bradman Drive west of Attrill Avenue, identified as '**Retail Core**' on [Concept Plan Map WeTo/19 - Hilton Neighbourhood Centre](#), accommodates a modern integrated shopping and service facility and associated car parking located behind the building.

The area on the southern side of Sir Donald Bradman Drive, west of Somerset Avenue, identified as '**Civic/Community**' on [Concept Plan Map WeTo/19 - Hilton Neighbourhood Centre](#), accommodates a range of civic, community and office facilities and associated car parking. Buildings are positioned on the Sir Donald Bradman Drive and Brooker Terrace boundaries.

The remaining area on both sides of Sir Donald Bradman Drive, identified as '**Office/Commercial**' on [Concept Plan Map WeTo/19 - Hilton Neighbourhood Centre](#) will accommodate small scale offices, bulky goods outlets, a range of recreation and community uses, and multi-storey residential development. Development in this area will occur on amalgamated sites with integrated features such as shared access points, driveways, car parking areas, pedestrian connections and verandas and pergolas to the Sir Donald Bradman Drive frontage.

Residential development in the form of two and three storey residential flat buildings, row dwellings or dwellings above consulting rooms, offices, shops, bulky goods outlets and community uses, are envisaged within the zone.

Buildings located on the south side of Sir Donald Bradman Drive, Bagot Avenue and Brooker Terrace frontages will be of a high architectural standard, accentuate the corner and be designed in a manner which reflects and emphasizes the prominent and important role of Sir Donald Bradman Drive as a gateway and major interconnection between the City and the Adelaide Airport.

Buildings will focus on a direct and convenient pedestrian network within the policy area which links the civic/community, office/commercial and retail core areas.

Car parking between shops and Sir Donald Bradman Drive, within the area identified as '**Retail Core**' on [Concept Plan Map WeTo/19 - Hilton Neighbourhood Centre](#), will be minimised, with landscaping and free standing buildings located to screen and soften the appearance of car parking from Sir Donald Bradman Drive. Landscaping, including paving, tree planting and a pedestrian linkage complementing the tree planting and landscaping on adjoining sections and along the median dividers of Sir Donald Bradman Drive is envisaged.

Buffer features including building setbacks, landscape areas, 2 metre high screen fences and screened or obscured building openings will be provided to residential zone boundaries and to the Attrill Avenue frontage.

A characteristic style of formal street tree planting with large canopy trees on both sides of Sir Donald Bradman Drive is envisaged.

## PRINCIPLES OF DEVELOPMENT CONTROL

### Land Use

1 The following forms of development are envisaged specifically in the policy area:

- bank
- child care facility
- civic centre
- consulting room
- dwelling above non-residential land use
- library
- health centre
- office
- petrol filling station
- place of worship
- playing field
- pre-school
- primary school
- recreation area
- residential flat building above non-residential land use
- restaurant
- row dwelling
- shop
- supermarket.

### Form and Character

2 Development should not be undertaken unless it is consistent with the desired character for the policy area.

3 A dwelling or residential flat building should not exceed three storeys (12.5 metres) in height.

4 Development should occur in accordance with [Concept Plan Map WeTo/19 - Hilton Neighbourhood Centre](#) and in particular:

(a) the area marked '**Retail Core**' should:

- (i) contain the major retail tenancy of a supermarket together with speciality shops and personal service establishments
- (ii) not contain large scale bulky goods outlets
- (iii) not exceed three storeys (12.5 metres) in height where a building faces onto Sir Donald Bradman Drive.

(b) the area marked '**Civic/Community**' should:

- (i) contain the civic centre, offices, library, meeting hall and other community and administrative facilities
- (ii) except for residential development, measure between two storeys (8.5 metres) and five storeys (20.5 metres) in height where a building faces onto Sir Donald Bradman Drive
- (iii) not accommodate car parking spaces between a building and Sir Donald Bradman Drive.

(c) the area marked '**Office/Commercial**' should:

- (i) contain office and commercial buildings which complement the shop, service facilities and community components of the '**Retail Core**' and '**Civic/Community Areas**'. Medium density

**West Torrens Council**  
**Zone Section**  
**Neighbourhood Centre Zone**  
**Hilton Policy Area 11**

residential development, bulky goods outlets, restaurants and limited other shops which retail food and convenience goods and personal service establishments should occur within this area

- (ii) except for residential development, not exceed three storeys (12.5 metres) in height where buildings face onto Sir Donald Bradman Drive.
  - (d) not accommodate off-street vehicle parking areas between a building and Sir Donald Bradman Drive.
- 5 Non-residential development, which does not face Sir Donald Bradman Drive, should not exceed two storeys (8.5 metres) in height.
  - 6 Buildings should be located to the property boundary of Brooker Terrace and Bagot Avenue.
  - 7 Buildings should be set back at least 5 metres from Somerset Avenue and Davenport Terrace.

## Marleston Policy Area 12

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 Development that contributes to the desired character of the policy area.

### DESIRED CHARACTER

This policy area will function as a small neighbourhood centre providing a range of services and facilities to cater for the daily and weekly needs of the surrounding population. The centre will have an emphasis on small-scale offices, bulky goods outlets and recreation and community activities which will predominate in the 'Recreation/Community' area as identified on [Concept Plan Map WeTo/20 - Marleston Neighbourhood Centre](#). Small-scale retail facilities selling primarily food and other convenience goods to service the daily and weekly needs of the surrounding population should be concentrated in the 'Retail Core' area as identified on [Concept Plan Map WeTo/20 - Marleston Neighbourhood Centre](#).

The total maximum gross leasable retail floor space in the policy area should be in the order of 1500 square metres, the majority of which should occur within the 'Retail Core' area.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development are envisaged specifically in the policy area:
  - bank
  - child care facility
  - commercial premises
  - community facility
  - consulting room
  - dwelling
  - entertainment facility
  - health centre
  - office
  - petrol filling station
  - place of worship
  - recreation area
  - residential flat building
  - restaurant
  - shop
  - supermarket.
- 2 The maximum gross leasable retail floor space within the Policy Area should be in the order of 1500 square metres.
- 3 Residential development should be in the form of:
  - (a) residential flat buildings and row dwellings which measure between two and three storeys in height
  - (b) dwellings above a non-residential land use.

## Form and Character

- 4 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 5 Development should occur in accordance with [Concept Plan Map WeTo/20 - Marleston Neighbourhood Centre](#), and in particular:
- (a) the area marked '**Retail Core**' should contain shops, selling primarily food and other convenience goods
  - (b) the area marked '**Recreation/Community**' should primarily accommodate recreation and community activities, particularly hotels, motels, theatres, indoor recreation centre, community centres and places of worship
  - (c) establish a landscape area with a minimum width of 3 metres along Richmond Road and Grove Avenue.
- 6 Development should be designed in accordance with the following parameters:

Location	Maximum number of storeys and maximum vertical wall height
Non-residential development where the site adjoins a residential zone	two storeys and 8.5 metres
Development elsewhere in the policy area	three storeys and 12.5 metres

- 7 Off-street vehicle parking areas should not be located between buildings and Richmond Road.



## Novar Gardens Policy Area 13

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 Development that contributes to the desired character of the policy area.

### DESIRED CHARACTER

This policy area will function as a small neighbourhood centre providing a range of services and facilities to cater for the daily and weekly needs of the surrounding population. The retail floor space of the centre will consist of primarily convenience goods outlets including food premises with a limited range of the more frequently required comparison goods.

An area to the south of the existing shopping centre has been designated for future expansion. Any future expansion of the centre will achieve full integration with existing facilities through such measures as shared car parking spaces and new buildings blending in with the existing.

Buildings within the centre will exhibit similar architectural features in terms of roof pitches, windows, external materials, colours and style of advertisements.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development are envisaged specifically in the policy area:
  - commercial
  - community facility
  - entertainment facility
  - office
  - shop.
- 2 The maximum gross leasable retail floor space of the policy area should be in the order of 2500 square metres.

#### Form and Character

- 3 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 4 Development should occur in accordance with [Concept Plan Map WeTo/21 - Novar Gardens Neighbourhood Centre](#) and in particular:
  - (a) development should be set back no less than 5 metres from Morphett Road
  - (b) development should not exceed two storeys (8.5 metres) in height
  - (c) landscaping should measure no less than 3 metres in width.

## Richmond Policy Area 14

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 Development that contributes to the desired character of the policy area.

### DESIRED CHARACTER

This policy area will function as a neighbourhood centre providing a range of services and facilities to cater for the daily and weekly needs of the surrounding population. Currently, a diverse range of facilities is provided, with the exception of a supermarket. The policy area should have a maximum total gross leasable retail floor space in the order of 2500 square metres.

Retailing will be the predominant activity in the area marked 'Retail Core' on the [Concept Plan Map WeTo/22 - Richmond Neighbourhood Centre](#).

The eastern side of Marion Road will accommodate a range of offices, service, commercial, community and entertainment facilities, bulky goods outlets and a limited range of small-scale low traffic generating retail outlets.

New development will have limited setbacks from the Marion Road property boundary and provide a continuous built-edge to the street with unifying features such as verandas and similar façade treatment. Buildings will incorporate verandas along the street frontage for pedestrian comfort. Car parking areas will be provided to the rear of buildings with access from the side streets. Densely planted landscape buffers will be provided along the interface of the centre and adjoining zones.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development are envisaged specifically in the policy area:
  - bank
  - child care facility
  - commercial premises
  - consulting room
  - entertainment facility
  - library
  - health centre
  - office
  - shop
  - supermarket.
- 2 The policy area should have a maximum gross leasable retail floor space in the order of 2500 square metres.
- 3 The retail focus of the policy area, in terms of the major proportion of gross leasable floor space dedicated for retail purposes, should be located between Trennery Street and Passmore Street.

#### Form and Character

- 4 Development should not be undertaken unless it is consistent with the desired character for the policy area.

- 5 Development should occur in accordance with [Concept Plan Map WeTo/22 - Richmond Neighbourhood Centre](#) and in particular:
- (a) the area marked '**Retail Core**' should predominantly accommodate retail land uses
  - (b) the area marked '**Retail/Office/Commercial**' should primarily accommodate a range of office, service, commercial, community and entertainment facilities, bulky goods outlets and small-scale, low traffic-generating retail outlets
  - (c) be setback on the front property boundary, except where development is located within 10 metres of a residential zone, in which case, the development should be set back at least 5 metres from the front property boundary
  - (d) should not exceed two storeys (8.5 metres) in height
  - (e) provide vehicle access primarily from side streets
  - (f) provide landscaping areas measuring no less than 3 metres in width.

## Tennyson Street Policy Area 15

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 Development that contributes to the desired character of the policy area.

### DESIRED CHARACTER

This policy area will function as a neighbourhood centre providing a range of services and facilities to cater for the daily and weekly needs of the surrounding population. The centre will have an emphasis on retailing with a range of other services and facilities appropriate to a neighbourhood centre. It is intended that total maximum gross leasable retail floor space (excluding bulky goods outlet floor space) be in the order of 2500 square metres in the centre. Such a limit reflects its neighbourhood centre function, its proximity to the **Kurralita Park Policy Area 7** of the **District Centre Zone** and its proximity to a local centre.

Development will face onto and have a consistent setback from South Road with car parking areas provided to the rear of buildings. Access will be in accordance with the [Concept Plan Map WeTo/23 - Tennyson Street Neighbourhood Centre](#). Off street parking areas within the centre zone will be linked to avoid the use of public roads for circulation.

Landscaping will be provided along the South Road property boundary and along the interface of the zone with adjoining residential areas. Re-development of this centre will ensure that the interface between the centre and residential areas is sensitively treated and potential adverse impacts associated with the centre are minimised.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development are envisaged specifically in the policy area:
  - bank
  - bulky goods outlet
  - child care facility
  - commercial
  - consulting room
  - entertainment facility
  - library
  - health centre
  - office
  - shop
  - supermarket.
- 2 The maximum total gross leasable retail floor space of the policy area, excluding bulky goods outlets, should be in the order of 2500 square metres.

#### Form and Character

- 3 Development should not be undertaken unless it is consistent with the desired character for the policy area.

- 4 Development should occur in accordance with [Concept Plan Map WeTo/23 - Tennyson Street Neighbourhood Centre](#) and in particular:
- (a) the area marked '**Retail Core**' should primarily contain shops, selling food and other convenience goods, banks and personal service establishments.
  - (b) the area marked '**Retail/Office/Commercial**' should primarily contain offices, consulting rooms, warehousing and bulky goods outlets.
  - (c) be setback a minimum of 5 metres from the road property boundary.
  - (d) not exceed two storeys (12 metres) in height.
  - (e) provide a landscaping buffer with a minimum width of 1 metre.

## Thebarton Policy Area 16

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 Development that contributes to the desired character of the policy area.

### DESIRED CHARACTER

Development within the policy area should comprise primarily small-scale specialty shops, offices and community facilities sited so as to complement the existing shops.

Larger shops in the form of a supermarket are also envisaged to provide a focus for services in the locality.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development are envisaged specifically in the policy area:
  - community facility
  - small scale office
  - small scale specialty shop.

#### Form and Character

- 2 Development should not be undertaken unless it is consistent with the desired character for the policy area.

## PROCEDURAL MATTERS

### Complying Development

Complying developments are prescribed in schedule 4 of the *Development Regulations 2008*.

In addition, the following forms of development (except where the development is non-complying) are complying:

A change of use to a shop, office, consulting room or any combination of these uses where all of the following are achieved:

- (a) the area to be occupied by the proposed development is located in an existing building and is currently used as a shop, office, consulting room or any combination of these uses
- (b) the building is not a State heritage place
- (c) it will not involve any alterations or additions to the external appearance of a local heritage place as viewed from a public road or public space
- (d) if the proposed change of use is for a shop that primarily involves the handling and sale of foodstuffs, it achieves either (i) or (ii):
  - (i) all of the following:
    - (A) areas used for the storage and collection of refuse are sited at least 10 metres from any Residential Zone boundary or a dwelling (other than a dwelling directly associated with the proposed shop)
    - (B) if the shop involves the heating and cooking of foodstuffs in a commercial kitchen and is within 30 metres of any Residential Zone boundary or a dwelling (other than a dwelling directly associated with the proposed shop), an exhaust duct and stack (chimney) exists or is capable of being installed for discharging exhaust emissions
  - (ii) the development is the same or substantially the same as a development, which has previously been granted development approval under the *Development Act 1993* or any subsequent Act and Regulations, and the development is to be undertaken and operated in accordance with the conditions attached to the previously approved development
- (e) if the change in use is for a shop with a gross leasable floor area greater than 250 square metres and has direct frontage to an arterial road, it achieves either (i) or (ii):
  - (i) the primary vehicle access (being the access where the majority of vehicles access / egress the site of the proposed development) is from a road that is not an arterial road
  - (ii) the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared
- (f) off-street vehicular parking is provided in accordance with the rate(s) specified in [Table WeTo/2 - Off Street Vehicle Parking Requirements](#) or the desired minimum in rate in [Table WeTo/6 - Off Street Vehicle Parking Requirements for Designated Areas](#) (whichever table applies) to the nearest whole number, except in any one or more of the following circumstances:
  - (i) the building is a local heritage place

- (ii) the development is the same or substantially the same as a development, which has previously been granted development approval under the *Development Act 1993* or any subsequent Act and Regulations, and the number and location of parking spaces is the same or substantially the same as that which was previously approved
- (iii) the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared.

### Non-complying Development

Development (including combinations thereof, or more than one of a particular kind, alterations and/or additions to existing buildings or structures building work, a change in the use of land, or division of an allotment) for the following is non-complying:

Form of development	Exceptions				
Advertisement and/or advertising hoarding	Except where: <ul style="list-style-type: none"> <li>(a) it measures 9 metres or less in height</li> <li>(b) the advertisement area satisfies the following criteria:</li> </ul> <table border="1" style="margin-left: 40px;"> <thead> <tr> <th>Advertisement area (square metres)</th> <th>Additional area per metre of site frontage with a public road or public thoroughfare (square metres)</th> </tr> </thead> <tbody> <tr> <td>6</td> <td>0.15</td> </tr> </tbody> </table>	Advertisement area (square metres)	Additional area per metre of site frontage with a public road or public thoroughfare (square metres)	6	0.15
Advertisement area (square metres)	Additional area per metre of site frontage with a public road or public thoroughfare (square metres)				
6	0.15				
Auction room where it is located within the <b>Thebarton Policy Area 16</b>					
Bus depot where it is located within the <b>Thebarton Policy Area 16</b>					
Bus station where it is located within the <b>Thebarton Policy Area 16</b>					
Caravan park where it is located within the <b>Thebarton Policy Area 16</b>					
Cemetery where it is located within the <b>Thebarton Policy Area 16</b>					
Crematorium where it is located within the <b>Thebarton Policy Area 16</b>					
Demolition of all or part of a building comprising a State heritage place					
Dwelling	Except where it is above a non-residential development.				
Fuel depot where it is located within any of the following policy areas: <ul style="list-style-type: none"> <li>(a) <b>Marleston Policy Area 12.</b></li> <li>(b) <b>Novar Gardens Policy Area 13</b></li> <li>(c) <b>Richmond Policy Area 14</b></li> <li>(d) <b>Tennyson Street Policy Area 15.</b></li> </ul>					
Horticulture					
Industry					



Form of development	Exceptions
Major public service depot	
Motor repair station	
Nursing home where it is located within any of the following policy areas: (a) <b>Hilton Policy Area 11</b> (b) <b>Marleston Policy Area 12</b> (c) <b>Novar Gardens Policy Area 13</b> (d) <b>Richmond Policy Area 14</b> (e) <b>Tennyson Street Policy Area 15.</b>	
Plant nursery where it is located within any of the following policy areas: (a) <b>Novar Gardens Policy Area 13</b> (b) <b>Richmond Policy Area 14</b> (c) <b>Thebarton Policy Area 16.</b>	
Racecourse where it is located within the <b>Thebarton Policy Area 16</b>	
Residential flat building	Except where it is located above non-residential development.
Row dwelling	Except where it is located within the <b>Hilton Policy Area 11.</b>
Service trade premises	
Road transport terminal	
Showground where it is located within the <b>Thebarton Policy Area 16</b>	
Stadium where it is located within the <b>Thebarton Policy Area 16</b>	
Store	
Warehouse	
Waste reception, storage, treatment or disposal	
Wrecking yard	

### Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

## Open Space Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

### OBJECTIVES

- 1 A zone:
  - (a) in which the open space character is preserved to provide a visual contrast to the surrounding urban area
  - (b) comprising open space that accommodates a range of public and private activities in an open and natural setting, including:
    - (i) passive and active recreation land uses
    - (ii) habitat conservation and restoration.
- 2 Private land within the Metropolitan Open Space System (MOSS) contributing to regional open space networks and providing an open, natural and rural character accommodating low-scale uses such as non-intensive agriculture
- 3 Public land within the MOSS that provides for recreation areas and facilities, sporting facilities and conservation of the open, natural character.
- 4 A linear park that:
  - (a) provides an open space corridor across Greater Adelaide/Regional South Australia
  - (b) promotes the use, awareness and preservation of the riverine environment and enhancement of natural or semi natural habitats for the movement of wildlife and conservation of biodiversity.
- 5 A river system which provides for the maintenance of stormwater capacity and flood mitigation measures for adjoining areas.
- 6 Development that contributes to the desired character of the zone.

### DESIRED CHARACTER

The area along the River Torrens / Karrawirra Parri is primarily an open space with aesthetic, stormwater management and recreation functions. Development within the zone is appropriate where it is integral to any of these functions provided any structures are not a dominant feature.

It will provide for the establishment and maintenance of a linear open space and biological corridor based on the existing watercourse channel, associated floodplains and vegetation. Water quality of the river and the recreational opportunities will be retained and enhanced.

Pedestrian, cycle and recreation facilities, such as drinking fountains and barbecues, will be incorporated into the Linear Park; however, facilities for sports competitions or events are inappropriate.

Landscaping should ensure that any remnant native vegetation is incorporated into the landscape design and that locally indigenous vegetation is utilised wherever possible.

Development will not be undertaken if the establishment, operation or management of such development is likely to result in pollution of the River Torrens, unnecessary loss of damage to native vegetation, erosion, creation of dust, nuisance from noise, the introduction of or an increase in the number of pest plants or vermin, reduction in capacity of the river channel, landfill or landslide.

## **PRINCIPLES OF DEVELOPMENT CONTROL**

### **Land Use**

- 1 The following forms of development are envisaged in the zone:
  - conservation work
  - farming
  - lighting
  - outbuilding associated with open space maintenance
  - playground
  - recreation area
  - sporting club facility
  - structure associated with a public facility such as car parking, picnic/barbeque area, shelter and toilet
  - toilet block and barbeque facility.
- 2 Development listed as non-complying is generally inappropriate.
- 3 Publicly owned land within the MOSS should be used for any of the following:
  - (a) to provide natural or landscaped open space using locally indigenous plant species
  - (b) to accommodate a range of public recreation, sporting and institutional facilities and uses
  - (c) to accommodate stormwater retention and management
  - (d) to conserve and restore areas of remnant native vegetation and wildlife habitats and corridors
  - (e) to conserve sites of scientific, cultural or heritage interest
  - (f) for revegetation purposes using locally indigenous plant species
  - (g) to provide a buffer to adjoining areas of conservation significance.
- 4 Privately owned land within the MOSS should be used for any of the following:
  - (a) rural activities and agriculture (but not intensive animal keeping)
  - (b) low-impact sporting facilities
  - (c) conservation purposes.
- 5 Development should allow for unstructured passive and active recreation

### **Form and Character**

- 6 Development should not be undertaken unless it is consistent with the desired character for the zone.
- 7 Development should only occur where it is integral to the aesthetic, drainage or recreation function of the zone.
- 8 Buildings should be:
  - (a) restricted in size and number
  - (b) sited so as not to detract from the open natural character of the zone
  - (c) constructed of materials which blend with the riverine landscape.

- 9 No infrastructure associated with adjoining development should intrude into the zone.
- 10 Development should ensure co-ordinated design with an emphasis on the creation of pedestrian areas.
- 11 Landscaped buffers should be provided around the perimeter of recreation or sporting facilities.
- 12 Landscaping should comprise locally indigenous species and incorporate existing remnant vegetation.
- 13 Vehicular access or crossings should not occur through the Linear Park.
- 14 Development should ensure that public access to the Linear Park is retained and enhanced for cyclists and pedestrians.
- 15 Development, landscaping, and paths for pedestrians and cyclists should:
- (a) take into account the changing flow regime and width of waterways
  - (b) be constructed of permeable material where practical to reduce stormwater runoff.

### Land Division

- 16 Land division should not be undertaken except where:
- (a) it will facilitate the development of envisaged uses in the zone
  - (b) no additional allotments are created.

## PROCEDURAL MATTERS

### Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

- recreation area.

### Non-complying Development

Development (including combinations thereof, or more than one of a particular kind, alterations and/or additions to existing buildings or structures building work, a change in the use of land, or division of an allotment) for the following is non-complying:

Form of development	Exceptions
Advertisement and/or advertising hoarding	
Amusement machine centre	
Consulting room	
Crematorium	
Dairy	
Dwelling	
Educational establishment	
Fuel depot	
Horticulture	

Form of development	Exceptions
Hospital	
Hotel	
Industry	
Intensive animal keeping	
Land division	Except where no additional allotments are created partly or wholly within the zone.
Motel	
Motor repair station	
Nursing home	
Office	Except where in association with recreation facilities.
Petrol filling station	
Place of worship	
Pre-school	
Prescribed mining operations	
Public service depot	
Residential flat building	
Restaurant	
Road transport terminal	
Service trade premises	
Shop or group of shops	
Stock sales yard	
Stock slaughter works	
Store	
Tourist accommodation	
Warehouse	
Waste reception, storage, treatment or disposal	
Wrecking yard	

### Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

## Residential Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

### OBJECTIVES

- 1 A residential zone comprising a range of dwelling types, including a minimum of 15 per cent affordable housing.
- 2 Dwellings of various types at very low, low and medium densities.
- 3 Increased dwelling densities in close proximity to centres, public transport routes and public open spaces.
- 4 Development that contributes to the desired character of the zone.

### DESIRED CHARACTER

This zone will contain predominantly residential development. There may also be some small-scale non-residential activities such as offices, shops, consulting rooms and educational establishments in certain locations. Non-residential activities will be complementary to surrounding dwellings.

Allotments will be at very low, low and medium densities to provide a diversity of housing options in different parts of the zone. The range of allotment sizes will support the desired dwelling types anticipated in each policy area, and the minimum allotment sizes shall be treated as such in order to achieve the Desired Character for each policy area and, in turn, reinforce distinction between policy areas. Row dwellings and residential flat buildings will be common near centres and in policy areas where the desired density is higher, in contrast to the predominance of detached dwellings in policy areas where the distinct established character is identified for protection and enhancement. There will also be potential for semi-detached dwellings and group dwellings in other policy areas.

Residential development in the form of a multiple dwelling, residential flat building or group dwelling will not be undertaken in a **Historic Conservation Area**.

Landscaping will be provided throughout the zone to enhance the appearance of buildings from the street as viewed by pedestrians, provide an appropriate transition between the public and private realm and reduce heat loads in summer.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development are envisaged in the zone:
  - affordable housing
  - domestic outbuilding in association with a dwelling
  - dwelling
  - dwelling addition
  - small scale non-residential use that serves the local community, for example:
    - child care facility
    - health and welfare service
    - open space
    - primary and secondary school
    - recreation area

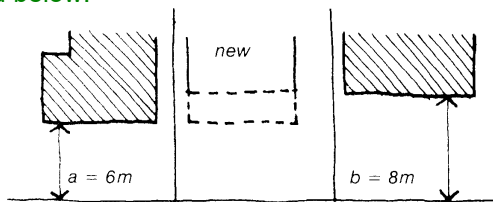
- shop measuring 250 square metres or less in gross leasable floor area supported accommodation.
- 2 Development listed as non-complying is generally inappropriate.
  - 3 Non-residential development such as shops, schools and consulting rooms should be of a nature and scale that:
    - (a) serves the local community
    - (b) is consistent with the character of the locality
    - (c) does not detrimentally impact on the amenity of nearby residents.
  - 4 The use and placement of outbuildings should be ancillary to and in association with a dwelling or dwellings.

### Form and Character

- 5 Development should not be undertaken unless it is consistent with the desired character for the zone and policy area.
- 6 Except where otherwise specified by a policy area, dwellings and buildings containing dwellings should be designed within the following parameters:

Parameter	Value
Maximum number of storeys	two storeys (above natural ground level)
Maximum vertical side wall height	6 metres (measured from the natural ground level)

- 7 Dwellings should be set back from allotment or site boundaries to:
  - (a) contribute to the desired character of the relevant policy area
  - (b) provide adequate visual privacy by separating habitable rooms from pedestrian and vehicle movement.
- 8 Except where specified in **Medium Density Policy Area 18** and **Medium Density Policy Area 19**, development (including any veranda, porch, etc) should be set back from the primary road frontage in accordance with the following table:

Setback difference between buildings on adjacent allotments	Setback of new building
Up to 2 metres	The same setback as one of the adjacent buildings, as illustrated below:  <p style="text-align: center;"><i>When <math>b - a \leq 2</math>, setback of new dwelling = a or b</i></p>
Greater than 2 metres	At least the average setback of the adjacent buildings.

9 Except where otherwise specified by a policy area, dwellings and buildings containing dwellings should be set back from secondary road frontages in accordance with the following table:

Vertical height of wall	Minimum setback including balconies (metres)
Less than 3 metres	2
3 metres or greater	3

10 Dwelling setbacks from side and rear boundaries should be progressively increased as the height of the building, (with the total wall height of the building being measured from the existing ground level at the boundary of the adjacent property as shown by [Figure 1](#)), increases to:

- (a) minimise the visual impact of buildings from adjoining properties
- (b) minimise the overshadowing of adjoining properties.

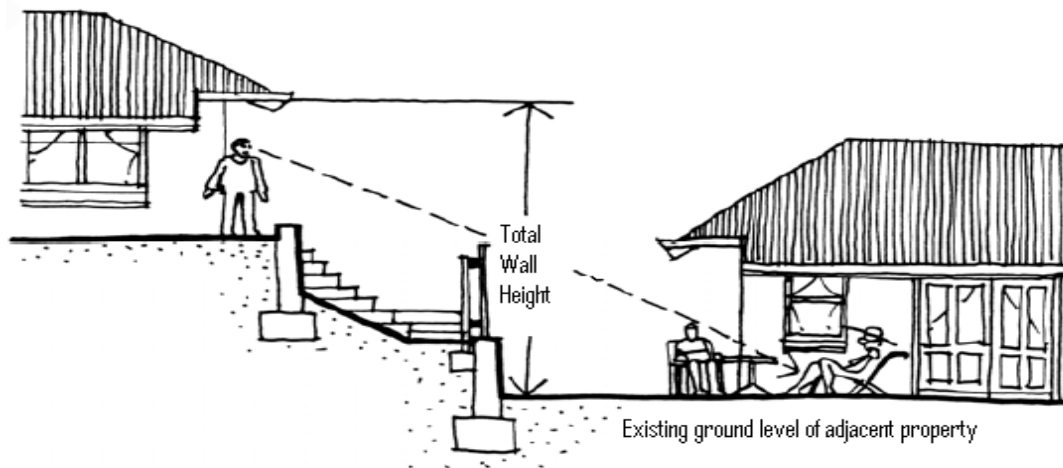


Figure 1

11 Except where otherwise specified by a policy area, and for party walls, dwellings and buildings containing dwellings should be set back from the side and rear boundaries in accordance with the following table:

Parameter	Minimum value (metres)
Side boundary setback where the vertical side wall is 3 metres or less in height (measured from the existing ground level at the boundary of the adjacent property as per <a href="#">Figure 1</a> )	1
Side boundary setback where vertical side wall measures between 3 to 6 metres in height (measured from the existing ground level at the boundary of the adjacent property as per <a href="#">Figure 1</a> )	2
Side boundary setback where the vertical side wall is greater than 6 metres in height (measured from the existing ground level at the boundary of the adjacent property as per <a href="#">Figure 1</a> )	2 metres plus an additional setback which is equal to the increase in wall height above 6 metres.
Rear boundary setback for single storey components of a building	3
Rear boundary setback for two or more storey components of a building	8



- 12 Side boundary walls in residential areas should be limited in length and height to:
  - (a) minimise their visual impact on adjoining properties
  - (b) minimise the overshadowing of adjoining properties.
- 13 Except where otherwise specified by a policy area, side boundary walls comply with the following:
  - (a) side boundary walls should be located immediately abutting the wall of an existing or simultaneously constructed building on the adjoining site and constructed to the same or to a lesser length and height
  - (b) side boundary walls:
    - (i) should have a maximum vertical wall height of 3 metres
    - (ii) should have a maximum length of 8 metres
    - (iii) should be constructed along one side of the allotment only and no further than 14 metres from the front boundary
  - (c) where there is an existing adjacent boundary wall which is setback greater than 1 metre from the front setback standard established for the rest of the street, side boundary walls should be located not more than 1 metre closer to the primary street frontage.
- 14 Development should ensure that sunlight to solar panels of existing buildings is maintained for a minimum of 2 consecutive hours between 9.00am and 3.00pm on 22 June.

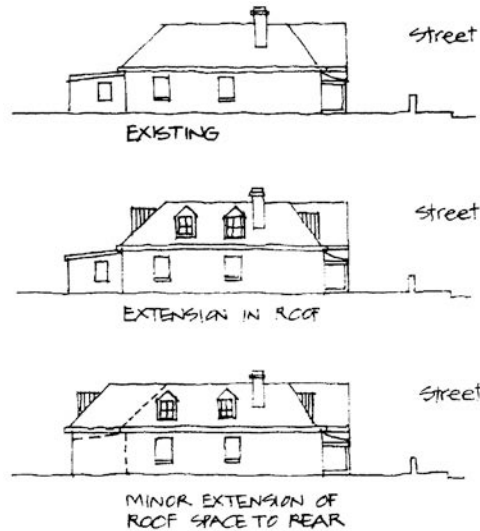
### **Affordable Housing**

- 15 Development should include a minimum 15 per cent of residential dwellings for affordable housing.
- 16 Affordable housing should be distributed throughout the zone to avoid over-concentration of similar types of housing in a particular area.

### **Character Areas**

The following Principles of Development Control apply to the **Ashford Character Policy Area 22, Cowandilla /Mile End West Character Policy Area 23, Glandore Character Policy Area 24, Lockleys Character Policy Area 25, Novar Gardens Character Policy Area 26, Thebarton Character Policy Area 27** and the **Torrensville Character Policy Area 28**.

- 17 Development should be limited to one storey, except where a dwelling faces a public road (ie is not sited on a battleaxe allotment or at the rear of a development site) and any of the following is proposed:
  - (a) sympathetic two-storey additions that use existing roof space or incorporate minor extensions of roof space to the rear of the dwelling (refer to the figure below)



- (b) in new dwellings, a second storey within the roof space where the overall building height, scale and form is compatible with existing single-storey development in the locality (refer to the figure below)



- (c) dormer windows with a total length less than 30 per cent of the total roof length along each elevation.
- 18 Development should preserve and enhance streetscapes by:
- (a) the incorporation of fences and gates in keeping with the height, scale and type of fences in the locality
  - (b) limiting the number of driveway crossovers.
- 19 The conversion of an existing dwelling into two or more dwellings may be undertaken provided that the building and front yard retain the original external appearance to the public road.
- 20 Where a new dwelling is constructed alongside or within a group of older style residential buildings, the new dwelling should be of a similar height, scale and proportions and be constructed of materials that complement and reinforce the character and design elements of existing buildings.
- 21 Other than in **Novar Gardens Character Policy Area 26**, garages and carports facing the street (other than an access lane way) should be designed with a maximum width of 3.66 metres.
- 22 The division of land should occur only where it will be consistent with the existing pattern and scale of allotments.

## Medium Density Policy Area 18

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 Development that contributes to the desired character of the policy area.

### DESIRED CHARACTER

Allotments in this policy area will be at medium density, accommodating a range of dwelling types including residential flat buildings, row dwellings, group dwellings, semi-detached dwellings and some detached dwellings on small allotments. Allotment amalgamation to create larger development sites will occur to maximise the density of development while also achieving integrated design outcomes, particularly within a comfortable walking distance of centre zones. Vehicle access will occur from side streets and new rear public and private laneways wherever possible, also supporting the retention of existing street trees.

New buildings will contribute to a highly varied streetscape. Buildings will be up to 3 storeys and provide a strong presence to streets, other than in the part of the policy area in Underdale, Ashford (other than allotments adjacent to **Residential Character Ashford Policy Area 22**) and allotments bounded by Anzac Highway, Morphett Road and Cromer Street in Camden Park where buildings will be up to 4 storeys. Parking areas and garages will be located behind the front facade of buildings.

Buildings on the edge of the policy area which adjoin residential policy areas at lower densities will pay particular attention to managing the interface with adjoining dwellings, especially in terms of the appearance of building height and bulk, and overshadowing.

Development will be interspersed with landscaping, particularly behind the main road frontage, to enhance the appearance of buildings from the street as viewed by pedestrians, provide an appropriate transition between the public and private realm and reduce heat loads in summer.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development are envisaged in the policy area:
  - affordable housing
  - detached dwelling
  - domestic outbuilding in association with a dwelling
  - domestic structure
  - dwelling addition
  - group dwelling
  - residential flat building
  - row dwelling
  - semi-detached dwelling
  - small scale non-residential use that serves the local community, for example:
    - child care facility
    - open space
    - recreation area
    - shop, office or consulting room
  - supported accommodation.

## Form and Character

- 1 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 2 Development adjacent the River Torrens should be undertaken in accordance with [Concept Plan Map WeTo/24 - Residential Promenade](#).
- 3 Development on sites adjoining or including the 'Natural Amphitheatre' area as shown on [Concept Plan Map WeTo/24 - Residential Promenade](#) should:
  - (a) not intrude into the amphitheatre area
  - (b) minimise the extent of cut and/or fill.
- 4 Medium density development that achieves gross densities of between 23 and 45 dwellings per hectare (which translates to net densities of between 40 and 67 dwellings per hectare, where net density can be calculated by dividing 10000 by the site area and multiplying that number by the number of proposed dwellings for the site) should be in the form of 2 to 4 storey buildings.
- 5 Dwellings should be designed within the following parameters:

Parameter	Value
Minimum setback from primary road frontage	3 metres
Minimum setback from secondary road frontage	1 metres
Minimum setback from back boundary	4 metres
Maximum site coverage (the area of a site covered by the ground floor level of a building, including the dwelling, garage, carport and outbuilding, but excluding unroofed balconies, verandas and pergolas)	70 per cent
Maximum building height (from natural ground level)	<p>Allotments bounded by Holbrooks Road, Garden Terrace and the River Torrens in Underdale: four storeys or 16.5 metres.</p> <p>Allotments bounded by Anzac Highway, Morphett Road and Cromer Street in Camden Park: four storeys or 16.5 metres</p> <p>Allotments in Ashford (other than those adjacent to <b>Residential Character Ashford Policy Area 22</b>): four storeys or 16.5 metres</p> <p>All other allotments: three storeys or 12.5 metres.</p>

- 6 A dwelling should have a minimum site area (and in the case of residential flat buildings, an average site area per dwelling) and a frontage to a public road not less than that shown in the following table:

Dwelling type	Site area (square metres)	Minimum frontage (metres)
Detached	250 minimum	9
Semi-detached	200 minimum	9
Group dwelling	150 minimum	7

Dwelling type	Site area (square metres)	Minimum frontage (metres)
Residential flat building	150 average	15 (for complete building)
Row dwelling	150 minimum	5

- 7 Dwellings constituting affordable housing (as defined by the *South Australian Housing Trust Regulations as amended*) should be designed within the following parameters and have a minimum site area (and in the case of residential flat buildings, an average site area per dwelling) not less than that shown in the following table:

Parameter	Detached Dwelling	Semi-detached Dwelling	Group Dwelling	Residential flat building	Row Dwelling
Site area (square metres)	150 minimum	150 minimum	100 minimum	100 average	100 minimum
Minimum area of private open space for ground level dwellings (square metres)	24 with a minimum dimension of 3 metres	24 with a minimum dimension of 3 metres	24 with a minimum dimension of 3 metres	24 with a minimum dimension of 3 metres	24 with a minimum dimension of 3 metres
Minimum area of private open space in the form of a balcony for dwellings above ground level (square metres)	8 with a minimum dimension of 2 metres	8 with a minimum dimension of 2 metres	8 with a minimum dimension of 2 metres	8 with a minimum dimension of 2 metres	8 with a minimum dimension of 2 metres
Minimum number of on-site car parking spaces (one of which should be covered)	1	1	1	1	1

### Land Division

- 8 Land division should create allotments with an area of greater than 250 square metres and a minimum frontage width of 9 metres, other than where the land division is combined with an application for dwellings, or follows an approval for dwellings on the site.

## Medium Density Policy Area 19

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 Development that contributes to the desired character of the policy area.

### DESIRED CHARACTER

Allotments in this policy area will be at medium density, accommodating a range of dwelling types including semi-detached, row and group dwellings, as well as some residential flat buildings and some detached dwellings on small allotments. There will be a denser allotment pattern close to centre zones where it is desirable for more residents to live and take advantage of the variety of facilities focused on centre zones.

New buildings will contribute to a highly varied streetscape. Buildings will be up to 2 storeys, except for allotments fronting Brooker Terrace, Marion Road and Henley Beach Road, and overlooking the Westside Bikeway, where buildings will be up to 3 storeys in height and provide a strong presence to streets. Garages and carports will be located behind the front facade of buildings.

Development will be interspersed with landscaping, particularly behind the main road frontage, to enhance the appearance of buildings from the street as viewed by pedestrians, provide an appropriate transition between the public and private realm and reduce heat loads in summer.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development are envisaged specifically in the policy area:
  - affordable housing
  - detached dwelling
  - domestic outbuilding in association with a dwelling
  - domestic structure
  - dwelling addition
  - group dwelling
  - residential flat building
  - row dwelling
  - semi detached dwelling
  - small scale non-residential use that serves the local community, for example:
    - child care facility
    - health and welfare service
    - open space
    - primary and secondary school
    - recreation area
    - shop measuring 250 square metres or less in gross leasable floor area
  - supported accommodation.

#### Form and Character

- 2 Development should not be undertaken unless it is consistent with the desired character for the policy area.

- 3 Dwellings should be designed within the following parameters:

Parameter	Value
Minimum setback from primary road frontage	3 metres
Minimum setback from secondary road frontage	2 metres
Minimum setback from back boundary	6 metres
Maximum site coverage (the area of a site covered by the ground floor level of a building, including the dwelling, garage, carport and outbuilding, but excluding unroofed balconies, verandas and pergolas)	60 per cent
Maximum building height (from natural ground level)	Allotments fronting Brooker Terrace, Marion Road and Henley Beach Road, and overlooking the Westside Bikeway: three storeys or 12.5 metres  All other locations: two storeys or 8.5 metres

- 4 Except when located within 400 metres of a centre zone, a dwelling should have a minimum site area (and in the case of residential flat buildings, an average site area per dwelling) and a frontage to a public road not less than that shown in the following table:

Dwelling type	Site area (square metres)	Minimum frontage (metres)
Detached	270 minimum	9
Semi-detached	270 minimum	9
Group dwelling	270 minimum	9
Residential flat building	270 average	15 (for complete building)
Row dwelling	270 minimum	9

- 5 When a dwelling is located within 400 metres of a centre zone, it should have a minimum site area (and in the case of residential flat buildings, an average site area per dwelling) and a frontage to a public road not less than that shown in the following table:

Dwelling type	Site area (square metres)	Minimum frontage (metres)
Detached	250 minimum	9
Semi-detached	200 minimum	9
Group dwelling	170 minimum	9
Residential flat building	150 average	15 (for complete building)
Row dwelling	150 minimum	5

- 6 When a dwelling constituting affordable housing (as defined by the *South Australian Housing Trust Regulations* as amended) is located within 400 metres of a centre, it should be designed within the following parameters and have a minimum site area (and in the case of residential flat buildings, an average site area per dwelling) not less than that shown in the following table:

West Torrens Council  
 Zone Section  
 Residential Zone  
 Medium Density Policy Area 19

Parameter	Detached Dwelling	Semi-detached Dwelling	Group Dwelling	Residential flat building	Row Dwelling
Site area (square metres)	230 minimum	180 minimum	250 minimum	130 average	130 minimum
Minimum area of private open space for ground level dwellings (square metres)	24 with a minimum dimension of 3 metres	24 with a minimum dimension of 3 metres	24 with a minimum dimension of 3 metres	24 with a minimum dimension of 3 metres	24 with a minimum dimension of 3 metres
Minimum area of private open space in the form of a balcony for dwellings above ground level (square metres)	8 with a minimum dimension of 2 metres	8 with a minimum dimension of 2 metres	8 with a minimum dimension of 2 metres	8 with a minimum dimension of 2 metres	8 with a minimum dimension of 2 metres
Minimum number of on site car parking spaces (one of which should be covered)	1	1	1	1	1

### Land Division

- 7 Land division should create allotments with an area of greater than 270 square metres and a minimum frontage width of 9 metres, other than where the land division is combined with an application for dwellings or follows an approval for dwellings on the site.



## Low Density Policy Area 20

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 Development that contributes to the desired character of the policy area.

### DESIRED CHARACTER

Allotments in the policy area will be at low density, accommodating predominantly detached dwellings and some other dwellings types such as semi-detached and group dwellings. There will be a denser allotment pattern close to centre zones where it is desirable for more residents to live and take advantage of the variety of facilities focused on centre zones. Battleaxe subdivision will not occur in the policy area to preserve a pattern of rectangular allotments developed with buildings that have a direct street frontage.

Buildings will be up to 2 storeys in height. Garages and carports will be located behind the front façade of buildings.

Development will be interspersed with landscaping, particularly behind the main road frontage, to enhance the appearance of buildings from the street as viewed by pedestrians, provide an appropriate transition between the public and private realm and reduce heat loads in summer. Low and open-style front fencing will contribute to a sense of space between buildings.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development are envisaged specifically in the policy area:
  - affordable housing
  - detached dwelling
  - domestic outbuilding in association with a dwelling
  - domestic structure
  - dwelling addition
  - group dwelling
  - semi-detached dwelling
  - small scale non-residential use that serves the local community, for example:
    - child care facility
    - health and welfare service
    - open space
    - primary and secondary school
    - recreation area
    - shop measuring 250 square metres or less in gross leasable floor area
  - supported accommodation.

#### Form and Character

- 2 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 3 A dwelling should have a minimum site area and a frontage to a public road not less than that shown in the following table:
  - (a) when located 400 metres or more from a centre zone, or

(b) when located within 400 metres of the Neighbourhood Centre Zone on Marion Road

Dwelling type	Site area (square metres)	Minimum frontage (metres)
Detached	340 minimum	10
Semi-detached	340 minimum	10
Group dwelling	340 minimum	10

- 4 When a dwelling is located within 400 metres of a centre zone (other than the Neighbourhood Centre Zone on Marion Road), it should have a minimum site area and a frontage to a public road not less than that shown in the following table:

Dwelling type	Site area (square metres)	Minimum frontage (metres)
Detached	300 minimum	9
Semi-detached	300 minimum	9
Group dwelling	300 minimum	9

### Land Division

- 5 Land division should create allotments with an area of greater than 340 square metres and a minimum frontage width of 10 metres, other than where the land division is combined with an application for dwellings or follows an approval for dwellings on the site.

## Low Density Policy Area 21

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 Development that contributes to the desired character of the policy area.

### DESIRED CHARACTER

This policy area will have a low density character. In order to preserve this, development will predominantly involve the replacement of detached dwellings with the same (or buildings in the form of detached dwellings).

There will be a denser allotment pattern and some alternative dwelling types, such as semi-detached and row dwellings, close to centre zones where it is desirable for more residents to live and take advantage of the variety of facilities focused on centre zones. Battleaxe subdivision will not occur in the policy area to preserve a pattern of rectangular allotments developed with buildings that have a direct street frontage. In the area bounded by Henley Beach Road, Torrens Avenue and the Linear Park, where the consistent allotment pattern is a significant positive feature of the locality, subdivision will reinforce the existing allotment pattern.

Buildings will be up to 2 storeys in height. Garages and carports will be located behind the front façade of buildings. Buildings in the area bounded by Henley Beach Road, Torrens Avenue and the Linear Park will be complementary to existing dwellings through the incorporation of design features such as pitched roofs, eaves and variation in the texture of building materials.

Development will be interspersed with landscaping, particularly behind the main road frontage, to enhance the appearance of buildings from the street as viewed by pedestrians, provide an appropriate transition between the public and private realm and reduce heat loads in summer. Low and open-style front fencing will contribute to a sense of space between buildings.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development are envisaged specifically in the policy area:
  - affordable housing
  - domestic outbuilding in association with a dwelling
  - domestic structure, such as a veranda, porch
  - detached dwelling
  - dwelling addition
  - row dwelling within 400 metres of an existing centre zone
  - semi detached dwelling within 400 metres of an existing centre zone
  - small scale non-residential use that serves the local community, for example:
    - child care facility
    - health and welfare service
    - open space
    - primary and secondary school
    - recreation area
    - shop measuring 250 square metres or less in gross leasable floor area
  - supported accommodation.

## Form and Character

- 2 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 3 Except when located within 400 metres of a centre zone, a dwelling should have a minimum site area (and in the case of residential flat buildings, an average site area per dwelling) and a frontage to a public road not less than that shown in the following table:

Dwelling type	Site area (square metres)	Minimum frontage (metres)
Detached	420 minimum	12
Semi-detached	420 minimum	12

- 4 When a dwelling is located within 400 metres of a centre zone, it should have a minimum site area (and in the case of residential flat buildings, an average site area per dwelling) and a frontage to a public road not less than that shown in the following table:

Dwelling type	Site area (square metres)	Minimum frontage (metres)
Detached	350 minimum	9
Semi-detached	350 minimum	9
Row dwelling	350 average	9

- 5 When a dwelling constituting affordable housing (as defined by the *South Australian Housing Trust Regulations* as amended) is located within 400 metres of a centre zone, it should be designed within the following parameters and have a minimum site area not less than that shown in the following table:

Parameter	Detached Dwelling	Semi-detached Dwelling	Row Dwelling
Site area (square metres)	300 minimum	300 minimum	300 minimum
Minimum area of private open space for ground level dwellings (square metres)	24 with a minimum dimension of 3 metres	24 with a minimum dimension of 3 metres	24 with a minimum dimension of 3 metres
Minimum area of private open space in the form of a balcony for dwellings above ground level (square metres)	8 with a minimum dimension of 2 metres	8 with a minimum dimension of 2 metres	8 with a minimum dimension of 2 metres
Minimum number of on site car parking spaces (one of which should be covered)	1	1	1

## Land Division

- 6 Land division should create allotments with an area of greater than 420 square metres and a minimum frontage width of 12 metres, other than where the land division is combined with an application for dwellings or follows an approval for dwellings on the site.

## Ashford Character Policy Area 22

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 Development that contributes to the desired character of the policy area.

### DESIRED CHARACTER

The policy area will contain predominantly detached dwellings (or buildings that look like detached dwellings).

Allotments will vary in size from low density to very low density, with wide frontages. Subdivision will reinforce the existing allotment pattern of wide and deep allotments, which is a significant positive feature of the policy area.

There will be a unity of built-form, particularly as viewed from the street, where all new development is complementary to the predominant character of inter-war bungalows and tudor-style dwellings rather than dominating or detracting from them. Key elements of this character include pitched roofs, verandas /porticos and masonry building materials. There will be predominantly one storey buildings, with some two storey buildings designed in a manner that is complementary to the single storey character of nearby buildings.

Setbacks will be complementary to the boundary setbacks of inter-war bungalows and tudor-style dwellings in the policy area, preserving considerable space in private yards for landscaping.

There will be no garages/carports forward of the main facade of buildings. Fencing forward of dwellings will be low to provide views of built-form that define the character of the policy area. Any driveway crossovers will be carefully designed and located to ensure the preservation of street trees which have an important positive impact on the streetscape.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development are envisaged specifically in the policy area:
  - domestic outbuilding in association with a dwelling
  - domestic structure, such as a veranda, porch
  - detached dwelling
  - dwelling addition
  - small scale non-residential use that serves the local community, for example:
    - child care facility
    - health and welfare service
    - open space
    - primary and secondary school
    - recreation area
    - shop measuring 250 square metres or less in gross leasable floor area
  - supported accommodation.

#### Form and Character

- 2 Development should not be undertaken unless it is consistent with the desired character for the policy area.

## Cowandilla / Mile End West Character Policy Area 23

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 Development that contributes to the desired character of the policy area.

### DESIRED CHARACTER

The policy area will contain predominantly detached dwellings and semi-detached dwellings. There will also be some small-scale non-residential activities such as offices, shops and consulting rooms in certain locations. Non-residential activities will be complementary to surrounding dwellings.

Allotments will vary in size from low density to very low density and are generally deep, with narrow frontages to main streets. Subdivision will reinforce the existing allotment pattern which is a significant positive feature of the policy area.

There will be a unity of built-form, particularly as viewed from the street, where all new development is complementary to the key character elements of Victorian-era villas, cottages, inter-war bungalows, Spanish mission and Dutch colonial-style dwellings, rather than dominating or detracting from them. Key elements of this character include pitched roofs, verandas /porticos and masonry building materials. There will be predominantly one storey buildings, with some two storey buildings designed in a manner that is complementary to the single storey character of nearby buildings. Setbacks will be complementary to the boundary setbacks of older dwellings in the policy area, preserving considerable space in private yards for landscaping.

There will be no garages/carports forward of the main facade of buildings. Fencing forward of dwellings will be low to provide views of built-form that define the character of the policy area. Any driveway crossovers will be carefully designed and located to ensure the preservation of street trees which have an important positive impact on the streetscape.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development are envisaged specifically in the policy area:
  - domestic outbuilding in association with a dwelling
  - domestic structure, such as a veranda, porch
  - detached dwelling
  - dwelling addition
  - semi-detached dwelling
  - small scale non-residential use that serves the local community, for example:
    - child care facility
    - health and welfare service
    - open space
    - primary and secondary school
    - recreation area
    - shop measuring 250 square metres or less in gross leasable floor area
  - supported accommodation.

**Form and Character**

- 2 Development should not be undertaken unless it is consistent with the desired character for the policy area.

**Land Division**

- 3 Land division should create allotments with an area of greater than 270 square metres east of Bagot Avenue and 340 square metres west of Bagot Avenue.

## Glandore Character Policy Area 24

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 Development that contributes to the desired character of the policy area.

### DESIRED CHARACTER

The policy area will contain predominantly detached dwellings (or buildings that look like detached dwellings).

Allotments will be very low density and are deep and wide. Subdivision will reinforce the existing allotment pattern which is a significant positive feature of the policy area.

There will be a unity of built-form, particularly as viewed from the street, where all new development is complementary to the key character elements of villas, inter-war bungalows, Spanish mission and Dutch colonial-style dwellings, rather than dominating or detracting from them. Key elements of this character include pitched roofs, verandas /porticos and masonry building materials. There will be predominantly one storey buildings, with some two storey buildings designed in a manner that is complementary to the single storey character of nearby buildings.

Setbacks will be complementary to the boundary setbacks of older dwellings in the policy area, preserving considerable space in private yards for landscaping.

There will be no garages/carports forward of the main facade of buildings. Any driveway crossovers will be carefully designed and located to ensure the preservation of street trees which have an important positive impact on the streetscape. Fencing forward of dwellings will be low to provide views of built-form that define the character of the policy area.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development are envisaged specifically in the policy area:
  - domestic outbuilding in association with a dwelling
  - domestic structure, such as a veranda, porch
  - detached dwelling
  - dwelling addition
  - small scale non-residential use that serves the local community, for example:
    - child care facility
    - health and welfare service
    - open space
    - primary and secondary school
    - recreation area
    - shop measuring 250 square metres or less in gross leasable floor area
  - supported accommodation.

#### Form and Character

- 2 Development should not be undertaken unless it is consistent with the desired character for the policy area.



## Lockleys Character Policy Area 25

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 Development that contributes to the desired character of the policy area.

### DESIRED CHARACTER

The policy area will contain detached dwellings (or buildings that look like detached dwellings).

Allotments will be very low density with wide street frontages and even deeper side boundaries. Subdivision will reinforce the existing allotment pattern which is a significant positive feature of the policy area.

There will be a unity of built-form, particularly as viewed from the street, where all new development is complementary to the key character elements of inter-war bungalows, Dutch colonial-style and art-deco style dwellings, rather than dominating or detracting from them. Key elements of this character include pitched roofs, verandas / porticos and masonry building materials.

There will be no garages/carports forward of the main facade of buildings. Large front setbacks provide space for generous landscaping in front yards which, in turn, have a positive impact on the streetscape given the low front boundary fencing. Any driveway crossovers will be carefully designed and located to ensure the preservation of street trees which have an important positive impact on the streetscape.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development are envisaged specifically in the policy area:
  - domestic outbuilding in association with a dwelling
  - domestic structure, such as a veranda, porch
  - detached dwelling
  - dwelling addition
  - small scale non-residential use that serves the local community, for example:
    - child care facility
    - health and welfare service
    - open space
    - primary and secondary school
    - recreation area
    - shop measuring 250 square metres or less in gross leasable floor area
  - supported accommodation.

#### Form and Character

- 2 Development should not be undertaken unless it is consistent with the desired character for the policy area.

## Novar Gardens Character Policy Area 26

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 Development that contributes to the desired character of the policy area.

### DESIRED CHARACTER

The policy area will contain detached dwellings (or buildings that looks like detached dwellings).

Allotments will be very low density and will be relatively square, with comparatively wide frontages. The consistency of this allotment pattern is a significant positive feature of the policy area, which subdivision will reinforce.

There will be a unity of built-form, particularly as viewed from the street, where all new development is complementary to the predominant low and wide single storey detached dwellings on wide, generously landscaped allotments. This means that new development will preserve the consistency of front and side boundary setbacks, and any two storey elements will be discretely integrated.

Carports and garages will be in line with or behind the building facade. Alterations and additions will be primarily located at the rear of existing dwellings so that they have minimal impact on the streetscape and do not disrupt the consistent front set back.

There will be no formal footpaths on the Council owned verge and there will be no front fences or side fences forward of the building line to preserve the appearance of continuous, generously landscaped front yards extending to the street.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development are envisaged specifically in the policy area:
  - domestic outbuilding in association with a dwelling
  - domestic structure, such as a veranda, porch
  - detached dwelling
  - dwelling addition
  - small scale non-residential use that serves the local community, for example:
    - child care facility
    - health and welfare service
    - open space
    - primary and secondary school
    - recreation area
    - shop measuring 250 square metres or less in gross leasable floor area
  - supported accommodation.

#### Form and Character

- 2 Development should not be undertaken unless it is consistent with the desired character for the policy area.

## Thebarton Character Policy Area 27

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 Development that contributes to the desired character of the policy area.

### DESIRED CHARACTER

The policy area will contain detached, semi-detached and row dwellings.

Allotments will vary in size considerably from medium to very low density. Some allotments have rear access vehicle parking via service laneways, reducing the number of garages and carports visible in the main streets. Due to the variance in allotment sizes, subdivision will reinforce the existing allotment pattern in the immediate locality.

New development will be complementary to the key character elements of Victorian-era villas, cottages, and bungalow-style dwellings in the policy area, rather than dominating or detracting from them, particularly when viewed from the street. Key elements of this character include pitched roofs, verandas / porticos and masonry building materials. There will be predominantly one storey buildings, with some two storey buildings designed in a manner that is complementary to the single storey character of nearby buildings. Setbacks will be complementary to the setbacks of adjacent properties.

There will be no garages / carports forward of the main facade of buildings. Fencing forward of dwellings will be low to provide views of built-form that define the character of the policy area. Any driveway crossovers will be carefully designed and located to ensure the preservation of street trees which have an important positive impact on the streetscape.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development are envisaged specifically in the policy area:
  - domestic outbuilding in association with a dwelling
  - domestic structure, such as a veranda, porch
  - detached dwelling
  - dwelling addition
  - row dwelling
  - semi-detached dwelling
  - small scale non-residential use that serves the local community, for example:
    - child care facility
    - health and welfare service
    - open space
    - primary and secondary school
    - recreation area
    - shop measuring 250 square metres or less in gross leasable floor area
  - supported accommodation.

**Form and Character**

- 2 Development should not be undertaken unless it is consistent with the desired character for the policy area.

**Land Division**

- 3 Land division should create allotments with an area of greater than 270 square metres.

## Torrensville Character Policy Area 28

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 Development that contributes to the desired character of the policy area.

### DESIRED CHARACTER

The policy area will contain predominantly detached dwellings and some semi-detached dwellings.

Allotments will vary in size from low to very low density with wide street frontages and even deeper side boundaries. Subdivision will reinforce the existing allotment pattern which is a significant positive feature of the policy area.

New development will be complementary to the key character elements of Victorian-era villas, cottages, inter-war bungalow and tudor-style dwellings in the policy area, rather than dominating or detracting from them, particularly as viewed from the street. Key elements of this character include pitched roofs, veranda / porticos and masonry building materials. There will be predominantly one storey buildings, with some two storey buildings designed in a manner that is complementary to the single storey character of nearby buildings. Setbacks will be complementary to the boundary setbacks of nearby older dwellings.

There will be no garages/carports forward of the main facade of buildings. Fencing forward of dwellings will be low to provide views of built-form that define the character of the policy area. Any driveway crossovers will be carefully designed and located to ensure the preservation of street trees which have an important positive impact on the streetscape.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development are envisaged specifically in the policy area:
  - domestic outbuilding in association with a dwelling
  - domestic structure, such as a veranda, porch
  - detached dwelling
  - dwelling addition
  - semi detached dwelling
  - small scale non-residential use that serves the local community, for example:
    - child care facility
    - health and welfare service
    - open space
    - primary and secondary school
    - recreation area
    - shop measuring 250 square metres or less in gross leasable floor area
  - supported accommodation.

#### Form and Character

- 2 Development should not be undertaken unless it is consistent with the desired character for the policy area.

#### Land Division

- 3 Land division should create allotments with an area of greater than 340 square metres.

## Elston Street Conservation Policy Area 29

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 Development that contributes to the desired character of the policy area.

### DESIRED CHARACTER

The provisions of the Historic Conservation Area apply to this policy area.

The policy area will contain predominantly detached dwellings (or buildings that look like detached dwellings).

Allotments are at low to very low density and are generally deep, with wide frontages to main streets. Subdivision will reinforce the existing allotment pattern which is a significant positive feature of the Policy Area and there will be no shared driveways.

Streetscape character elements including low front fencing, landscaping space in front yards, regular street trees and on-street visitor car parking will be supported by limiting vehicle cross-overs.

There will be a unity of built-form, where all new development is complementary to historic buildings rather than dominating or detracting from them. There will be predominantly one storey buildings, with some two storey buildings where the upper level is contained within the roof space in a manner that is complementary to the single storey character of nearby buildings.

New dwellings will incorporate building elements common to older structures such as pitched roofs, verandas and simple detailing, as well as building materials such as stone, bricks, limited rendered masonry and corrugated iron/steel.

Alterations and additions will be primarily located at the rear of existing dwellings so that they have minimal impact on the streetscape. Setbacks will be complementary to the boundary setbacks of existing buildings in the policy area.

There will be no garages/carports forward of the main facade of buildings. There may be single width garages/carports attached to the side of dwellings set back from the principal facade. Double garages or carports will be located behind dwellings and accessed from a single width driveway along the side of the associated dwelling.

Historic front fencing, such as post and rail with woven wire and timber picket fences, will be preserved and new front fencing will be complementary in form and materials, facilitating views into front yards and of building facades.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development are envisaged specifically in the policy area:
  - domestic outbuilding in association with a dwelling
  - domestic structure
  - detached dwelling
  - dwelling addition.

### **Form and Character**

- 2 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 3 Development to the principal frontage of the allotment should be single storey, except where there may be dormer windows facing the street.
- 4 The building setback from the street boundary should align with the buildings situated on allotments on either side. Where the buildings on either side are not setback at the same distance, the building should be set back the average of the setback of the buildings situated on allotments on either side.

### **Land Division**

- 5 Land division may only occur where there is an accompanying application for dwellings that are consistent with the desired character statement for the policy area.

## Mile End Conservation Policy Area 30

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 Development that contributes to the desired character of the policy area.

### DESIRED CHARACTER

The provisions of the Historic Conservation Area apply to this policy area.

The policy area will contain detached and semi-detached dwellings.

Allotments are at low to very low density and are generally deep, with narrow frontages to wide main streets. In many cases, there is also rear access to service laneways. Subdivision will reinforce the existing allotment pattern which is a significant positive feature of the policy area.

It is envisaged that the long and wide streets running east-west within the policy area will continue to provide pedestrian access to the main frontages of dwellings, and the laneways will be used for rear vehicular access. Streetscape character elements including continuous front fencing, landscaping space in front yards, regular street trees and on-street visitor car parking will be supported by having vehicle cross-overs in laneways.

There will be a unity of built-form, where all new development is complementary to historic buildings rather than dominating or detracting from them. There will be predominantly one storey buildings, with some two storey buildings where the upper level is contained within the roof space in a manner that is complementary to the single storey character of nearby buildings.

New dwellings will incorporate building elements common to older structures such as pitched roofs, verandas and simple detailing, as well as building materials such as stone, bricks, limited rendered masonry and corrugated iron/steel.

Alterations and additions will be primarily located at the rear of existing dwellings so that they have minimal impact on the streetscape. Setbacks will be complementary to the boundary setbacks of existing buildings in the policy area.

Historic front fencing, such as post and rail with woven wire and timber picket fences will be preserved, and new front fencing will be complementary in form and materials, facilitating views into front yards and of building facades.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development are envisaged specifically in the policy area:
  - domestic outbuilding in association with a dwelling
  - domestic structure
  - detached dwelling
  - dwelling addition
  - semi-detached dwelling.



### **Form and Character**

- 2 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 3 Development to the principal frontage of the allotment should be single storey, except where there may be dormer windows facing the street.
- 4 The building setback from the street boundary should align with the buildings situated on allotments on either side. Where the buildings on either side are not setback at the same distance, the building should be set back the average of the setback of the buildings situated on allotments on either side.

### **Land Division**

- 5 Land division may only occur where there is an accompanying application for dwellings that are consistent with the desired character statement for the policy area.

## Richmond Conservation Policy Area 31

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 Development that contributes to the desired character of the policy area.

### DESIRED CHARACTER

The provisions of the Historic Conservation Area apply to this policy area.

The policy area will contain predominantly semi-detached and row dwellings, and limited detached dwellings.

Allotments are at medium to low density with some rear public and private laneways.

Streetscape character elements including continuous minimal/low front fencing and a combination of garages/carports accessed from main street frontages and rear laneways.

There will be a mix of one and two storey buildings and the consistency of front boundary setbacks will remain a significant feature of the policy area.

New dwellings will incorporate building elements common to older structures such as pitched roofs, verandas and simple detailing, as well as building materials such as bricks, limited rendered masonry and corrugated iron/steel.

Alterations and additions will be primarily located at the rear of existing dwellings so that they have minimal impact on the streetscape. Setbacks will be complementary to the boundary setbacks of existing buildings in the policy area.

Many garages/carports will be located off rear laneways, but where vehicle access is necessary off main roads there will be no garages/carports forward of the main facade of buildings.

There will be either no fencing forward of dwellings, or fencing in this location will be low facilitating views into front yards and of building facades.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development are envisaged specifically in the policy area:
  - domestic outbuilding in association with a dwelling
  - domestic structure
  - detached dwelling
  - dwelling addition
  - row dwelling
  - semi-detached dwellings.

#### Form and Character

- 2 Development should not be undertaken unless it is consistent with the desired character for the policy area.

- 3 The building setback from the street boundary should align with the buildings situated on allotments on either side. Where the buildings on either side are not setback at the same distance, the building should be set back the average of the setback of the buildings situated on allotments on either side.

#### **Land Division**

- 4 Land division may only occur where there is an accompanying application for dwellings that are consistent with the desired character statement for the policy area.

## Rose Street Conservation Policy Area 32

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 Development that contributes to the desired character of the policy area.

### DESIRED CHARACTER

The provisions of the Historic Conservation Area apply to this policy area.

The policy area will contain detached and semi-detached dwellings.

Allotments are at low to very low density and are generally deep, with narrow frontages to wide main streets. In many cases, there is also rear access to service laneways in both public and private ownership. Subdivision will reinforce the existing allotment pattern which is a significant positive feature of the Policy Area.

Streetscape character elements including continuous minimal/low front fencing, landscaping space in front yards and on-street visitor car parking will be supported by having vehicle cross-overs in laneways.

There will be a unity of built-form, where all new development is complementary to historic buildings rather than dominating or detracting from them. There will be predominantly one storey buildings, with two storey buildings only where the upper level is contained within the roof space in a manner that is complementary to the single storey character of nearby buildings. The consistency of front boundary setbacks will remain a significant feature of the policy area.

New dwellings will incorporate building elements common to older structures such as pitched roofs, verandas and simple detailing, as well as building materials such as stone, bricks, limited rendered masonry and corrugated iron/steel.

Alterations and additions will be primarily located at the rear of existing dwellings so that they have minimal impact on the streetscape. Setbacks will be complementary to the boundary setbacks of existing buildings in the policy area.

Garages/carports will primarily be located off rear laneways, but where vehicle access is necessary off main roads there will be no garages/carports forward of the main facade of buildings.

There will be either no fencing forward of dwellings, or fencing in this location will be low facilitating views into front yards and of building facades.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development are envisaged specifically in the policy area:
  - domestic outbuilding in association with a dwelling
  - domestic structure
  - detached dwelling
  - dwelling addition
  - semi-detached dwelling.

### **Form and Character**

- 2 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 3 Development of the Workmen's Cottages on Rose Street, as identified within [Table WeTo/5 - State Heritage Places](#) and on *Overlay Maps - Heritage*, other than maintenance / restoration work, should only occur on those parts of the building which are not visible from the street.
- 4 Development to the street front of the allotment should be single storey, except where there may be dormer windows facing the street.
- 5 The building setback from the street boundary should align with the buildings situated on allotments on either side. Where the buildings on either side are not setback at the same distance, the building should be set back the average of the setback of the buildings situated on allotments on either side.

### **Land Division**

- 6 Land division may only occur where there is an accompanying application for dwellings that are consistent with the desired character statement for the policy area.

## Torrensville East Conservation Policy Area 33

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 Development that contributes to the desired character of the policy area.

### DESIRED CHARACTER

The provisions of the Historic Conservation Area apply to this policy area.

The policy area will contain predominantly detached dwellings and limited semi-detached dwellings.

Allotments are at low to very low density and are generally deep, with wide frontages to main streets. Subdivision will reinforce the existing allotment pattern which is a significant positive feature of the policy area.

Streetscape character elements including low front fencing, landscaping space in front yards, regular street trees and on-street visitor car parking will be supported by limiting vehicle cross-overs.

There will be a unity of built-form, where all new development is complementary to historic buildings rather than dominating or detracting from them. There will be predominantly one storey buildings, with some two storey buildings where the upper level is contained within the roof space in a manner that is complementary to the single storey character of nearby buildings. New dwellings will incorporate building elements common to older structures such as pitched roofs, verandas and simple detailing, as well as building materials such as stone, bricks, limited rendered masonry and corrugated iron/steel.

Alterations and additions will be primarily located at the rear of existing dwellings so that they have minimal impact on the streetscape. Setbacks will be complementary to the boundary setbacks of existing buildings in the policy area.

There will be no garaging/carports forward of the main facade of buildings.

Historic front fencing, such as post and rail with woven wire and timber picket fences, will be preserved and new front fencing will be complementary in form and materials, facilitating views into front yards and of building facades.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development are envisaged specifically in the policy area:
  - domestic outbuilding in association with a dwelling
  - domestic structure
  - detached dwelling
  - dwelling addition
  - semi-detached dwelling.

#### Form and Character

- 2 Development should not be undertaken unless it is consistent with the desired character for the policy area.

- 3 Development to the street front of the allotment should be single storey, except where there may be dormer windows facing the street.
- 4 The building setback from the street boundary should align with the buildings situated on allotments on either side. Where the buildings on either side are not setback at the same distance, the building should be set back the average of the setback of the buildings situated on allotments on either side.

**Land Division**

- 5 Land division may only occur where there is an accompanying application for dwellings that are consistent with the desired character statement for the policy area.

## PROCEDURAL MATTERS

### Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

In addition, the following forms of development are designated as complying subject to the conditions contained in [Table WeTo/1 - Conditions for Complying Development](#):

Form of Development	Exceptions
Detached dwelling	
Free standing carport	
Free standing garage	
Outbuilding	
Row dwelling within any of the following policy areas:	
(a) <b>Medium Density Policy Area 18</b>	
(b) <b>Medium Density Policy Area 19</b>	
Semi-detached dwelling	Except where located within any of the following policy areas: (a) <b>Ashford Character Policy Area 22</b> (b) <b>Glandore Character Policy Area 24</b> (c) <b>Lockleys Character Policy Area 25</b> (d) <b>Novar Gardens Character Policy Area 26</b>

### Non-complying Development

Development (including combinations thereof, or more than one of a particular kind, alterations, extensions and/or additions to existing buildings or structures, building work, a change in the use of land, or division of an allotment) for the following is non-complying:

Form of Development	Exceptions
Advertisement and /or advertising hoarding	
Amusement machine centre	
Amusement park	
Auction room	
Bus depot	
Caravan park	
Car wash	
Consulting room	Except where: (a) the total floor area is 100 square metres or less (b) the site does not front an arterial road.
Crematorium	
Demolition of all or part of a building comprising a State heritage place	



Form of Development	Exceptions
Dairy	
Farming	
Fuel depot	
Hall	
Horse keeping	
Horticulture	
Hospital	
Hotel	
Indoor recreation centre	
Industry	
Integrated service station complex	
Intensive animal keeping	
Motel	
Motor repair station	
Office	Except where: (a) the total floor area is 100 square metres or less (b) the site does not front an arterial road.
Petrol filling station	
Public service depot	
Restaurant	
Road transport terminal	
Service trade premises	
Shop or group of shops	Except where the gross leasable area is 250 square metres or less.
Showground	
Stadium	
Stock sales yard	
Stock slaughter works	
Store	
Theatre	
Warehouse	
Waste reception, storage, treatment or disposal	
Wrecking yard	

## Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development (except where the development is classified as non-complying) are designated:

Category 1	Category 2
	<p data-bbox="770 499 1350 562">Dwelling exceeding one storey within Residential Policy Areas 22-33</p> <p data-bbox="770 577 1370 640">Demolition of a building or structure listed in any of the following tables:</p> <ul data-bbox="826 640 1342 703" style="list-style-type: none"><li data-bbox="826 640 1294 672">(a) <a href="#">Table WeTo/3 - Contributory Items</a></li><li data-bbox="826 672 1342 703">(b) <a href="#">Table WeTo/4 - Local Heritage Places</a>.</li></ul>

## Urban Core Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

### OBJECTIVES

- 1 A mixed use zone accommodating a mix of employment generating land uses and medium to high density residential development in close proximity to a high frequency public transport corridor.
- 2 Development within a mixed use environment that is compatible with surrounding development and which does not unreasonably compromise the amenity of the zone or any adjoining residential zone.
- 3 Smaller dwellings, including innovative housing designs, located close to local services and public transport stops.
- 4 Mixed use development integrated with a high quality public realm that promotes walking, cycling, public transport patronage and positive social interaction.
- 5 A zone that provides a spatial separation, or transitions down, in scale and intensity to adjacent lower density residential zones and areas.
- 6 Development that contributes to the desired character of the zone.

### DESIRED CHARACTER

This zone supports housing at medium and high densities and a range of dwelling types which are conveniently located in proximity to high frequency public transport services, recreation, commercial, shop, office and other mixed use activities. Development within this zone will result in significant employment generating activity closely aligned to nearby public transport infrastructure and services.

Medium and high density housing, primarily in the form of row dwellings, residential flat buildings and mixed use buildings, will be developed in the zone. Overall, the zone is intended to achieve an average net residential site density of 200 dwellings per hectare. Development in proximity to Industrial Zones will consist of non-residential land uses to limit environmental impacts.

The zone will focus around a central Core Area where development intensities and heights will be at their greatest and transition down to a scale and intensity that is compatible with neighbouring developments.

Overlooking, overshadowing and noise impacts will be moderated through good design, setbacks and noise attenuation techniques. Impacts on adjoining zones will also be addressed through appropriate building envelopes, transition of building heights, design and location of windows and balconies, and use of landscaping. Buildings will also be designed to maximise solar access within the development site.

Development in the zone will achieve high quality, contemporary urban design, reflective of its role as a premier development and an event destination. The visual massing and height of buildings should be progressively reduced towards the interface with lower density residential development and will reflect the desired character of the locality while incorporating contemporary designs. Urban design will reflect the 'grid' format of the surrounding residential areas. Buildings will contribute to the provision of a coherent public realm by articulating buildings with setbacks, canopies, modelled façades and balconies that make use of light and shade, and by providing architectural detail. Development adjacent existing public roads, tramways, civic and open spaces should be orientated to face those accessways. Development should be designed to take advantage of key views to the Adelaide Hills and coastal areas, open spaces, the Morphettville Racecourse, as well as existing roadways, tramline and bikeways to optimise safety of the vicinity and amenity for residents.

Development in proximity to the tramline will be designed to avoid any detrimental impacts on the efficiency of tram operations, cyclists on the Mike Turtur Bikeway and / or the safety of pedestrians. Tram corridors, and the Bikeway, will be separated from pedestrian areas through grade-separation and / or other appropriate means.

The site will continue to play host to major events, separate to or in conjunction with the adjacent racecourse. All development will be located and designed to be cognisant of and mitigate the impacts of crowd management, parking and accessibility, and ensure community safety. Development in proximity to horse keeping, racing or handling areas will be additionally designed to protect inhabitants from the negative impacts of associated noise, odour and waste. In addition, sensitive development in proximity to the equine activities, major events infrastructure and tram infrastructure (including the tram barns) will be located and designed in a manner that will enable existing and envisaged land uses to operate without prejudice. This may include the incorporation of open space, fencing and / or landscaping to reduce the impacts of activities that result in noise, air quality or waste management.

A significant emphasis will be placed on the provision of landscaping, including mature trees, to soften built form, provide pedestrian shade, urban biodiversity and streetscape amenity. The streetscape will be further improved with the provision of landscaping, public art and street furniture to assist in establishing a sense of place within the zone for the enjoyment and benefit of users.

Wide footpaths and cycling routes with extensive landscaping will be provided throughout the zone and integrate with existing bikeways and walkways in the vicinity, including the Mike Turtur Bikeway. A hierarchy of public open space throughout the zone, including small pocket parks and formal plazas will enable a variety of experiences and functions, including play and activation and incorporate stormwater management functions where appropriate. Buildings will be designed to encourage the development of active land uses at street level along key thoroughfares as well as ensuring that open space areas have access to sunlight throughout the year.

Anzac Highway is a strategic route and its function as a major transport corridor will be protected with minimal on-street vehicle parking and access points. Access will be provided from secondary road frontages and rear access ways. Controlled pedestrian crossings points will be focussed and consolidated at key locations. On-site parking areas, including a park'n'ride facility, will be consolidated, shared and, where possible, not visible from the street or public spaces.

Morphettville Racecourse will be developed to showcase water sensitive urban design as part of its master planned development. Where practical, stormwater disposal from the zone or individual development sites within the zone will be managed to June 2017 levels, and incorporate water sensitive urban design systems at the site and building level, including the harvest, treatment, storage and reuse of stormwater. Harvested stormwater will improve the aesthetic and functional value of open spaces and public access ways.

## **Core Area**

The Core Area will provide the greatest intensity of land use and activity in the zone with a mix of residential, commercial and employment generating activities integrated with adjacent public transit stops. The Core Area will be focused around a central plaza area that includes a tram stop and will be framed by active building frontages, public art, community facilities, open space and landscaping. Open and amenable pedestrian and cycle access will be provided throughout the Core Area and linking to surrounding destinations, including bus stops surrounding roadways.

The Core Area will continue to accommodate activities associated with Morphettville Racecourse, particularly for race meetings. Within the Core Area, development associated with horse racing activities, including the spectator stadiums, an equine promenade area, mounting yards, race day stables, parade rings, race day administration, bars and betting facilities, will be incorporated in a manner that is cognisant of the role of the site as the premier facility for horse racing in the state.

Building height will be limited to a maximum of 12 storeys in the centre of the Core Area with a maximum of 6 storeys fronting Anzac Highway. Development, particularly adjacent to a fixed transit stop, will be mixed both vertically and horizontally, with non-residential uses encouraged on the ground floor to create visual interest and invite personal interaction on street frontages. The exterior storage or display of goods will not compromise pedestrian movement.

Neighbourhood scale retail activity is envisaged within the Core Areas of the zone, containing in the order of 5500 square metres of retail floor space in addition to other business and community facilities.

Student and aged accommodation, serviced apartments and affordable housing are also strongly encouraged in the Core Area to assist in delivering an overall mix of residential activity in this area.

### Transition Area

The Transition Area will provide a buffer between the Core Area and adjacent residential areas with development taking the form of high quality medium density housing where the range of housing will provide for a diverse community. Buildings in the Transition Area will front the public streetscape, have a maximum height of 4 storeys and respond to the context, scale and intensity of existing development in the vicinity.

There will be some provision for mixed use activities of a local scale that does not negatively impact on the predominant residential character of the area.

## PRINCIPLES OF DEVELOPMENT CONTROL

### Land Use

- 1 The following types of development, or combination thereof, are envisaged in the zone:
  - affordable housing
  - aged persons accommodation
  - community centre
  - consulting room
  - dwelling
  - educational establishment
  - emergency service facility
  - licensed premises
  - nursing home
  - office
  - pre-school
  - recreation area
  - residential flat building
  - retirement village
  - shop or group of shops
  - supported accommodation.
  
- 2 The following additional types of development, or combination thereof, are envisaged within the Core Area of the zone, identified on [Concept Plan Map WeTo/26 – Morphettville Racecourse](#):
  - conference facilities
  - clubrooms
  - entertainment venue
  - hall
  - hotel
  - indoor recreation centre
  - place of worship
  - restaurant
  - spectator stand
  - horse racing activities and facilities ancillary to horse racing activities including promenade area, mounting yards, race day stables and parade rings
  - special events
  - tourist accommodation.
  
- 3 Development listed as non-complying is generally inappropriate.
  
- 4 Core Areas, Transition Areas and other identified features should be developed in accordance with the relevant [Concept Plan Map WeTo/26 – Morphettville Racecourse](#).

- 5 Core Areas should be developed to include a range of land uses that are high pedestrian generators, directly promote public transport use and provide opportunities for multi-purpose trips.
- 6 Core Areas should incorporate integrated public open spaces, sport and recreation facilities, and community areas that act as social hubs for communal activity.
- 7 Except in Core Areas where a higher intensity of development is envisaged, non-residential development should comprise uses that:
  - (a) are of local or neighbourhood scale
  - (b) encourage walking to local shopping, community services and other activities
  - (c) do not detrimentally impact on the amenity of nearby residents.
- 8 Shop or groups of shops should have the following gross leasable areas in the order of:

Designated area	Shop or group of shops (square metres)
Core Area	5500
Transition Area	500

### **Form and Character**

- 9 Development should be consistent with the desired character for the zone.
- 10 Residential development (other than residential development in mixed use buildings), should achieve a minimum net residential site density in accordance with the following, except where shown on [Concept Plan Map WeTo/26 – Morphettville Racecourse](#):

Designated area	Minimum net residential site density
Core Area	200 dwellings per hectare net
Transition Area	70 dwellings per hectare net

- 11 In Transition Areas, development should deliver a built form that provides the transition between an intense core of development and neighbouring lower intensity development.

### **Design and Appearance**

- 12 In Core Areas:
  - (a) the ground and first floors of buildings should be built to dimensions including a minimum ceiling height of 4.5 metres to allow for adaptation to a range of land uses including shops, office and residential without the need for significant change to the building
  - (b) a minimum of 50 per cent of the ground floor primary frontage of buildings should be visually permeable, transparent or clear glazed to promote active street frontages and maximise passive surveillance.
- 13 Except in Core Areas, development of three or more storeys in height should ensure that:
  - (a) north-facing windows to habitable rooms of existing dwelling(s) on the same allotment, and on adjacent allotments, receive at least 3 hours of direct sunlight over a portion of their surface between 9.00 am and 3.00 pm on 21 June

- (b) ground level open space of existing buildings receives direct sunlight for a minimum of 2 hours between 9.00 am and 3.00 pm on 21 June to at least the smaller of the following:
  - (i) half of the existing ground level open space
  - (ii) 35 square metres of the existing ground level open space (with at least one of the area's dimensions measuring 2.5 metres).
- 14 Buildings should address public open space and defined pedestrian and cycle routes as illustrated in [Concept Plan Map WeTo/26 – Morphettville Racecourse](#).
- 15 Masonry fences should be no more than 1.2 metres in height to maintain sight lines between buildings and the street and to improve safety through passive surveillance.
- 16 Buildings should predominantly provide vehicle access via a side street or rear lane access way.
- 17 Side streets and rear lane access ways should be designed to:
  - (a) provide space between buildings that reduces building mass and creates a more interesting public realm
  - (b) achieve active frontages at a lower intensity than the primary street frontage.

## Building Envelope

### Building Height

- 18 Except where airport building height restrictions prevail, building heights (excluding any rooftop located mechanical plant or equipment) should be consistent with the following parameters:

Designated area	Minimum building height	Maximum building height
Core Area	4 storeys	12 storeys and up to 48.5 metres
Transition Area	2 storey	4 storeys and up to 16.5 metres

### Setbacks from the Primary Road Frontage

- 19 Buildings (excluding verandas, porticos and the like) should be set back from the primary road frontage in accordance with the following parameters:

Designated area	Minimum setback from the primary road frontage
Core Area	No minimum
Transition Area	2 metres

### Setbacks from side boundaries

- 20 Buildings (excluding verandas, porticos and the like) should be set back from side boundaries in accordance with the following parameters:

Designated area	Minimum setback from side boundaries
Core Area	No minimum
Transition Area	0.9 metres

### Other Setbacks

- 21 Buildings (excluding verandas, porticos and the like) should be set back in accordance with the following parameters:

Setback parameter	Value
Minimum setback from secondary road frontage	0.9 metres
Minimum setback from a rear access way	No minimum where the access way is 6.5 metres or more OR Where the access way is less than 6.5 metres in width, the distance equal to the additional width required to make the access way 6.5 metres or more, to provide adequate manoeuvrability for vehicles
Minimum setback from the rear allotment boundary	6 metres where the subject land directly abuts an allotment of a different zone 3 metres in all other cases, except where development abuts the wall of an existing or simultaneously constructed building on the adjoining land

### Land Division

- 22 A traditional street grid pattern should be reinforced in any comprehensive development of areas for mixed use activity to maintain clear sightlines and ensure maximum connectivity.
- 23 Wherever practicable, land division and site amalgamation should:
- create allotments that vary in size and are suitable for a variety of residential and commercial uses
  - improve the level of integration associated with the design and layout of buildings, vehicle parking areas, access points and landscaping, including retention of existing trees.

## PROCEDURAL MATTERS

### Complying Development

Complying developments are prescribed in Schedule 4 of the Development Regulations 2008.

### Non-complying Development

Development (including building work, a change in the use of land or division of an allotment) involving any of the following is non-complying:

Form of development	Exceptions
Fuel depot	
General industry	
Public service depot	
Road transport terminal	
Service trade premises	
Special industry	



Form of development	Exceptions
Store	
Transport depot	
Warehouse	
Waste reception, storage, treatment or disposal	

## Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

In addition, the following forms of development, or any combination of (except where the development is classified as non-complying), are designated:

Category 1	Category 2
Advertisement	All forms of development not listed as Category 1.
Aged persons accommodation	
All forms of development that are ancillary and in association with residential development	
All forms of development that are ancillary and in association with racecourse activities within the Core Area, identified on identified on <a href="#">Concept Plan Map WeTo/26 – Morphettville Racecourse</a>	
Consulting room	
Dwelling	
Educational establishment	
Emergency Services	
Hotel where located within the Core Area	
Indoor recreation facility	
Nursing home	
Office	
Pre-school	
Primary school	
Residential flat buildings	
Retirement village	
Shop or group of shops where located:	
(a) within the Core Area of less than 5500 square metres	
(b) within the Transition Area of less than 500 square metres.	
Supported accommodation	
Tourist accommodation	

## Urban Corridor Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

### OBJECTIVES

- 1 A mixed use zone accommodating a range of compatible non-residential and medium and high density residential land uses orientated towards a high frequency public transport corridor.
- 2 Integrated, mixed use, medium and high rise buildings with ground floor uses that create active and vibrant streets with residential development above.
- 3 A mix of land uses that enable people to work, shop and access a range of services close to home.
- 4 Adaptable and flexible building designs that can accommodate changes in land use and respond to changing economic and social conditions.
- 5 A built form that provides a transition down in scale and intensity at the zone boundary to maintain the amenity of residential properties located within adjoining zones.
- 6 A safe, comfortable and appealing street environment for pedestrians that is sheltered from weather extremes, is of a pedestrian scale and optimises views or any outlook onto spaces of interest.
- 7 Noise and air quality impacts mitigated through appropriate building design and orientation.
- 8 To identify and remediate contaminated land appropriate for its intended use.
- 9 Development that contributes to the desired character of the zone.

### DESIRED CHARACTER

This zone will contain an innovative mix of medium density (45-70 dwellings per hectare) and high density (70-200 dwellings per hectare) residential development, together with community and employment land uses, along the Port Road, Anzac Highway, Richmond Road and Henley Beach Road corridors. The combination of land uses will vary within these corridors. Some locations will contain a genuine land use mix with ground floor shops, restaurants and offices, and upper level residential, while other areas will give primacy to residential development. Other parts of the zone will have a strong employment focus.

The function of main roads in the zone, particularly Port Road, Richmond Road and Anzac Highway, as major transport corridors will be protected by providing access to allotments from secondary road frontages and rear access ways as much as possible. Parking areas will be consolidated, shared (where possible) and screened from the street or public spaces. Allotments with car parking fronting Port Road, Anzac Highway, Richmond Road and Henley Beach Road will be redeveloped with built form closer to the road and reconfigured car parking areas.

As one of the key zones in the City of West Torrens where there will be transformation in built form, new buildings will be recognised for their design excellence. These buildings will establish an interesting pedestrian environment and human-scale at ground level through careful building articulation and fenestration, verandas, balconies, canopies and landscaping. In general, the greatest height, mass and intensity of development will be focussed at the main road frontage. Buildings of 3 or more storeys will be the predominant built form. It is for these reasons that dwellings other than detached dwellings will be the predominant form of residential development.

Overlooking, overshadowing and noise impacts will be moderated through careful design. Impacts on adjoining zones where development is lower in scale and intensity will be minimised through transition of building heights and setbacks, judicious design and location of windows and balconies, and the use of landscaping. The transition of building heights and setbacks, and judicious design is especially important adjacent Character Policy Areas, including those Character Policy Areas at Glandore and Ashford. The use of blank walls in these transitional areas, especially at the rear and side of allotments, will be avoided. Plant and service equipment will be enclosed and screened from view from the street and neighbouring allotments.

Where buildings are set back from main roads, landscaping will contribute to a pleasant pedestrian environment and provide an attractive transition between the public and private realm. Large scale development in the zone will facilitate the establishment of areas of communal and public open space, and create links with existing movement patterns and destinations in the zone. Front fencing in the zone will be kept low and/or visually permeable.

Some parts of the zone, including allotments in Thebarton and Keswick, are potentially contaminated because of previous and current industrial activities. In these circumstances, development is expected to occur on a precautionary basis if site contamination investigations identify potential site contamination, particularly where it involves sensitive uses such residential development.

The Thebarton brewery has potential to cause nuisance to future users and residents within this zone through noise and odour. To mitigate potential adverse impacts, residential development north of Smith Street that is likely to be sensitive to brewery operations should generally be avoided unless interface mitigation measures have been implemented (or will be implemented within an acceptable period) such that the anticipated impacts are within acceptable limits.

Noise and air amenity with the zone is not expected to be equivalent to that expected from living in a purely residential zone.

## **PRINCIPLES OF DEVELOPMENT CONTROL**

### **Land Use**

- 1 The following types of development, or combination thereof, are envisaged in the zone:
  - affordable housing
  - aged persons accommodation
  - community centre
  - consulting room
  - dwelling
  - educational establishment
  - entertainment venue
  - licensed premises
  - office
  - pre-school
  - primary school
  - residential flat building
  - retirement village
  - shop or group of shops
  - supported accommodation
  - tourist accommodation.
- 2 Development listed as non-complying is generally inappropriate.
- 3 Residential development on land within the zone north of Smith Street should be avoided unless interface measures for noise and odour have been implemented (or will be implemented within an acceptable period) at the source such that the anticipated impacts are within acceptable limits.

### **Form and Character**

- 4 Development should be consistent with the desired character for the zone.

- 5 Residential development (other than residential development in mixed use buildings on allotments less than 5000 square metres), should achieve a minimum net residential allotment density in accordance with the following:

Policy Area	Minimum net residential site density
Boulevard Policy Area 34	100 dwellings per hectare net
High Street Policy Area 35	70 dwellings per hectare net
Transit Living Policy Area 36	45 dwellings per hectare net
Business Policy Area 37	No minimum

- 6 Vehicle parking should be located to the rear of development or not be visible from public land along the primary road frontage.

## Design and Appearance

- 7 Buildings should maintain a pedestrian scale at street level, and should:
- (a) include a clearly defined podium, or street wall with a parapet, and a maximum building height of 2 storeys from natural ground level
  - (b) have levels above the defined podium or street wall setback a minimum of 2 metres from that wall.
- 8 Buildings on allotments with a frontage greater than 10 metres should be well articulated through variations in forms, materials, openings and colours.
- 9 Buildings should be designed to:
- (a) enable suitable sunlight access to public open space
  - (b) overlook or orientate towards public open space and defined pedestrian and cycle routes.
- 10 To maintain sight lines between buildings and the street, and to improve safety through passive surveillance, solid fencing should not be constructed between the front building line and the primary or secondary street.
- 11 Development should minimise the number of access points onto an arterial road, by providing vehicle access:
- (a) from side streets or rear access ways
  - (b) via co-ordinated through-property access rights of way or common rear vehicle parking areas.
- 12 Vehicle access points on side streets and rear access ways should be located and designed to:
- (a) minimise the impacts of headlight glare and noise on nearby residents
  - (b) avoid excessive traffic flows into residential streets.

## Building Envelope

### Building Height

- 13 Except where airport building height restrictions prevail, ~~or~~ the interface height provisions require a lesser height, building heights (excluding any rooftop mechanical plant or equipment) should be consistent with the following parameters:

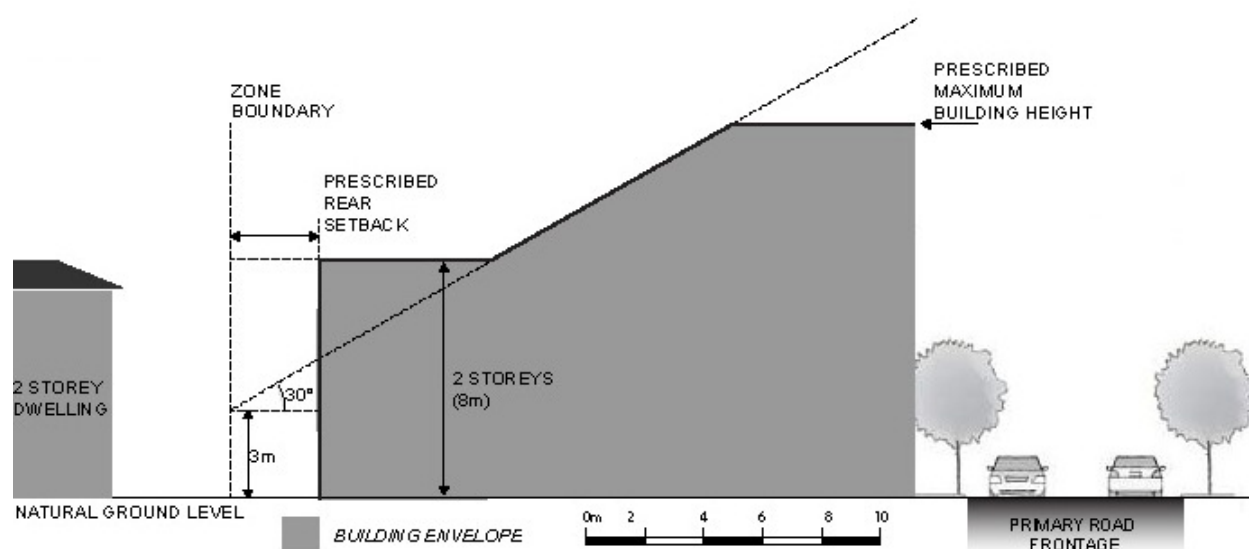
Policy area	Maximum building height (above natural ground height)
<b>Boulevard Policy Area 34</b>	Allotments abutting <b>Residential Character Glandore Policy Area 24</b> , and allotments between Syme Street and South Road: 3 storeys and 12.5 metres  All other allotments: 8 storeys and up to 32.5 metres
<b>High Street Policy Area 35</b>	Allotments west of Marion Road: 3 storeys and up to 12.5 metres  Allotments between South Road and Marion Road: 4 storeys and up to 16.5 metres  All allotments east of South Road: 6 storeys and up to 24.5 metres
<b>Transit Living Policy Area 36</b>	Allotments adjoining Henley Beach Road west of Marion Road: 3 storeys and up to 12.5 metres  Allotments adjoining Henley Beach Road east of Marion Road: 4 storeys and up to 16.5 metres  All other allotments: 6 storeys and up to 24.5 metres
<b>Business Policy Area 37</b>	6 storeys and up to 24.5 metres

- 14 Building(s) on land that is directly adjacent to or facing the Adelaide Parklands should be a minimum height of 4 storeys.

#### Interface Height Provisions

- 15 Any portion of a development above two storeys (8 metres) in height should be constructed within a building envelope provided by a 30 degree plane measured from a point 3 metres above natural ground level at the zone boundary (except where this boundary is a primary road frontage), as illustrated in Figure 1, unless it is demonstrated that the proposed development minimises interface impacts including from building massing, overshadowing and overlooking with adjoining residential development:

Figure 1



- 16 To minimise overshadowing of sensitive development outside of the zone, buildings should ensure that:
- north-facing windows to habitable rooms of existing dwellings in adjacent zones receive at least 3 hours of direct sunlight over a portion of their surface between 9.00 am and 3.00 pm on 21 June

- (b) ground level open space of existing residential buildings in adjacent zones receive direct sunlight for a minimum of 2 hours between 9.00 am and 3.00 pm on 21 June to at least the smaller of the following:
- (i) half of the existing ground level open space
  - (ii) 35 square metres of the existing ground level open space (with at least one of the area's dimensions measuring **no less than 2.5 metres**)
- (c) sunlight to solar panels should be maintained for a minimum of 2 consecutive hours between 9.00 am and 3.00 pm on 22 June.

### Setbacks from Road Frontages

- 17 Buildings (excluding verandahs, porticos and the like) should be set back from the primary road frontage in accordance with the following parameters:

Policy area	Minimum setback from the primary road frontage where it is Port Road, Anzac Highway, Richmond Road or Henley Beach Road	Minimum setback from the primary road frontage in all other cases
<b>Boulevard Policy Area 34</b>	No minimum at Port Road 3 metres at Anzac Highway	2 metres
<b>High Street Policy Area 35</b>	No minimum	2 metres
<b>Transit Living Policy Area 36</b>	3 metres	3 metres
<b>Business Policy Area 37</b>	3 metres	3 metres

- 18 Buildings (excluding verandahs, porticos and the like) should be set back from the secondary road frontage or a vehicle access way in accordance with the following parameters:

Designated Area	Minimum setback from secondary road	Minimum setback from a rear access way
<b>Boulevard Policy Area 34</b>	No minimum	No minimum where the access way is 6.5 metres <b>wide</b> or more  OR Where the access way is less than 6.5 metres in width, the distance equal to the additional width required to make the access way 6.5 metres or more, to provide adequate manoeuvrability for vehicles
<b>High Street Policy Area 35</b>	No minimum	No minimum where the access way is 6.5 metres <b>wide</b> or more  OR Where the access way is less than 6.5 metres in width, the distance equal to the additional width required to make the access way 6.5 metres or more, to provide adequate manoeuvrability for vehicles

Designated Area	Minimum setback from secondary road	Minimum setback from a rear access way
<b>Transit Living Policy Area 36</b>	2 metres	No minimum where the access way is 6.5 metres wide or more  OR Where the access way is less than 6.5 metres in width, the distance equal to the additional width required to make the access way 6.5 metres or more, to provide adequate manoeuvrability for vehicles
<b>Business Policy Area 37</b>	2 metres	No minimum where the access way is 6.5 metres wide or more  OR Where the access way is less than 6.5 metres in width, the distance equal to the additional width required to make the access way 6.5 metres or more, to provide adequate manoeuvrability for vehicles

#### Other Setbacks

- 19 Buildings (excluding verandahs, porticos and the like) should be set back in accordance with the following parameters:

Designated area	Minimum setback from rear allotment boundary	Minimum setback from side boundaries (where not on a street boundary)
<b>Boulevard Policy Area 34</b>	3 metres where the subject land directly abuts an allotment of a different zone  No minimum in all other cases	For allotments with a frontage width of 20 metres or less: no minimum up to a height of 2 storeys and 3 metres above this height.  For allotments with a frontage width of more than 20 metres: 3 metres.
<b>High Street Policy Area 35</b>	3 metres where the subject land directly abuts an allotment of a different zone  No minimum in all other cases	No minimum
<b>Transit Living Policy Area 36</b>	3 metres where the subject land directly abuts an allotment of a different zone  No minimum in all other cases	For allotments with a frontage width of 20 metres or less: no minimum up to a height of 2 storeys and 3 metres above this height  For allotments with a frontage width of more than 20 metres: 3 metres
<b>Business Policy Area 37</b>	3 metres where the subject land directly abuts an allotment of a residential zone  No minimum in all other cases	For allotments with a frontage width of 20 metres or less: no minimum up to a height of 2 storeys and 3 metres above this height  For allotments with a frontage width of more than 20 metres: 3 metres

## Vehicle Parking

- 20 Development should provide off-street vehicle parking and specifically marked accessible car parking places to meet anticipated demand in accordance with [Table WeTo/6 - Off Street Vehicle Parking Requirements for Designated Areas](#).
- 21 Loading areas and designated parking spaces for service vehicles should:
  - (a) be provided within the boundary of the allotment
  - (b) not be located where there is parking provided for any other purpose.
- 22 Vehicle parking spaces and multi-level vehicle parking structures within buildings should:
  - (a) enhance active street frontages by providing land uses such as commercial, retail or other non-car park uses along ground floor street frontages
  - (b) complement the surrounding built form in terms of height, massing and scale
  - (c) incorporate facade treatments along major street frontages that are sufficiently enclosed and detailed to complement neighbouring buildings consistent with the desired character of the locality.
- 23 In mixed use buildings, the provision of vehicle parking may be reduced in number and shared where the operating hours of commercial activities complement the residential use of the allotment.

## Land Division

- 24 Land division in the zone is appropriate provided new allotments are of a size and configuration to ensure the objectives of the zone can be achieved.



## Boulevard Policy Area 34

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 Medium and high rise development framing the street, including mixed use buildings that contain shops, offices and commercial development at lower floors with residential land uses above.
- 2 A uniform streetscape edge established through a largely consistent front setback and tall, articulated building façades.
- 3 Development that does not compromise the transport functions of the road corridor.
- 4 Development that contributes to the desired character of the policy area.

### DESIRED CHARACTER

The policy area will contain a mix of land uses that complement the function of Port Road as a strategic transport route linking central Adelaide with the north western suburbs, and Anzac Highway linking central Adelaide with Glenelg.

The redevelopment of existing commercial and industrial allotments into medium-to-high scale, mixed-use development will occur. Where development has a mix of land uses, non-residential activities such as shops, offices and consulting rooms will be located on lower levels with residential land uses above. In order to achieve the desired transformation of the policy area, dwellings other than detached dwellings will be the predominant form of residential development.

A mix of complementary land uses will assist in extending the usage of the policy area beyond normal working hours to enhance its vibrancy and safety.

Development will take place at medium and high densities, at a scale that is proportionate to the width of Port Road and Anzac Highway respectively. To achieve this, development will take place on large, often amalgamated allotments. Vehicle access points will be located off side streets and new rear laneways where possible, so that vehicle flows, safety and efficient pedestrian movement along Port Road and Anzac Highway are maintained.

Pedestrian areas will be enhanced to maximise safety and strong links will be made between development and tram stops along Port Road, and Bonython Park.

While the use and address of buildings will be designed to be easily interpreted when driving in a vehicle, the footpath will be sheltered with awnings, verandas and similar structures.

Buildings of up to eight storeys will have a strong presence to Port Road and Anzac Highway. At lower levels, buildings will have a human scale through the use of design elements such as balconies, verandas and canopies. Development on corner allotments will enhance the gateway function of such corners by providing strong, built-form edges combined with careful detailing at a pedestrian scale to both street frontages.

Podium elements, where higher floors of the building are set back further than lower level floors, may be used to improve air quality (through greater air circulation), as well as enhancing solar access, privacy and outlook for both the residents of the building and neighbours.

Buildings along Port Road will have zero setback from the front boundary in order to establish a strong and imposing presence to the road, while short front setbacks along Anzac Highway will allow for some landscaping to contribute to a more open landscaped character.

On-site vehicle parking will not be visible from Port Road and Anzac Highway, by locating parking areas behind building façades and shielding undercroft parking areas with landscaping and articulated screens.

## PRINCIPLES OF DEVELOPMENT CONTROL

### Land use

- 1 Development should predominantly comprise mixed use buildings, with non-residential development at the ground and first floor and residential development above, and wholly residential buildings.
- 2 Residential Development should create a medium-to-high density urban environment incorporating residential flat buildings and dwellings in mixed-use buildings, and not lower density residential development such as detached dwellings.

### Form and Character

- 3 Development should be consistent with the desired character for the policy area.
- 4 Shops or groups of shops contained in a single building should have a gross leasable area of less than 2000 square metres.
- 5 The ground floor of buildings should be built to dimensions including a minimum floor to ceiling height of 4.5 metres to allow for adaptation to a range of land uses including retail, office and residential without the need for significant change to the building.
- 6 A minimum of 50 per cent of the ground floor primary frontage of buildings should be visually permeable, transparent or clear glazed to promote active street frontages and maximise passive surveillance.

## High Street Policy Area 35

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 A mix of land uses including retail, office, commercial, community, civic and medium and high density residential development that support the economic vitality of the area.
- 2 Buildings sited to provide a continuous and consistent built edge with verandas / awnings over the public footpath and an intimate built scale, with fine-grained detailing of buildings in the public realm.
- 3 An interesting and varied skyline as viewed from the street and afar, provided by modulation in roof forms and the use of parapets.
- 4 An intimate public realm with active streets created by buildings designed with frequently repeated frontage form and narrow tenancy footprints.
- 5 A high degree of pedestrian activity and a vibrant street-life with well lit and engaging shop fronts and business displays including alfresco seating and dining facilities and licensed areas.
- 6 Development that contributes to the desired character of the policy area.

### DESIRED CHARACTER

The policy area will predominantly contain a mix of retail, offices, commercial, community and medium density residential development. The mix of complementary land uses will assist in extending the usage of the policy area beyond normal working hours to enhance its vibrancy and safety.

Development will take place at medium densities. The fine grained subdivision pattern, where allotments are narrow but deep, will be maintained to encourage development that supports a variety of tenancies and provides visual interest. Where there are larger/wider allotments, built-form will reinforce this desirable fine grained appearance of older development in the policy area (i.e. many, separate, narrower tenancies). In order to achieve the desired transformation of the policy area, dwellings other than detached dwellings will be the predominant form of residential development.

Where development has a mix of land uses, non-residential activities such as shops, offices and consulting rooms will be located on lower levels with residential land uses above.

The public realm along Henley Beach Road will provide a comfortable and interesting place for pedestrians. To achieve this, development will shelter the footpath with verandas, awnings and similar structures, as well as providing frequent pedestrian entries and clear windows to the street. Buildings will be built with zero set back from the main street, with the occasional section of building set further back to create intimate but active spaces for outdoor dining and interesting building entrances. Buildings west of Marion Road will have a maximum height of 3 storeys and between Marion Road and South Road buildings will have a maximum height of 4 storeys. East of South Road buildings will be 3- 6 storeys and these buildings will include parapets at lower levels to create a clear demarcation between the taller levels (4-6 storeys in total), which will be setback further from Henley Beach Road.

Buildings and structures within **Historic Conservation Areas** identified on the *Overlay Map WeTo/4 - Heritage* and *Overlay Map WeTo/5 - Heritage* will be adapted and reused while maintaining their heritage qualities, with development encouraged towards the rear and behind the front facades. Buildings adjacent to State Heritage places, Local Heritage places and contributory items will contain design elements and building materials that are complementary to such buildings.

Vehicle access points will be located off side streets and new rear laneways so that vehicle flows, safety and efficient pedestrian movement on Henley Beach Road are maintained. In many cases vehicle access points and car parking areas will be shared. On-site vehicle parking will not be visible from Henley Beach Road through the use of design solutions such as locating parking areas behind the front building façade and shielding undercroft parking areas with landscaping and articulated screening.

## **PRINCIPLES OF DEVELOPMENT CONTROL**

### **Land Use**

- 1 Parts of a development that abut the primary road corridor should provide continuity of ground floor shops, offices and other non-residential land uses.
- 2 Shops or groups of shops contained in a single building, other than a restaurant, should have a gross leasable area of less than 2000 square metres.

### **Form and Character**

- 3 Development should be consistent with the desired character for the policy area.
- 4 Pedestrian shelter and shade should be provided over footpaths through the use of structures such as awnings, canopies and verandas.
- 5 The ground level street frontages of buildings should contribute to the appearance and retail function of the area by providing at least 5 metres or 60 per cent of the street frontage (whichever is greater) as an entry/ foyer or display window to a shop (including a café or restaurant) or other community or commercial use which provides pedestrian interest and activation.
- 6 The ground floor of buildings should be built to dimensions including a minimum floor to ceiling height of 3.5 metres to allow for adaptation to a range of land uses including retail, office and residential without the need for significant change to the building.
- 7 A minimum of 50 per cent of the ground floor primary frontage of buildings should be visually permeable, transparent or clear glazed to promote active street frontages and maximise passive surveillance.

## Transit Living Policy Area 36

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 A medium density residential area supported by local shops, offices and community land uses.
- 2 A highly varied built streetscape allowing multiple built form design responses that support innovative housing and mixed use development.
- 3 Development that contributes to the desired character of the policy area.

### DESIRED CHARACTER

The policy area will contain primarily medium density residential development, together with some local shops, offices and community land uses to support the daily needs of residents. Larger retail formats may be appropriate on prominent or large-scale sites where proposed as part of a coordinated, mixed-use development with a residential focus. This includes the former TAFE site bound by Richmond Road, Sutton Terrace and Desmond Avenue at Marleston. There will be a variety of building forms and design, creating housing opportunity for people of various life stages and accommodating a variety of small businesses that do not compromise residential amenity.

Development will take place at medium densities. This will result in the replacement of existing detached dwellings at low to very low density with row dwellings and residential flat buildings, possibly involving allotment amalgamation. Vehicle access will occur from side streets and new rear laneways where possible.

Safe and efficient pedestrian movement along arterial roads and associated transport networks and facilities will be supported by limiting vehicle access points to allotments from side streets and new rear laneways where possible. This will also support the retention/planting of street trees on arterial roads.

New buildings will contribute to a highly varied built streetscape, allowing multiple built form design responses that support innovative housing and mixed-use development. Buildings in the part of the policy area around Henley Beach Road will be up to 3 storeys west of Marion Road and 4 storeys east of Marion Road. Buildings in the part of the policy area along Richmond Road will be up to 6 storeys in height toward Richmond Road, transitioning down in height to provide a 2 to 3 storey building interface toward adjacent residential areas and local streets. Balconies and windows will face the street to provide passive surveillance. Parking and garages will be located behind the front façade of buildings. State heritage places and Local heritage places will be adapted, maintaining their heritage qualities with redevelopment occurring to the rear and behind the front facades. Buildings adjacent to these heritage buildings will contain design elements and building materials that are complementary to such buildings.

Development will be interspersed with landscaping, particularly behind the main street frontage, to soften the appearance of buildings from the street and reduce heat load in summer.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land use

- 1 Shops or groups of shops contained in a single building should have a gross leasable area of less than 500 square metres, except where located on sites greater than one hectare fronting Richmond Road.

**Form and Character**

- 2 Development should be consistent with the desired character for the policy area.
- 3 Development should be undertaken in accordance with [Concept Plan Map WeTo/2 – Prominent Development Sites](#).

## Business Policy Area 37

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 A mixed use business policy area that accommodates a range of commercial and light industrial land uses together with compatible medium and high density residential development.
- 2 Development that minimises any adverse impacts upon the amenity of the locality within the zone.
- 3 A high standard of development which promotes distinctive building, landscape and streetscape design, with high visual and environmental amenity.
- 4 Development that contributes to the desired character of the policy area.

### DESIRED CHARACTER

The policy area will have a strong employment focus, containing predominantly shops, offices, commercial activities such as research establishments and bioscience, and light industrial uses, together with residential development. A mix of complementary land uses will assist in extending the usage of the policy area beyond normal working hours to enhance its vibrancy and safety.

Parking will be predominantly at the rear or underneath buildings. Vehicle access points will be located off side streets and rear laneways where possible, so that vehicle flows, safety and efficient pedestrian movement are achieved, acknowledging that there may be some parking in front of buildings for short-term use. In many cases, vehicle access points and car parking areas will be shared.

Large buildings between 3 to 6 storeys in height will occur. Development will be varied in form to accommodate a mix of land uses, although in all cases will use design elements such as balconies, variation in the texture of building materials and articulation of facades will contribute to an interesting and attractive pedestrian environment. The footpath will be sheltered with awnings, verandas and street trees, which will also soften the appearance of buildings.

State heritage places, local heritage places and contributory items will be adapted, maintaining their heritage qualities with redevelopment occurring to the rear and behind the front facades. Buildings adjacent to these heritage buildings will contain design elements and building materials that are complementary to such buildings.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following types of development or combination thereof, are envisaged in the Business Policy Area and are additional to those identified in the zone:
  - dwelling
  - office
  - light industry
  - service industry
  - service trade premises
  - shop.
- 2 Land uses on the ground floor of buildings should be non-residential.

- 3 Shops or groups of shops should have a gross leasable area of less than 500 square metres.
- 4 Light industry should comprise high technology and/or research and development related uses.

**Form and Character**

- 5 Development should be consistent with the desired character for the policy area.
- 6 Vehicle parking should be predominantly located at the rear or underneath buildings with limited short-term parking located to the front.
- 7 The ground floor of buildings should be built to dimensions including a minimum floor to ceiling height of 3.5 metres to allow for adaptation to a range of land uses including retail, office and residential without the need for significant change to the building.
- 8 A minimum of 50 per cent of the ground floor primary frontage of buildings should be visually permeable, transparent or clear glazed to promote active street frontages and maximise passive surveillance.



## PROCEDURAL MATTERS

### Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

In addition, the following forms of development (except where the development is non-complying) are complying:

- 1 Subject to the conditions contained in [Table WeTo/1 - Conditions for Complying Development](#) and [Table WeTo/6 - Off-street Vehicle Parking Requirements for Designated Areas](#):
  - (a) change in the use of land, from residential to office on the ground or first floor of a building
  - (b) change in the use of land from residential to shop less than 250 square metres on the ground floor of a building.
- 2 A change of use to a shop, office, consulting room or any combination of these uses where all of the following are achieved:
  - (a) the area to be occupied by the proposed development is located in an existing building and is currently used as a shop, office, consulting room or any combination of these uses
  - (b) the development is located inside any of the following area(s):
    - **High Street Policy Area 35**
  - (c) the building is not a State heritage place
  - (d) it will not involve any alterations or additions to the external appearance of a local heritage place as viewed from a public road or public space
  - (e) if the proposed change of use is for a shop that primarily involves the handling and sale of foodstuffs, it achieves either (i) or (ii):
    - (i) all of the following:
      - (A) areas used for the storage and collection of refuse are sited at least 10 metres from any **Residential Zone** boundary or a dwelling (other than a dwelling directly associated with the proposed shop)
      - (B) if the shop involves the heating and cooking of foodstuffs in a commercial kitchen and is within 30 metres of any **Residential Zone** boundary or a dwelling (other than a dwelling directly associated with the proposed shop), an exhaust duct and stack (chimney) exists or is capable of being installed for discharging exhaust emissions
    - (ii) the development is the same or substantially the same as a development, which has previously been granted development approval under the *Development Act 1993* or any subsequent Act and Regulations, and the development is to be undertaken and operated in accordance with the conditions attached to the previously approved development
  - (f) if the change in use is for a shop with a gross leasable floor area greater than 250 square metres and has direct frontage to an arterial road, it achieves either (i) or (ii):
    - (i) the primary vehicle access (being the access where the majority of vehicles access / egress the site of the proposed development) is from a road that is not an arterial road

- (ii) the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared
- (g) off-street vehicular parking is provided in accordance with the rate(s) specified in [Table WeTo/2 - Off Street Vehicle Parking Requirements](#) or the desired minimum rate in [Table WeTo/6 - Off-street Vehicle Parking Requirements for Designated Areas](#) (whichever table applies) to the nearest whole number, except in any one or more of the following circumstances:
  - (i) the building is a local heritage place
  - (ii) the development is the same or substantially the same as a development, which has previously been granted development approval under the *Development Act 1993* or any subsequent Act and Regulations, and the number and location of parking spaces is the same or substantially the same as that which was previously approved
  - (iii) the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared.

### Non-complying Development

Development (including building work, a change in the use of land or division of an allotment) involving any of the following is non-complying:

Form of development	Exceptions
Detached dwelling on sites fronting Port Road	Except where 4 or more storeys in height.
Industry	Except light industry or service industry located within the <b>Business Policy Area 37</b> .
Fuel depot	
Group dwelling on sites fronting Port Road	Except where 4 or more storeys in height.
Petrol filling station	
Public service depot	
Road transport terminal	
Semi-detached dwelling on sites fronting Port Road	Except where 4 or more storeys in height.
Service trade premises	
Store	Except where located within the <b>Business Policy Area 37</b> .
Transport depot	
Waste reception storage treatment and disposal	

## Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

In addition, the following forms of development, or any combination of (except where the development is classified as non-complying), are designated:

Category 1	Category 2
Advertisement	All forms of development not listed as Category 1
Aged persons accommodation	Any development listed as Category 1 and located on adjacent land to a <b>Residential Zone</b> or <b>Historic Conservation Area</b> that:
All forms of development that are ancillary and in association with residential development	(a) is 3 or more storeys, or 11.5 metres or more, in height
Consulting room	(b) exceeds the 'Building Envelope - Interface Height Provisions'.
Dwelling	
Educational establishment	
Light industry within the <b>Business Policy Area 37</b>	
Office	
Pre-school	
Primary school	
Residential flat building	
Retirement village	
Supported accommodation	
Shop or group of shops located in the <b>High Street Policy Area 35</b>	
Shop or group of shops with a gross leasable area of 500 square metres or less located within the <b>Business Policy Area 37</b> or <b>Transit Living Policy Area 36</b>	
Shop or group of shops with a gross leasable area of 2000 square metres or less located within the <b>Boulevard Policy Area 34</b>	
Tourist accommodation	
Warehouse within the <b>Business Policy Area 37</b>	

## Urban Renewal Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

### OBJECTIVES

- 1 A zone providing for medium density residential development comprising a range of dwelling types and forms, integrated with a range of non-residential uses and usable open spaces that may also have a dual purpose for stormwater management.
- 2 The orderly and efficient redevelopment of existing and transitioning industrial activities to land uses that are compatible with residential development and other sensitive uses.
- 3 Redevelopment and intensification of urban areas to support the effective and economic provision of public infrastructure and community services.
- 4 High quality public realm through the design and layout of development to encourage walking and cycling access to, and use of, open space, activity centres, fixed transit and high frequency public transport stops.
- 5 A built form that provides a transition down in scale and intensity between those areas where higher building heights are envisaged and to maintain the amenity of adjacent residential properties.
- 6 The minimisation of environmental impacts upon human health, local amenity and the environment.
- 7 Development that contributes to the desired character of the zone.

### DESIRED CHARACTER

A vibrant and rejuvenated, medium density neighbourhood, offering diverse residential, affordable housing and mixed use housing choice as well as employment opportunities in close proximity to public transport, services and the River Torrens Linear Park.

Currently, the zone contains a mixture of land uses with a strong focus on non-residential activities such as industry, warehouse, storage and service industry. However, the zone also contains a significant number of residential properties – particularly along Wilford Avenue.

Over time, existing industries and other commercial activities in the zone will transition to medium density residential and compatible mixed use development and older housing stock will be redeveloped.

Non-residential land uses of a small-scale that serve the community and are of a nature and scale consistent with the character of the locality will be encouraged. In particular, non-residential land uses will be single storey in height (or be located on the ground floor of a multi-storey building) and will feature setbacks, building design and floor areas which complement surrounding residential development. Non-residential land uses will be integrated with surrounding residential uses and areas of open space in order to provide a mixture of small shops, offices, consulting rooms, pre-schools, cafes and restaurants (providing outdoor eating places).

In addition to the range of small-scale non-residential land uses envisaged in the zone, a wider range of larger-scale non-residential land uses will be located in the **Mixed Use Transition Policy Area 38** and will primarily contain existing industries and commercial activities. Within this policy area, existing uses will continue together with a range of low-impact mixed-use development. Over time, a transition to medium density residential development will occur where environmental impacts and site contamination issues have been addressed.

Development will address interface issues associated with existing non-residential land uses in the zone as well as nearby industrial land uses to the north of Ashwin Parade through the application of buffers and/or design techniques to mitigate potential impacts associated with noise, light, dust, traffic, odour and air quality.

Development will allow for variety in housing forms and styles. The zone will comprise primarily medium density residential development of up to 2 storeys. Taller buildings up to 3 storeys and development achieving a net residential density of around 35 to 65 dwellings per hectare, will be located near the centre of the zone between Sherriff Street and Hardys Road and north of Wilford Avenue.

Within this area, development will be in accordance with [Concept Plan WeTo/25 - Underdale Urban Renewal](#), including higher density mixed use apartments (that may incorporate ground floor retailing and commercial uses) that are adjacent public open space. Development should be set back from Hardys Road and Sherriff Street. The provision of public open space, shared access ways and local road connections will be in accordance with [Concept Plan WeTo/25 - Underdale Urban Renewal](#). Local street connections will enable local and indirect vehicle, bicycle and pedestrian connectivity through the area.

Generally, the greatest height, mass and intensity of development will be focussed near the centre of the zone, reducing in scale to transition down at the interface with low rise residential development within the adjoining **Residential Zone**.

Housing diversity will be delivered through land division layout comprising smaller allotments together with innovative housing design, access and parking arrangements. Larger and/or amalgamated sites will achieve a coordinated development outcome where new medium density housing integrates smoothly with surrounding residential development. Medium rise residential development in the order of two storeys, at the interface of low rise residential development, should through good design, sympathetic scale and the location of windows and balconies, ensure transitioning down in scale thereby limiting the extent of overlooking and overshadowing.

Development in the zone will proceed based on the ability of stormwater related public infrastructure to accommodate the anticipated additional demands being placed on it. Anticipated upgrades of stormwater infrastructure may need to be provided in order to meet future demands created by new, infill development.

There are sites within the zone that are known to be affected by potentially contaminating activities or which have the potential to impact on new development through the emission of noise, dust, light or odour. In particular, the zone contains sites that are licensed by the Environment Protection Authority (EPA) under the *Environment Protection Act 1993*. The EPA licence sites and activities which do not require an EPA licence may have recommended evaluation distances and new development within these distances will ensure that potential impacts from the licenced activity have been appropriately addressed. In addition, sensitive development will take into account separation distances to existing activities which may have been imposed through other forms of legislation such as those administered by SafeWork SA.

Offsite contamination may also exist on adjacent or nearby land, which may impact the use of land within the zone. Appropriate investigations will confirm the potential for site contamination and, where applicable, the extent of such contamination. Development of sites or areas affected by actual or potential site contamination will not proceed unless appropriate investigations and remediation (where required) has been undertaken. This may include in some situations, a Preliminary Site investigation (PSI), Detailed Site Investigation (DSI) or a Site Contamination Audit Report (SCAR) being prepared.

The zone will be enhanced through high quality development and associated improvements to the public realm. Redevelopment of existing sites will occur using high quality design, materials and vegetation (including green walls) to significantly improve the site's appearance. In the public realm, and particularly at the interface with the adjoining **Residential Zone**, it is desirable to use trees and verge landscaping, swales, rain gardens, parks, parklets and other green infrastructure that complement and enhance the visual impacts of buildings and structures, and add to overall amenity of the zone. An increased building setback from Hardys Road and the northern side of Ashley Street will allow for future street infrastructure upgrades.

## PRINCIPLES OF DEVELOPMENT CONTROL

### Land Use

- 1 The following types of development, or combination thereof, are envisaged in the zone:
  - affordable housing
  - aged persons accommodation
  - community centre
  - consulting room
  - domestic outbuilding
  - dwelling(s)
  - educational establishment
  - office
  - pre-school
  - primary school
  - recreation facilities
  - residential flat building
  - retirement village
  - shop or group of shops
  - supported accommodation
  - tourist accommodation.
- 2 Development should not be undertaken unless it is consistent with the desired character for the zone.
- 3 Vacant or underutilised land should be developed in an efficient and co-ordinated manner.
- 4 A range of residential development types, formats and sizes is encouraged at increased densities to support a diverse population.
- 5 Sensitive development, within the evaluation distance of an active *Environment Protection Act 1993* licenced activity should seek to mitigate impacts of noise and air quality including, odour from the licenced activity, through built form, landscaping, design and orientation.
- 6 Non-residential development should mitigate impacts from visual appearance, building bulk and scale, overshadowing, noise, vibration, chemical over-spray, air quality, odour, dust, hours of operation and on-street parking.
- 7 Sensitive development that increases the overall net density (other than non-habitable structures such as garages, carports and the like) including land division for residential purposes, should not occur where the site(s) are located within the evaluation distance of an active licenced activity (under the *Environment Protection Act 1993*) unless, the following minimum impact requirements are met to mitigate potential risk to human health and the environment:

Type of Impact	Threshold Criteria
Noise	where noise levels measured at the boundary of the development site(s) not exceeding 57dB(A) between 7am and 10pm and 50dB(A) between 10pm and 7am measured and adjusted in accordance with the Environment Protection (Noise) Policy 2007
Odour	where odour measured at the boundary of the development site(s) not exceeding 2 OU measured in accordance with the Environment Protection (Air Quality) Policy 2016
Air quality	where air quality at the boundary of the development site(s) not exceeding maximum concentrations specified in Schedule 2 Clause 2 of the Environment Protection (Air Quality) Policy 2016 measured in accordance with that policy

- 8 Sensitive development, that increases the overall net density (other than non-habitable structures such as garages, carports and the like) including land division for residential purposes, should not occur where the site(s) are located within required separation or buffer distances, of other licenced activities as required by relevant legislation, to mitigate potential risk to human health.
- 9 Development that results in greater intensity of land use should ensure appropriate stormwater infrastructure is provided to meet the needs and consider implications of the development on upstream and downstream areas of the catchment (which could include regional solutions) including:
  - (a) stormwater management aimed at reducing flow rates
  - (b) integrated Water Sensitive Urban Design techniques.
- 10 Development should ensure movement networks are provided to create a safe and highly connected environment, including:
  - (a) road design and access points to address localised traffic impacts
  - (b) adequate and accessible public open spaces, walkways and cycling pathways.
- 11 Development should not occur until it is demonstrated that the land is suitable for its intended use (including where impacts from historical onsite contamination or where offsite contamination has been identified as impacting the site) in particular where there is:
  - (a) a land use change to a sensitive use, and/or
  - (b) a land division creating allotments intended for a sensitive use.
- 12 Non-residential development should be located on arterial road corridors; collector roads; adjacent open space; or within the **Mixed Use Transition Policy Area 38** except where comprising small scale uses that:
  - (a) serve the local community
  - (b) are of a nature and scale consistent with the character of the locality
  - (c) do not compromise the capacity to achieve coordinated activity centre development
  - (d) do not detrimentally impact on the amenity of nearby residents.
- 13 Development listed as non-complying is generally inappropriate.

### **Form and Character**

- 14 The bulk and scale of development should be compatible with adjoining land uses.
- 15 Development should be up to 2 storeys, except near the centre of the zone in between Sherriff Street and Hardys Road and north of Wilford Avenue, where development may be up to 3 storeys.
- 16 To minimise building mass at the interface of residential development, buildings over 2 storeys should seek to minimise interface impacts by scaling down buildings and articulating building facades to complement adjoining developments.
- 17 Development of multiple dwellings on one site should ensure access to parking and garaging areas from public roads is via a minimum number of common or shared driveways.
- 18 Development of three or more storeys in height should ensure that:
  - (a) north-facing windows to habitable rooms of existing dwelling(s) on the same allotment, and on adjacent allotments, receive at least 3 hours of direct sunlight over a portion of their surface between 9.00 am and 3.00 pm on 21 June

- (b) ground level open space of existing buildings receives direct sunlight for a minimum of 2 hours between 9.00 am and 3.00 pm on 21 June to at least the smaller of the following:
- (i) half of the existing ground level open space
  - (ii) 35 square metres of the existing ground level open space (with at least one of the area's dimensions measuring 2.5 metres).
- 19 Development fronting Ashwin Parade and the northern portion of Hardys Road should provide a physical buffer to noise and air emissions from those roads and the industrial activities to the north.
- 20 Residential development along Hardys Road within the area subject to [Concept Plan WeTo/25 – Underdale Urban Renewal](#) should achieve vehicle access via a rear laneway and should not have direct vehicle access to Hardys Road or, if direct access is necessary, shared access between dwellings should be provided together with forward entry and exit movements.

## Dwellings and Residential Flat Buildings

### Building to the Side Boundary

- 21 Walls of dwellings and residential flat buildings sited on side boundaries should be in accordance with at least one of the following:
- (a) be located immediately abutting the wall of an existing or simultaneously constructed building on the adjoining land to the same or lesser length and height
  - (b) constructed in accordance with any approved building envelope plan
  - (c) the exposed section of the wall is less than 8 metres in length and 3.5 metres in height.
- 22 Dwellings and residential flat buildings developed to both side boundaries, except where there is less than 25 square metres of private open space, should provide ground level access to the rear of the site via a carport, garage, access way, service lane or the like.

### Setbacks from the Side Boundary

- 23 Walls of dwellings and residential flat buildings set back from the side boundary should be designed in accordance with the following:

Wall height (measured from natural ground level)	Minimum setback from side boundaries except if it is a boundary wall (metres)
For any portion of the wall less than or equal to 3 metres	1
For any portion of the wall greater than 3 metres	2

### Front Setbacks

- 24 Dwellings and residential flat buildings (excluding verandas, porticos and the like) should be set back from road frontages in accordance with the following parameters:

Minimum setback	Value (metres)
From the primary road frontage of an arterial road	8 or the average of any existing dwellings on any adjoining allotments with the same primary frontage (or, if there is only one such dwelling, the setback of that dwelling)
From the primary frontage of Ashley Street and Hardys Road	5



Minimum setback	Value (metres)
From the primary road frontage of all other roads	3 or the average of any existing dwellings on any adjoining allotments with the same primary frontage (where there is only one such dwelling, the setback of that dwelling) or a lesser setback is provided as part of an approved building envelope plan  1.5 where the allotment is located adjacent to a public reserve greater than 2000 square metres and the River Torrens Linear Park, the dwelling faces that reserve and vehicle access is provided to the rear of the allotment
From a secondary road frontage that is an arterial road	8 or the average of any existing dwellings on any adjoining allotments with the same primary frontage (or, if there is only one such dwelling, the setback of that dwelling)
Minimum setback to a secondary road frontage that is not an arterial road	0.9

### Setbacks from Rear Boundaries

- 25 The walls of dwellings and residential flat buildings should be set back from rear boundaries, except where the rear boundary adjoins an access way, in accordance with the following parameters:

Allotment size (square metres)	Ground floor minimum setback (metres)	Second storey minimum setback (metres)	Third storey or more minimum setback (metres)
≤300	2	5	5 plus any increase in wall height over 6 metres
>300	4	6	6 plus any increase in wall height over 6 metres

- 26 Walls of dwellings and residential flat buildings sited on rear boundaries should be in accordance with at least one of the following:
- (a) be located immediately abutting the wall of an existing or simultaneously constructed building on the adjoining land to the same or lesser length and height
  - (b) constructed in accordance with any approved building envelope plan.
- 27 Walls located on the side boundary should be devoid of windows, unless located adjacent public space.
- 28 Carports and garages should not be located in front of the building line of dwelling; and
- (a) should have a maximum opening of 6 metres wide
  - (b) if accessed from a lane, alley or right-of way, the lane should be at least 6.2 metres in width.

### Site Area

- 29 A dwelling, **except where located in the area identified for reduced minimum/average site areas in [Concept Plan WeTo/25 – Underdale Urban Renewal](#)**, should have a minimum site area (and in the case of group dwellings and residential flat buildings, an average site area per dwelling) and a frontage to a public road not less than that shown in the following table:

Dwelling type	Area (square metres)	Minimum frontage (metres)
Detached (except where constructed boundary to boundary)	250 minimum allotment area	9
Semi-detached	200 minimum allotment area	8
Row dwelling and detached dwelling constructed boundary to boundary	150 minimum allotment area	5
Group dwellings and / or residential flat building	150 average site area per dwelling	9

- 30 A dwelling, where located in the area identified for reduced minimum/average site areas in [Concept Plan WeTo/25 – Underdale Urban Renewal](#) and located adjacent to public open space, should have a minimum site area (and in the case of group dwellings and residential flat buildings, an average site area per dwelling) and a frontage to a public road not less than that shown in the following table:

Dwelling type	Area (square metres)	Minimum frontage (metres)
Detached (except where constructed boundary to boundary)	200 minimum allotment area	9
Semi-detached	150 minimum allotment area	8
Row dwelling and detached dwelling constructed boundary to boundary	150 minimum allotment area	5
Group dwellings and / or residential flat building	80 average site area per dwelling	9

### Private Open Space

- 31 Dwellings at ground level should provide private open space in accordance with the following table:

Site area per dwelling (square metres)	Minimum area excluding any area at ground level at the front of the dwelling (square metres)	Minimum dimension (metres)	Minimum area provided at the rear or side of the dwelling, directly accessible from a habitable room (square metres)
≥200	40, of which 16 may comprise balconies, roof patios and the like, provided they have a minimum dimension of 2 metres	2.5	24
<200	15, of which 5 may comprise balconies, roof patios and the like, provided they are a minimum dimension of 2 metres	2	10

- 32 Where a building comprises one or more dwellings located above ground, provision should be made for:

- 25 square metres of communal open space per above-ground dwelling
- private open space for each above ground dwelling in accordance with the table below:

Site area of dwelling (square metres)	Minimum area of private open space (metres)	Minimum dimension of private open space (square metres)
≥200	10	2
<200	8	2

### **Affordable Housing**

- 33 Residential development should include a minimum 15 per cent of dwellings for affordable housing.
- 34 Affordable housing should be distributed throughout the zone to avoid over concentration of similar types of housing in a particular area.

### **Non-Residential Development**

- 35 Non-residential development should:
- (a) be physically connected with surrounding residential areas by avoiding large expanses of vehicle parking that physically separates the development from surrounding residential areas
  - (b) orient development towards and near to public spaces and street frontages
  - (c) include shelter for pedestrians along public streets and internal access ways
  - (d) ensure building façades create diversity of interest and have the appearance of an aggregation of small buildings
  - (e) ensure roof forms are varied and do not include large expanses of roofline that are visible from the public domain
  - (f) ensure buildings address the street frontage and open spaces with servicing areas generally accessed via rear lanes or internal to the centre and not visible from public streets.
- 36 Non-residential development should have front, rear and side setbacks consistent with the setbacks sought for dwellings and residential flat buildings.

### **Land Division**

- 37 Land division should create new allotments that are of a size and configuration to ensure the objectives of the zone can be achieved.
- 38 Land division should result in north-south / east-west grid design to facilitate allotments and dwellings that primarily orientate north-south to maximise passive solar efficiency.
- 39 Land division should result in allotments that allow building orientation that minimises the impact of garages and driveway crossovers on the streetscape and should be designed to provide a maximum number of on street car parks, particularly adjacent to allotments intended to be developed for affordable housing.
- 40 Land division in the zone should only occur where the land is suitable for the intended use having considered access, contamination, noise impacts and air quality (including odour).

## Mixed Use Transition Policy Area 38

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 A policy area accommodating a range of business, commercial, warehouse, storage and light industrial land uses that are envisaged to transition in an orderly and efficient manner to compatible mixed use development, including residential development.
- 2 The effective location and management of industrial and commercial activities at the interface of adjacent areas that support residential and other sensitive land uses.
- 3 A high standard of development which promotes distinctive building, landscape and streetscape design, with high visual and environmental amenity, particularly at the interface of residential areas, along arterial roads and the boundaries of adjoining zones.
- 4 Development that contributes to the desired character of the policy area.

### DESIRED CHARACTER

The policy area will, through changing land uses and urban renewal, transition to a revitalised modern, urban location providing for a mix of land uses, including complementary small scale retail, community facilities, commercial uses, light industry and generally, medium density housing in appropriate locations.

As existing industrial and commercial activities continue to operate, some under various licencing requirements, opportunities for such activities to transition to new land uses is anticipated, providing for new employment opportunities and mixed use developments.

Residential and other forms of sensitive development will only occur within the policy area as adverse impacts of adjoining industrial and commercial activities relating to noise, dust, traffic movements, air quality and odour are lessened or a better understanding of the impacts are determined and associated appropriate separation distances are applied.

The policy area will be enhanced through high quality development which creates visual interest through the use of articulated façades and utilises a mixture of building materials and canopies or verandahs while avoiding long blank walls fronting the public realm. Redevelopment of existing sites will occur using high quality materials, landscaping and other elements to create a mixed use area. Where non-residential development is sited adjacent residential development, landscaping and street trees will complement and enhance the visual impacts of buildings and structures, adding to overall amenity and streetscape quality.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following additional types of development, or combination thereof, are envisaged in the policy area:
  - light industry
  - service industry
  - service trade premises
  - store
  - warehouse.

- 2 Development should promote the redevelopment and transition of the area towards medium density residential development supported by a mix of compatible land uses as set out in the **Urban Renewal Zone**.
- 3 The expansion, alteration or replacement of existing industrial activities that have environmental impacts which currently extend over residential areas should not occur unless the impacts can be significantly reduced and managed in accordance with *Environment Protection Act* requirements.
- 4 Development, including land division, should only occur on land which is suitable for its intended use(s) with respect to site contamination.
- 5 Development and redevelopment of industrial areas should be orderly and efficient and compatible with residential development and other sensitive uses.
- 6 Residential development should only occur where it can be demonstrated that the environmental impacts of nearby industrial land uses can be mitigated to a satisfactory level.
- 7 Development should not be undertaken if it will be prejudicial to the orderly and economic development of future residential or associated land uses within the zone.

### **Form and Character**

- 8 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 9 Development at the direct interface with residential **and/or non-residential development** should:
  - (a) incorporate a range of operational, physical and/or spatial design techniques to improve the amenity of the area
  - (b) seek to minimise impacts from visual appearance, building bulk and scale, overshadowing, noise, vibration, chemical over-spray, air quality, odour, dust, hours of operation and on-street parking
  - (c) incorporate planting of street trees and a landscape buffer to improve amenity through planting of appropriate scale trees.
- 10 Development should through appropriate building design and orientation attempt to mitigate adverse environmental impacts including noise and air quality (including odour) impacts.
- 11 Development generating high levels of heavy vehicle use should direct traffic movements of this nature away from established residential areas.

## PROCEDURAL MATTERS

### Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

### Non-complying Development

Development (including building work, a change in the use of land or division of an allotment) for the following is non-complying:

Form of development	Exceptions
Advertisement and / or advertising hoarding	Except where it is ancillary to and in association with non-residential development.
Fuel depot	
General industry	
Industry	Except where it involves the following (a) and/or (b): (a) light industry (b) service industry.
Intensive animal keeping	
Landfill that constitutes solid waste disposal required to be licensed as a waste depot under the <i>Environment Protection Act 1993</i> .	
Road transport terminal	
Special industry	
Telecommunications facility	Except where located at least 100 metres from: (a) any Local Heritage Place described within the extent of listing within <a href="#">Table WeTo/4 - Local Heritage Places</a> (b) any State Heritage Place described in the extent of listing within <a href="#">Table WeTo/4 - Local Heritage Places</a> or listed within the South Australian Heritage Register.
Wrecking yard	

### Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

In addition, the following forms of development (except where the development is classified as non-complying) are designated:

Category 1	Category 2
<p>Advertisement and / or advertising hoarding <b>where it is ancillary to and in association with non-residential development.</b></p> <p>The following forms of development other than where the development exceeds two storeys in height:</p> <ul style="list-style-type: none"><li>(a) aged persons accommodation</li><li>(b) all forms of development that are ancillary and in association with residential development</li><li>(c) community centre</li><li>(d) consulting room</li><li>(e) dwelling</li><li>(f) educational establishment</li><li>(g) land division</li><li>(h) nursing home</li><li>(i) Office <b>in the Mixed Use Transition Policy Area 38</b> or with a gross leasable floor area of no more than 250 square metres</li><li>(j) pre-school</li><li>(k) primary school</li><li>(l) retirement village</li><li>(m) residential flat building</li><li>(n) shop or group of shops <b>in the Mixed Use Transition Policy Area 38</b> or with a gross leasable floor area of no more than 250 square metres</li><li>(o) supported accommodation.</li></ul>	<p>All forms of development not listed as Category 1.</p>





# Table Section



## Table WeTo/1 - Conditions for Complying Development

Form of Development	Compliance Criteria / Conditions																												
All development	No building being erected, added to or altered on any land so that any portion of such building will be erected, added to or altered nearer than eight metres to the existing boundary of any road, or to the boundary of any land shown as being required for road widening on the Plan deposited under the provisions of the <i>Metropolitan Adelaide Road Widening Plan Act, 1972-1976</i> .																												
Consulting room	<ol style="list-style-type: none"> <li>1 Off street vehicle parking is provided on site at the rate of 10 car parking spaces per 100 square metres of total floor area.</li> <li>2 The site is not located within an area subject to the <b>Historic Conservation Area</b> as depicted by the <i>Overlay Maps - Heritage</i>.</li> <li>3 The development is not located on an allotment that is in whole or in part within a 1-in-100 year average return interval flood event area as depicted by <i>Overlay Maps - Development Constraints</i>.</li> <li>4 No part of the development is located within 60 metres of either side of the centre line of the River Torrens / Karrawirra Parri.</li> </ol>																												
Detached dwelling Row dwelling Semi detached	<ol style="list-style-type: none"> <li>1 The site of the detached, semi detached or row dwelling, has an area and dimensions equal to or greater than those specified below: <table border="1" style="width: 100%; border-collapse: collapse; margin-top: 10px;"> <thead> <tr> <th style="background-color: #cccccc;">Parameter</th> <th style="background-color: #cccccc;">Residential Policy Area 18</th> <th style="background-color: #cccccc;">Residential Policy Areas 19 and 20</th> <th style="background-color: #cccccc;">Residential Policy Area 21</th> </tr> </thead> <tbody> <tr> <td>Site area (square metres)</td> <td style="text-align: center;">250</td> <td style="text-align: center;">380</td> <td style="text-align: center;">420</td> </tr> <tr> <td>Width of frontage of site of regular shape (metres)</td> <td style="text-align: center;">9</td> <td style="text-align: center;">10</td> <td style="text-align: center;">12</td> </tr> <tr> <td>Depth of site of regular shape (metres)</td> <td style="text-align: center;">25</td> <td style="text-align: center;">25</td> <td style="text-align: center;">25</td> </tr> <tr> <td>Width of frontage of irregular shape (metres)</td> <td style="text-align: center;">9</td> <td style="text-align: center;">9</td> <td style="text-align: center;">9</td> </tr> <tr> <td>Average width of irregular shape (metres)</td> <td style="text-align: center;">11</td> <td style="text-align: center;">11</td> <td style="text-align: center;">11</td> </tr> <tr> <td>Average depth of irregular shape (metres)</td> <td style="text-align: center;">28</td> <td style="text-align: center;">28</td> <td style="text-align: center;">28</td> </tr> </tbody> </table> </li> <li>2 Except for fascias, gutters, downpipes and eaves up to 0.45 metres, masonry chimneys, flues and pipes, and unroofed balconies, landings, steps or ramps measuring a maximum of 1 metre in height, a dwelling, semi detached dwelling or row dwelling, and associated carport or garage, satisfies the following criteria:</li> </ol>	Parameter	Residential Policy Area 18	Residential Policy Areas 19 and 20	Residential Policy Area 21	Site area (square metres)	250	380	420	Width of frontage of site of regular shape (metres)	9	10	12	Depth of site of regular shape (metres)	25	25	25	Width of frontage of irregular shape (metres)	9	9	9	Average width of irregular shape (metres)	11	11	11	Average depth of irregular shape (metres)	28	28	28
Parameter	Residential Policy Area 18	Residential Policy Areas 19 and 20	Residential Policy Area 21																										
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Width of frontage of irregular shape (metres)	9	9	9																										
Average width of irregular shape (metres)	11	11	11																										
Average depth of irregular shape (metres)	28	28	28																										

Form of Development	Compliance Criteria / Conditions	
	Parameter	Value
	Minimum setback from primary road frontage excluding garages/carports attached to a detached, semi detached or row dwelling	The same distance as one or the other of the adjoining buildings, provided the difference between the setbacks of the two adjoining buildings is less than or equal to 2 metres or not less than the average of the setbacks of the adjoining dwellings, if the difference between the setbacks of the two adjoining buildings is greater than 2 metres.
	Minimum setback from primary road frontage for single or double garages/carports where attached to a detached, semi detached or row dwelling	<p>(a) not less than 5.5 metres from the street from which they are accessed and when the associated dwelling faces the same street as the garage/carport, the dwelling incorporates elements such as projecting windows, verandas and porticos or other elements that articulates the building when viewed from the street</p> <p>(b) not less than the setback of the associated dwelling to the side street, when facing the side street on a corner site.</p>
	Minimum setback from side boundaries	<p>(a) 1 metre where the vertical side wall height measures a maximum of 3 metres</p> <p>(b) 1 metre plus one third of the increase in vertical wall height above 3 metres where the vertical side wall height is between 3 metres and 6 metres.</p>
	Minimum setback from rear boundary	3 metres plus the increase in vertical wall height above 3 metres.
	Maximum number of storeys/vertical wall height where it is not sited on side boundaries	one storey / 3 metres or less
	Maximum vertical side wall height where it is sited on side boundaries	<p>(a) side boundary walls are located immediately abutting the wall of an existing or simultaneously constructed building on the adjoining site and are constructed to the same or to a lesser length and height</p> <p>(b) where the side boundary walls are not located immediately abutting the wall of an existing or simultaneously constructed building on the adjoining site, the side boundary walls:</p> <p>(i) have a maximum vertical wall height of 3 metres</p> <p>(ii) have a maximum length of 8 metres</p> <p>(iii) are constructed along one side of the allotment only and within 14 metres of the front boundary, except where it abuts a side service lane in which case the maximum vertical wall height is 6 metres.</p>

Form of Development	Compliance Criteria / Conditions
	<p>Minimum area of private open space, which comprises:</p> <ul style="list-style-type: none"> <li>(a) unbuilt upon areas (excluding driveways and parking areas) having a minimum dimension of 2.5 metres, and screened from external view to a height of 1.8 metres</li> <li>(b) where in the form of balconies, patios or other elevated outdoor areas, it is directly accessible from a habitable room at the ground floor level and has a minimum area of 10 square metres with a minimum dimension of 2 metres.</li> </ul>
	<p>Minimum number of on site car parking spaces (one of which should be covered)                      2 car parking spaces per dwelling.</p>
<p>3</p> <p>4</p> <p>5</p> <p>6</p> <p>7</p> <p>8</p> <p>9</p> <p>10</p> <p>11</p> <p>12</p>	<p>Ground floor windows of habitable rooms do not face directly towards ground floor windows of habitable rooms in adjacent buildings.</p> <p>Windows of habitable rooms at or above the second storey and which face the side or rear site boundaries have either sill heights of at least 1.7 metres above floor level or permanently fixed translucent glazing in any part of the window below 1.7 metres above floor level.</p> <p>There are no balconies, terraces or decks at or above the second storey.</p> <p>Bedrooms do not share a wall with a living room (ie lounge, family or dining room) or a garage of an adjacent dwelling.</p> <p>Bedroom windows are located at least 3 metres from road reserve boundaries.</p> <p>Dwelling and dwelling additions incorporating a day living area, where such additions have a floor area equal to or greater than 50 per cent of the existing dwelling, have a day living area incorporating a window facing between 20° west and 30° east of true north.</p> <p>All north facing windows are shaded to allow winter sun access and provide complete shading during summer.</p> <p>Habitable rooms have windows:</p> <ul style="list-style-type: none"> <li>(a) totalling in area at least 10 per cent of the rooms floor area</li> <li>(b) located to face an area of open space which is clear to the sky (of minimum area 3 square metres and dimension 1 metre), an open veranda or an open carport or with a horizontal distance between any facing building, measured perpendicular to the face of the window, of at least 1 metre which is clear to the sky (ie at least 1 metre between eaves).</li> </ul> <p>It is not located within an area subject to the <b>Historic Conservation Area</b> as depicted by the <i>Overlay Maps - Heritage</i>.</p> <p>It is not located within one of the following policy areas:</p> <ul style="list-style-type: none"> <li>(a) <b>Ashford Character Policy Area 22</b></li> </ul>

Form of Development	Compliance Criteria / Conditions
	<ul style="list-style-type: none"> <li>(b) <b>Cowandilla / Mile End West Character Policy Area 23</b></li> <li>(c) <b>Glandore Character Policy Area 24</b></li> <li>(d) <b>Lockleys Character Policy Area 25</b></li> <li>(e) <b>Novar Gardens Character Policy Area 26</b></li> <li>(f) <b>Thebarton Character Policy Area 27</b></li> <li>(g) <b>Torrensville Character Policy Area 28.</b></li> </ul>
	13 It is not located on an allotment that is in whole or in part within a 1-in-100 year average return interval flood event area as depicted by <i>Overlay Maps - Development Constraints</i> .
	14 No part of the development is on land affected by aircraft noise as shown on <i>Overlay Map - Development Constraints</i> .
	15 No part of the development is located within 60 metres of either side of the centre line of the River Torrens / Karrawirra Parri.
Domestic carport or garage	<ul style="list-style-type: none"> <li>1 The carport or garage is free standing.</li> <li>2 It is setback: <ul style="list-style-type: none"> <li>(a) no less than 5.5 metres from the primary street frontage</li> <li>(b) no less than 500 millimetres behind the main face of the associated dwelling</li> <li>(c) no less than 2 metres from the secondary street frontage.</li> </ul> </li> <li>3 It is setback a minimum of 600 millimetres from any side or rear boundary, except where the building adjoins a rear lane, in which case a 1 metre rear setback is required.</li> <li>4 No window faces a side or rear boundary of the allotment closer than 900 millimetres to that boundary.</li> <li>5 It has a maximum vertical wall height to the eaves of 2.8 metres.</li> <li>6 It has a maximum floor area of 40 square metres.</li> <li>7 Not less than 20 per cent of the site area and including a minimum ground level private open space area of 40 square metres is available as private open space. Private open space comprises: <ul style="list-style-type: none"> <li>(a) unbuilt upon areas (excluding driveways and parking areas) having a minimum dimension of 2.5 metres, and screened from external view to a height of 1.8 metres</li> <li>(b) where in the form of balconies, patios or other elevated outdoor areas, it is directly accessible from a habitable room at the ground floor level and has a minimum area of 10 square metres with a minimum dimension of 2 metres.</li> </ul> </li> <li>8 No part of the development is located within 60 metres of either side of the centre line of the River Torrens / Karrawirra Parri.</li> </ul>
Office	<ul style="list-style-type: none"> <li>1 Off street vehicle parking is provided on site at the rate of 4 per 100 square metres of total floor area, except when located in the <b>Urban Corridor Zone</b> and off street vehicle parking is provided in accordance with <a href="#">Table WeTo/6 – Off-street Vehicle Parking Requirements for Designated Areas</a>.</li> <li>2 It is not located within an area subject to the <b>Historic Conservation Area</b> as depicted by the <i>Overlay Maps - Heritage</i>.</li> </ul>

Form of Development	Compliance Criteria / Conditions
	<p>3 It is not located on an allotment that is in whole or in part within a 1-in-100 year average return interval flood event area as depicted by <i>Overlay Maps - Development Constraints</i>.</p> <p>4 No part of the development is located within 60 metres of either side of the centre line of the River Torrens/Karrawirra Parri.</p>
Domestic, non habitable outbuilding	<p>1 Being setback a minimum of 600 millimetres from any side or rear boundary, or in the case of a rear lane, being setback 1 metre from the rear lane.</p> <p>2 No window facing a side or rear boundary of the allotment being closer than 900 millimetres to that boundary.</p> <p>3 Setback not less than 500 millimetres behind the main face of the associated dwelling and screened from view.</p> <p>4 Has a maximum vertical wall height to the eaves of 2.8 metres.</p> <p>5 Does not have a floor more than 1 metre above ground level.</p> <p>6 Has a maximum floor area of 40 square metres.</p> <p>7 Not less than 20 per cent of the site area and including a minimum ground level private open space area of 40 square metres is available as private open space. Private open space comprises:</p> <p>(a) unbuilt upon areas (excluding driveways and parking areas) having a minimum dimension of 2.5 metres, and screened from external view to a height of 1.8 metres</p> <p>(b) where in the form of balconies, patios or other elevated outdoor areas, it is directly accessible from a habitable room at the ground floor level and has a minimum area of 10 square metres with a minimum dimension of 2 metres.</p> <p>8 It is ancillary or ordinarily incidental to the residential use of the land.</p> <p>9 No part of the development is located within 60 metres of either side of the centre line of the River Torrens / Karrawirra Parri.</p>
Recreation area	<p>1 No building exceeds two storeys in height in that part of the <b>District Commercial Policy Area 2</b> at East Terrace and Henley Beach Road, Thebarton and Mile End.</p> <p>2 It is not located within that part of the <b>District Commercial Policy Area 2</b> between Taylors Lane and George Street.</p> <p>3 The site is not located within an area subject to the <b>Historic Conservation Area</b> as depicted by the <i>Overlay Maps - Heritage</i>.</p> <p>4 The development is not located on an allotment that is in whole or in part within a 1-in-100 year average return interval flood area as depicted by <i>Overlay Maps - Development Constraints</i>.</p> <p>5 No part of the development is located within 60 metres of either side of the centre line of the River Torrens/Karrawirra Parri.</p>

Form of Development	Compliance Criteria / Conditions
Store	<ol style="list-style-type: none"> <li>1 All access driveways and manoeuvring areas is designed to enable all vehicle entry/exit from the site in a forward direction and pass in the driveway.</li> <li>2 No building exceeds 12 metres in overall height.</li> <li>3 Buildings are setback at least 3 metres from the street boundary after road widening, except sites located in areas depicted on the following concept plan maps, where alternative setback distances are specified for complying purposes: <ol style="list-style-type: none"> <li>(a) <a href="#"><u>Concept Plan Map WeTo/1 - Mile End South Bulky Goods</u></a></li> <li>(b) <a href="#"><u>Concept Plan Map WeTo/2 - Prominent Development Sites</u></a></li> </ol> </li> <li>4 Buildings are setback not less than 3 metres from at least one side boundary.</li> <li>5 The front facade of any building principally composed of one or more of the following materials: masonry, brick stone, concrete block, concrete tilt-up slab, timber, rendered masonry, painted-patterned cement fibre sheeting, and glass.</li> <li>6 No buildings or structures is elevated on posts without enclosing the space between the floor and the finished ground surface including undercroft space.</li> <li>7 A minimum of 10 per cent of the site is to be landscaped. A landscaped buffer strip of at least 3 metre width along all road frontages and adjacent to at least one side boundary using trees and shrubs suitable for screening purposes and enhanced with lawn, ground cover, paving and other landscaping features.</li> <li>8 Any security fencing to be setback at the building line or at least 2 metres from the street boundary.</li> <li>9 Storage areas screened by either densely planted trees and shrubs or fencing comprised of timber, masonry or double-sided Colorbond.</li> <li>10 Fencing of the side and rear boundaries of the site being of timber, masonry, double-sided Colorbond, or black or green plastic-coated mesh used in conjunction with dense shrub or tree planting.</li> <li>11 Buildings and structures do not exceed 60 per cent coverage of the site.</li> <li>12 Off street vehicle parking and access is designed in accordance with <i>Australian Standard 2890 Parking Facilities</i> and: <ol style="list-style-type: none"> <li>(a) is surfaced with impervious paving</li> <li>(b) divided into groups of not more than ten bays with dividing strips planted with suitable trees and shrubs.</li> <li>(c) separated from service and heavy vehicle access, loading and unloading areas.</li> </ol> </li> <li>13 The development is not located within 50 metres of a residential zone.</li> <li>14 No part of the development is located within Airport Building Heights '<b>Zone A</b>' as depicted on <i>Overlay Map - Development Constraints</i>.</li> <li>15 No part of the development is located within 60 metres of either side of the centre line of the River Torrens / Karrawirra Parri.</li> </ol>



Form of Development	Compliance Criteria / Conditions
Warehouse	<ol style="list-style-type: none"> <li data-bbox="531 293 1474 383">1 All access driveways and manoeuvring areas is designed to enable all vehicle entry/exit from the site in a forward direction and pass in the driveway.</li> <li data-bbox="531 398 1474 430">2 No building exceeds 12 metres in overall height.</li> <li data-bbox="531 448 1474 658">3 Buildings are setback at least 3 metres from the street boundary after road widening, except sites located in areas depicted on following concept plan maps, where alternative setback distances are specified for complying purposes: <ol style="list-style-type: none"> <li data-bbox="592 584 1474 616">(a) <a href="#">Concept Plan Map WeTo/1 - Mile End South Bulky Goods</a></li> <li data-bbox="592 631 1474 663">(b) <a href="#">Concept Plan Map WeTo/2 - Prominent Development Sites</a>.</li> </ol> </li> <li data-bbox="531 678 1474 745">4 Buildings are setback not less than 3 metres from at least one side boundary.</li> <li data-bbox="531 761 1474 873">5 The front facade of any building principally composed of one or more of the following materials: masonry, brick stone, concrete block, concrete tilt-up slab, timber, rendered masonry, painted-patterned cement fibre sheeting, and glass.</li> <li data-bbox="531 889 1474 978">6 No buildings or structures is elevated on posts without enclosing the space between the floor and the finished ground surface including undercroft space.</li> <li data-bbox="531 994 1474 1151">7 A minimum of 10 per cent of the site is to be landscaped. A landscaped buffer strip of at least 3 metre width along all road frontages and adjacent to at least one side boundary using trees and shrubs suitable for screening purposes and enhanced with lawn, ground cover, paving and other landscaping features.</li> <li data-bbox="531 1167 1474 1234">8 Any security fencing to be setback at the building line or at least 2 metres from the street boundary.</li> <li data-bbox="531 1249 1474 1317">9 Storage areas screened by either densely planted trees and shrubs or fencing comprised of timber, masonry or double-sided Colorbond.</li> <li data-bbox="531 1332 1474 1422">10 Fencing of the side and rear boundaries of the site being of timber, masonry, double-sided Colorbond, or black or green plastic-coated mesh used in conjunction with dense shrub or tree planting.</li> <li data-bbox="531 1438 1474 1469">11 Buildings and structures do not exceed 60 per cent coverage of the site.</li> <li data-bbox="531 1485 1474 1731">12 Off street vehicle parking and access is designed in accordance with <i>Australian Standard 2890 Parking Facilities</i> and: <ol style="list-style-type: none"> <li data-bbox="592 1545 1474 1576">(a) is surfaced with impervious paving</li> <li data-bbox="592 1592 1474 1659">(b) divided into groups of not more than ten bays with dividing strips planted with suitable trees and shrubs.</li> <li data-bbox="592 1675 1474 1742">(c) separated from service and heavy vehicle access, loading and unloading areas.</li> </ol> </li> <li data-bbox="531 1747 1474 1778">13 The development is not located within 50 metres of a residential zone.</li> <li data-bbox="531 1794 1474 1883">14 The development is not located wholly within or partly within an allotment that is in whole or in part within a 1-in-100 year average return interval flood event as depicted by <i>Overlay Maps - Development Constraints</i>.</li> <li data-bbox="531 1899 1474 1966">15 No part of the development is located within Airport Building Heights 'Zone A' as depicted on <i>Overlay Map - Development Constraints</i>.</li> <li data-bbox="531 1982 1474 2049">16 No part of the development is located within 60 metres of either side of the centre line of the River Torrens / Karrawirra Parri.</li> </ol>

## Table WeTo/2 - Off Street Vehicle Parking Requirements

The following off street vehicle parking requirements apply except where otherwise stated in [Table WeTo/6 - Off Street Vehicle Parking Requirements for Designated Areas](#).

Form of development	Number of Required Car Parking Spaces
<b>All development</b>	
Parking for disabled drivers	1 space for development with a total of 10 to 25 parking spaces and 1 space per 25 spaces thereafter to a maximum of 5 spaces.
<b>Recreation</b>	
Amusement machine centre	7 per 100 square metres of total floor area
Bowling club	10 per bowling green
Cinema complex	1 per 4 seats
Concert hall/theatre	1 per 4 seats
Hotels and taverns:	
<ul style="list-style-type: none"> <li>▪ public bar</li> <li>▪ lounge or beer garden</li> <li>▪ dining room</li> <li>▪ gaming room</li> <li>▪ accommodation.</li> </ul>	<ul style="list-style-type: none"> <li>▪ 1 per 2 square metres of total floor area +</li> <li>▪ 1 per 6 square metres of total floor area +</li> <li>▪ 1 per 3 seats +</li> <li>▪ 1 per 2 machines +</li> <li>▪ assessed on needs basis.</li> </ul>
Restaurant (traditional)	Greater of 1 per 3 seats or 1 per 15 square metres of total floor area
Restaurant (fast food/family):	
<ul style="list-style-type: none"> <li>▪ without dine-in and drive through facilities</li> <li>▪ with dine-in facilities but no drive through</li> <li>▪ with dine-in and drive through facilities.</li> </ul>	<ul style="list-style-type: none"> <li>▪ 12 per 100 square metres of total floor area</li> <li>▪ greater of 1 per 5 square metres of total floor area (internal and external seating) or 1 per 2 seats (internal seating)</li> <li>▪ greater of 1 per 3 square metres of total floor area (internal and external seating) or 1 per 2 seats (internal seating) and a car queuing area for a maximum of 12 vehicles with 4 car spaces back from the ordering point.</li> </ul>
Squash/tennis courts with no spectator seats	4 per court
Tab facility	8 per 100 square metres of total floor area
<b>Medical</b>	
Consulting room	10 per 100 square metres of total floor area
Hospital	2.5 per bed
Medical centre/day surgery	10 per 100 square metres of total floor area
Nursing home	1 per 4 beds

Table WeTo/2 - Off Street Vehicle Parking Requirements

Form of development	Number of Required Car Parking Spaces
<b>Industry and Warehouse</b>	
Office component PLUS Non-office component	3.3 per 100 square metres of total floor area
<ul style="list-style-type: none"> <li>▪ up to 200 square metres</li> <li>▪ plus 200 - 2000 square metres</li> <li>▪ plus greater than 2000 square metres</li> </ul>	<ul style="list-style-type: none"> <li>▪ 2 per 100 square metres of total floor area</li> <li>▪ 1.33 per 100 square metres of total floor area</li> <li>▪ 0.67 per 100 square metres of total floor area</li> <li>▪ 0.75 per employee</li> </ul>
OR	
<ul style="list-style-type: none"> <li>▪ for labour intensive industries (inclusive of office component).</li> </ul>	
<b>Community/Civic</b>	
Child care centre	1 per 4 children
Civic administration offices	4 per 100 square metres of total floor area
Community centre	10 per 100 square metres of total floor area
Educational institutions (long term and short term)	1 per full time employee plus 1 space for wheelchair users plus an additional 10 per cent of the total for visitors
<ul style="list-style-type: none"> <li>▪ pre-school</li> <li>▪ primary school</li> <li>▪ secondary school</li> </ul>	
Tertiary institution	0.6 per full time student, plus 0.2 per part time student
Library	4 per 100 square metres of total floor area
Meeting hall	1 per 5 seats
Place of worship	1 per 3 seats
<b>Commercial</b>	
Auction depot	1 per 100 square metres of total floor area plus 2 spaces
Bank	6 per 100 square metres of total floor area
Bulky goods outlet	4 per 100 square metres of gross leasable area
Hardware and other bulky goods outlet	4 per 100 square metres gross leasable area
Motor repair station	greater of 2 per 100 square metres of total floor area or 4 per service bay
Motor showroom	2 per 100 square metres of total floor area
Office	4 per 100 square metres of total floor area
Petrol filling station	6 per service bay plus 5 per 100 square metres retail floorspace
Post office	7 per 100 square metres of total floor area
Service trade premises	4 per 100 square metres of total floor area
Shop (not within a centre)	7 per 100 square metres gross leasable area
Used car lot/vehicle sales yard	2 per 100 square metres of the total area of the site
Video store	6 per 100 square metres gross leasable area

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Table WeTo/2 - Off Street Vehicle Parking Requirements

Form of development	Number of Required Car Parking Spaces
<b>Accommodation</b>	
Dwelling	For detached, semi detached, row dwelling and multiple dwellings to provide:
<ul style="list-style-type: none"> <li>▪ detached</li> <li>▪ semi-detached</li> <li>▪ row</li> <li>▪ multiple</li> <li>▪ group</li> <li>▪ within a residential flat building</li> </ul>	<p>2 car parking spaces per dwelling, one of which is covered.</p> <p>For group dwelling and residential flat building to provide:  2 car parking spaces per dwelling, one of which is covered  + an additional 0.25 car parking spaces per dwelling.</p>
Aged care retirement homes	1 per unit
Guest house/hostel	1 per 3 beds
Motel	1 per room plus 1 per employee
Serviced apartments	1 per room plus 1 per employee
Tourist accommodation (bed and breakfast)	1 per guest room plus 1 per employee
Tourist accommodation (bed and breakfast)	1 per guest room plus 1 per employee
<b>Other</b>	
Funeral parlour	4 per 100 square metres of total floor area plus 1 per 4 seats (chapel)
Radio and TV studio	5 per 100 square metres of total floor area

## Table WeTo/3 - Contributory Items

Property Address	Description of Contributory item	Certificate of Title	DPLG ID
4 ELSTON Street BROOKLYN PARK	Dwelling	CT 5775/64	21597
6 ELSTON Street BROOKLYN PARK	Dwelling	CT 5629/905	21599
7 ELSTON Street BROOKLYN PARK	Dwelling	CT 5783/426	21596
8 ELSTON Street BROOKLYN PARK	Dwelling	CT 5281/978	21601
9 ELSTON Street BROOKLYN PARK	Dwelling	CT 6020/917	21598
10 ELSTON Street BROOKLYN PARK	Dwelling	CT 5713/306	21603
11 ELSTON Street BROOKLYN PARK	Dwelling	CT 5773/673	21600
12 ELSTON Street BROOKLYN PARK	Dwelling	CT 5704/313	21604
13 ELSTON Street BROOKLYN PARK	Dwelling	CT 5283/566	21602
14 ELSTON Street BROOKLYN PARK	Dwelling	CT 5081/811	21606
17 ELSTON Street BROOKLYN PARK	Dwelling	CT 5488/59	21605
21 ELSTON Street BROOKLYN PARK	Dwelling	CT 5277/988	21607
22 ELSTON Street BROOKLYN PARK	Dwelling	CT 5275/110	21609
23 ELSTON Street BROOKLYN PARK	Dwelling	CT 5238/9	21608
24 ELSTON Street BROOKLYN PARK	Dwelling	CT 5694/725	21610
26 ELSTON Street BROOKLYN PARK	Dwelling	CT 5257/655	21611
2 BALLARA Street MILE END	Dwelling	CT 5871/612	21446
2A BALLARA Street MILE END	Dwelling	CT 5871/624	21447
4A BALLARA Street MILE END	Dwelling	CT 5874/324	21448
6 BALLARA Street MILE END	Dwelling	CT 6051/205	21449
6A BALLARA Street MILE END	Dwelling	CT 6051/206	21450
8A BALLARA Street MILE END	Dwelling	CT 5776/465	21451
10 & 10A BALLARA Street MILE END	Dwelling	CT 5003/443 CT 5003/444 CT 5003/445	21452
12 BALLARA Street MILE END	Dwelling	CT 5127/363	21453
14 & 14A BALLARA Street MILE END	Dwelling	CT 5553/852 CT 5874/323	21454
16 BALLARA Street MILE END	Dwelling	CT 5830/804	21401
18 BALLARA Street MILE END	Dwelling	CT 5866/168	21400

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<b>Property Address</b>	<b>Description of Contributory item</b>	<b>Certificate of Title</b>	<b>DPLG ID</b>
20 BALLARA Street MILE END	Dwelling	CT 5548/12	21399
22 BALLARA Street MILE END	Dwelling	CT 5526/966	21398
24 BALLARA Street MILE END	Dwelling	CT 5840/631	21397
26 BALLARA Street MILE END	Dwelling	CT 5724/76	21462
28 BALLARA Street MILE END	Dwelling	CT 5532/785	21461
30 BALLARA Street MILE END	Dwelling	CT 5529/849	21460
1/1 & 2/1A CLAREMONT Street MILE END	Dwelling	CT 5026/393 CT 5026/394 CT 5026/395	21458
2 CLAREMONT Street MILE END	Dwelling	CT 5683/187	21404
3A CLAREMONT Street MILE END	Dwelling	CT 5826/873	21457
4 CLAREMONT Street MILE END	Dwelling	CT 5739/576	21403
5 & 5A CLAREMONT Street MILE END	Dwelling	CT 5392/525 CT 5871/622	21456
6 CLAREMONT Street MILE END	Dwelling	CT 5119/287	21402
7 & 7A CLAREMONT Street MILE END	Dwelling	CT 5724/68 CT 5892/436	21455
1 CUMING Street MILE END	Dwelling	CT 5279/811	21378
3 CUMING Street MILE END	Dwelling	CT 5281/840	21377
5 CUMING Street MILE END	Dwelling	CT 5731/859	21376
7 CUMING Street MILE END	Dwelling	CT 5814/766	21375
8 & 8A CUMING Street MILE END	Dwelling	CT 5071/33	21367
1/ 9 CUMING Street MILE END	Dwelling	CT 5005/311	21374
10 CUMING Street MILE END	Dwelling	CT 5071/34	21366
12 & 12A CUMING Street MILE END	Dwelling	CT 5071/35	21365
13 CUMING Street MILE END	Dwelling	CT 5658/796	21373
14 CUMING Street MILE END	Dwelling	CT 5126/399	21364
15 CUMING Street MILE END	Dwelling	CT 5521/234	21372
16 CUMING Street MILE END	Dwelling	CT 5804/282	21363
17 CUMING Street MILE END	Dwelling	CT 5665/275	21371
18 CUMING Street MILE END	Dwelling	CT 5900/720	21362
19 CUMING Street MILE END	Dwelling	CT 5476/755	21370
20 CUMING Street MILE END	Dwelling	CT 5283/456	21361

Property Address	Description of Contributory item	Certificate of Title	DPLG ID
21 CUMING Street MILE END	Dwelling	CT 5582/680	21369
22 CUMING Street MILE END	Dwelling	CT 5719/122	21360
23 CUMING Street MILE END	Dwelling	CT 6057/2	21368
24 CUMING Street MILE END	Dwelling	CT 5658/800	21359
25 CUMING Street MILE END	Dwelling	CT 5273/676	21574
25A CUMING Street MILE END	Dwelling	CT 5802/160	21575
26 CUMING Street MILE END	Dwelling	CT 5577/775	21358
27 CUMING Street MILE END	Dwelling	CT 5556/424	21576
28 & 28A CUMING Street MILE END	Dwelling	CT 5818/721 CT 5818/722	21357
29 CUMING Street MILE END	Dwelling	CT 5755/453	21577
30 & 30A CUMING Street MILE END	Dwelling	CT 5235/211 CT 5857/879	21356
31 CUMING Street MILE END	Dwelling	CT 5716/681	21578
33 CUMING Street MILE END	Dwelling	CT 5996/260	21579
34 CUMING Street MILE END	Dwelling	CT 5722/966	21726
35 & 35B CUMING Street MILE END	Dwelling	CT 5825/748	21581
1/36 & 2/36 CUMING Street MILE END	Dwelling	CT 5172/16 CT 5172/17 CT 5172/18	21725
37 CUMING Street MILE END	Dwelling	CT 5814/667	21580
39 CUMING Street MILE END	Dwelling	CT 5818/886	21582
40 CUMING Street MILE END	Dwelling	CT 5678/407	21724
41 CUMING Street MILE END	Dwelling	CT 5377/63	21583
42 CUMING Street MILE END	Dwelling	CT 6104/534 CT 6104/536	21723
43 CUMING Street MILE END	Dwelling	CT 5660/494	21584
44 CUMING Street MILE END	Dwelling	CT 5805/436	21722
45 CUMING Street MILE END	Dwelling	CT 5790/75	21585
46 CUMING Street MILE END	Dwelling	CT 5394/509	21721
47 CUMING Street MILE END	Dwelling	CT 5781/980	21586
48 CUMING Street MILE END	Dwelling	CT 5819/351	21720
49 CUMING Street MILE END	Dwelling	CT 5668/64	21587
50 CUMING Street MILE END	Dwelling	CT 5472/45	21719

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<b>Property Address</b>	<b>Description of Contributory item</b>	<b>Certificate of Title</b>	<b>DPLG ID</b>
51 CUMING Street MILE END	Dwelling	CT 5628/656	21588
52 & 52A CUMING Street MILE END	Dwelling	CT 5672/879	21718
53 CUMING Street MILE END	Dwelling	CT 5218/94	21589
54 & 54A CUMING Street MILE END	Dwelling	CT 5196/211 CT 5841/324	21716
55 CUMING Street MILE END	Dwelling	CT 5813/138	21590
1/56A & 2/56 CUMING Street MILE END	Dwelling	CT 5016/72 CT 5016/73 CT 5016/74	21715
57 CUMING Street MILE END	Dwelling	CT 5480/984	21591
58 CUMING Street MILE END	Dwelling	CT 5812/757	21714
59 CUMING Street MILE END	Dwelling	CT 5983/350	21592
60 CUMING Street MILE END	Dwelling	CT 5135/55	21713
61 CUMING Street MILE END	Dwelling	CT 5246/248	21593
62 CUMING Street MILE END	Dwelling	CT 5794/701	21712
62A CUMING Street MILE END	Dwelling	CT 5434/680	21711
63 CUMING Street MILE END	Dwelling	CT 5387/480	21594
64 CUMING Street MILE END	Dwelling	CT 5717/28	21710
65 CUMING Street MILE END	Dwelling	CT 5546/181	21694
66 CUMING Street MILE END	Dwelling	CT 5797/533	21709
67 CUMING Street MILE END	Dwelling	CT 5543/884	21695
68 CUMING Street MILE END	Dwelling	CT 5389/48	21708
70 CUMING Street MILE END	Dwelling	CT 5697/615	21707
72 CUMING Street MILE END	Dwelling	CT 6113/68	21706
74 CUMING Street MILE END	Dwelling	CT 5531/123	21705
76 CUMING Street MILE END	Dwelling	CT 5817/945	21704
78 CUMING Street MILE END	Dwelling	CT 5176/153	21703
80 CUMING Street MILE END	Dwelling	CT 5553/260	21702
82 CUMING Street MILE END	Dwelling	CT 5931/387	21701
2 & 2A DEW Street MILE END	Dwelling	CT 5748/601 CT 5748/602	21508
4 & 4A DEW Street MILE END	Dwelling	CT 5973/143 CT 5973/144	21507
6 DEW Street MILE END	Dwelling	CT 5144/556	21506



Property Address	Description of Contributory item	Certificate of Title	DPLG ID
8 & 8A DEW Street MILE END	Dwelling	CT 5027/39 CT 5027/40 CT 5027/41	21505
10 & 10A DEW Street MILE END	Dwelling	CT 5124/687 CT 5288/606	21504
1 GLADSTONE Road MILE END	Dwelling	CT 5779/442	21355
2 GLADSTONE Road MILE END	Dwelling	CT 5496/242	21334
2A GLADSTONE Road MILE END	Dwelling	CT 5198/585	21333
3 GLADSTONE Road MILE END	Dwelling	CT 5779/441	21354
4 GLADSTONE Road MILE END	Dwelling	CT 5178/342	21332
4A GLADSTONE Road MILE END	Dwelling	CT 5159/59	21331
5 GLADSTONE Road MILE END	Dwelling	CT 5665/674	21353
6 GLADSTONE Road MILE END	Dwelling	CT 5690/770	21330
7 GLADSTONE Road MILE END	Dwelling	CT 5316/832	21352
8 GLADSTONE Road MILE END	Dwelling	CT 5139/857	21329
8A GLADSTONE Road MILE END	Dwelling	CT 5510/593	21328
9 GLADSTONE Road MILE END	Dwelling	CT 5071/18	21351
10 GLADSTONE Road MILE END	Dwelling	CT 5319/870	21327
11 GLADSTONE Road MILE END	Dwelling	CT 5071/17	21350
12 GLADSTONE Road MILE END	Dwelling	CT 5839/359	21326
13 GLADSTONE Road MILE END	Dwelling	CT 5714/718	21349
14 GLADSTONE Road MILE END	Dwelling	CT 5508/430	21325
14A GLADSTONE Road MILE END	Dwelling	CT 5839/358	21324
15 GLADSTONE Road MILE END	Dwelling	CT 5071/16	21348
16 Gladstone Road MILE END	Dwelling	CT 5644/968 CT 5644/971	21323
17 GLADSTONE Road MILE END	Dwelling	CT 5071/15	21347
18 GLADSTONE Road MILE END	Dwelling	CT 5427/660	21322
19 GLADSTONE Road MILE END	Dwelling	CT 5470/913	21346
20 GLADSTONE Road MILE END	Dwelling	CT 5567/446	21321
21 GLADSTONE Road MILE END	Dwelling	CT 5813/46	21345
22 GLADSTONE Road MILE END	Dwelling	CT 5344/844	21320
23 GLADSTONE Road MILE END	Dwelling	CT 5505/415	21344

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<b>Property Address</b>	<b>Description of Contributory item</b>	<b>Certificate of Title</b>	<b>DPLG ID</b>
1&2/ 24 GLADSTONE Road MILE END	Dwelling	CT 5025/738 CT 5025/739	21319
25 GLADSTONE Road MILE END	Dwelling	CT 6053/96	21343
27 GLADSTONE Road MILE END	Dwelling	CT 5304/799	21342
29 GLADSTONE Road MILE END	Dwelling	CT 5799/257	21341
30 GLADSTONE Road MILE END	Dwelling	CT 5658/333	21767
31 GLADSTONE Road MILE END	Dwelling	CT 5720/86	21340
33 GLADSTONE Road MILE END	Dwelling	CT 5799/259	21339
1/ 34 GLADSTONE Road MILE END	Dwelling	CT 5000/926	21766
35-35A GLADSTONE Road MILE END	Dwelling	CT 5837/743	21338
36 GLADSTONE Road MILE END	Dwelling	CT 5717/539	21765
36A GLADSTONE Road MILE END	Dwelling	CT 5646/207	21764
38 GLADSTONE Road MILE END	Dwelling	CT 6156/508	21763
39 GLADSTONE Road MILE END	Shop	CT 5893/575	21748
40 GLADSTONE Road MILE END	Dwelling	CT 5490/336	21762
41 GLADSTONE Road MILE END	Dwelling	CT 5775/493	21746
42 & 42A GLADSTONE Road MILE END	Dwelling	CT 5376/542 CT 5376/543	21761
43 GLADSTONE Road MILE END	Dwelling	CT 5867/777	21745
45-45A GLADSTONE Road MILE END	Dwelling	CT 5210/910	21744
47 GLADSTONE Road MILE END	Dwelling	CT 5199/149	21743
48 GLADSTONE Road MILE END	Dwelling	CT 5704/532	21760
49 GLADSTONE Road MILE END	Dwelling	CT 5908/827	21742
50 GLADSTONE Road MILE END	Dwelling	CT 5803/866	21759
50A GLADSTONE Road MILE END	Dwelling	CT 5258/942	21758
53 GLADSTONE Road MILE END	Dwelling	CT 5657/408	21741
54 GLADSTONE Road MILE END	Dwelling	CT 6056/199	21757
55 GLADSTONE Road MILE END	Dwelling	CT 5291/487	21740
2/ 57 & 57A GLADSTONE Road MILE END	Dwelling	CT 5037/127 CT 5037/129 CT 5867/447	21739
58 GLADSTONE Road MILE END	Dwelling	CT 5813/921	21756
58A GLADSTONE Road MILE END	Dwelling	CT 5364/246	21755

Property Address	Description of Contributory item	Certificate of Title	DPLG ID
59 & 59A GLADSTONE Road MILE END	Dwelling	CT 5018/523 CT 5018/524 CT 5018/525	21738
61 GLADSTONE Road MILE END	Dwelling	CT 5797/492	21737
62 GLADSTONE Road MILE END	Dwelling	CT 5173/505	21754
62A GLADSTONE Road MILE END	Dwelling	CT 5302/61	21753
65 GLADSTONE Road MILE END	Dwelling	CT 5546/677	21736
67 GLADSTONE Road MILE END	Dwelling	CT 5805/104	21735
69 GLADSTONE Road MILE END	Dwelling	CT 5292/104	21734
71 GLADSTONE Road MILE END	Dwelling	CT 5281/380	21733
75 GLADSTONE Road MILE END	Dwelling	CT 5135/123 CT 5135/125	21732
77 GLADSTONE Road MILE END	Dwelling	CT 5292/763	21731
79 GLADSTONE Road MILE END	Dwelling	CT 5810/580	21730
81 & 81A GLADSTONE Road MILE END	Dwelling	CT 6013/410 CT 6013/411	21729
83 & 83A GLADSTONE Road MILE END	Dwelling	CT 5153/703 CT 6064/664	21728
85 & 87 GLADSTONE Road MILE END	Dwelling	CT 5974/305 CT 5974/306	21727
34 HENLEY BEACH Road MILE END	Shops	CT 5225/163	21810
36 HENLEY BEACH Road MILE END	Shop	CT 5365/802	21809
40 HENLEY BEACH Road MILE END	Shop	CT 5221/224	21808
42 HENLEY BEACH Road MILE END	Shop	CT 5882/170	21807
44 HENLEY BEACH Road MILE END	Shop	CT 5514/551	21806
50-60 HENLEY BEACH Road MILE END	Shops	CT 5225/682 CT 5309/298	21804
62-62A HENLEY BEACH Road MILE END	Shops and Office	CT 5339/775	21803
1-4/ 68 & 70 HENLEY BEACH Road MILE END	Shop	CT 5875/176 CT 5875/177 CT 5875/178 CT 5875/179	21801
72 HENLEY BEACH Road MILE END	Shop	CT 5239/425	21800
3 HUGHES Street MILE END	Dwelling	CT 5786/303	21311
4 HUGHES Street MILE END	Dwelling	CT 5355/857	21554
5 HUGHES Street MILE END	Dwelling	CT 5319/860	21310

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<b>Property Address</b>	<b>Description of Contributory item</b>	<b>Certificate of Title</b>	<b>DPLG ID</b>
6 HUGHES Street MILE END	Dwelling	CT 5511/407	21553
7 HUGHES Street MILE END	Dwelling	CT 5319/859	21309
8 & 8A HUGHES Street MILE END	Dwelling	CT 6150/474 CT 6150/475 CT 6150/476	21552
9 HUGHES Street MILE END	Dwelling	CT 5378/252	21308
10 HUGHES Street MILE END	Dwelling	CT 5256/39	21551
11-11A HUGHES Street MILE END	Dwelling	CT 5319/858	21307
12 HUGHES Street MILE END	Dwelling	CT 5113/962	21550
13 HUGHES Street MILE END	Dwelling	CT 5327/210	21306
14A HUGHES Street MILE END	Dwelling	CT 5506/603	21549
16 HUGHES Street MILE END	Community	CT 5506/602	21548
17 HUGHES Street MILE END	Dwelling	CT 5813/825	21305
18 HUGHES Street MILE END	Dwelling	CT 5824/219	21547
19 HUGHES Street MILE END	Dwelling	CT 5830/501	21304
20 HUGHES Street MILE END	Dwelling	CT 5814/661	21546
21 HUGHES Street MILE END	Dwelling	CT 5839/356	21303
22 & 24 HUGHES Street MILE END	Dwelling	CT 6014/148	21544
23 HUGHES Street MILE END	Dwelling	CT 5566/801	21302
25 HUGHES Street MILE END	Dwelling	CT 5775/543	21301
26 HUGHES Street MILE END	Dwelling	CT 5875/809	21543
27 HUGHES Street MILE END	Dwelling	CT 5775/545	21300
28 HUGHES Street MILE END	Dwelling	CT 5875/811	21542
29 HUGHES Street MILE END	Dwelling	CT 5442/347	21299
30 & 30A HUGHES Street MILE END	Dwelling	CT 5825/934 CT 5864/566	21541
31 HUGHES Street MILE END	Dwelling	CT 6155/925	21298
32 & 32A HUGHES Street MILE END	Dwelling	CT 5509/293 CT 5672/137	21540
33A HUGHES Street MILE END	Dwelling	CT 5476/14	21561
34 HUGHES Street MILE END	Dwelling	CT 5898/135	21539
35 HUGHES Street MILE END	Dwelling	CT 5475/735	21560
36 HUGHES Street MILE END	Dwelling	CT 5839/355	21538

Property Address	Description of Contributory item	Certificate of Title	DPLG ID
37 HUGHES Street MILE END	Dwelling	CT 5935/585 CT 5935/588	21559
38 HUGHES Street MILE END	Dwelling	CT 5362/751	21537
39 HUGHES Street MILE END	Dwelling	CT 5671/421	21558
40 HUGHES Street MILE END	Dwelling	CT 5786/301	21536
41 HUGHES Street MILE END	Dwelling	CT 5649/930	21557
42 & 42A HUGHES Street MILE END	Dwelling	CT 5235/436 CT 5864/567	21535
43 HUGHES Street MILE END	Dwelling	CT 5117/351	21556
46 HUGHES Street MILE END	Dwelling	CT 5065/750	21531
1-2/ 47 HUGHES Street MILE END	Dwelling	CT 5026/459 CT 5026/460 CT 5026/461	21774
48 HUGHES Street MILE END	Dwelling	CT 5141/306	21530
49 HUGHES Street MILE END	Dwelling	CT 5859/698	21775
1/ 50 & 52 HUGHES Street MILE END	Dwelling	CT 5038/212 CT 5038/214 CT 5859/198	21529
51 HUGHES Street MILE END	Dwelling	CT 5392/732	21776
53 HUGHES Street MILE END	Dwelling	CT 5762/389	21777
54 HUGHES Street MILE END	Dwelling	CT 5823/830	21528
55 HUGHES Street MILE END	Dwelling	CT 5584/988	21778
56 HUGHES Street MILE END	Dwelling	CT 5820/465	21527
57 HUGHES Street MILE END	Dwelling	CT 5201/810	21779
58 HUGHES Street MILE END	Dwelling	CT 5402/299	21526
59 HUGHES Street MILE END	Dwelling	CT 5647/916	21780
60 HUGHES Street MILE END	Dwelling	CT 5811/733	21525
61 HUGHES Street MILE END	Dwelling	CT 6123/943	21781
62 HUGHES Street MILE END	Dwelling	CT 5926/3	21524
63 HUGHES Street MILE END	Dwelling	CT 5324/172	21782
64 HUGHES Street MILE END	Dwelling	CT 6027/604 CT 6027/606	21523
65-65A HUGHES Street MILE END	Dwelling	CT 5837/277	21783
67 HUGHES Street MILE END	Dwelling	CT 5780/4	21784
68 HUGHES Street MILE END	Dwelling	CT 5582/703	21522

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<b>Property Address</b>	<b>Description of Contributory item</b>	<b>Certificate of Title</b>	<b>DPLG ID</b>
71 HUGHES Street MILE END	Dwelling	CT 6164/230 CT 6164/233	21785
72 HUGHES Street MILE END	Dwelling	CT 5838/463	21521
73 HUGHES Street MILE END	Dwelling	CT 5378/916 CT 5378/997	21786
75 HUGHES Street MILE END	Dwelling	CT 5378/917	21787
80 HUGHES Street MILE END	Dwelling	CT 5297/412	21520
81 HUGHES Street MILE END	Dwelling	CT 5280/951	21788
83 HUGHES Street MILE END	Dwelling	CT 5925/878	21789
1 JUNCTION Street MILE END	Dwelling	CT 5589/905	21812
2 JUNCTION Street MILE END	Dwelling	CT 5100/685	21813
3 JUNCTION Street MILE END	Dwelling	CT 6106/446	21814
4 JUNCTION Street MILE END	Dwelling	CT 5192/942	21815
5 JUNCTION Street MILE END	Dwelling	CT 5233/658	21816
6 JUNCTION Street MILE END	Dwelling	CT 5138/94	21817
7 JUNCTION Street MILE END	Dwelling	CT 5713/912	21818
8 JUNCTION Street MILE END	Dwelling	CT 5094/539	21819
1 KING Street MILE END	Dwelling	CT 5372/200	21430
1A KING Street MILE END	Dwelling	CT 5880/499	21396
3 KING Street MILE END	Dwelling	CT 5164/956	21395
4 KING Street MILE END	Dwelling	CT 5778/523	21389
5 KING Street MILE END	Dwelling	CT 5164/957	21394
8A KING Street MILE END	Dwelling	CT 5786/823	21388
10 KING Street MILE END	Dwelling	CT 5558/490	21387
12 KING Street MILE END	Dwelling	CT 5787/645	21386
14 & 14A KING Street MILE END	Dwelling	CT 5862/965 CT 5862/968	21384
15 KING Street MILE END	Dwelling	CT 5633/946	21445
16 & 16A KING Street MILE END	Dwelling	CT 5862/973 CT 5862/975	21382
17 KING Street MILE END	Dwelling	CT 5900/470	21444
18 KING Street MILE END	Dwelling	CT 6111/971	21381
19 KING Street MILE END	Dwelling	CT 5404/616	21443

Property Address	Description of Contributory item	Certificate of Title	DPLG ID
20 KING Street MILE END	Dwelling	CT 5713/916	21380
21 KING Street MILE END	Dwelling	CT 5392/787	21442
23 KING Street MILE END	Dwelling	CT 5810/545	21441
25 & 25A KING Street MILE END	Dwelling	CT 5846/413 CT 5846/414	21440
26 KING Street MILE END	Dwelling	CT 5887/848	21567
27 KING Street MILE END	Dwelling	CT 5824/39	21439
28 KING Street MILE END	Dwelling	CT 5638/470	21566
29 KING Street MILE END	Dwelling	CT 5892/437	21438
30 KING Street MILE END	Dwelling	CT 5763/913	21565
31 & 31A KING Street MILE END	Dwelling	CT 5892/438 CT 5892/439	21437
32A & 32B KING Street MILE END	Dwelling	CT 5018/467 CT 5018/468 CT 5018/469	21564
33A KING Street MILE END	Dwelling	CT 5335/27	21436
34 KING Street MILE END	Dwelling	CT 5658/379	21563
1/ 35 & 35A KING Street MILE END	Dwelling	CT 5202/718 CT 5202/719 CT 5335/28	21435
36 KING Street MILE END	Dwelling	CT 5795/86	21562
37 KING Street MILE END	Dwelling	CT 5892/435	21434
38 KING Street MILE END	Dwelling	CT 5512/292	21429
39 KING Street MILE END	Dwelling	CT 5211/23	21433
40 KING Street MILE END	Dwelling	CT 5802/345	21428
41 KING Street MILE END	Dwelling	CT 5816/266	21432
42 KING Street MILE END	Dwelling	CT 5886/683	21427
43 KING Street MILE END	Dwelling	CT 5853/88	21431
44 KING Street MILE END	Dwelling	CT 5793/469	21426
45 KING Street MILE END	Dwelling	CT 5334/638	21405
46 KING Street MILE END	Dwelling	CT 5515/653	21425
47 KING Street MILE END	Dwelling	CT 5739/760	21406
48 KING Street MILE END	Dwelling	CT 5578/94	21424
49 KING Street MILE END	Dwelling	CT 5792/226	21407

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<b>Property Address</b>	<b>Description of Contributory item</b>	<b>Certificate of Title</b>	<b>DPLG ID</b>
50 KING Street MILE END	Dwelling	CT 5316/362	21423
51 KING Street MILE END	Dwelling	CT 5418/579	21408
52 KING Street MILE END	Dwelling	CT 5723/894	21422
53 KING Street MILE END	Dwelling	CT 5773/791	21409
54 & 54A KING Street MILE END	Dwelling	CT 6065/820 CT 6065/821 CT 6065/822	21421
55 KING Street MILE END	Dwelling	CT 5771/101	21410
56 KING Street MILE END	Dwelling	CT 5167/790	21420
58 KING Street MILE END	Dwelling	CT 5983/351	21419
60 KING Street MILE END	Dwelling	CT 5346/584	21418
61 KING Street MILE END	Dwelling	CT 5673/320	21411
62A-62D KING Street MILE END	Dwelling	CT 5201/772	21417
64 KING Street MILE END	Dwelling	CT 5466/557	21416
66 KING Street MILE END	Dwelling	CT 5803/849	21415
1 PARKER Street MILE END	Dwelling	CT 5748/337	21513
2 PARKER Street MILE END	Dwelling	CT 5102/555	21519
3 PARKER Street MILE END	Dwelling	CT 5435/74	21512
4 PARKER Street MILE END	Dwelling	CT 5704/943	21518
5 PARKER Street MILE END	Dwelling	CT 5283/558	21511
6 PARKER Street MILE END	Dwelling	CT 5544/779	21517
7 PARKER Street MILE END	Dwelling	CT 5748/321	21510
8 PARKER Street MILE END	Dwelling	CT 5707/768	21516
10 PARKER Street MILE END	Dwelling	CT 5557/104	21515
6-10 RAILWAY Terrace MILE END	Dwelling	CT 5124/414 CT 5124/415	21555
12 RAILWAY Terrace MILE END	Dwelling	CT 5108/322 CT 5786/304	21312
14 RAILWAY Terrace MILE END	Dwelling	CT 5501/71	21313
16 RAILWAY Terrace MILE END	Dwelling	CT 5313/340	21314
18 RAILWAY Terrace MILE END	Dwelling	CT 5839/357	21337
20 RAILWAY Terrace MILE END	Dwelling	CT 5284/536	21336
22 RAILWAY Terrace MILE END	Dwelling	CT 5786/296	21335



Property Address	Description of Contributory item	Certificate of Title	DPLG ID
28 RAILWAY Terrace MILE END	Dwelling	CT 5351/80	21379
30 RAILWAY Terrace MILE END	Dwelling	CT 5889/778	21393
32 RAILWAY Terrace MILE END	Dwelling	CT 5164/972	21392
34 RAILWAY Terrace MILE END	Dwelling	CT 5164/971	21391
36 RAILWAY Terrace MILE END	Dwelling	CT 5878/354	21390
34 ROSE Street MILE END	Dwelling	CT 5141/578	21514
37A ROSE Street MILE END	Dwelling	CT 5869/141	21509
1 & 1A VICTORIA Street MILE END	Dwelling	CT 5028/47 CT 5028/48 CT 5028/49	21811
4 VICTORIA Street MILE END	Dwelling	CT 5188/558	21534
6 VICTORIA Street MILE END	Dwelling	CT 5747/604	21533
8 VICTORIA Street MILE END	Dwelling	CT 5813/152	21532
10 VICTORIA Street MILE END	Dwelling	CT 5343/277	21773
12 VICTORIA Street MILE END	Dwelling	CT 5883/859	21772
14 VICTORIA Street MILE END	Dwelling	CT 5561/153	21771
15 VICTORIA Street MILE END	Dwelling	CT 5497/182	21315
16 VICTORIA Street MILE END	Dwelling	CT 5837/278	21770
17 VICTORIA Street MILE END	Dwelling	CT 5355/832	21316
17A VICTORIA Street MILE END	Dwelling	CT 5554/417	21317
18 VICTORIA Street MILE END	Dwelling	CT 5558/383	21769
19 VICTORIA Street MILE END	Dwelling	CT 5786/297	21318
20 VICTORIA Street MILE END	Dwelling	CT 5826/146	21768
22 VICTORIA Street MILE END	Dwelling	CT 5874/686	21747
24 VICTORIA Street MILE END	Dwelling	CT 5874/768	21573
24A VICTORIA Street MILE END	Dwelling	CT 5874/789	21572
26 & 26A VICTORIA Street MILE END	Dwelling	CT 6059/459 CT 6059/460	21571
28 VICTORIA Street MILE END	Dwelling	CT 5874/769	21570
28A VICTORIA Street MILE END	Dwelling	CT 5124/376	21569
1/ 30 & 30A VICTORIA Street MILE END	Dwelling	CT 5049/507 CT 5049/508 CT 5049/509	21568
8 ALBERT Street RICHMOND	Dwelling	CT 6031/109	21680

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<b>Property Address</b>	<b>Description of Contributory item</b>	<b>Certificate of Title</b>	<b>DPLG ID</b>
21 Albert Street RICHMOND	Dwelling	CT 6037/622	26033
23 ALBERT Street RICHMOND	Dwelling	CT 6037/623	21626
24 & 26 ALBERT Street RICHMOND	Dwelling	CT 6031/62 CT 6031/63	21625
25 & 27 ALBERT Street RICHMOND	Dwelling	CT 6037/624 CT 6037/625	21677
28 ALBERT Street RICHMOND	Dwelling	CT 6031/61	21624
30 ALBERT Street RICHMOND	Dwelling	CT 6031/60	21676
8 & 10 DAVENPORT Terrace RICHMOND	Dwelling	CT 6031/67 CT 6031/68	21619
12 & 14 DAVENPORT Terrace RICHMOND	Dwelling	CT 6031/70 CT 6031/71	21618
16 & 18 DAVENPORT Terrace RICHMOND	Dwelling	CT 6117/857 CT 6117/858	21616
20 & 22 DAVENPORT Terrace RICHMOND	Dwelling	CT 6117/861 CT 6117/862	21615
24 & 26 DAVENPORT Terrace RICHMOND	Dwelling	CT 6117/865 CT 6117/866	21614
1 & 3 MARTIN Avenue RICHMOND	Dwelling	CT 6031/73 CT 6031/74	21617
5 & 7 MARTIN Avenue RICHMOND	Dwelling	CT 6058/5 CT 6058/6	21627
6 & 8 MARTIN Avenue RICHMOND	Dwelling	CT 6037/614 CT 6037/615	21663
9 & 11 MARTIN Avenue RICHMOND	Dwelling	CT 6058/3 CT 6058/4	21664
13 & 15 MARTIN Avenue RICHMOND	Dwelling	CT 6058/1 CT 6058/2	21678
14 & 16 MARTIN Avenue RICHMOND	Dwelling	CT 6031/64 CT 6031/65	21665
17 & 19 MARTIN Avenue RICHMOND	Dwelling	CT 6037/96 CT 6037/97	21679
33 MILNER Road RICHMOND	Dwelling	CT 6117/869	21670
35 MILNER Road RICHMOND	Dwelling	CT 6117/871	21613
37 MILNER Road RICHMOND	Dwelling	CT 6117/872	21612
39 & 41 MILNER Road RICHMOND	Dwelling	CT 6037/600 CT 6037/601	21622
43 MILNER Road RICHMOND	Dwelling	CT 6037/599	21621

Property Address	Description of Contributory item	Certificate of Title	DPLG ID
45 Milner Road RICHMOND	Dwelling	CT 6037/597	26032
47 MILNER Road RICHMOND	Dwelling	CT 6037/596	21668
1 ASHLEY Street TORRENSVILLE	Dwelling	CT 5546/676	21167
3 ASHLEY Street TORRENSVILLE	Dwelling	CT 5787/337	21168
5 ASHLEY Street TORRENSVILLE	Dwelling	CT 5704/277	21169
7 ASHLEY Street TORRENSVILLE	Dwelling	CT 5785/966	21170
9 ASHLEY Street TORRENSVILLE	Dwelling	CT 5785/965	21171
11 ASHLEY Street TORRENSVILLE	Dwelling	CT 5785/964	21172
13 ASHLEY Street TORRENSVILLE	Dwelling	CT 5830/140	21466
2 CARLTON Parade TORRENSVILLE	Dwelling	CT 5813/461	21156
4 CARLTON Parade TORRENSVILLE	Dwelling	CT 5815/864	21155
6 CARLTON Parade TORRENSVILLE	Dwelling	CT 5856/215	21154
8 CARLTON Parade TORRENSVILLE	Dwelling	CT 5856/220	21153
10 CARLTON Parade TORRENSVILLE	Dwelling	CT 5820/797	21152
14 CARLTON Parade TORRENSVILLE	Dwelling	CT 5439/45	21138
15A CARLTON Parade TORRENSVILLE	Dwelling	CT 5147/187	21243
16 CARLTON Parade TORRENSVILLE	Dwelling	CT 5797/398	21137
18 CARLTON Parade TORRENSVILLE	Dwelling	CT 5743/875	21136
20 CARLTON Parade TORRENSVILLE	Dwelling	CT 5781/684	21135
22 CARLTON Parade TORRENSVILLE	Dwelling	CT 5682/159	21134
24 CARLTON Parade TORRENSVILLE	Dwelling	CT 5729/957	21260
26 CARLTON Parade TORRENSVILLE	Dwelling	CT 5797/412	21261
28 CARLTON Parade TORRENSVILLE	Dwelling	CT 5832/644	21262
30 CARLTON Parade TORRENSVILLE	Dwelling	CT 5895/872	21263
32 CARLTON Parade TORRENSVILLE	Dwelling	CT 5545/280	21264
34 CARLTON Parade TORRENSVILLE	Dwelling	CT 5160/981	21265
36 CARLTON Parade TORRENSVILLE	Dwelling	CT 5779/514	21279
1A DANBY Street TORRENSVILLE	Dwelling	CT 5700/472	21908
3 DANBY Street TORRENSVILLE	Dwelling	CT 5455/6	21907
4 DANBY Street TORRENSVILLE	Dwelling	CT 5287/980	21912
5 DANBY Street TORRENSVILLE	Dwelling	CT 5108/225	21906

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<b>Property Address</b>	<b>Description of Contributory item</b>	<b>Certificate of Title</b>	<b>DPLG ID</b>
6 DANBY Street TORRENSVILLE	Club	CT 5722/895	21911
7 DANBY Street TORRENSVILLE	Dwelling	CT 5193/557	21905
8 DANBY Street TORRENSVILLE	Club	CT 5547/977	21910
9 DANBY Street TORRENSVILLE	Dwelling	CT 5732/317	21904
10 DANBY Street TORRENSVILLE	Dwelling	CT 5700/85	21909
11 DANBY Street TORRENSVILLE	Dwelling	CT 5766/488	21231
12 DANBY Street TORRENSVILLE	Dwelling	CT 5546/673	21227
13 DANBY Street TORRENSVILLE	Dwelling	CT 6008/343	21230
15 DANBY Street TORRENSVILLE	Dwelling	CT 5563/94	21229
17 DANBY Street TORRENSVILLE	Dwelling	CT 5167/631	21228
19 DANBY Street TORRENSVILLE	Dwelling	CT 5553/102	21151
20 DANBY Street TORRENSVILLE	Dwelling	CT 5119/762	21210
21 DANBY Street TORRENSVILLE	Dwelling	CT 5109/129	21149
22 DANBY Street TORRENSVILLE	Dwelling	CT 5119/947	21209
23 DANBY Street TORRENSVILLE	Dwelling	CT 5515/900	21147
24 DANBY Street TORRENSVILLE	Dwelling	CT 5119/946	21208
25 DANBY Street TORRENSVILLE	Dwelling	CT 5211/820	21144
26 DANBY Street TORRENSVILLE	Dwelling	CT 5118/179	21207
27 DANBY Street TORRENSVILLE	Dwelling	CT 5773/157	21142
28 DANBY Street TORRENSVILLE	Dwelling	CT 5118/351	21206
30 DANBY Street TORRENSVILLE	Dwelling	CT 5118/173	21205
31 DANBY Street TORRENSVILLE	Dwelling	CT 5818/989	21164
32 DANBY Street TORRENSVILLE	Dwelling	CT 5118/353	21204
33 DANBY Street TORRENSVILLE	Dwelling	CT 5408/303	21165
34 DANBY Street TORRENSVILLE	Dwelling	CT 5119/944 CT 5119/945	21203
35 DANBY Street TORRENSVILLE	Dwelling	CT 5783/789	21463
36 DANBY Street TORRENSVILLE	Dwelling	CT 5119/777	21202
37 DANBY Street TORRENSVILLE	Dwelling	CT 5560/390	21464
38 DANBY Street TORRENSVILLE	Dwelling	CT 5119/943	21201
39 DANBY Street TORRENSVILLE	Dwelling	CT 5802/327	21465
40 DANBY Street TORRENSVILLE	Dwelling	CT 5119/766	21200

Property Address	Description of Contributory item	Certificate of Title	DPLG ID
42 DANBY Street TORRENSVILLE	Dwelling	CT 5119/795	21199
44 DANBY Street TORRENSVILLE	Dwelling	CT 5119/807	21198
46 DANBY Street TORRENSVILLE	Dwelling	CT 6079/392 CT 6079/393	21185
46A DANBY Street TORRENSVILLE	Dwelling	CT 5596/991	21184
48 DANBY Street TORRENSVILLE	Dwelling	CT 5756/887	21183
50 DANBY Street TORRENSVILLE	Dwelling	CT 5786/281	21182
52 DANBY Street TORRENSVILLE	Dwelling	CT 5786/280	21181
54 DANBY Street TORRENSVILLE	Dwelling	CT 5656/755	21180
56 DANBY Street TORRENSVILLE	Dwelling	CT 5786/659	21179
58 DANBY Street TORRENSVILLE	Dwelling	CT 5791/71	21178
60 DANBY Street TORRENSVILLE	Dwelling	CT 5546/227	21177
62 DANBY Street TORRENSVILLE	Dwelling	CT 6110/951	21176
64 DANBY Street TORRENSVILLE	Dwelling	CT 5785/967	21175
66 DANBY Street TORRENSVILLE	Dwelling	CT 5787/863	21174
68 DANBY Street TORRENSVILLE	Dwelling	CT 5785/963	21173
168 HENLEY BEACH Road TORRENSVILLE	Shops	CT 5312/864	21797
170 HENLEY BEACH Road TORRENSVILLE	Shops	CT 5304/775 CT 5304/776 CT 5304/783	21795
174 HENLEY BEACH Road TORRENSVILLE	Shop and Office	CT 5108/661	21794
178 HENLEY BEACH Road TORRENSVILLE	Shop	CT 5108/659	21793
192 & 192B HENLEY BEACH Road TORRENSVILLE	Shops	CT 5580/707 CT 5739/737	21798
200 HENLEY BEACH Road TORRENSVILLE	Shop	CT 5360/845	21799
1 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5406/789	21882
2 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5763/895	21895
3 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5797/521	21883
4 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5790/93	21891
5 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5797/520	21884
6 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5609/300	21890
8 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5790/94	21889
9 & 11 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5002/983 CT 5002/984	21885

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<b>Property Address</b>	<b>Description of Contributory item</b>	<b>Certificate of Title</b>	<b>DPLG ID</b>
		CT 5002/986	
10 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5305/855	21888
12 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5752/293	21887
14 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5856/368	21247
15 & 15A HUNTRISS Street TORRENSVILLE	Dwelling	CT 5437/806	21278
15B HUNTRISS Street TORRENSVILLE	Dwelling	CT 6002/92	21276
1/16 & 2/18 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5005/928 CT 5005/929	21246
17 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5405/577	21275
20 & 22 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5300/760 CT 5398/804	21245
21 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5721/756	21274
23 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5814/67	21273
25 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5352/606	21277
26 HUNTRISS Street TORRENSVILLE	Shop and Dwelling	CT 5814/192	21244
28 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5840/283	21266
29 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5831/886	21503
30 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5881/822	21267
31 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5104/921	21502
32 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5116/476	21268
33 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5294/164	21501
34 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5349/501	21269
35 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5892/381	21500
36 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5840/3	21270
37 HUNTRISS Street TORRENSVILLE	Dwelling	CT 6068/805	21499
38 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5806/773	21271
39 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5758/921	21498
40 & 42 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5578/347 CT 5578/348	21272
41 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5247/181	21497
44 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5075/378	21485
45 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5783/639	21496
46 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5814/948	21486

Property Address	Description of Contributory item	Certificate of Title	DPLG ID
47 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5539/624	21495
48 & 1/50 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5014/873 CT 5014/875 CT 5958/984	21487
52 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5386/287	21488
54 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5839/125	21489
56 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5561/626	21490
58 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5461/508	21491
60 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5876/107	21492
62 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5406/817	21493
64 HUNTRISS Street TORRENSVILLE	Dwelling	CT 5310/618	21494
2 NORTH Parade TORRENSVILLE	Dwelling	CT 5152/754	21188
4 NORTH Parade TORRENSVILLE	Dwelling	CT 5448/278	21187
1-2/ 6 NORTH Parade TORRENSVILLE	Dwelling	CT 5193/494 CT 5193/495	21186
8 NORTH Parade TORRENSVILLE	Dwelling	CT 5769/365	21157
10 NORTH Parade TORRENSVILLE	Dwelling	CT 5813/723	21158
12 NORTH Parade TORRENSVILLE	Dwelling	CT 5815/453	21159
14 NORTH Parade TORRENSVILLE	Dwelling	CT 5815/450	21160
16 NORTH Parade TORRENSVILLE	Dwelling	CT 5815/451	21161
18 NORTH Parade TORRENSVILLE	Dwelling	CT 5512/282	21941
20 NORTH Parade TORRENSVILLE	Dwelling	CT 5524/900	21162
21 NORTH Parade TORRENSVILLE	Dwelling	CT 5142/511	21253
23 NORTH Parade TORRENSVILLE	Dwelling	CT 5485/708	21252
24 NORTH Parade TORRENSVILLE	Dwelling	CT 5202/961	21480
26 NORTH Parade TORRENSVILLE	Dwelling	CT 5838/4	21481
28 NORTH Parade TORRENSVILLE	Dwelling	CT 5356/342	21482
30 NORTH Parade TORRENSVILLE	Dwelling	CT 5495/428	21483
32 NORTH Parade TORRENSVILLE	Dwelling	CT 5892/33	21484
1 NORTHCOTE Street TORRENSVILLE	Office	CT 5087/638	21899
1A NORTHCOTE Street TORRENSVILLE	Office	CT 5761/14	21898
2 NORTHCOTE Street TORRENSVILLE	Dwelling	CT 6058/857	21903
6 NORTHCOTE Street TORRENSVILLE	Dwelling	CT 5811/702	21902

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<b>Property Address</b>	<b>Description of Contributory item</b>	<b>Certificate of Title</b>	<b>DPLG ID</b>
7 NORTHCOTE Street TORRENSVILLE	Dwelling	CT 5808/540	21897
8 NORTHCOTE Street TORRENSVILLE	Dwelling	CT 5704/570	21901
9 NORTHCOTE Street TORRENSVILLE	Dwelling	CT 5338/297	21239
10 NORTHCOTE Street TORRENSVILLE	Dwelling	CT 5559/23	21900
11 NORTHCOTE Street TORRENSVILLE	Dwelling	CT 5780/965	21238
12 NORTHCOTE Street TORRENSVILLE	Dwelling	CT 6168/626 CT 6168/628	21235
13 NORTHCOTE Street TORRENSVILLE	Dwelling	CT 5780/738	21237
14 NORTHCOTE Street TORRENSVILLE	Dwelling	CT 5548/373	21234
15 NORTHCOTE Street TORRENSVILLE	Dwelling	CT 5074/772	21236
16 NORTHCOTE Street TORRENSVILLE	Dwelling	CT 6004/761 CT 6004/763	21233
17 NORTHCOTE Street TORRENSVILLE	Dwelling	CT 5365/316	21133
18 NORTHCOTE Street TORRENSVILLE	Dwelling	CT 5810/328	21232
19 NORTHCOTE Street TORRENSVILLE	Dwelling	CT 5857/875	21296
20 NORTHCOTE Street TORRENSVILLE	Dwelling	CT 5232/871	21150
21 NORTHCOTE Street TORRENSVILLE	Dwelling	CT 5850/535	21294
22 NORTHCOTE Street TORRENSVILLE	Dwelling	CT 5198/236	21148
23 NORTHCOTE Street TORRENSVILLE	Dwelling	CT 5614/488	21292
24 NORTHCOTE Street TORRENSVILLE	Dwelling	CT 5274/381	21146
25 NORTHCOTE Street TORRENSVILLE	Dwelling	CT 5898/884	21290
26 NORTHCOTE Street TORRENSVILLE	Dwelling	CT 5494/809	21145
27 NORTHCOTE Street TORRENSVILLE	Dwelling	CT 5821/398	21288
28 NORTHCOTE Street TORRENSVILLE	Dwelling	CT 5606/489	21143
29 NORTHCOTE Street TORRENSVILLE	Dwelling	CT 5855/780	21287
30 NORTHCOTE Street TORRENSVILLE	Dwelling	CT 5856/439	21141
32 NORTHCOTE Street TORRENSVILLE	Dwelling	CT 5856/375	21140
33 NORTHCOTE Street TORRENSVILLE	Dwelling	CT 5883/877	21283
34 NORTHCOTE Street TORRENSVILLE	Dwelling	CT 5856/372	21139
35 NORTHCOTE Street TORRENSVILLE	Dwelling	CT 5448/745	21281
94 SOUTH Road TORRENSVILLE	Dwelling	CT 5158/721	21166
116-118 SOUTH Road TORRENSVILLE	Office and Dwelling	CT 5771/143	21223



Property Address	Description of Contributory item	Certificate of Title	DPLG ID
154 SOUTH Road TORRENSVILLE	Office and Dwelling	CT 5848/479	21913
156 & 158 SOUTH Road TORRENSVILLE	Office and Dwelling	CT 6151/604 CT 6161/603	21914
160 SOUTH Road TORRENSVILLE	Dwelling	CT 5575/282	21915
162 SOUTH Road TORRENSVILLE	Office and Dwelling	CT 5749/298	21916
164 SOUTH Road TORRENSVILLE	Office	CT 5789/676 CT 5802/36	21917
5 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 6084/926 CT 6084/927	21894
7 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 5789/459	21893
9 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 5780/625	21892
11 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 5225/787	21248
12 WAINHOUSE Street TORRENSVILLE	Office	CT 5355/912	21896
13 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 5515/237	21249
15 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 5843/5	21250
16 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 6037/728	21241
17 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 5135/713	21251
19 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 5821/403	21259
20 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 6037/727	21242
21 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 5846/25	21258
22 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 5547/525	21297
23 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 5547/906	21257
24 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 5871/212	21295
25 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 5547/578	21256
26 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 5895/880	21293
27 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 5545/166	21255
28 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 5895/632	21291
29 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 5803/658	21254
30 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 5928/964	21289
31 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 5164/89	21479
32 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 5871/421	21286
33 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 5285/324	21478
34 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 5896/422	21285

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Property Address	Description of Contributory item	Certificate of Title	DPLG ID
36 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 5876/843	21284
37 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 5558/956	21477
38 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 5845/131	21282
39 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 5803/884	21476
40 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 5896/448	21280
41 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 5546/718	21475
42 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 5813/984	21163
45 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 5769/277	21474
47 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 5376/6	21473
49 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 5547/492	21472
52 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 5623/1	21471
54 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 5795/809	21470
56 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 5764/151	21469
58 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 5780/396	21468
60 WAINHOUSE Street TORRENSVILLE	Dwelling	CT 5780/395	21467

## Table WeTo/4 - Local Heritage Places

Property Address	Description and/or Extent of Listed Place	Lot No. or Part Sec	Plan No.	Certificate of Title	Section 23(4) Criteria	DPLG ID
85-87 Anzac Highway ASHFORD	Ashford House School & Fence; Extent of 1882 residence including pillared front portico, front verandahs, bay windows, face stone and brickwork, slate roof, moulded chimneys, cast iron verandah balustrading and all other original architectural details. Front fence with 4 masonry pillars, masonry and cast iron fence sections, and cast iron double gates. Remaining garden elements including 2 mature Port Jackson figs ( <i>Ficus benjamina</i> ) 1998 Heritage Survey Ref AS01	700	F10556	CT 5841/495	a c d e f	21070
85-87 Anzac Highway ASHFORD	Tree- Gum Tree Stump (rear of Ashford House); Extent and remains of gum tree stump 1998 Heritage Survey Ref AS02	700	F10556	CT 5841/495	a e f	21071
12 Tyson Street ASHFORD	Mature Oak ( <i>Quercus robur</i> ) Tree; Tree formed original part of Ashford House gardens 2003 Heritage Review Ref Page 3	83	D3108	CT 5642/65	a e f	21120
27-29 Elston Street BROOKLYN PARK	Lockleys Junior Primary School; External form and original detailing of the 1915 school building including brick coped gable ends, tall face red brick chimneys and other original detailing. Later extensions and additions do not form part of the listing. 1998 Heritage Survey Ref. BP02	31 32 33 34 35 49 50 51 52 53	D1898 D1898 D1898 D1898 D1898 D1898 D1898 D1898 D1898 D1898	CT 5551/685 CT 5551/685 CT 5551/685 CT 5551/685 CT 5551/685 CT 5551/685 CT 5551/685 CT 5551/685 CT 5551/685 CT 5551/685	a c d	21073
23 Lipsett Terrace BROOKLYN PARK	St John Bosco Parish Centre; External form and detailing of the circa 1910 former residence including masonry walls, timber joinery, return verandah and gabled roof form. Additions to the rear (north) of the property are not included in the listing. 1998 Heritage Survey Ref. BP01	148	D1127	CT 6109/525	a c	21072
31-47 Lipsett Terrace BROOKLYN PARK	Former Salesian College buildings; 1890 structures including chapel and hall constructed in red brick with masonry and timber detailing. 2003 Heritage Review Ref. Page 10	212	F5731	CT 5559/87	a c d e	21115

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<b>Property Address</b>	<b>Description and/or Extent of Listed Place</b>	<b>Lot No. or Part Sec</b>	<b>Plan No.</b>	<b>Certificate of Title</b>	<b>Section 23(4) Criteria</b>	<b>DPLG ID</b>
112-120 Marion Road BROOKLYN PARK	Former West Torrens Council Chambers; Extent of earliest sections of building including rendered mouldings to window and door openings and parapeted frontages to Marion Road, and masonry sections of buildings behind including early chimneys, walls and other original elements. Later additions and extensions do not form part of the listing. 1998 Heritage Survey Ref. BP04	76	F144404	CT 5861/944	a c d	21075
44-50 Marshall Terrace BROOKLYN PARK	House; Overall external original form and detailing of circa 1880s residence including sandstone and bluestone walls with rendered window and door dressings, double gable form, timber door and window joinery and moulded eaves brackets and window head detail. 1998 Heritage Survey Ref. BP05	109 110	D1127 D1127	CT 5795/828 CT 5795/829	a d	21076
5-7 May Terrace BROOKLYN PARK	Brooklyn Park Uniting Church; Overall external form and original detailing of 1913 church including gable end detailing, arch headed windows and face red brick walls. Later extensions and additions do not form part of the listing. 1998 Heritage Survey Ref. BP03	100	D17646	CT 5698/832	a c	21074
407 Anzac Highway CAMDEN PARK	House & Coach House; External form and detailing of residence fronting Anzac Highway and external form and detailing of two storeyed building along rear boundary of property. 1998 Heritage Survey Ref CP01	50	F7052	CT 5828/5	a d	21077
1-35 Jenkins Street (corner Brooker Terrace) COWANDILLA	Former Cowandilla Junior Primary School; U' shaped form of painted brick former school building including timber window and door joinery, and gable ventilated to roof form. 1998 Heritage Survey Ref. CO01	12	D57875	CT 5861/581	a d	21078
180-184 Sir Donald Bradman Drive COWANDILLA	Shops; Painted masonry shops with projecting parapet detail and remaining original shopfront elements. 1998 Heritage Survey Ref. HI04	3	F106772	CT 5177/612	a c	21079

Property Address	Description and/or Extent of Listed Place	Lot No. or Part Sec	Plan No.	Certificate of Title	Section 23(4) Criteria	DPLG ID
17-21 Davenport Terrace HILTON	Row Houses; External form and original materials and detailing of row of single storey masonry row houses. Later front porch additions and asbestos roof tiles are not included in the listing. 1998 Heritage Survey Ref. HI05	82	F145110	CT 5851/339	a b d	21080
81-85 Davenport Terrace (also known as 165-171 Sir Donald Bradman Drive) HILTON	Memorial Gardens and Cross; Stone memorial and plinth. 1998 Heritage Survey Ref. HI02	714	F211500	CT 2437/129	a c d f	21082
143-145 Sir Donald Bradman Drive HILTON	Theatre 62 (former Baptist Church & Hilton Soldier's Memorial Hall); Former 1872 Baptist Church with alterations and adjacent memorial hall (former Soldier's Memorial Institute). 1998 Heritage Survey Ref. HI01	28 9 4	F145156 D639 D3002	CT 5776/128 CT 5792/740 CT 5832/7	a c	21081
45 Farnham Road KESWICK	House 'Alpine'; The overall form and detailing of the circa 1890 residence including face stone front elevation, brickwork, pediment, balustrading to parapet, verandah and verandah balustrade. 2003 Heritage Survey Ref. KE02	55	F19512	CT 5438/104	a d	21084
419-433 South Road KESWICK	Richmond Primary School; Original 1898 red brick school building fronting South Road including gable end detailing, verandah form and other original detailing. 2003 Heritage Survey Ref. KE04	450 451	D1288 D1288	CT 5837/916 CT 5837/916	a c d	21085
41-47 Surrey Road KESWICK	Former Keswick Child Health Centre; Circa 1940 red brick domestic scaled building, including terracotta tiled roof, brick pillared verandah and concrete window hoods. The later infill mesh panels to doors and windows and window shade blind are not included. 2003 Heritage Survey Ref. KE05	421	D1288	CT 5812/597	a c	21086
2-4 Anthus Street LOCKLEYS	Dwelling; External form and detailing of 1883 bluestone residence including verandas, gable end detailing and timber door and joinery. 2003 Heritage Review Ref Page 3	4	F11111	CT 5086/130	a d e	21116

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2 Cross Street LOCKLEYS	Dwelling; External form and detailing of 1911 house including face stone and render walls, strapped timber gable end and roof gables, verandah form and timber detailing. 2003 Heritage Review Ref. Page 14	93	F120312	CT 5804/569	a d	21119
10 & 10A Cross Street LOCKLEYS	Attached Dwellings; External form and detailing of the pair of c1908 residences including masonry construction, projecting gables and central verandah, and red face brick chimney. 2003 Heritage Review Ref. Page 17	1 2 CP	S6804 S6804 S6804	CT 5015/345 CT 5015/346 CT 5015/347	a d	21123
362 Henley Beach Road LOCKLEYS	Soldier's Memorial Centre (now Odeon Cinema); External form and detailing of original memorial hall, particularly the elevation to Henley Beach Road. Later extensions and additions do not form part of the listing. 1998 Heritage Survey Ref. LO02	36	F124330	CT 5842/983	a c e	21088
382 Henley Beach Road LOCKLEYS	House; Overall form and external detailing of circa 1910 residence including masonry walls with rendered trim, distinctive conical corner tower and return verandah. 1998 Heritage Survey Ref. LO03	25	F124319	CT 5424/851	a d	21089
4 Rowells Road LOCKLEYS	House "Billilla"; Extent and original detailing of bluestone residence including unpainted stone and brickwork, gable ended rear bay, brick chimneys. 1998 Heritage Survey Ref. LO05	100	D52986	CT 5710/319	a d e	21090
46-48 Torrens Avenue LOCKLEYS	House; Overall external form and detailing of face bluestone residence with brick trim, timber gable end and verandah detailing. Later extensions to the north do not form part of the listing. 1998 Heritage Survey Ref. LO06	96	F120415	CT 5811/318	a d	21091
1 Elm Avenue MILE END	Dwelling; External form and details of c1920 houses. 1996 Heritage Survey Ref. Pages 78 & 79	1	D2422	CT 5192/119	a d	9103
3 Elm Avenue MILE END	Dwelling; External form and details of c1920 houses. 1996 Heritage Survey Ref. Pages 78 & 80	2	D2422	CT 5618/501	a d	9104
5 Elm Avenue MILE END	Dwelling; External form and details of c1920 houses. 1996 Heritage Survey Ref. Pages 78 & 80	3	D2422	CT 5408/926	a d	9105

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7 Elm Avenue MILE END	Dwelling; External form and details of c1920 houses. 1996 Heritage Survey Ref. Pages 78 & 80	4	D2422	CT 5719/489	a d	9106
8 Elm Avenue MILE END	Dwelling; External form and details of c1920 houses. 1996 Heritage Survey Ref. Pages 78 & 81	64	F144292	CT 5587/642	a d	9107
9 Elm Avenue MILE END	Dwelling; External form and details of c1920 houses. 1996 Heritage Survey Ref. Pages 78 & 81	5	D2422	CT 5559/254	a d	9108
10 Elm Avenue MILE END	Dwelling; External form and details of c1920 houses. 1996 Heritage Survey Ref. Pages 78 & 81	65	F144293	CT 5380/922	a d	9109
16 Falcon Avenue MILE END	St James Anglican Church; External form, details and materials of 1901 Church. 1996 Heritage Survey Ref. Pages 82-83	2	D27292	CT 5159/851	a c d	9110
6-8 Henley Street 11-19 Elm Avenue MILE END	Offices; Former MTT Converter Station. External form, details and materials of 1910 structure (excluding front office addition) 1996 Heritage Survey Ref. Page 92	13 15 6 8 11 17 19	C24490 C24490 C24490 C24490 C24490 C24490 C24490	CT 6007/583 CT 6007/584 CT 6007/585 CT 6007/586 CT 6007/587 CT 6007/588 CT 6007/589 CT 6007/590	a d	9114
28-32 Henley Beach Road MILE END	Mile End Hotel; External form, details and materials of the original two storey section of hotel. 1996 Heritage Survey Ref. Pages 84-85	1	F103915	CT 5140/590	a b f	9111
147 Henley Beach Road MILE END	Former dwelling; External form, details and materials of front section of house. 1996 Heritage Survey Ref. Pages 88-89	398 535 536	D1285 D42283 D42283	CT 5293/899 CT 5293/899 CT 5293/899	a d f	9112
173 Henley Beach Road MILE END	Former service station; External form, details and original materials of front section of building. 1996 Heritage Survey Ref. Page 90	526 524	D42051 D42052	CT 6126/302 CT 6162/303	a d	9113
19 James Congdon Drive MILE END	Former International Harvester LTD Showroom; External form, details and materials of two storey frontage. 1996 Heritage Survey Ref. Pages 76-77	14 22	F119833 F119841	CT 5224/126 CT 5224/127	a d	9102
200 South Road MILE END	Holder Memorial Church; External form, details and materials of 1914-15 church. 1996 Heritage Survey Ref. Page 96-97	732	D34701	CT 5104/806	a c d f	9116

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12a Victoria Street MILE END	Mile End Baptist Church; External form, details and materials of both the 1908 and 1912 sections of the church. Excluding rear hall. 1996 Heritage Survey Ref. Page 98	132	D1099	CT 5316/477	a c	9117
47-49 London Road MILE END SOUTH	Former Charlick's Mill; Multi storey brick mill complex and single storey brick section fronting London Road. Later corrugated iron and concrete sections are not included. 1998 Heritage Survey Ref. MS02	120	F19721	CT 5902/321	a c d	21095
328 Marion Road NETLEY	House 'Carlyle'; Circa 1900 residence including face stone walls, strapped gable ends, expansive roof form, brick chimneys, return verandah with timber posts and bracketing, and rear castellated wall sections. 1998 Heritage Survey Ref. NE01	301	D91273	CT 6113/204	a d	21097
Birdwood Terrace McArthur Avenue NORTH PLYMPTON	Former Railway Corridor; Extent of open land of former railway reserve 1998 Heritage Survey Ref PL02	120	F6137	CT 5868/745	a c f	21098
327-329 Marion Road NORTH PLYMPTON	House 'Kandahar'; Extent of 1906 residence including picked random coursed sandstone, asymmetric form, tower, bay windows, cast iron verandah posts and trim. 1998 Heritage Survey Ref. PL07	26	F7197	CT 5825/472	a c d	21099
333 Marion Road NORTH PLYMPTON	House and Gates 'Bucklands'; Extent of 2 storey mansion including face bluestone, brick projecting prominent front chimney, 2 storey verandah with cast iron and timber trim, cast iron posts, and significant mature trees including Bunya Pines and Figs. Note - under redevelopment at Jan. 2004 gate posts have been removed, rear section including conservatory may have been demolished. 1998 Heritage Survey Ref. PL08	28	F7197	CT 5681/664	a d	21100
336 Marion Road NORTH PLYMPTON	House 'The Pines'; Remaining section of 1880 residence including recessed entrance porch and arcaded entrance elements, window detailing and plasterwork. Elements of the garden including mature Moreton Bay fig driveway from Marion Road. 1998 Heritage Survey Ref. PL09	16	F17584	CT 5705/154	a d e	21101



Property Address	Description and/or Extent of Listed Place	Lot No. or Part Sec	Plan No.	Certificate of Title	Section 23(4) Criteria	DPLG ID
353 Marion Road NORTH PLYMPTON	House 'Nesfield'; Extent of substantial 1892 residence including face stone and brickwork, substantial tower with arched entrance, castellated front bay, circular access stair to tower and other detailing. Listing includes the boundary fence of brick pillars and plinth and cast iron railing. 1998 Heritage Survey Ref. PL10	27	F7395	CT 5498/947	a d e f	21102
354 Marion Road NORTH PLYMPTON	House; Circa 1880s symmetrically fronted residence with return verandah, gabled entrance porch, cast iron verandah posts and detailing, and moulded masonry chimneys. 1998 Heritage Survey Ref. PL11	10	D89930	CT 6100/800	a d e	21103
457-489 Anzac Highway NOVAR GARDENS	Avenue of River Red Gums (Eucalyptus camaldulensis); Retention of two rows of trees planted in avenue formation that formerly lined the driveway from Anzac Highway into Cummins House. 2003 Heritage Review Ref. Page 7	2	F34937	CT 5203/102	a e f	21121
457-489 Anzac Highway NOVAR GARDENS	Olive Grove (Olea europa); Retention of rows of mature olive trees originally part of the Cummins house garden. 2003 Heritage Review Ref. Page 7	2	F34937	CT 5203/102	a e f	21122
11-13 Clayton Avenue PLYMPTON	Anglican Church of the Good Shepherd; 1920s sections of red brick church with terracotta tiled roof, original detailing and square tower. Excluding later additions. 1998 Heritage Survey Ref PL04	10 11	D2693 D2693	CT 5849/475 CT 5849/476	a c d f	21105
30 Long Street PLYMPTON	Multi-use hall (Hayhurst RSL Hall); Face bluestone 1902 hall with brick quoins, original window dressings and strapped gable infill. The later red brick addition to the front is not included in the listing. 1998 Heritage Survey Ref. PL05	233 209	F21483 F21483	CT 5083/878 CT 5083/879	a c	21106
20 Owen Street PLYMPTON	Plympton Primary School; Including 1880s bluestone school building and 1950s red brick separate structure. 1998 Heritage Survey Ref. PL03	216	F779	CT 5785/646	a c d	21108

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68 Brooker Terrace RICHMOND	St Aloysius Catholic Church; External form and detailing of 1933 red brick church including simple roof form and projecting northern bay with coping detail and eastern porch. 1998 Heritage Survey Ref RI01	27	D2051	CT 5853/268	a c	21109
71 Kingston Avenue RICHMOND	House; External form and detailing of circa 1910 bluestone house including bull-nose return verandah. 1998 Heritage Survey Ref. RI02	11	F144939	CT 5642/714	a b d	21110
32 Chapel Street THEBARTON	Dwelling; External form and original detailing of circa 1860 cottage 1996 Heritage Survey Ref Pages 47 - 48	26	F6527	CT 5681/819	a d	9089
41 Chapel Street THEBARTON	Dwelling; External form and materials of circa 1850 cottage 1996 Heritage Survey Ref Pages 47 - 48	67	F6527	CT 5680/297	a b d	9090
39 George Street THEBARTON	Wheatsheaf Hotel; External form, details and material of 1919 hotel. 1996 Heritage Survey Ref. Pages 57-58	1	D17651	CT 5698/502	a c d	9078
65-67 George Street THEBARTON	St Nicholas Greek Orthodox Church; External form, details and materials of 1959 church. 1996 Heritage Survey Ref. Pages 59	3	D60043	CT 5878/474	a d	9079
116 George Street THEBARTON	Former St John the Baptist School; External form, details and materials of 1939 frontage. 1996 Heritage Survey Ref. Page 60	129 130 131	F199611 F199611 F199611	CT 6144/335 CT 6144/335 CT 6144/335	a d	9080
12 & 12A Kintore Street THEBARTON	Dwelling; External form of c1865 cottage. 1996 Heritage Survey - refer to Thebarton Village Page 47	1 2 CP	S14483 S14483 S14483	CT 5877/228 CT 5877/229 CT 5877/230	a d	9091
28 Kintore Street THEBARTON	Dwelling; External form, detailing and original elements of c1866 cottage. 1996 Heritage Survey - refer to Thebarton Village Page 47	181	F6531	CT 5634/240	a d	9092
30 Kintore Street THEBARTON	Dwelling; External form, gable detailing and other original materials of the c1850 cottage. 1996 Heritage Survey - refer to Thebarton Village Page 47	182	F6531	CT 5107/827	a d	9093
38 Kintore Street THEBARTON	Dwelling; External form, detailing and elements to gable ends and verandah of c1872 cottage. 1996 Heritage Survey - refer to Thebarton Village Page 47	50	F54653	CT 6072/994	a d	9094

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42 Kintore Street THEBARTON	Dwelling; External form and detailing, particularly verandah and chimneys of c1866 cottage. 1996 Heritage Survey - refer to Thebarton Village Page 47	187	F6531	CT 5508/996	a d	9095
72 Kintore Street THEBARTON	Dwelling; Original external form of c1880 cottage. 1996 Heritage Survey - refer to Thebarton Village Page 47	10	F37588	CT 5354/226	a d	9096
114 Kintore Street THEBARTON	Kilmara Primary School convent and chapel; External form, details and materials of 1869 chapel, 1883 church and 1885 school building. 1996 Heritage Survey - Pages 64-65	22	D1234	CT 6054/373	a c e f	9082
57 Light Terrace THEBARTON	Former butcher's shop and dwelling; External form and detailing of 1891 shop original detailing of 1840s dwelling behind. Added after Review of 1996 Heritage Survey	107	F17429	CT 5483/169	a c d	9097
61 Light Terrace THEBARTON	Dwelling; External form and materials of c1876 cottage. Added after Review of 1996 Heritage Survey	9	F6527	CT 5071/137	a d	9098
63 Light Terrace THEBARTON	Dwelling; External form and original details (not including current verandah) of 1852 cottage. Added after Review of 1996 Heritage Survey	8	F6527	CT 5661/999	a d	9099
47-49 Maria Street THEBARTON	Semi-detached dwellings; Original external form of attached c1868 cottages. 1996 Heritage Survey - refer to Thebarton Village Page 47-49	170	F6531	CT 5238/185	a d e	9129
51 Maria Street THEBARTON	Dwelling; External form, materials and details of this c1868 cottage. 1996 Heritage Survey - refer to Thebarton Village Page 47-49	169	F6531	CT 5235/989	a d e	9100
53-55 Maria Street THEBARTON	Dwelling; External form and original detailing of c1868 attached cottages. 1996 Heritage Survey - refer to Thebarton Village Page 32-33	167 168	F6531 F6531	CT 5783/801 CT 5783/801	a d e	9101
64 Maria Street THEBARTON	Dwelling; Barbara Hanrahan's Studio; External form, details and materials of 1920s house, excluding the side garden allotment. 1996 Heritage Survey Ref. Page 66	147	F6531	CT 5214/763	c e	9083

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107 Port Road THEBARTON	Southwark Brewery Gardens; Garden strip to River Torrens. 1996 Heritage Survey Ref. Pages 67-68	383	D1038	CT 5137/205	a c f	9084
		380	D1038	CT 5137/368		
		381	D1038	CT 5137/368		
		382	D1038	CT 5137/368		
		391	D1038	CT 5137/383		
		2	F103751	CT 5137/753		
		384	D1038	CT 5138/158		
		385	D1038	CT 5138/158		
		386	D1038	CT 5138/158		
		387	D1038	CT 5138/158		
		388	D1038	CT 5138/158		
		389	D1038	CT 5138/158		
		390	D1038	CT 5977/935		
107 Port Road THEBARTON	Plaque: Site of Colonel Light's Cottage; Attached to wall of warehouse. Plaque only 1996 Heritage Survey Ref Page 54	2	D64225	CT 5941/298	e	9077
107 Port Road THEBARTON	Electric Supply Company Transformer; External form, details and materials of building housing transformer. 1996 Heritage Survey Ref. Page 61	2	D64225	CT 5941/298	a d	9135
32 Queen Street (previously listed as 26-32 Stirling Street) THEBARTON	Alaska Towers (former Alaska Ice Cream Factory); External form of 1920's structure from Fauldings use of site. 1996 Heritage Survey Ref Pages 72-75	20 21	D683 D683	CT 5213/487 CT 5400/30	a c d f	21066
51-57 South Road THEBARTON	West Thebarton Hotel; External form of 1880s hotel. 1996 Heritage Survey Ref. Page 46	1	F14586	CT 5166/967	a c	9085
125-133 South Road THEBARTON	Former Town Hall; Original external form, details and materials of 1885 Town Hall which remain. 1996 Heritage Survey Ref. Pages 70-71	212	D88290	CT 6096/161	a c	9086
		213	D88290	CT 6096/161		
		62	F119781	CT 6096/161		
125-133 South Road THEBARTON	Girl Guide and Scout Hall; External form, details and materials of 1939 hall 1996 Heritage Survey Ref Page 53	25	D1357	CT 5838/327	c	9076
135 South Road THEBARTON	Former Thebarton Court House and Police Station; External form, details and materials of the rendered masonry 1940 frontage to South Road and George Street. Added after Review of 1996 Heritage Survey	1	D1234	CT 6054/361	a c d	9088
141 South Road THEBARTON	Queen of Angels Church; External form, details and materials of 1916 church. 1996 Heritage Survey Ref. Pages 62-63	23	D1234	CT 6054/375	a c d f	9081
		24	D1234	CT 6054/375		
		25	D1234	CT 6054/375		
		26	D1234	CT 6054/375		

Property Address	Description and/or Extent of Listed Place	Lot No. or Part Sec	Plan No.	Certificate of Title	Section 23(4) Criteria	DPLG ID
35-37 Stirling Street THEBARTON	University of Adelaide, Thebarton Campus - Factory Building (former Amdel Warehouse), Compound Building (former Caretakers Residence/Office), Hedge House Building (former Laboratories), former Boiler House and Chimney; External form of 1920s structures from Fauldings use of site. 1996 Heritage Survey Ref. Pages 72-75	117 19 18	D19274 D683 D683	CT 5110/939 CT 5110/939 CT 5110/940	a c d f	9087
38 Ashley Street TORRENSVILLE	Grandstand, Thebarton Soldiers' Memorial Oval.; 1921 Grandstand including overall form, seating and access steps. 1996 Heritage Survey Ref Pages 99-101	12	D14796	CT 5092/344	a c d	9119
40 Ashley Street TORRENSVILLE	Thebarton Senior College (former Technical School); External form, details and materials of section of building fronting Ashley Street. 1996 Heritage Survey Ref Pages 102-104	18	F26399	CT 5384/552	a c d	9120
86 Ashley Street TORRENSVILLE	House (Albert Villa); External form, details and materials of 1890's section of house. 1996 Heritage Survey ref Pages 105-106	42	F123336	CT 5410/518	a d e	9121
1/31 & 2/31 Carlton Parade TORRENSVILLE	Attached Dwelling (former Congregational Church); External form, details and materials of the 1935 church 1996 Heritage Survey Ref Pages 107-108	1 2	C22743 C22743	CT 5935/15 CT 5935/16	a c d	9122
40-42 Carlton Parade TORRENSVILLE	Former Fire Station; External form, details and materials of 1917 Fire Station 1996 Heritage Survey Ref Pages 109-111	111 110	D1771 D1771	CT 5349/52 CT 5349/53	a c d	9123
114 Henley Beach Road TORRENSVILLE	Mile End Church of Christ; External form, details and materials of both church buildings. 1996 Heritage Survey Ref. Pages 114-115	7	F122901	CT 5845/732	a c	9118
180 Henley Beach Road TORRENSVILLE	The Royal Hotel; External form of hotel based on 1880s structure. 1996 Heritage Survey Ref. Pages 16-117	102 103	D618 D618	CT 5139/14 CT 5139/14	a c	9125
184-188 Henley Beach Road TORRENSVILLE	RW Fielder & Son District Centre; External form, details and materials of 1936 corner section of shop. 1996 Heritage Survey Ref. Page 118	503	D35862	CT 5105/444	a d	9126
194 Henley Beach Road TORRENSVILLE	Pan Aracadian Hall; External form, details and materials of the frontage to street. 1996 Heritage Survey Ref. Pages 119-120	56	F122850	CT 5778/802	a c	9127

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8A Jervois Street TORRENSVILLE	Druids Hall; External form, details and materials of stone frontage to Jervois Street. 1996 Heritage Survey Ref. Pages 121-122	60	D618	CT 6157/474	a c d	9128
68 Meyer Street TORRENSVILLE	Dwelling; External form, original details and materials of circa 1900 residence including front bay drum window, stone front elements, brick rear sections and complex roof form. 2004 Heritage Review	68	D89567	CT 6098/307	a d e	21124
49 Hardys Road UNDERDALE	Outbuilding; External form, original details and materials of single storey stone and brick outbuilding. 2004 Heritage Review	63	F123157	CT 5720/413	a e	21125
264 Henley Beach Road UNDERDALE	House 'Roseneath'; External form and original detailing of large circa 1880 residence including face stone walls, verandah with cast iron columns, circular turret, arched porch entrance and other original details. 1998 Heritage Survey Ref. UN01	7	F131915	CT 5662/83	a d	21113
103 Holbrooks Road UNDERDALE	Former Holbrook's Winery; Overall form of remnant cellar buildings including local field stone walls, brick coping with gable ends and red brick detailing to quoins and other openings. 1998 Heritage Survey Ref. UN02	461	D72682	CT 5981/562	a b d	21114
Anzac Highway road reserve, VARIOUS SUBURBS	Avenue of Claret Ash Trees ( <i>Fraxinus raywoodii</i> ); Extends through the suburbs of Ashford, Keswick, Kurralta Park, Plympton, Camden Park and Novar Gardens 2003 Heritage Review Ref Page 3			RR	a e f	21126
364A Marion Road (corner Mooringe Avenue)	All Saints Uniting Church (former Methodist Church); Extent of 1856 rendered hall form church (including the bell tower) but excluding the later front porch and rear additions. 1998 Heritage Survey Ref. PL12	58 59 60	F6523 F6523 F6523	CT 5487/139 CT 5487/25 CT 5831/605	a c d	21104

## Table WeTo/5 - State Heritage Places

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511 Henley Beach Road FULHAM	Dwelling ('The Oaks') and row of cottages	A102	D21018	CT 5417/1		11921
14 Weetunga Street FULHAM	Dwelling ('Weetunga'), including Main House, former Kitchen, Servants' Quarters, Museum and Laundry	A100	D43690	CT 6035/471		11922
165-171 Sir Donald Bradman Drive HILTON	West Torrens Council Chambers (1935 Building only)	A714 A8	F211500 F145136	CT 2437/129 CT 5872/793	a e g	11949
Holland Street HINDMARSH	Sir William Goodman Bridge (formerly 'Holland Street Bridge')	S1 A71	D1038 D82876	CT 400/195 CT 6114/298		10987
Bagot Avenue MILE END	Former Council Boundary Marker	S48-ROAD RESERVE		N/A		14476
1A Falcon Avenue MILE END	Former St James' Anglican Church School	A45	D1285	CT 5335/459		14451
2-4 Henley Beach Road MILE END	Temple Christian College (Former Thomas Hardy & Sons Wine Cellars, Tintara House)	A2	D15490	CT 5546/689		12365
94 Henley Beach Road MILE END	Former BankSA (former Savings Bank of South Australia Mile End Branch)	A10	D883	CT 5342/715		11825

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Property Address	Description and/or Extent of Listed Place	Lot No. or Part Sec	Plan No.	Certificate of Title	Section 16 Criteria	SA Heritage Register ID
36-50A & 39-45A Rose Street MILE END	Dwelling - Row Cottages (Adelaide Workmen's Homes)	A39	D45807	CT 5381/463		11792
		A40	D45807	CT 5381/464		
		A41	D45807	CT 5381/465		
		A42	D45807	CT 5381/466		
		A43	D45807	CT 5381/467		
		A44	D45807	CT 5381/468		
		A45	D45807	CT 5381/469		
		A46	D45807	CT 5381/470		
		A136	D48398	CT 5510/545		
		A137	D48398	CT 5510/546		
		A138	D48398	CT 5510/547		
		A139	D48398	CT 5510/548		
		A140	D48398	CT 5510/549		
		A141	D48398	CT 5510/550		
		A142	D48398	CT 5510/551		
		A143	D48398	CT 5510/552		
		A144	D48398	CT 5510/553		
		A145	D48398	CT 5510/554		
		A146	D48398	CT 5510/555		
		A147	D48398	CT 5510/556		
A148	D48398	CT 5510/557				
A149	D48398	CT 5510/558				
A150	D48398	CT 5510/559				
A151	D48398	CT 5510/560				
32-56 Sir Donald Bradman Drive, MILE END	Former Adelaide Electric Supply Co Ltd – four former garages and two double storey office / workshop buildings	A171	D41533	CT 5289/572	a	26308
4 Lewis Crescent NORTH PLYMPTON	Dwelling ('Plympton House', sometime Parkin House) and former Gardener's Cottage	A34	D6084	CT 5189/107		11945
19-23 Sheoak Avenue NOVAR GARDENS	Dwelling ('Cummins')	A145	F6203	CT 5796/195		10552
39A Dew Street THEBARTON	Lady Gowrie Child Centre	A58	F119877	CT 5393/312	a b	14869
1 George Street THEBARTON	Squatters Arms Hotel	A286	F6536	CT 6049/352		11833
42 and 42A Phillips Street THEBARTON	Former Thebarton Baptist Church & Hall	A2 A3	F17710 F17710	CT 5986/242 CT 5986/243		11841
77 Port Road THEBARTON	Southwark Hotel	A216, 217	D1038	CT 5139/375		11831



Property Address	Description and/or Extent of Listed Place	Lot No. or Part Sec	Plan No.	Certificate of Title	Section 16 Criteria	SA Heritage Register ID
35-37 Stirling Street THEBARTON	Former Faulding's Eucalyptus Oil Distillery	A117	D19274	CT 5110/940		11827
34-36 West Thebarton Road THEBARTON	Former Thebarton Incinerator designed by Walter Burley Griffin	A47	F119966	CT 5675/845		10554
Ashley Street TORRENSVILLE	Gate/Ticket Office, Thebarton Soldiers Memorial Recreation Ground	A19	F26399	CT 5436/231		11835
4-8 Hayward Avenue TORRENSVILLE	Former Torrensville Uniting Church	A2	F106771	CT 5178/19	b	19118
112 Henley Beach Road TORRENSVILLE	Former Thebarton Council Chambers, Town Hall (Thebarton Theatre) & Hall	A2 & 3 A6	F125494 F122900	CT 5237/221 CT 5825/857		10645
Lot 21 Ashwin Parade TORRENSVILLE	Former Hoffman Brick Kiln, Brickworks Marketplace (former Hallett Brickworks)	A21	D96027	CT 6160/354		10553
92 South Road TORRENSVILLE	Australian Society of Magicians' Offices (former World War Two Civil Defence Sub-Control Station)	A19	F26399	CT 5436/231		14477

Note: this table was last updated on 16 August 2011 and is an extract from the South Australian Heritage Register established under Section 13 (1) of the *Heritage Places Act 1993*. In the event of a discrepancy between this extract and the South Australian Heritage Register, the South Australian Heritage Register shall prevail.

## Table WeTo/6 - Off-street Vehicle Parking Requirements for Designated Areas

### Interpretation

- 1 The vehicle parking rates table applies to Designated Areas listed below except where:
  - (a) any applicable condition(s) is/are not met
  - (b) the zone provisions require a lesser amount of on-site vehicular parking spaces than the amount determined using the vehicle parking rates table below.

### Designated Areas

- 2 The following are Designated Areas:

Designated Area	Conditions
Urban Core Zone	None
Urban Corridor Zone	None
District Centre Zone Local Centre Zone Neighbourhood Centre Zone	Any part of the development site is located in accordance with at least one of the following: <ol style="list-style-type: none"> <li>(a) within 200 metres of any section of road reserve along which a bus service operates as a high frequency public transit service<sup>(2)</sup></li> <li>(b) within 400 metres of a bus interchange<sup>(1)</sup> that is part of a high frequency public transit service<sup>(2)</sup></li> <li>(c) within 400 metres of an O-Bahn interchange<sup>(1)</sup></li> <li>(d) within 400 metres of a passenger rail station<sup>(1)</sup> that is part of a high frequency public transit service<sup>(2)</sup></li> <li>(e) within 400 metres of a passenger tram station<sup>(1)</sup></li> <li>(f) within 400 metres of the Adelaide Parklands.</li> </ol>

<sup>(1)</sup> Measured from an area that contains any platform(s), shelter(s) or stop(s) where people congregate for the purpose waiting to board a bus, tram or train, but does not include areas used for the parking of vehicles

<sup>(2)</sup> A high frequency public transit service is a route serviced every 15 minutes between 7.30am and 6.30pm Monday to Friday and every 30 minutes at night, Saturday, Sunday and public holidays until 10pm.

### Applicable off-street vehicular parking requirements

- 3 Development should provide off-street vehicle parking in accordance with the table(s) below. A lesser number of parking spaces may be provided based on the nature of development and parking condition in the wider locality including (but not limited to) the following:
  - (a) the development is a mixed use development with integrated (shared) parking where the respective peak parking demands across the range of uses occurs at different times
  - (b) the development is sited in a locality where the respective peak demands for parking for the range of uses (existing and proposed) occurs at different times and suitable arrangements are in place for the sharing of adjoining or nearby parking areas
  - (c) the development involves the retention and reuse of a place of heritage value, where the provision of on-site parking is constrained

**Table WeTo/6 - Off-street Vehicle Parking Requirements for Designated Areas**

- (d) suitable arrangements are made for any parking shortfall to be met elsewhere or by other means (including a contribution to a car parking fund)
- (e) generous on-street parking and/or public parking areas are available and in convenient proximity, other than where such parking may become limited or removed by future loss of access, restrictions, road modifications or widening
- (f) the site of the development is located within distances specified in the condition applicable to Designated Areas for at least two different public transit modes.

**VEHICLE PARKING RATES TABLES****Table 1: Non-residential development excluding tourist accommodation**

Location of development	Desired minimum number of vehicle parking spaces	Maximum number of vehicle parking spaces
All <b>Designated Areas</b> (unless otherwise stated)	3 spaces per 100 square metres of gross leasable floor area	6 spaces per 100 square metres of gross leasable floor area
<b>Urban Core Zone</b>	3 spaces per 100 square metres of gross leasable floor area 1.5 spaces per 100 square metres of gross leasable floor area above ground floor level other than for a shop	3 spaces per 100 square metres of gross leasable floor area
<b>Boulevard Policy Area 34</b> within the <b>Urban Corridor Zone</b>	3 spaces per 100 square metres of gross leasable floor area	5 spaces per 100 square metres of gross leasable floor area
<b>High Street Policy Area 35</b> within the <b>Urban Corridor Zone</b>	3 spaces per 100 square metres of gross leasable floor area	5 spaces per 100 square metres of gross leasable floor area

**Table 2: Tourist accommodation**

Location of development	Desired minimum number of required vehicle parking spaces	Maximum number of vehicle parking spaces
<b>Urban Core Zone</b> <b>Urban Corridor Zone</b>	1 space for every 4 bedrooms up to 100 bedrooms and 1 space for every 5 bedrooms over 100 bedrooms	1 space for every 2 bedrooms up to 100 bedrooms and 1 space for every 4 bedrooms over 100 bedrooms

**Table 3: Residential development, in the form of residential flat buildings and residential development in multi-storey buildings**

Location of development	Rate for each dwelling based on number of bedrooms per dwelling	Plus number of required visitor parking spaces
<b>Urban Core Zone</b>	1 per studio or 1 bedroom dwelling, except within 200 metres of a transit stop where the car parking rate should be 0.75 car parking spaces 1 per 2 bedroom dwelling 1.25 per 3+ bedroom dwelling	0.25 per dwelling

Location of development	Rate for each dwelling based on number of bedrooms per dwelling	Plus number of required visitor parking spaces
<b>Boulevard Policy Area 34</b> within the <b>Urban Corridor Zone</b>	0.25 per studio (no separate bedroom) 0.75 per 1 bedroom dwelling 1 per 2 bedroom dwelling 1.25 per 3 + bedroom dwelling	0.25 per dwelling
<b>High Street Policy Area 35</b> within the <b>Urban Corridor Zone</b>	0.25 per studio (no separate bedroom) 0.75 per 1 bedroom dwelling 1 per 2 bedroom dwelling 1.25 per 3 + bedroom dwelling	0.25 per dwelling
<b>Transit Living Policy Area 36</b> within the <b>Urban Corridor Zone</b>	0.5 per studio (no separate bedroom) 1 per 1 bedroom dwelling 1.5 per 2 bedroom dwelling 2 per 3 + bedroom dwelling	0.25 per dwelling
<b>Business Policy Area 37</b> within the <b>Urban Corridor Zone</b>	0.5 per studio (no separate bedroom) 1 per 1 bedroom dwelling 1.5 per 2 bedroom dwelling 2 per 3 + bedroom dwelling	0.25 per dwelling

Table 4: Row, semi-detached, group and detached dwellings

Location of development	Number of bedrooms, or rooms capable of being used as a bedroom	Number of required vehicle parking spaces
<b>Urban Core Zone</b>	1 or 2 bedrooms	1
	3 + bedrooms	2
<b>Urban Corridor Zone</b>	1 or 2 bedrooms	1
	3 + bedrooms	2

## Table WeTo/7 - Off-street Bicycle Parking Requirements for Urban Corridor Zone

The following bicycle parking requirements apply to development specifically in the **Urban Corridor Zone**.

- 1 In residential and mixed use development, the provision of bicycle parking may be reduced in number and shared where the operating hours of commercial activities complement the residential use of the site.
- 2 Residential and mixed use development, in the form of multi-storey buildings, should provide bicycle parking in accordance with the following rates:

Form of development	Employee/resident (bicycle parking spaces)	Visitor/shopper (bicycle parking spaces)
Residential component of multi-storey building/residential flat building	1 for every 4 dwellings	1 for every 10 dwellings
Office	1 for every 200 square metres of gross leasable floor area	2 plus 1 per 1000 square metres of gross leasable floor area
Shop	1 for every 300 square metres of gross leasable floor area	1 for every 600 square metres of gross leasable floor area
Tourist accommodation	1 for every 20 employees	2 for the first 40 rooms plus 1 for every additional 40 rooms



# Mapping Section

**Map Reference Tables**  
**Structure Plan Maps**  
**Spatial Extent Maps**  
**Concept Plan Maps**





## Map Reference Tables

### Index Maps

#### Map Reference

[Council Index Map](#)

### Zone Maps

Zone Name	Map Numbers
Adelaide Shores Zone	WeTo/6, WeTo/10
Airfield Zone	WeTo/6, WeTo/7, WeTo/8, WeTo/10, WeTo/11, WeTo/12
Bulky Goods Zone	WeTo/9
Coastal Marina Zone	WeTo/10
Coastal Open Space Zone	WeTo/10
Commercial Zone	WeTo/2, WeTo/3, WeTo/7, WeTo/8, WeTo/9, WeTo/13, WeTo/14
Community Zone	WeTo/3, WeTo/4, WeTo/5, WeTo/7, WeTo/9, WeTo/10, WeTo/11, WeTo/12
District Centre Zone	WeTo/4, WeTo/5, WeTo/13
Industry Zone	WeTo/4, WeTo/5, WeTo/8, WeTo/9, WeTo/10, WeTo/11, WeTo/12, WeTo/13, WeTo/14
Local Centre Zone	WeTo/2, WeTo/3, WeTo/6, WeTo/7, WeTo/9, WeTo/12, WeTo/13, WeTo/15
Neighbourhood Centre Zone	WeTo/5, WeTo/8, WeTo/9, WeTo/13, WeTo/14
Open Space Zone	WeTo/2, WeTo/3, WeTo/4, WeTo/5, WeTo/6
Residential Zone	WeTo/2, WeTo/3, WeTo/4, WeTo/5, WeTo/6, WeTo/7, WeTo/8, WeTo/9, WeTo/10, WeTo/11, WeTo/12, WeTo/13, WeTo/14, WeTo/15
Urban Core Zone	WeTo/14
Urban Corridor Zone	WeTo/4, WeTo/5, WeTo/8, WeTo/9, WeTo/12, WeTo/13, WeTo/15
Urban Renewal Zone	WeTo/4

### Policy Area Maps

Policy Area Name	Map Numbers
Arterial Roads Policy Area 1	WeTo/8, WeTo/9, WeTo/13, WeTo/14
District Commercial Policy Area 2	WeTo/3, WeTo/8
Local Commercial Policy Area 3	WeTo/2, WeTo/3, WeTo/7
Office Park Policy Area 4	WeTo/9

<b>Policy Area Name</b>	<b>Map Numbers</b>
Recreation Policy Area 5	WeTo/3, WeTo/4, WeTo/5, WeTo/7, WeTo/9, WeTo/11, WeTo/12
Brickworks Policy Area 6	WeTo/4, WeTo/5
Kurralta Park Policy Area 7	WeTo/13
Infrastructure Policy Area 8	WeTo/10
Netley Policy Area 9	WeTo/8, WeTo/12
Mixed Use Policy Area 10	WeTo/9, WeTo/13
Hilton Policy Area 11	WeTo/8, WeTo/9
Marleston Policy Area 12	WeTo/8, WeTo/9
Novar Gardens Policy Area 13	WeTo/14
Richmond Policy Area 14	WeTo/8
Tennyson Street Policy Area 15	WeTo/13
Thebarton Policy Area 16	WeTo/5
Medium Density Policy Area 18	WeTo/4, WeTo/5, WeTo/9, WeTo/12, WeTo/13, WeTo/14, WeTo/15
Medium Density Policy Area 19	WeTo/4, WeTo/5, WeTo/8, WeTo/9, WeTo/12, WeTo/13
Low Density Policy Area 20	WeTo/3, WeTo/4, WeTo/5, WeTo/7, WeTo/8, WeTo/9, WeTo/11, WeTo/12, WeTo/13, WeTo/14, WeTo/15
Low Density Policy Area 21	WeTo/2, WeTo/3, WeTo/4, WeTo/5, WeTo/6, WeTo/7, WeTo/10, WeTo/11, WeTo/14
Ashford Character Policy Area 22	WeTo/13
Cowandilla / Mile End West Character Policy Area 23	WeTo/4, WeTo/5, WeTo/8, WeTo/9
Glandore Character Policy Area 24	WeTo/13
Lockleys Character Policy Area 25	WeTo/3, WeTo/7
Novar Gardens Character Policy Area 26	WeTo/11, WeTo/14
Thebarton Character Policy Area 27	WeTo/5
Torrensville Character Policy Area 28	WeTo/4, WeTo/5
Elston Street Conservation Policy Area 29	WeTo/7
Mile End Conservation Policy Area 30	WeTo/5, WeTo/9
Richmond Conservation Policy Area 31	WeTo/9
Rose Street Conservation Policy Area 32	WeTo/5
Torrensville East Conservation Policy Area 33	WeTo/5
Boulevard Policy Area 34	WeTo/5, WeTo/9, WeTo/12, WeTo/13, WeTo/15

Policy Area Name	Map Numbers
High Street Policy Area 35	WeTo/4, WeTo/5
Transit Living Policy Area 36	WeTo/4, WeTo/8
Business Policy Area 37	WeTo/5, WeTo/9, WeTo/13
Mixed Use Transition Policy Area 38	WeTo/4

### Precinct Maps

Precinct Name	Map Numbers
Precinct 1 Intersection	WeTo/8, WeTo/9
Precinct 2 Richmond Road	WeTo/8, WeTo/9
Precinct 3 Sir Donald Bradman Drive (Mile End)	WeTo/9
Precinct 4 South Road (Keswick)	WeTo/9, WeTo/13
Precinct 5 South Road (Mile End)	WeTo/9
Precinct 6 South Road (Mile End South)	WeTo/9
Precinct 7 Mile End	WeTo/5, WeTo/9
Precinct 8 Open Space	WeTo/3, WeTo/4, WeTo/5, WeTo/7, WeTo/9, WeTo/11, WeTo/12

### Historic Conservation Area Maps

Area Name	<i>shown within Overlay Maps - Heritage</i>
Historic Conservation Area	WeTo/4, WeTo/5, WeTo/7, WeTo/9

### Overlay Maps

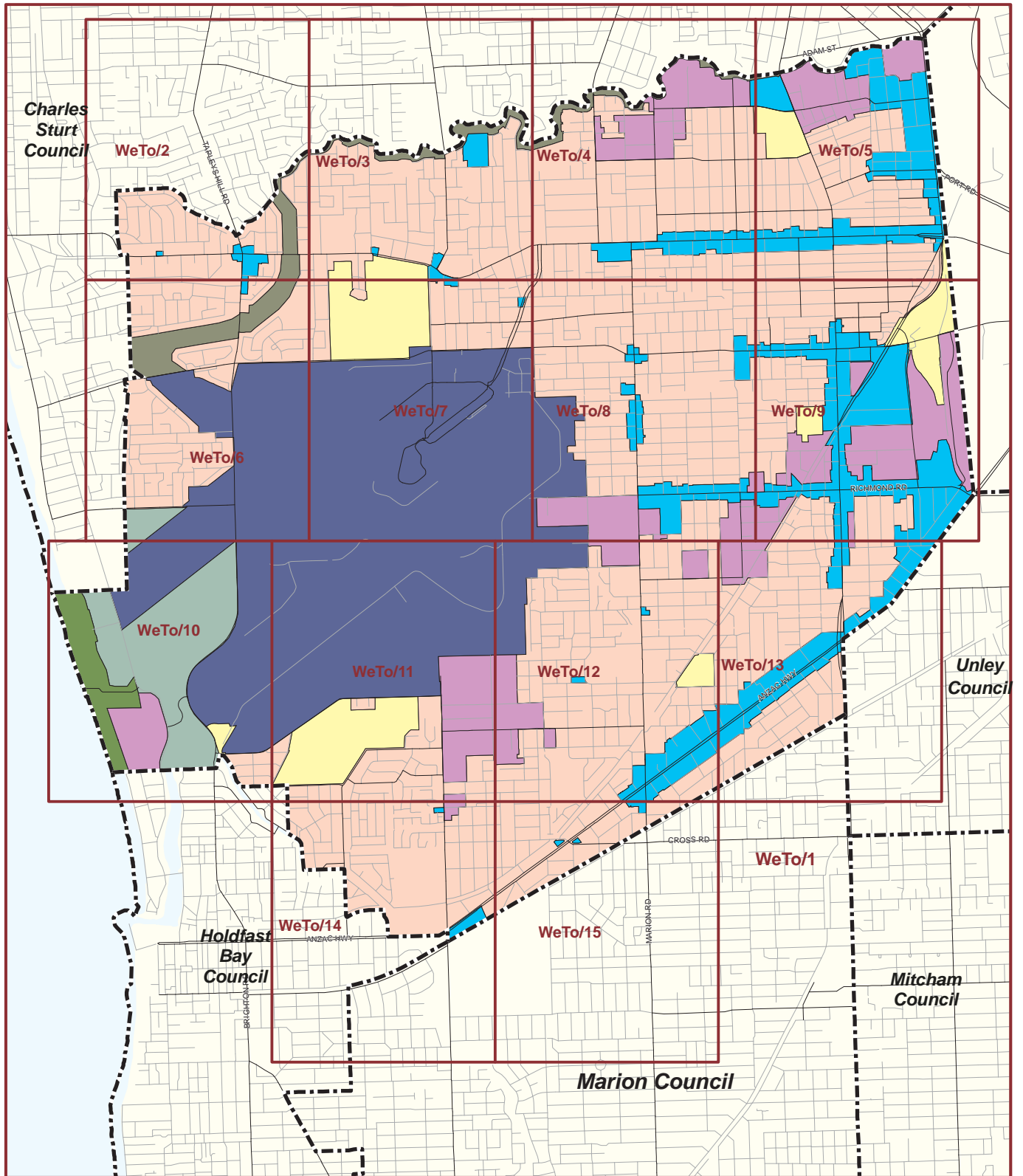
Issue	Map Numbers
Location	WeTo/1, WeTo/2, WeTo/3, WeTo/4, WeTo/5, WeTo/6, WeTo/7, WeTo/8, WeTo/9, WeTo/10, WeTo/11, WeTo/12, WeTo/13, WeTo/14, WeTo/15
Transport	WeTo/1, WeTo/2, WeTo/3, WeTo/4, WeTo/5, WeTo/6, WeTo/7, WeTo/8, WeTo/9, WeTo/10, WeTo/11, WeTo/12, WeTo/13, WeTo/14, WeTo/15
Development Constraints	WeTo/1, WeTo/2, WeTo/3, WeTo/4, WeTo/5, WeTo/6, WeTo/7, WeTo/8, WeTo/9, WeTo/10, WeTo/11, WeTo/12, WeTo/13, WeTo/14, WeTo/15
Heritage	WeTo/2, WeTo/3, WeTo/4, WeTo/5, WeTo/7, WeTo/8, WeTo/9, WeTo/12, WeTo/13, WeTo/14, WeTo/15
Natural Resources	WeTo/2, WeTo/3, WeTo/4, WeTo/5, WeTo/6, WeTo/7, WeTo/10, WeTo/14
Noise and Air Emissions	WeTo/4, WeTo/5, WeTo/8, WeTo/9, WeTo/12, WeTo/13, WeTo/14, WeTo/15
Strategic Transport Routes	WeTo/5, WeTo/8, WeTo/9, WeTo/10, WeTo/12, WeTo/13, WeTo/14, WeTo/15
Affordable Housing	WeTo/4, WeTo/5, WeTo/8, WeTo/9, WeTo/12, WeTo/13, WeTo/14, WeTo/15

## Concept Plan Maps

Concept Plan Title	Map Numbers
Mile End South Bulky Goods Zone	Concept Plan Map WeTo/1
Prominent Development Sites	Concept Plan Map WeTo/2
Sir Donald Bradman Drive & Marion Road Intersection Commercial	Concept Plan Map WeTo/3a
Marion Road & Richmond Road Intersection Commercial	Concept Plan Map WeTo/3b
Richmond Road & South Road Intersection Commercial	Concept Plan Map WeTo/3c
Richmond Road (Marleston) Commercial	Concept Plan Map WeTo/4a
Richmond Road (Richmond) Commercial	Concept Plan Map WeTo/4b
Richmond Road (Keswick/Mile End South) Commercial	Concept Plan Map WeTo/4c
Sir Donald Bradman Drive (Mile End) Commercial	Concept Plan Map WeTo/5
South Road (Keswick) Commercial	Concept Plan Map WeTo/6
South Road (Mile End) Commercial	Concept Plan Map WeTo/7
South Road (Mile End South) Commercial	Concept Plan Map WeTo/8
Office Park Commercial	Concept Plan Map WeTo/9
Mile End Recreation	Concept Plan Map WeTo/10
Mile End South Recreation	Concept Plan Map WeTo/11
Adelaide Shores	Concept Plan Map WeTo/12
Brickworks District Centre	Concept Plan Map WeTo/13
Kurralta Park District Centre	Concept Plan Map WeTo/14
Former Apollo Stadium Industry	Concept Plan Map WeTo/15
North Plympton/Camden Park Industry	Concept Plan Map WeTo/16
Novar Gardens Industry	Concept Plan Map WeTo/17
Netley Industry	Concept Plan Map WeTo/18
Hilton	Concept Plan Map WeTo/19
Marleston Neighbourhood Centre	Concept Plan Map WeTo/20
Novar Gardens Neighbourhood Centre	Concept Plan Map WeTo/21
Richmond Neighbourhood Centre	Concept Plan Map WeTo/22
Tennyson Street Neighbourhood Centre	Concept Plan Map WeTo/23
Residential Promenade	Concept Plan Map WeTo/24
Underdale Urban Renewal	Concept Plan Map WeTo/25
Morphettville Racecourse	Concept Plan Map WeTo/26

# **Spatial Extent Maps**

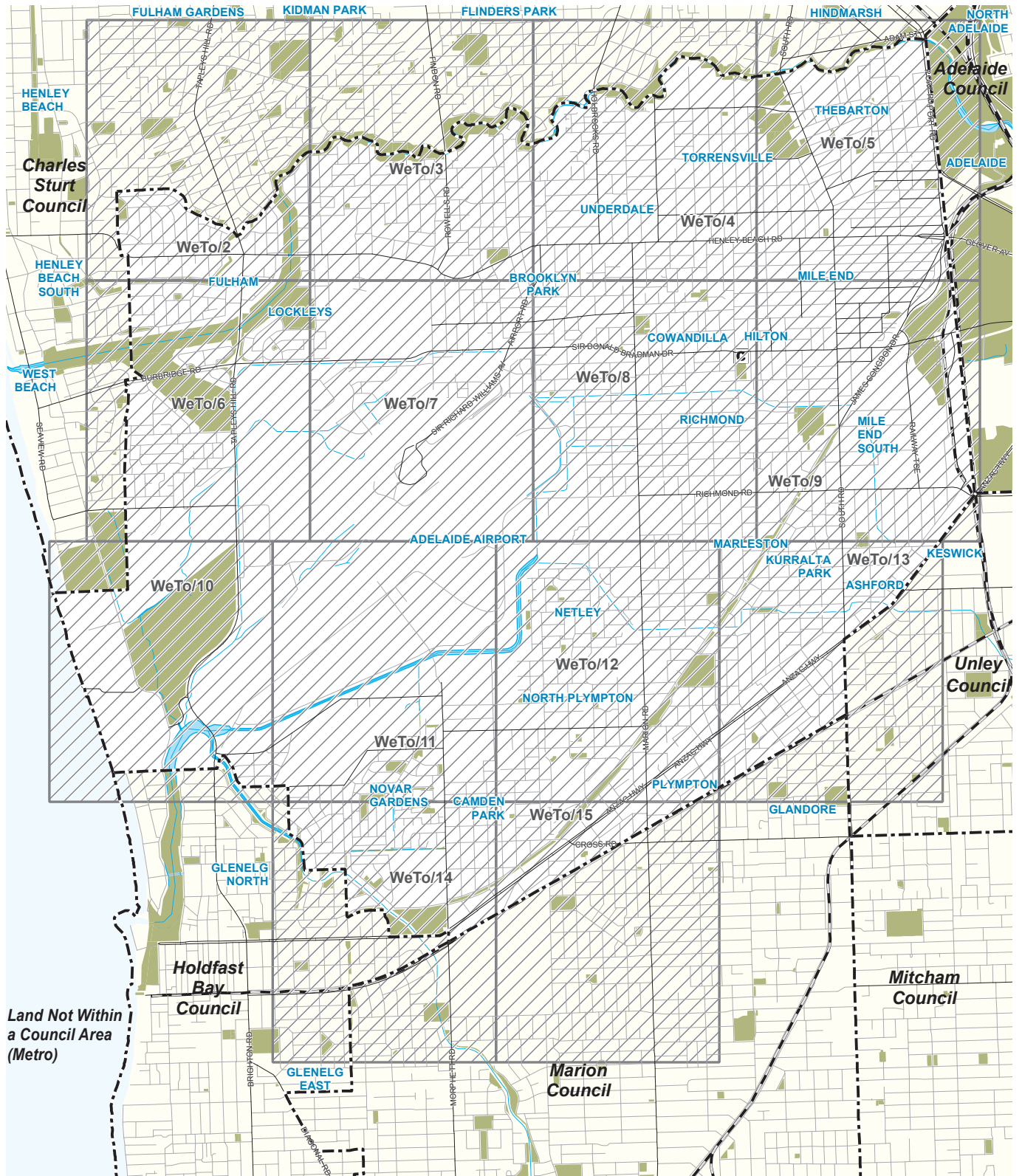




For the purposes of the Development Plan unless otherwise clearly indicated, the zone/policy area/precinct boundaries depicted on or intended to be fixed by Maps WeTo/1 to Map WeTo/15 inclusive shall be read as conforming in all respects (as the case may require) to the land division boundaries, to the centre line of roads or drain reserves or to the title boundaries, or to imaginary straight lines joining the positions defined by survey or by the measurements shown on the said maps against which the said zone/policy area/precinct boundaries are shown or otherwise indicated.








# Council Index Map



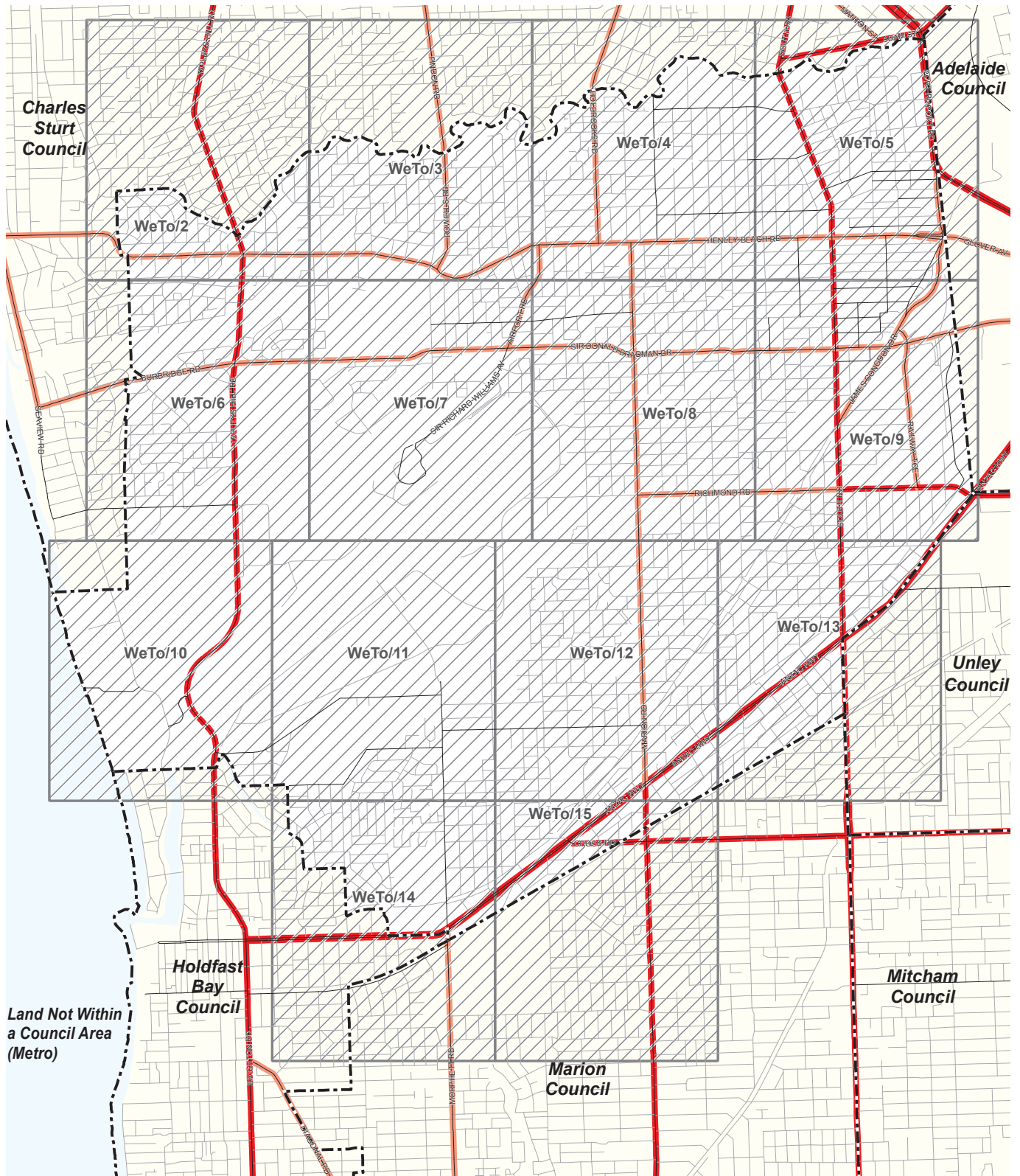
Land Not Within a Council Area (Metro)



-  Council Office
-  Railways
-  Waterway / drain / channel
-  Local Reserves
-  Development Plan Boundary

# Location Map WeTo/1

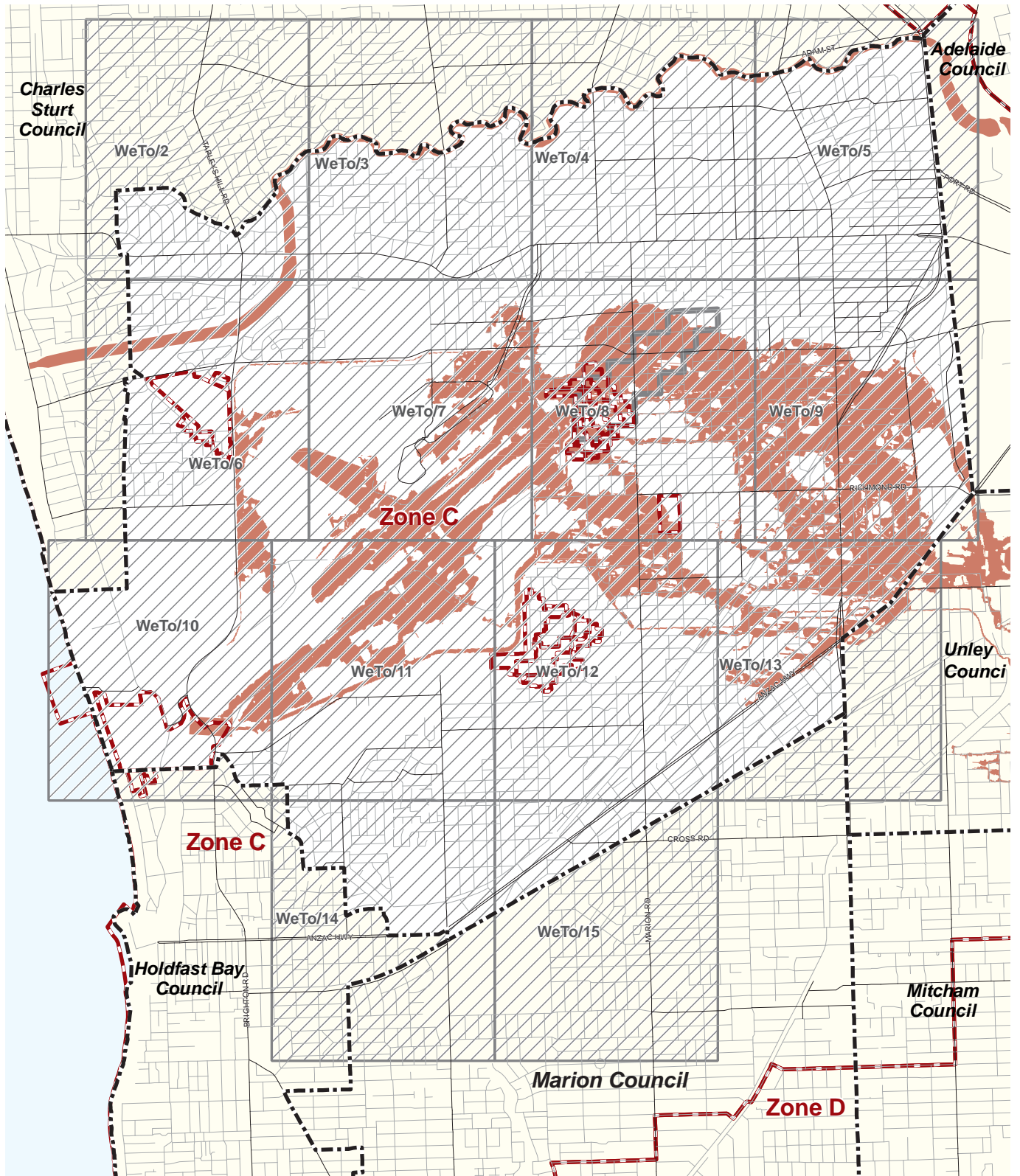




- Primary Arterial Roads
- Secondary Arterial Roads
- Development Plan Boundary

# Overlay Map WeTo/1

## TRANSPORT



**Airport Building Heights**  
 4m - 26m All Structures restricted to height identified on maps  
 (above existing ground level, measured from the top of the nearest roadside curb)

**Zone C** All Structures Exceeding 15 metres above existing ground level

**Zone D** All Structures Exceeding 45 metres above existing ground level

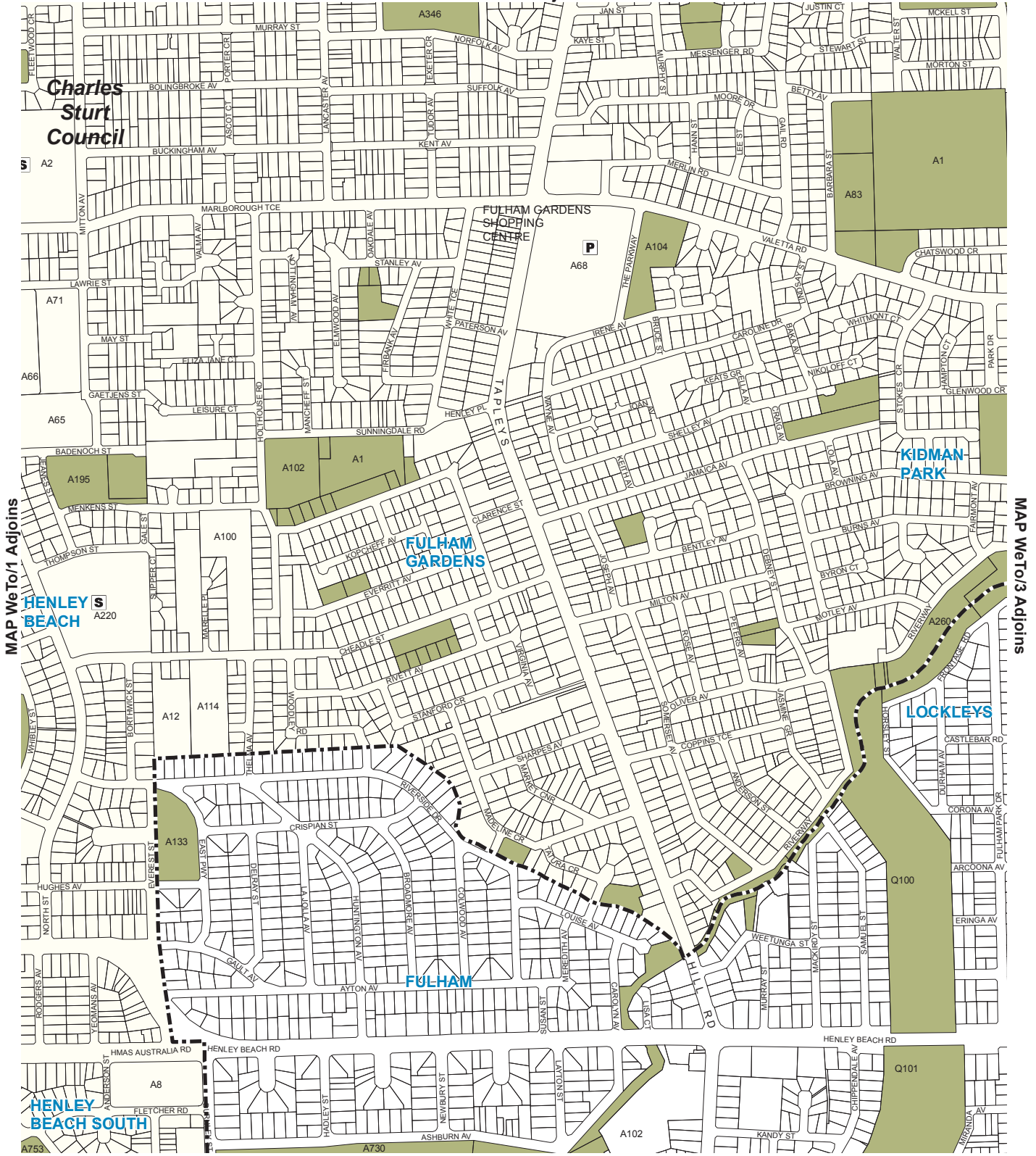


- Airport Building Heights
- Areas affected by aircraft noise
- Flood Hazard
- Development Plan Boundary

# Overlay Map WeTo/1

## DEVELOPMENT CONSTRAINTS

MAP WeTo/1 Adjoins



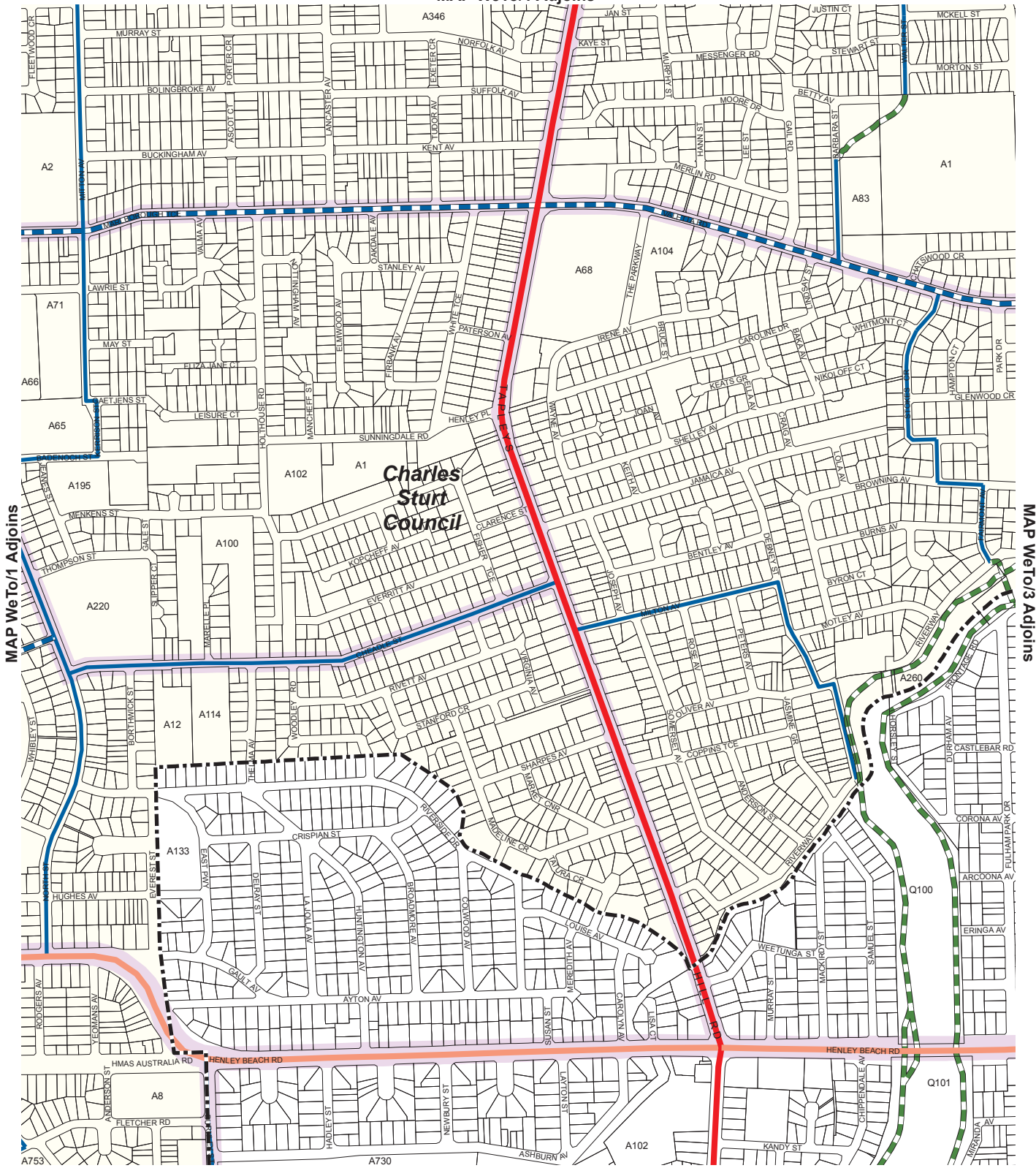
MAP WeTo/6 Adjoins



# Location Map WeTo/2

- S School
- P Post Office
- Local Reserves
- Development Plan Boundary

MAP WeTo/1 Adjoins



MAP WeTo/6 Adjoins

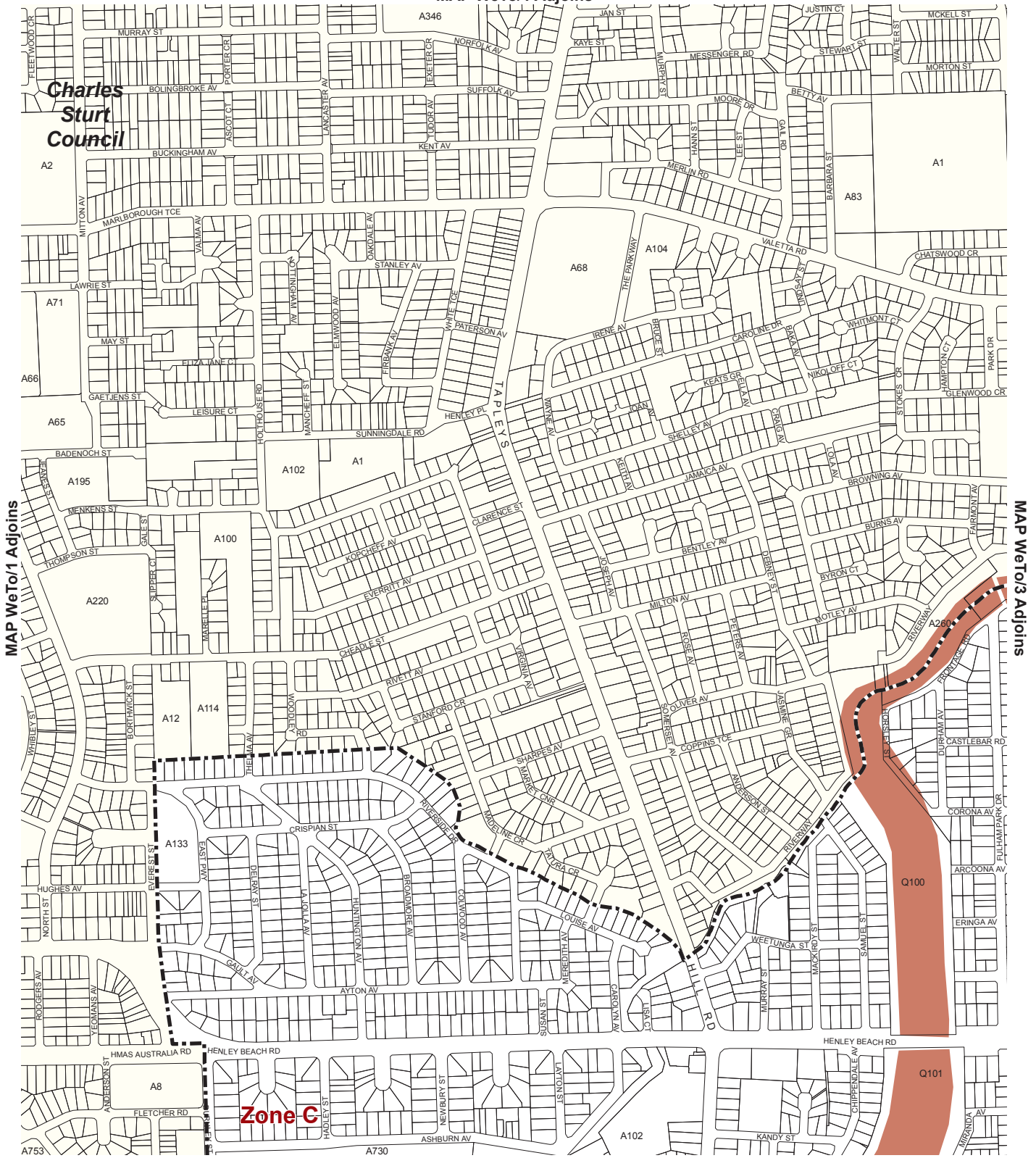
- Primary Arterial Roads
- Secondary Arterial Roads
- Bikedirect Network**
- Main Road - Bike Lane
- Main Road
- Secondary Road - Bike Lane
- Secondary Road
- Off Road Sealed Path
- Public Transport
- Development Plan Boundary



# Overlay Map WeTo/2

## TRANSPORT

MAP WeTo/1 Adjoins



**Airport Building Heights**  
**Zone C** All Structures Exceeding 15 metres above existing ground level

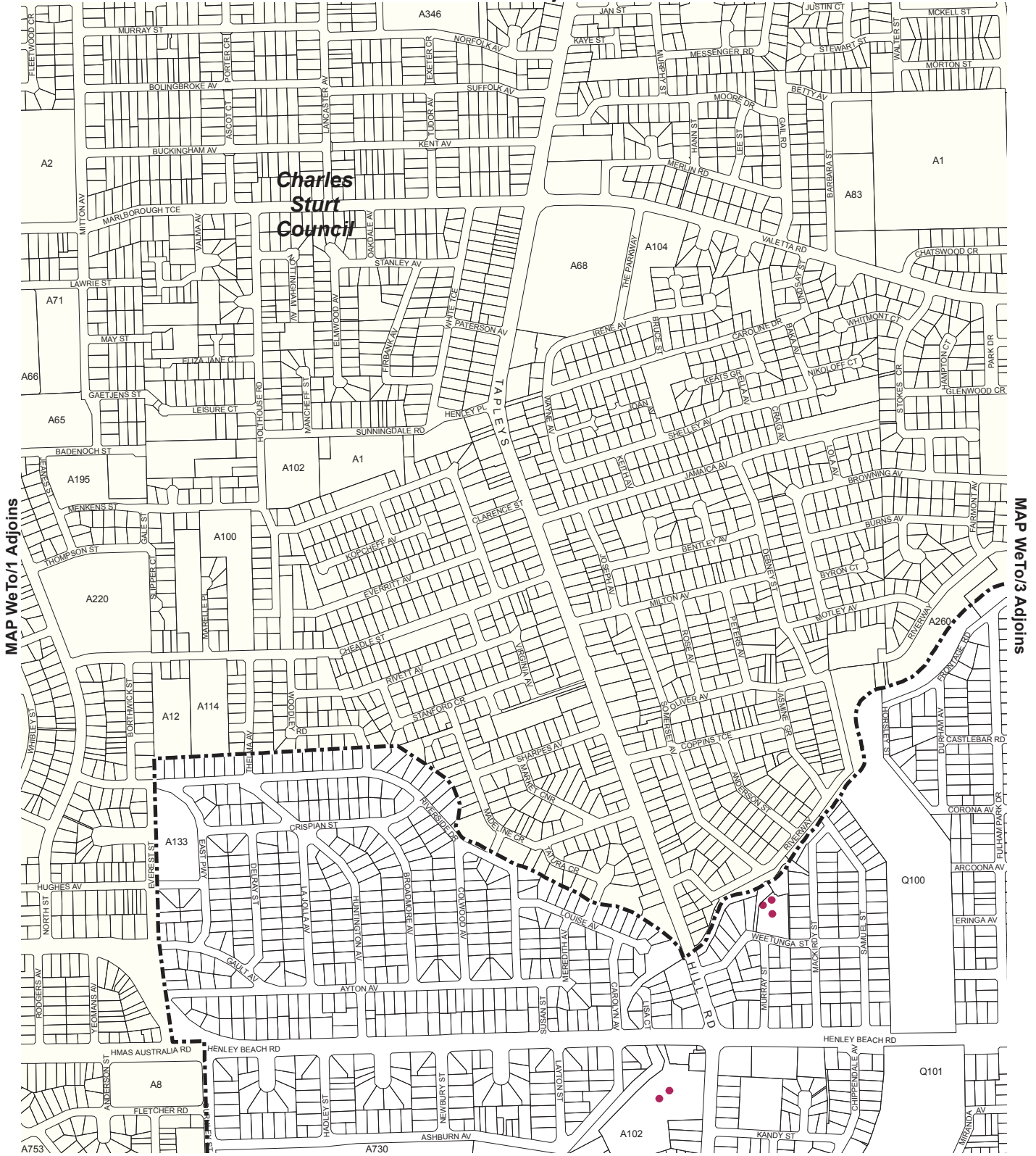


# Overlay Map WeTo/2

## DEVELOPMENT CONSTRAINTS

- Airport Building Heights
- Flood Hazard
- Development Plan Boundary

MAP WeTo/1 Adjoins



**Charles  
Sturt  
Council**

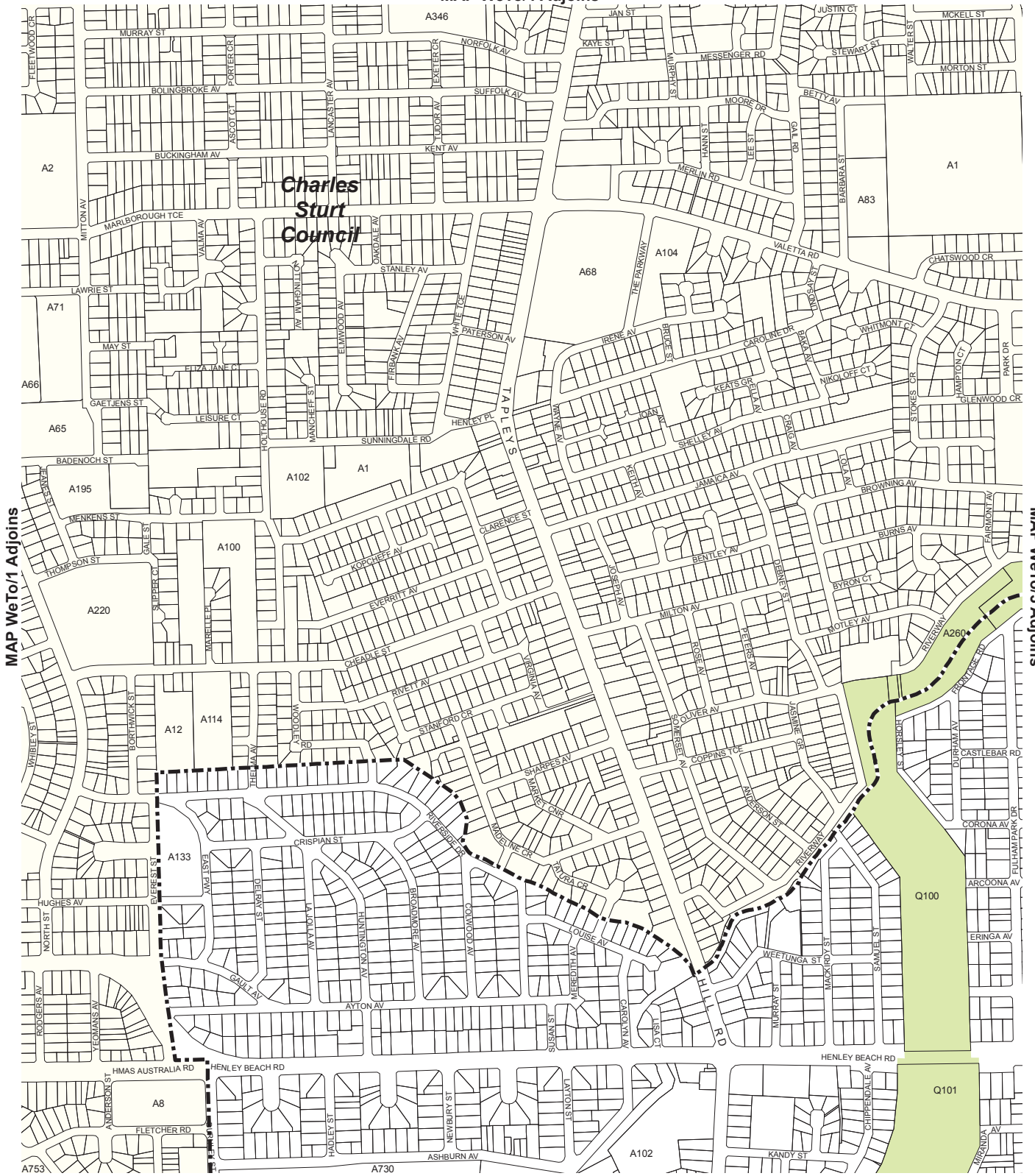
Heritage points are indicative only.  
For further information on State and Local Heritage Places and Contributory  
Items please refer to the relevant tables within this document.



# Overlay Map WeTo/2 HERITAGE

- State heritage place
- Development Plan Boundary

MAP WeTo/1 Adjoins

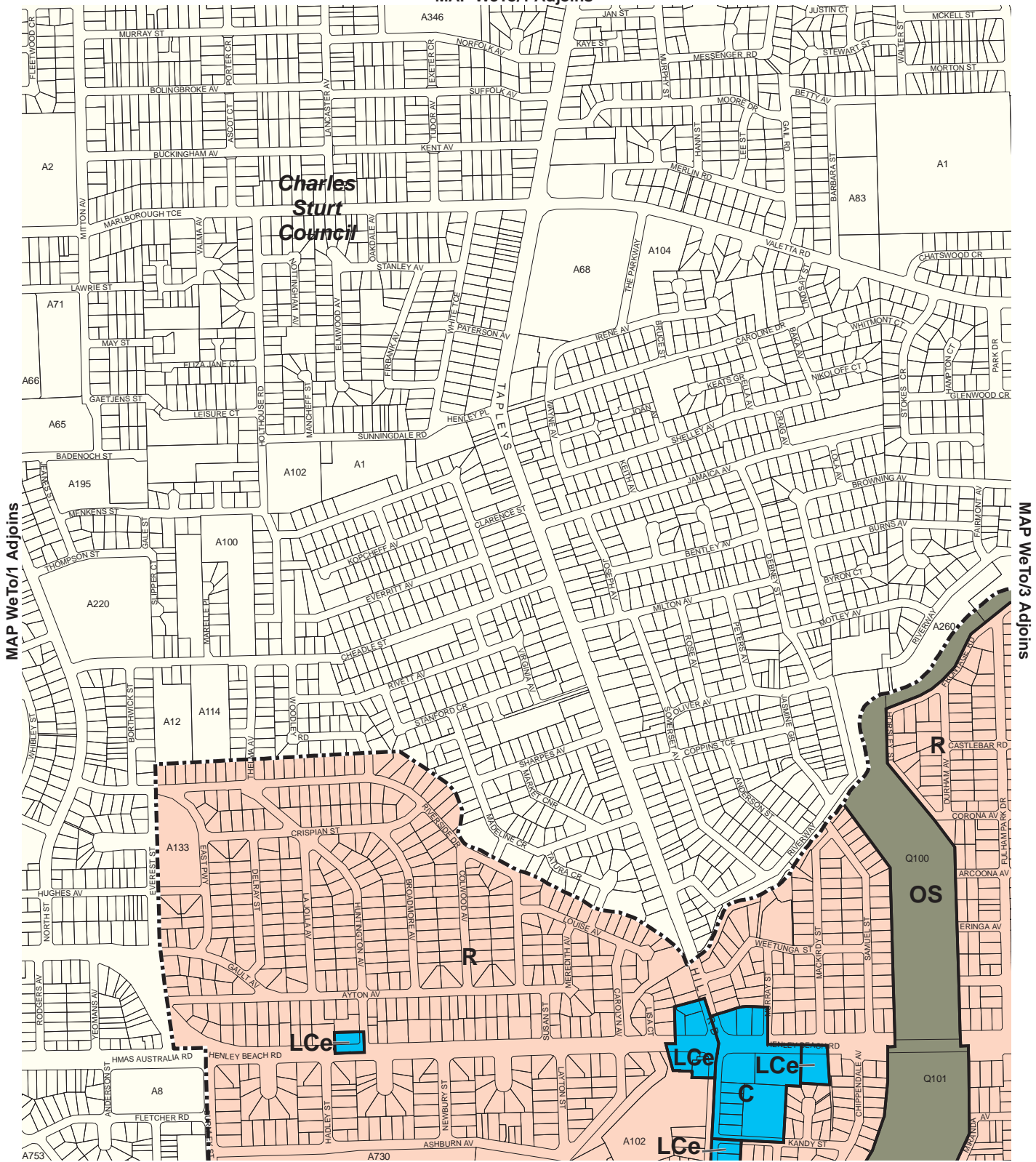


# Overlay Map WeTo/2 NATURAL RESOURCES

WEST TORRENS COUNCIL  
Consolidated - 21 May 2020

- MOSS Study Area
- Development Plan Boundary

MAP WeTo/1 Adjoins



MAP WeTo/6 Adjoins

Lamberts Conformal Conic Projection, GDA94

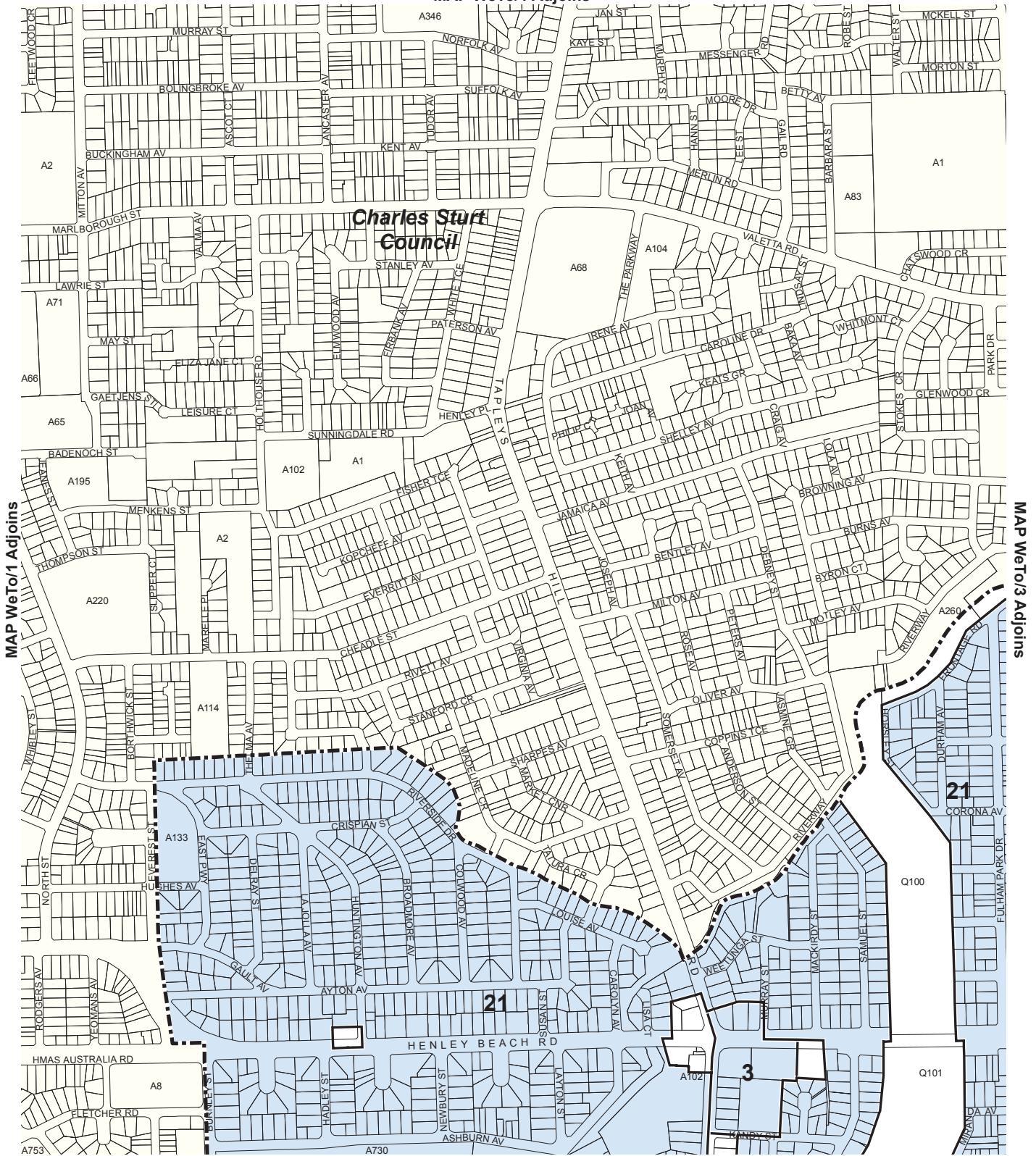


- Zones**
- Commercial
  - Local Centre
  - Open Space
  - Residential
  - Zone Boundary
  - Development Plan Boundary

# Zone Map WeTo/2



MAP WeTo/1 Adjoins



Lamberts Conformal Conic Projection, GDA94

**Policy Area**

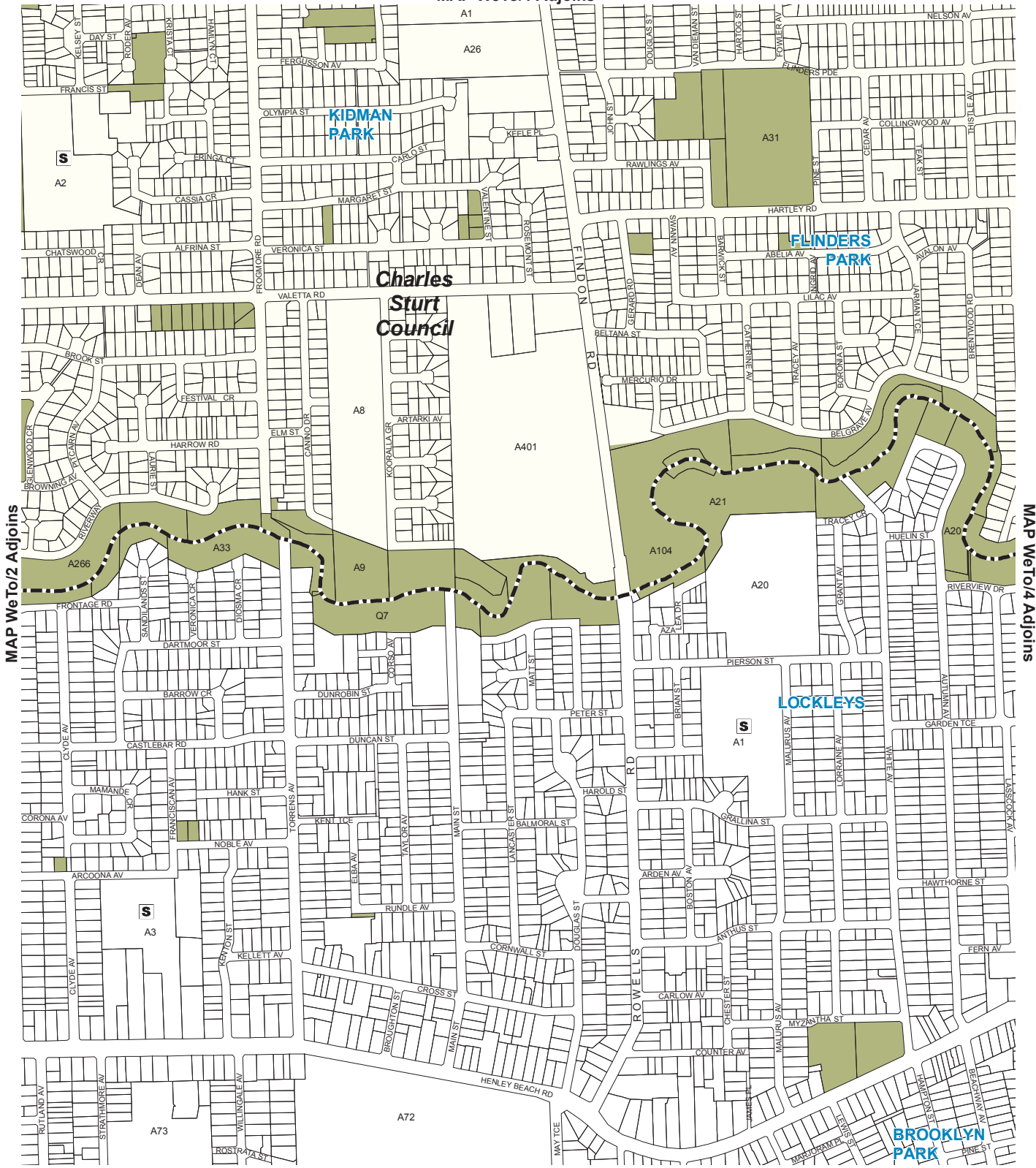
- 21 Low Density
- 3 Local Commercial

- Policy Area Boundary
- Development Plan Boundary

MAP WeTo/6 Adjoins

# Policy Area Map WeTo/2

MAP WeTo/1 Adjoins



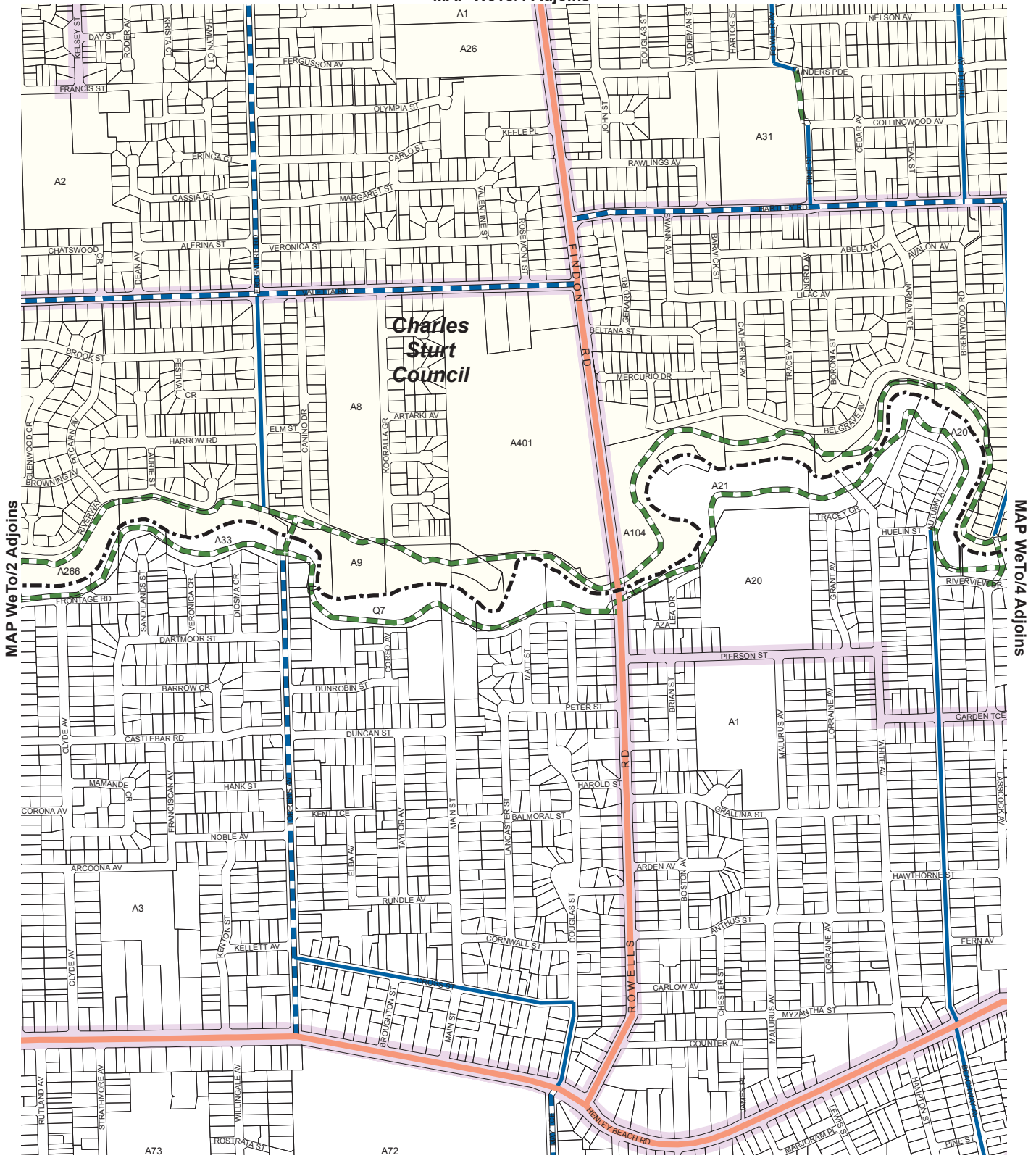
MAP WeTo/7 Adjoins



# Location Map WeTo/3

- S School
- Local Reserves
- Development Plan Boundary

MAP WeTo/1 Adjoins



MAP WeTo/7 Adjoins

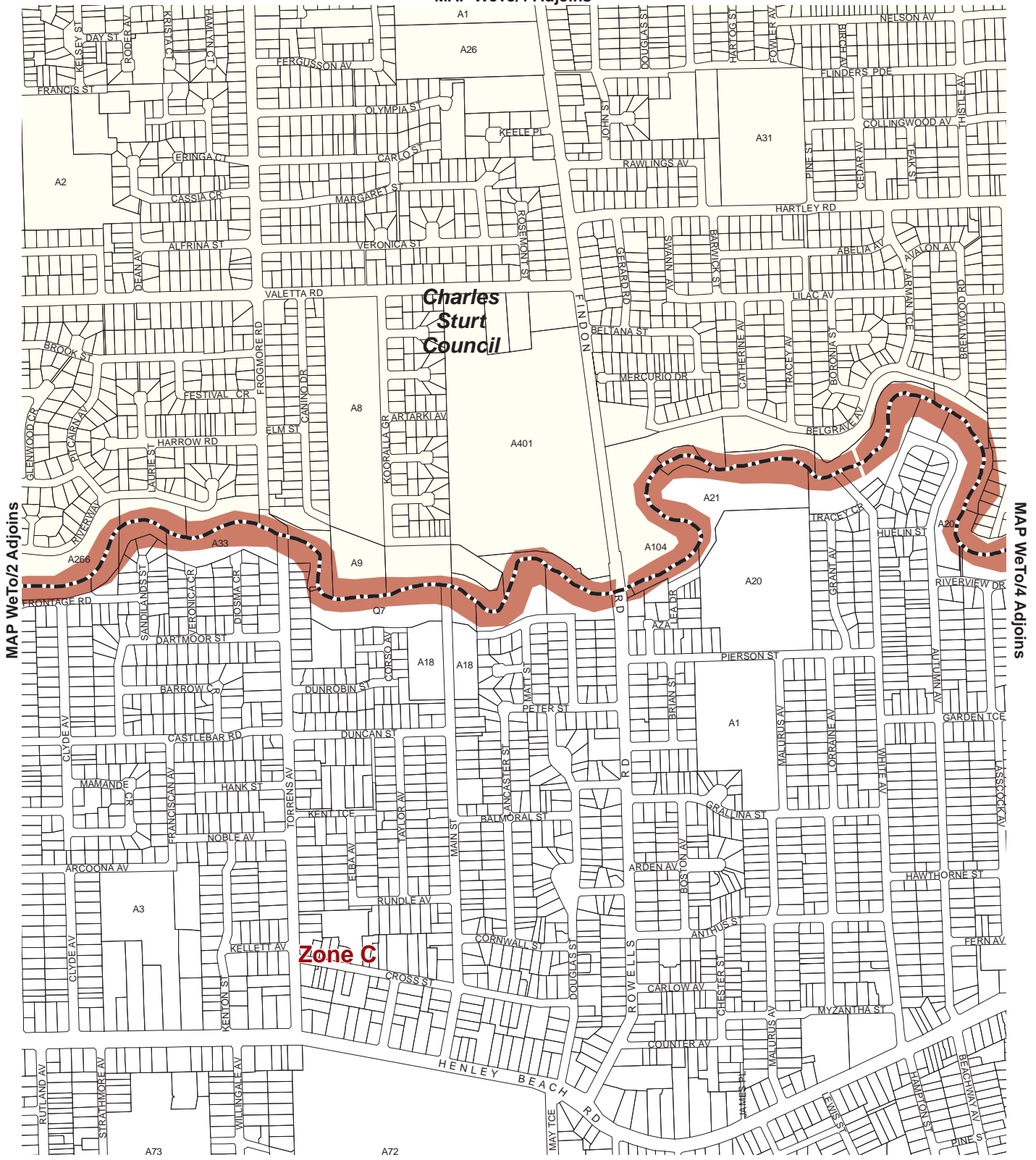
- Secondary Arterial Roads
- Bikedirect Network**
- Main Road - Bike Lane
- Main Road
- Secondary Road - Bike Lane
- Secondary Road
- - - Off Road Sealed Path
- Public Transport
- - - - Development Plan Boundary



# Overlay Map WeTo/3

## TRANSPORT




MAP WeTo/1 Adjoins



**Airport Building Heights**

**Zone C** All Structures Exceeding 15 metres above existing ground level

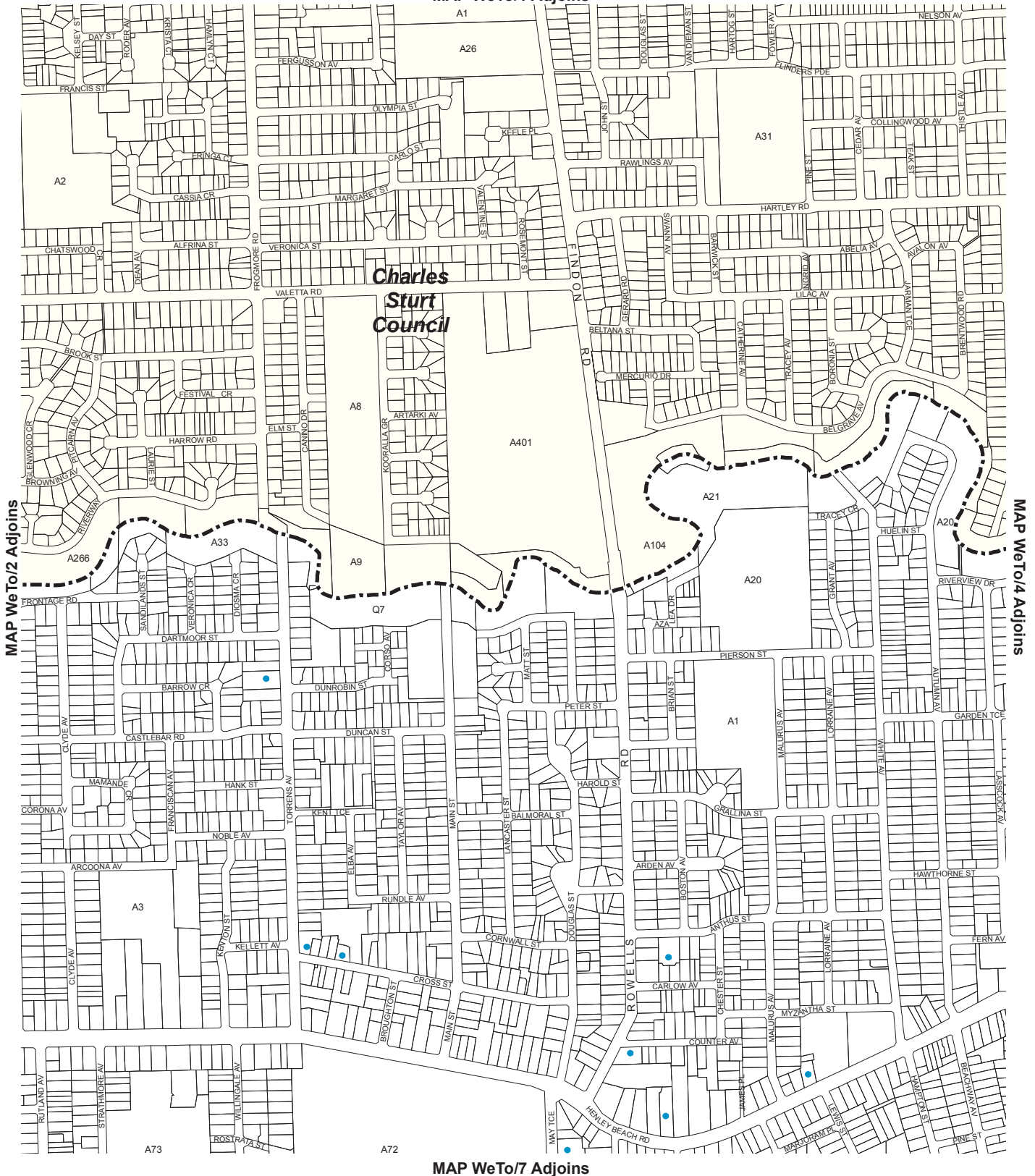


-  Airport Building Heights
-  Flood Hazard
-  Development Plan Boundary

# Overlay Map WeTo/3

## DEVELOPMENT CONSTRAINTS

MAP WeTo/1 Adjoins



Heritage points are indicative only.  
 For further information on State and Local Heritage Places and Contributory  
 Items please refer to the relevant tables within this document.

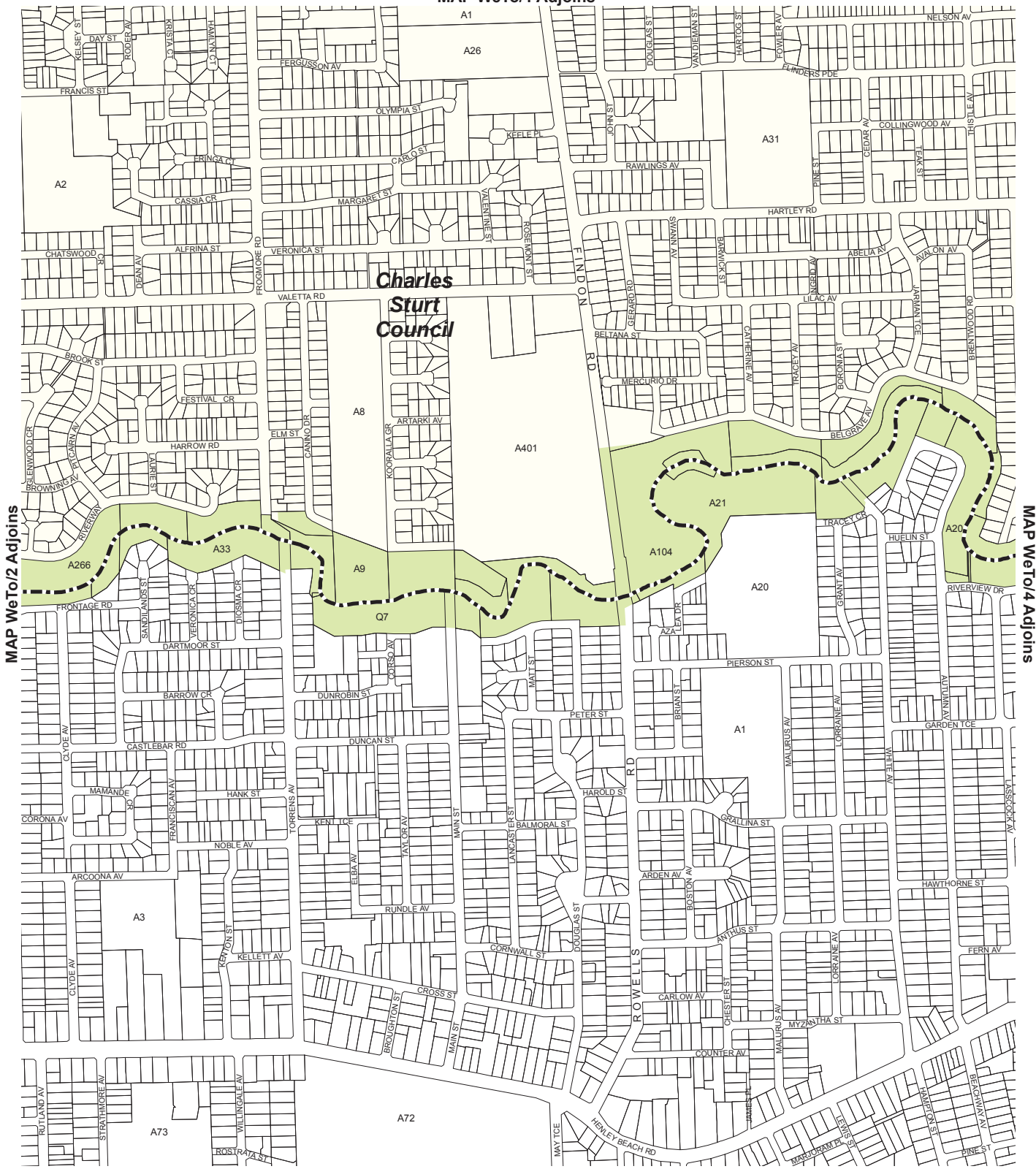


- Local heritage place
- Development Plan Boundary

# Overlay Map WeTo/3

## HERITAGE

MAP WeTo/1 Adjoins



MAP WeTo/2 Adjoins

MAP WeTo/4 Adjoins

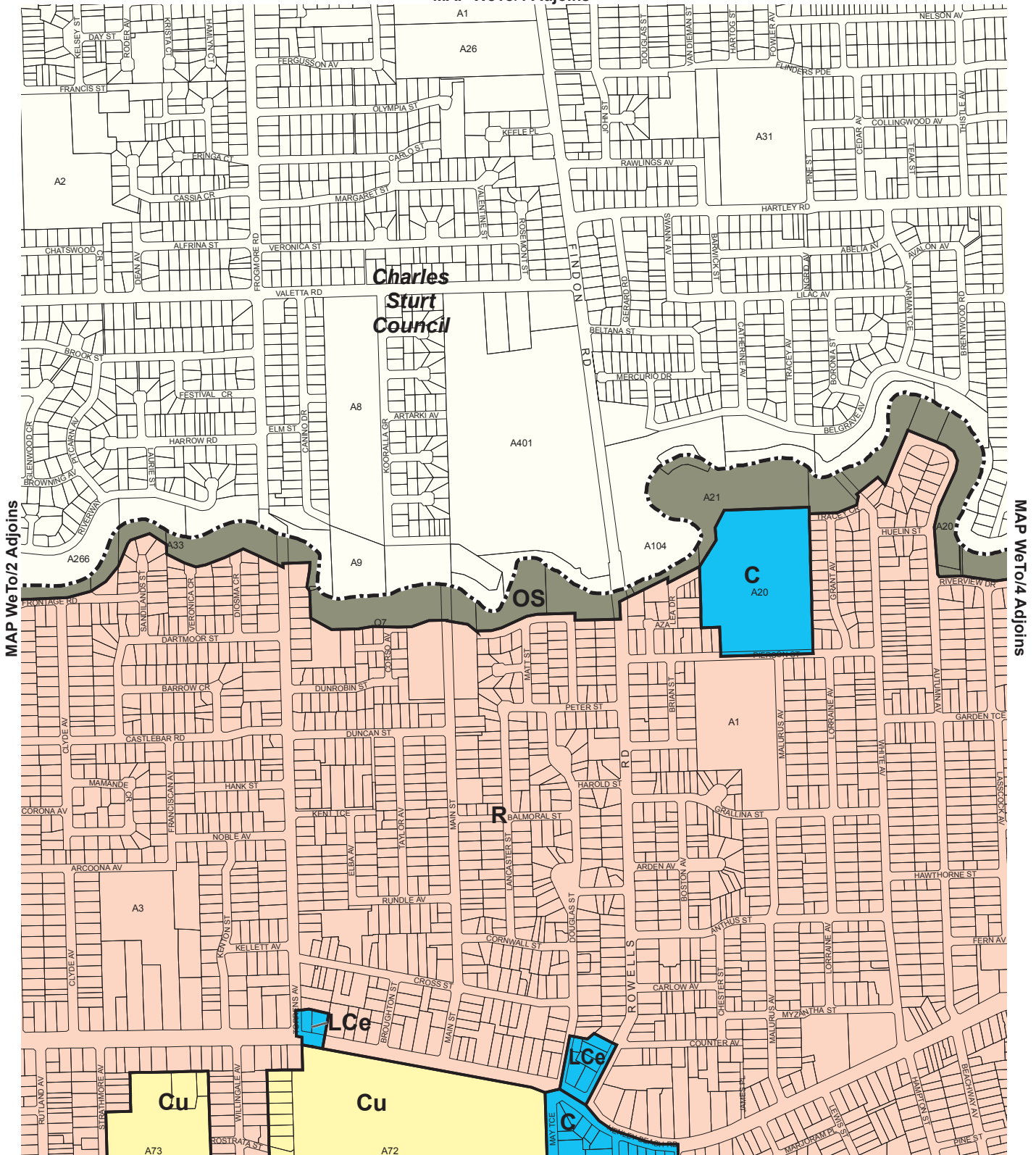
MAP WeTo/7 Adjoins



# Overlay Map WeTo/3 NATURAL RESOURCES

- MOSS Study Area
- Development Plan Boundary

MAP WeTo/1 Adjoins



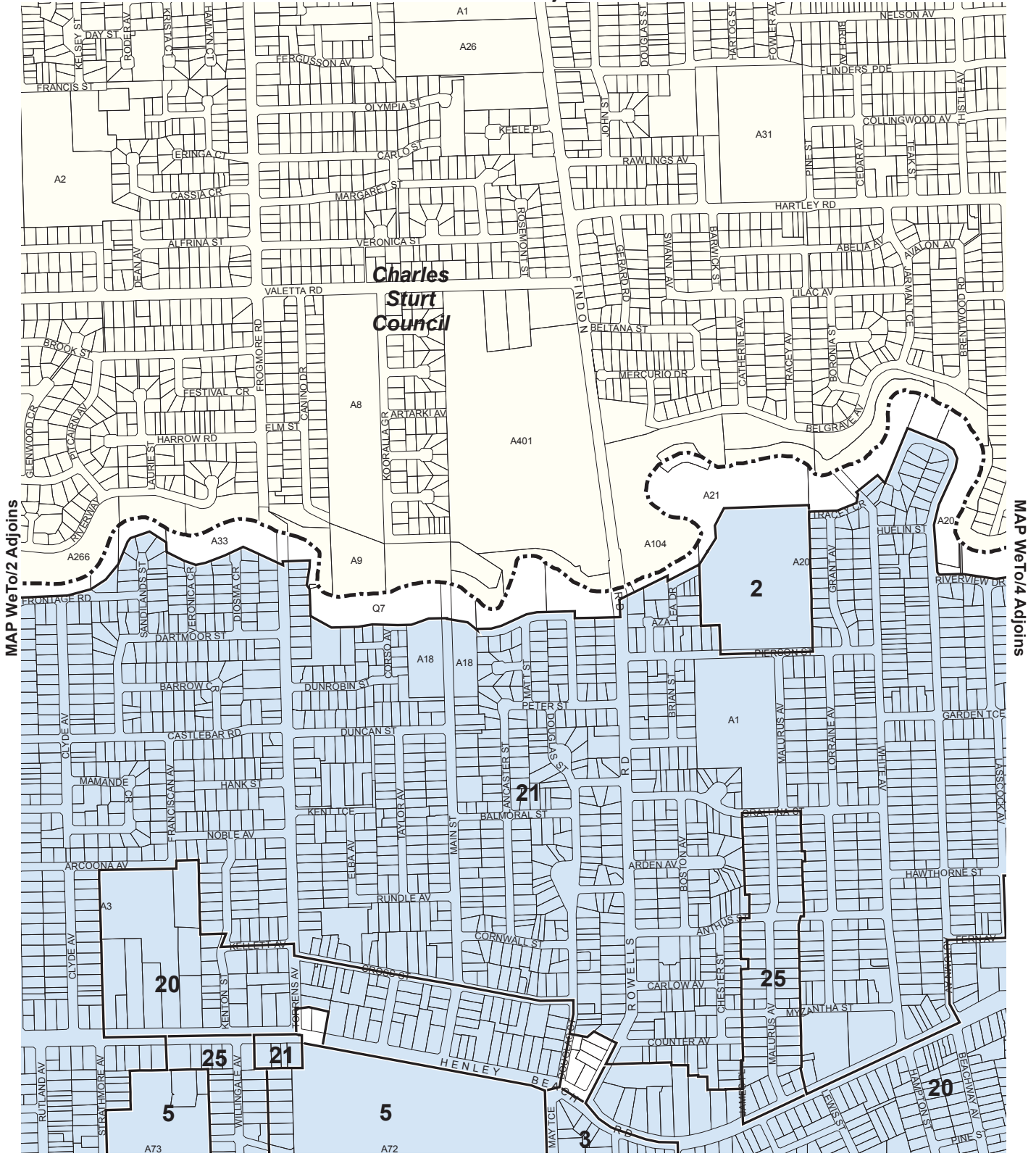
Lamberts Conformal Conic Projection, GDA94

- Zones**
- C** Commercial
  - Cu** Community
  - LCe** Local Centre
  - OS** Open Space
  - R** Residential
  - Zone Boundary
  - Development Plan Boundary



# Zone Map WeTo/3

MAP WeTo/1 Adjoins



Lamberts Conformal Conic Projection, GDA94

**Policy Area**

- 2 District Commercial
- 20 Low Density
- 21 Low Density
- 25 Lockleys Character
- 3 Local Commercial
- 5 Recreation



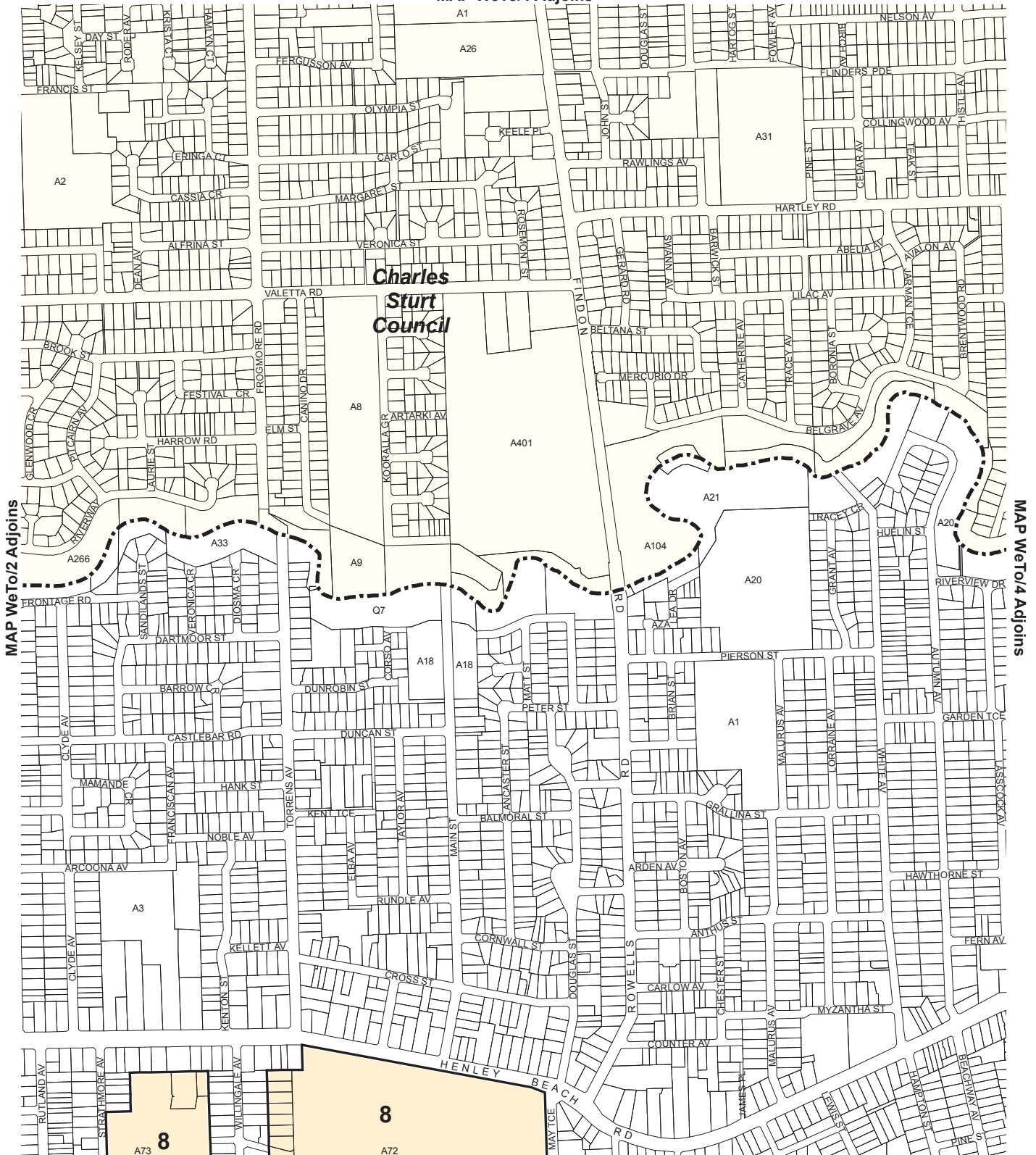
MAP WeTo/7 Adjoins

# Policy Area Map WeTo/3

- Policy Area Boundary
- Development Plan Boundary



MAP WeTo/1 Adjoins



MAP WeTo/7 Adjoins

Lamberts Conformal Conic Projection, GDA94

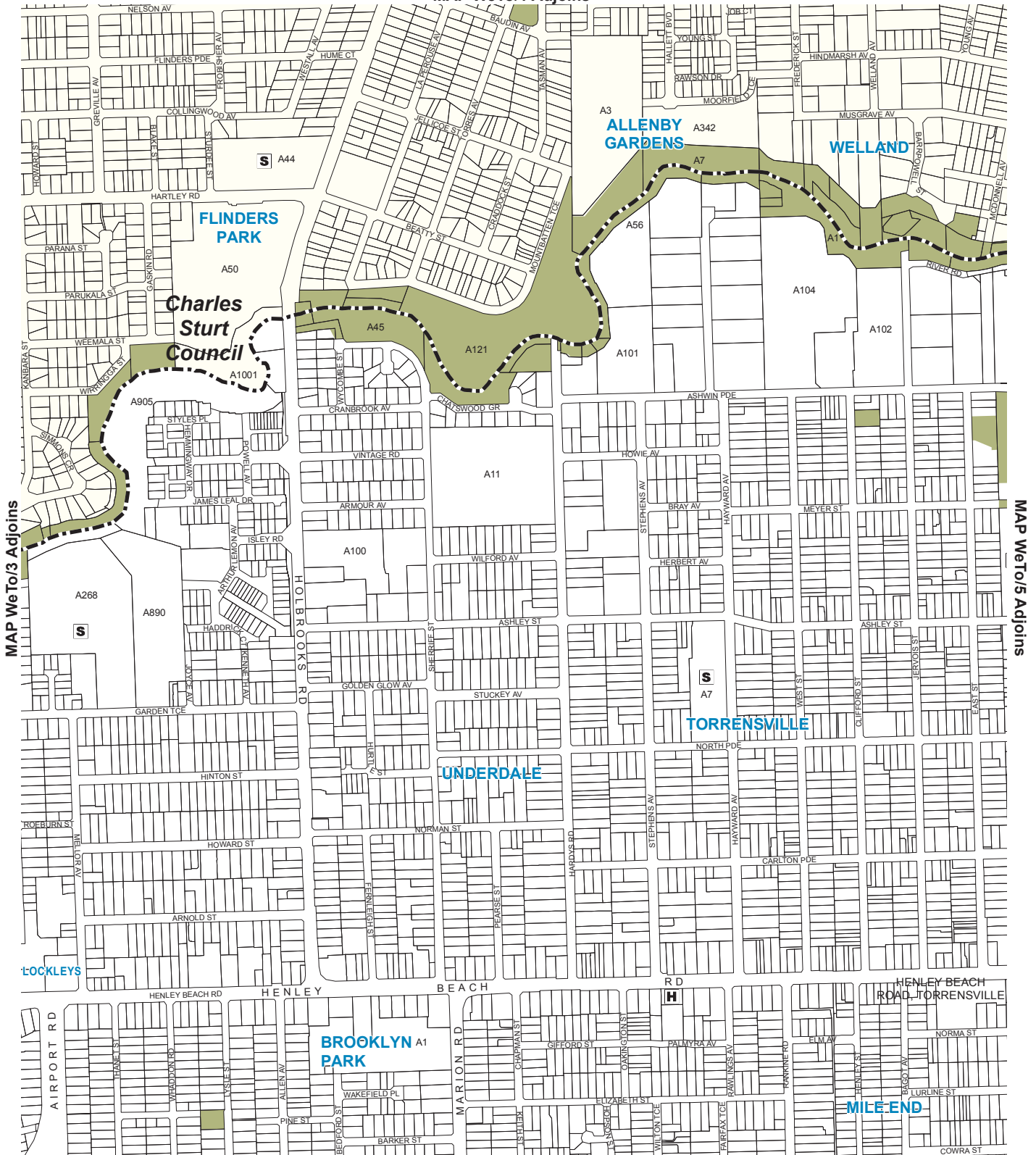
**Precinct**  
8 Open Space



# Precinct Map WeTo/3

- Precinct Boundary
- Development Plan Boundary


MAP WeTo/1 Adjoins



MAP We To/3 Adjoins

MAP WeTo/5 Adjoins

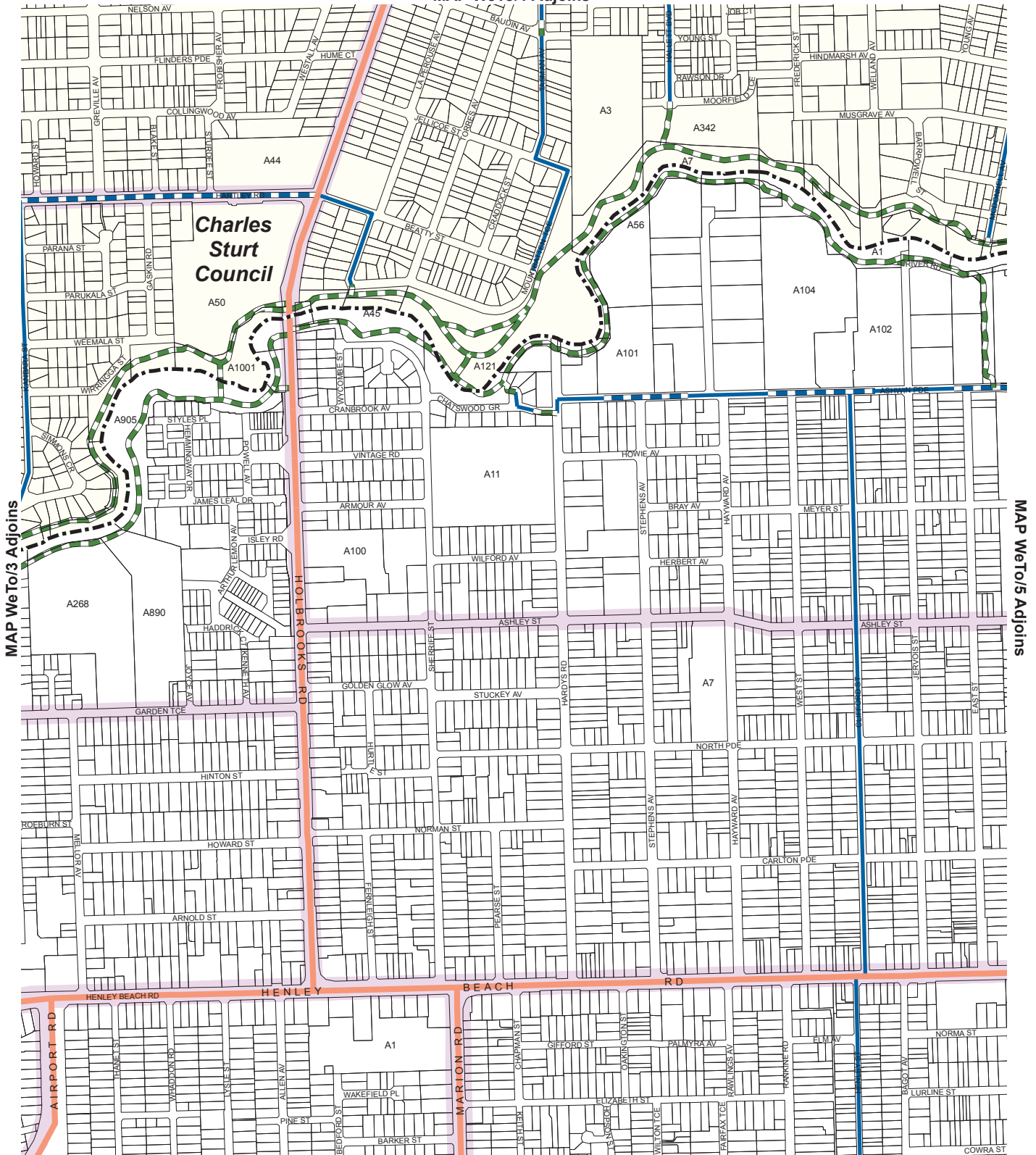
MAP WeTo/8 Adjoins

-  School
-  Other Health Services
-  Local Reserves
-  Development Plan Boundary



# Location Map WeTo/4

MAP WeTo/1 Adjoins



MAP WeTo/8 Adjoins

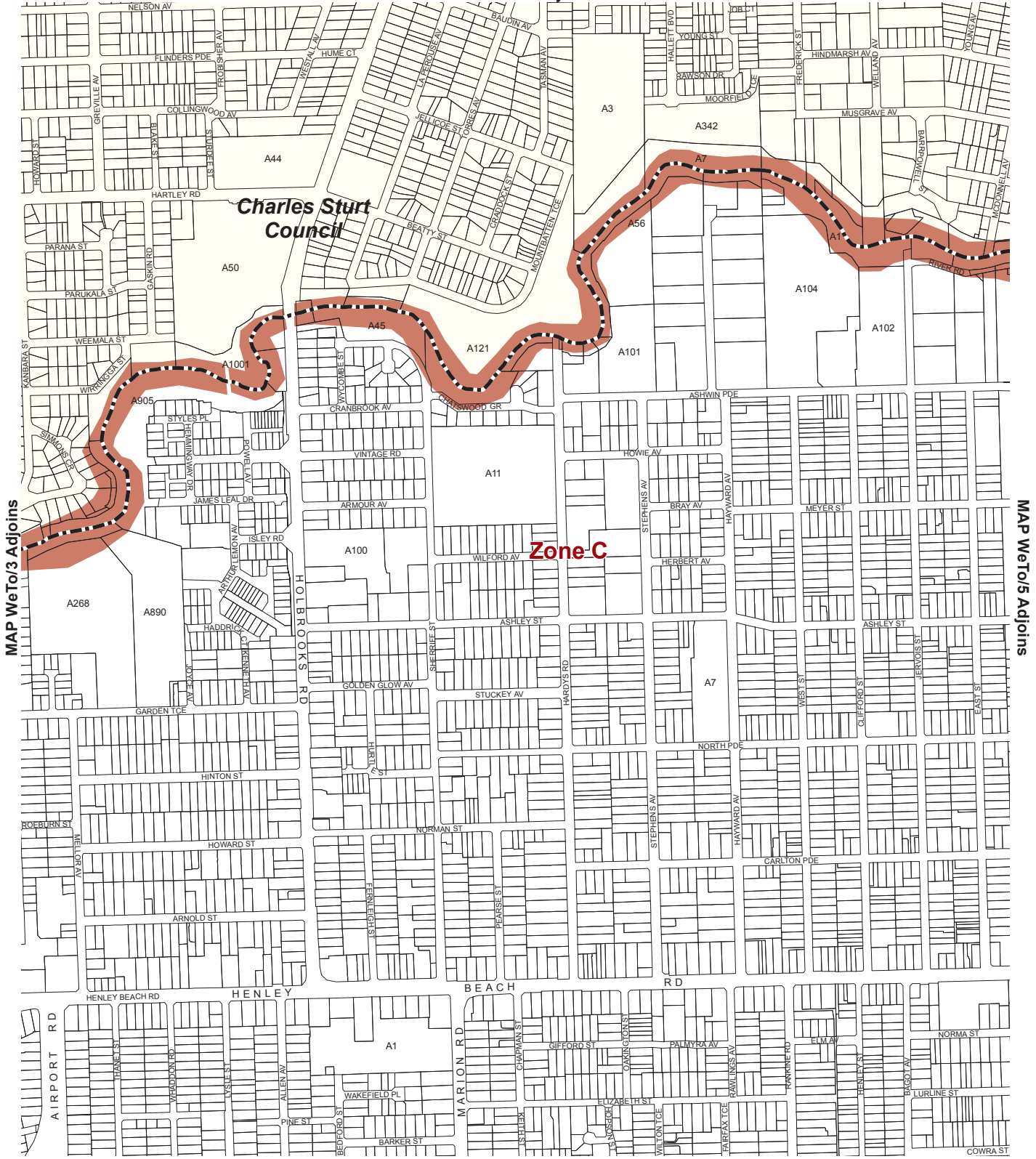


- Secondary Arterial Roads
- Bikedirect Network
- Main Road - Bike Lane
- Main Road
- Secondary Road - Bike Lane
- Secondary Road
- - - Off Road Sealed Path
- Public Transport
- - - - Development Plan Boundary

# Overlay Map WeTo/4

## TRANSPORT

MAP WeTo/1 Adjoins






MAP WeTo/8 Adjoins

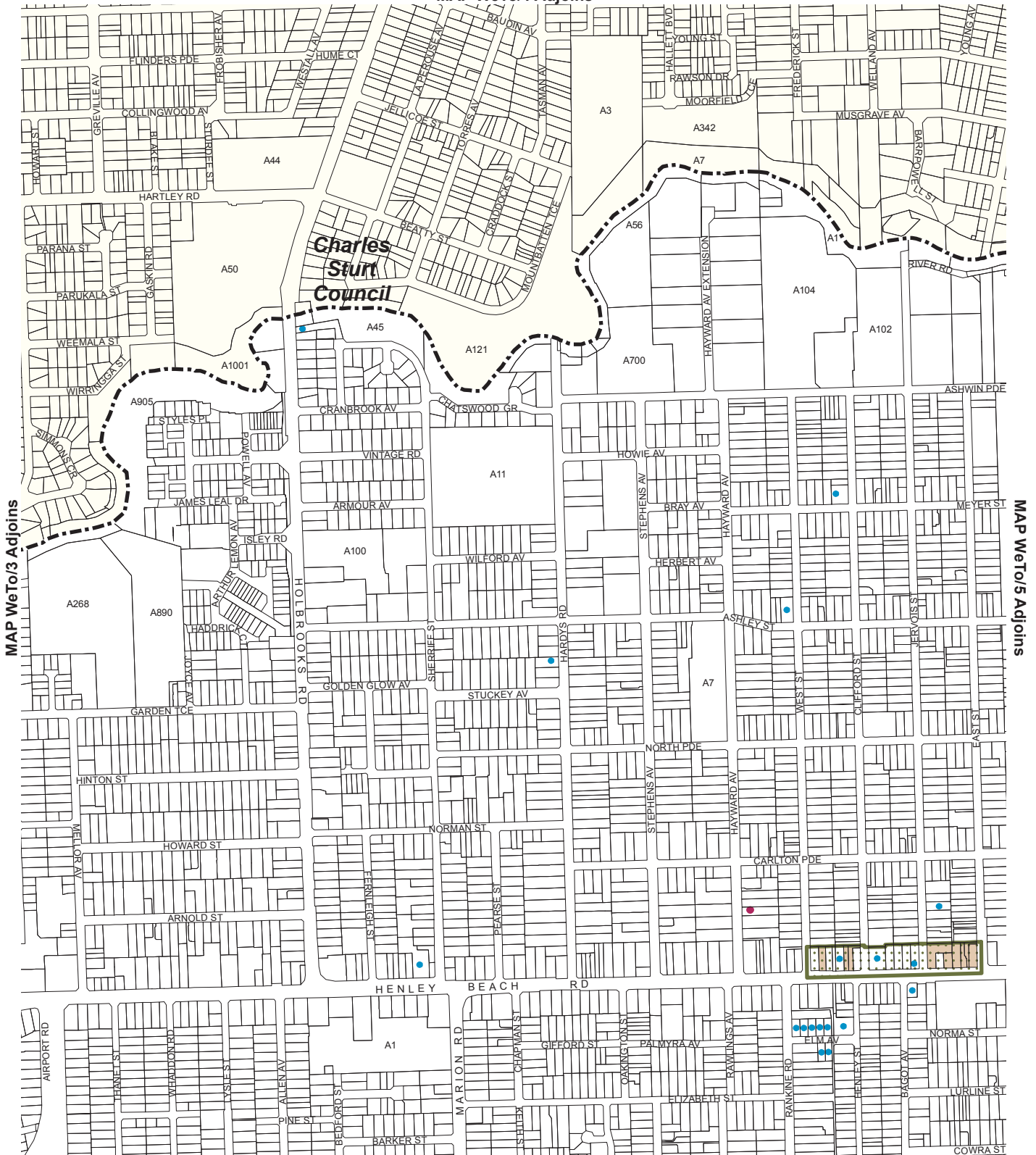
**Airport Building Heights**  
**Zone C** All Structures Exceeding 15 metres above existing ground level



# Overlay Map WeTo/4 DEVELOPMENT CONSTRAINTS

-  Airport Building Heights
-  Flood Hazard
-  Development Plan Boundary

MAP WeTo/1 Adjoins



Heritage points are indicative only.  
 For further information on State and Local Heritage Places and Contributory  
 Items please refer to the relevant tables within this document.



- State heritage place
- Local heritage place
- Historic Conservation Area
- Contributory item
- Development Plan Boundary

# Overlay Map WeTo/4

## HERITAGE

MAP WeTo/1 Adjoins



MAP WeTo/3 Adjoins

MAP WeTo/5 Adjoins

MAP WeTo/8 Adjoins

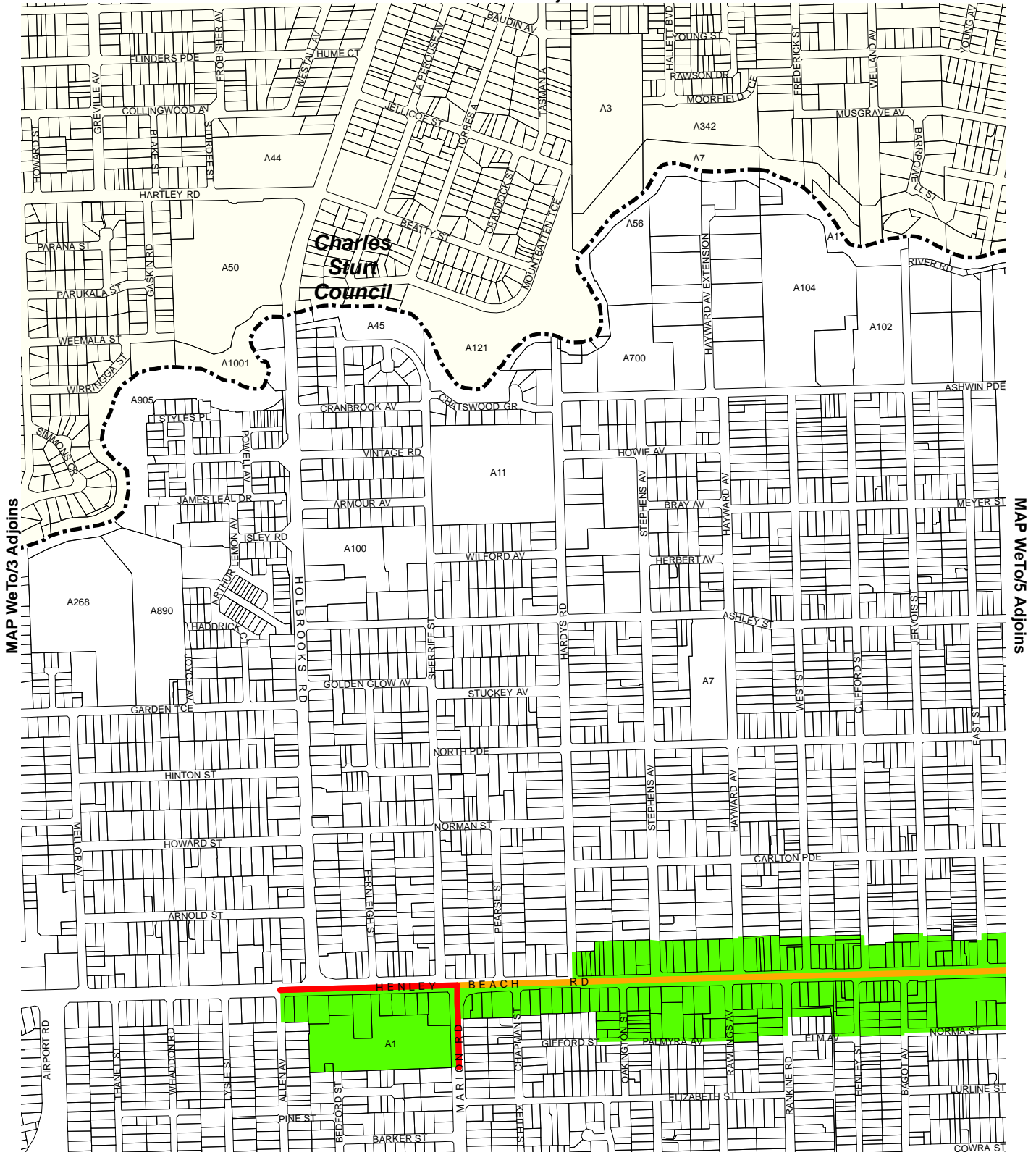


# Overlay Map WeTo/4

## NATURAL RESOURCES

- MOSS Study Area
- Development Plan Boundary

MAP WeTo/1 Adjoins



MAP WeTo/8 Adjoins

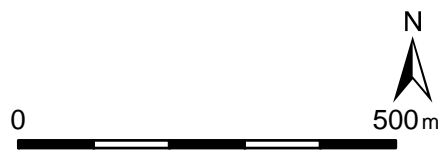
MAP WeTo/3 Adjoins

MAP WeTo/5 Adjoins

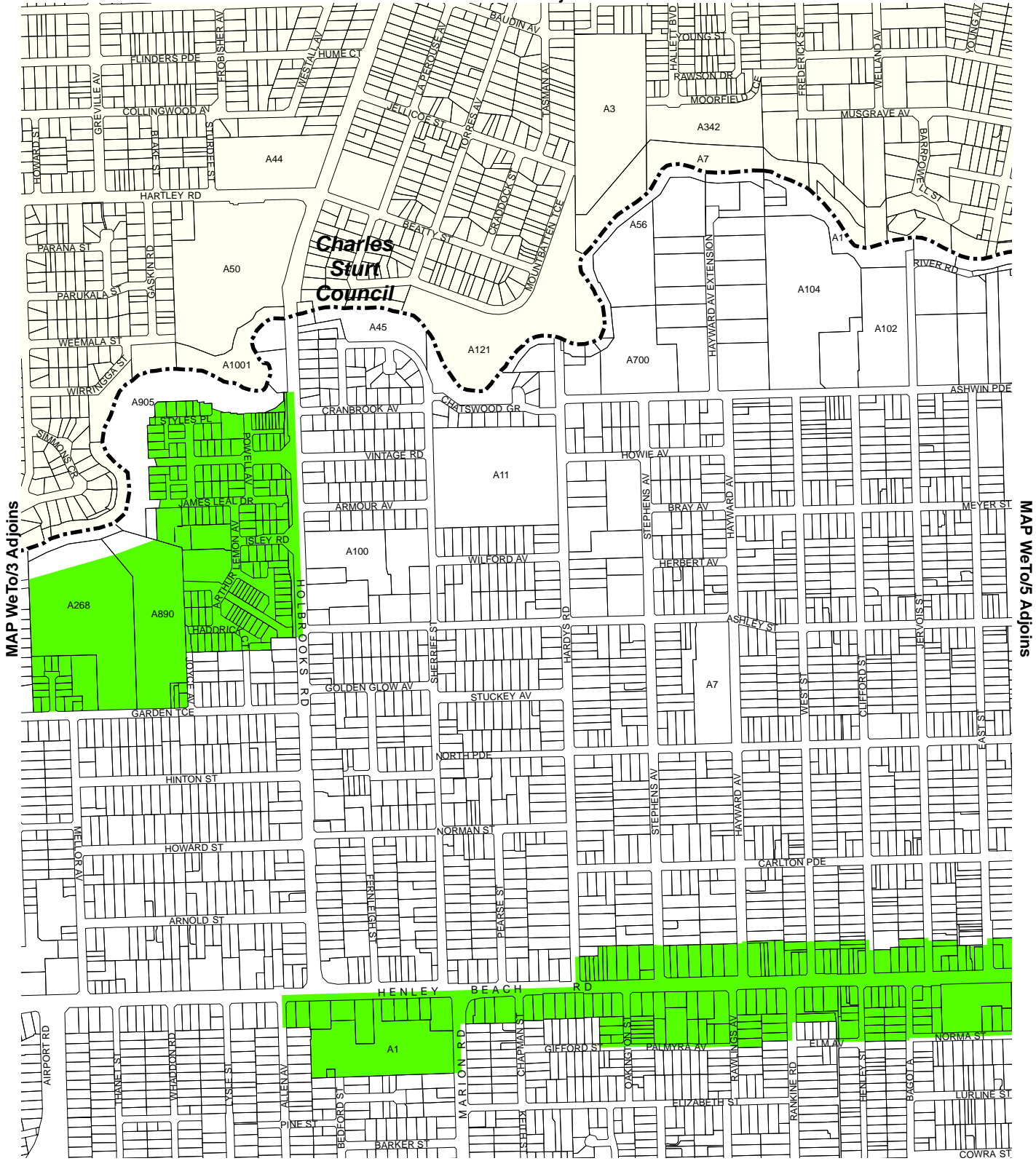
- Designated Road: type A road
- Designated Road: type B road
- Noise and Air Emissions Designated Area
- Development Plan Boundary

# Overlay Map WeTo/4

## NOISE AND AIR EMISSIONS



MAP WeTo/1 Adjoins



MAP WeTo/8 Adjoins



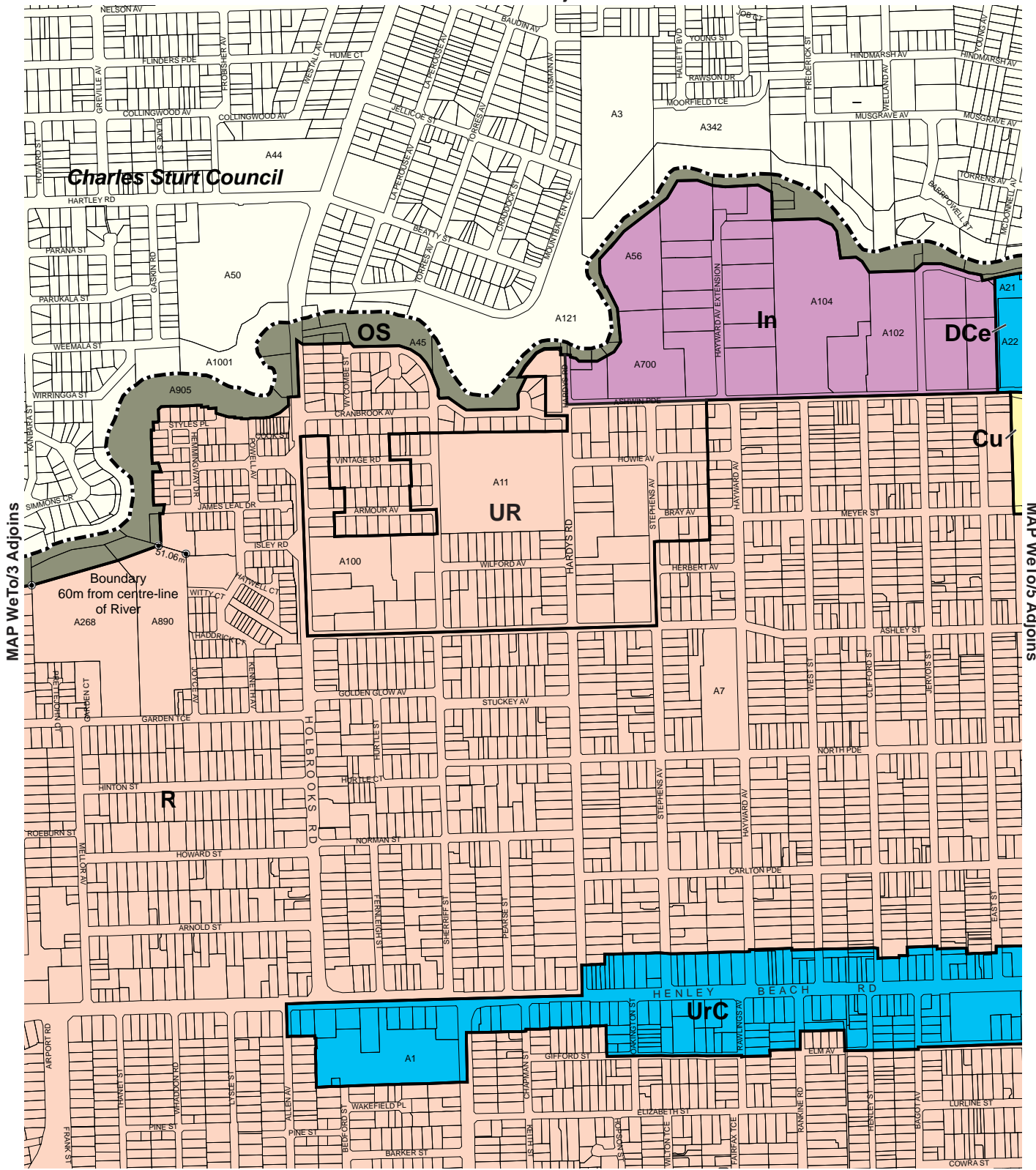
# Overlay Map WeTo/4

## AFFORDABLE HOUSING

- Affordable Housing Designated Area
- Development Plan Boundary



MAP WeTo/1 Adjoins



MAP WeTo/8 Adjoins

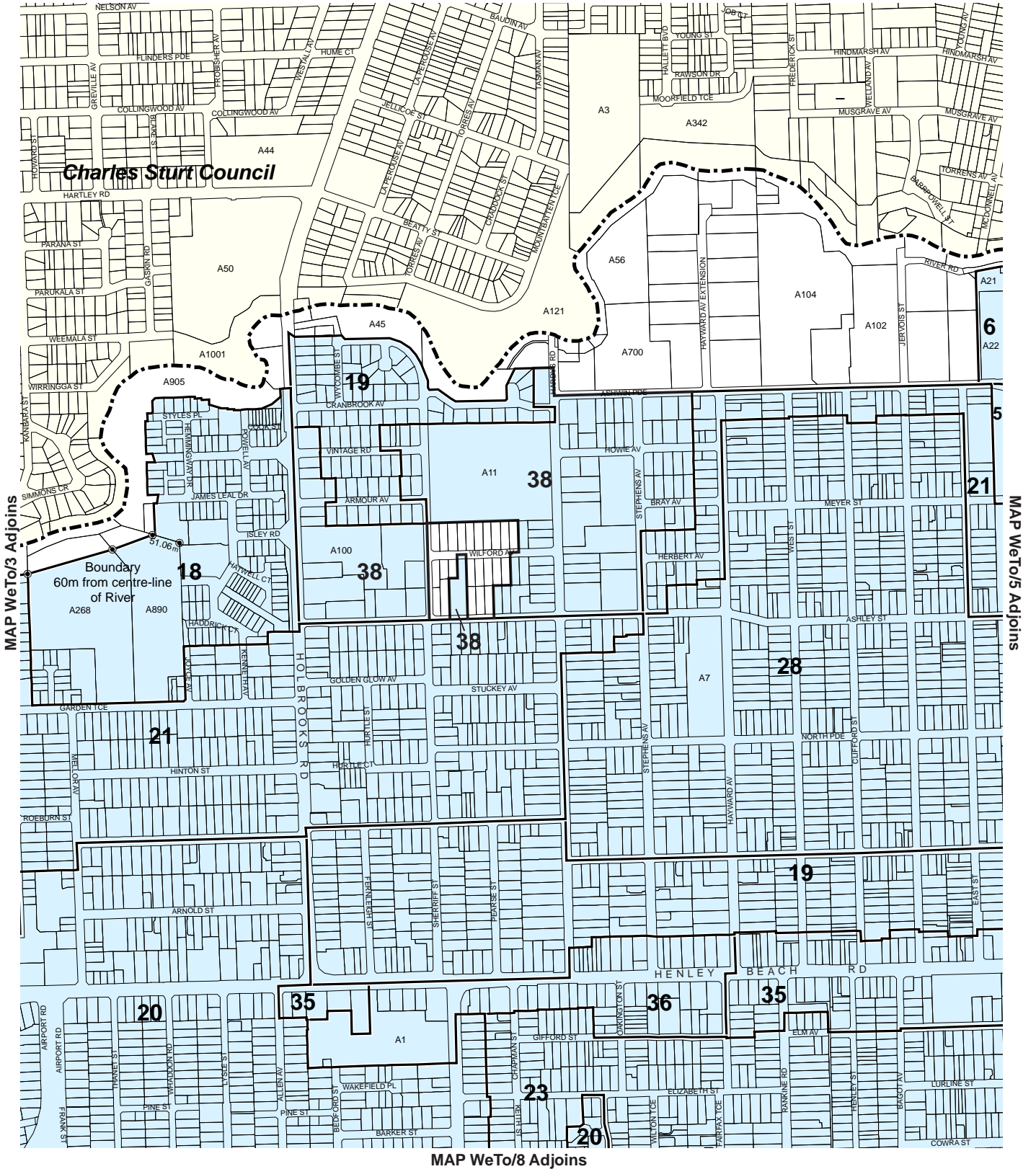
Lamberts Conformal Conic Projection, GDA94

- Zones**
- Community
  - District Centre
  - Industry
  - Open Space
  - Residential
  - Urban Corridor
  - Urban Renewal
  - Zone Boundary
  - Development Plan Boundary



# Zone Map WeTo/4

MAP WeTo/1 Adjoins



Lamberts Conformal Conic Projection, GDA94

**Policy Area**

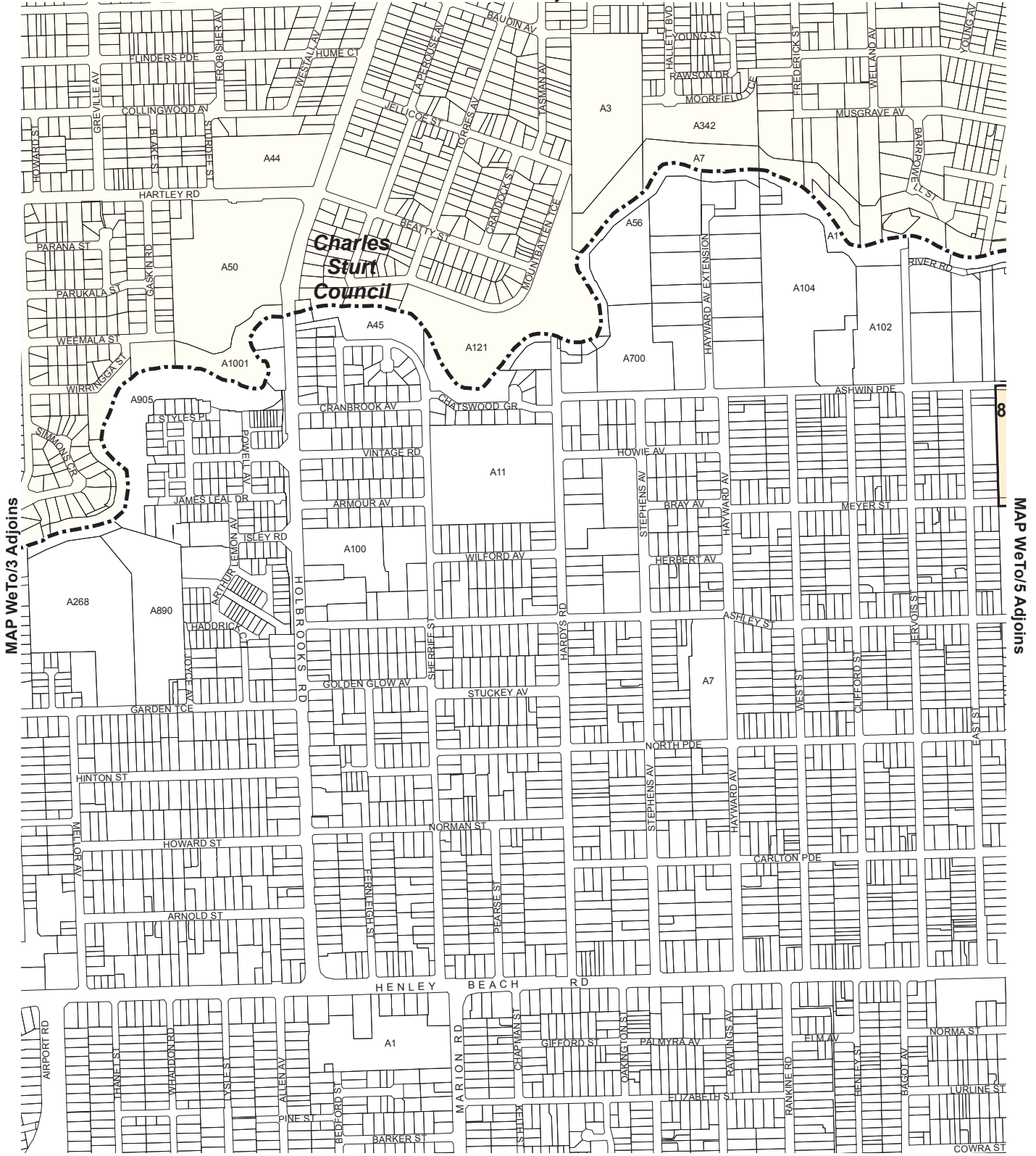
- 18 Medium Density
- 19 Medium Density
- 20 Low Density
- 21 Low Density
- 23 Cowandilla / Mile End West Character
- 28 Torrensville Character
- 35 High Street
- 36 Transit Living
- 5 Recreation
- 6 Brickworks
- 38 Mixed Use Transition

-  Policy Area Boundary
-  Development Plan Boundary

MAP WeTo/8 Adjoins

# Policy Area Map WeTo/4

MAP WeTo/1 Adjoins



MAP WeTo/8 Adjoins

Lamberts Conformal Conic Projection, GDA94

Precinct  
8 Open Space



# Precinct Map WeTo/4

- Precinct Boundary
- Development Plan Boundary

MAP WeTo/1 Adjoins



MAP WeTo/4 Adjoins

MAP WeTo/1 Adjoins

MAP WeTo/9 Adjoins

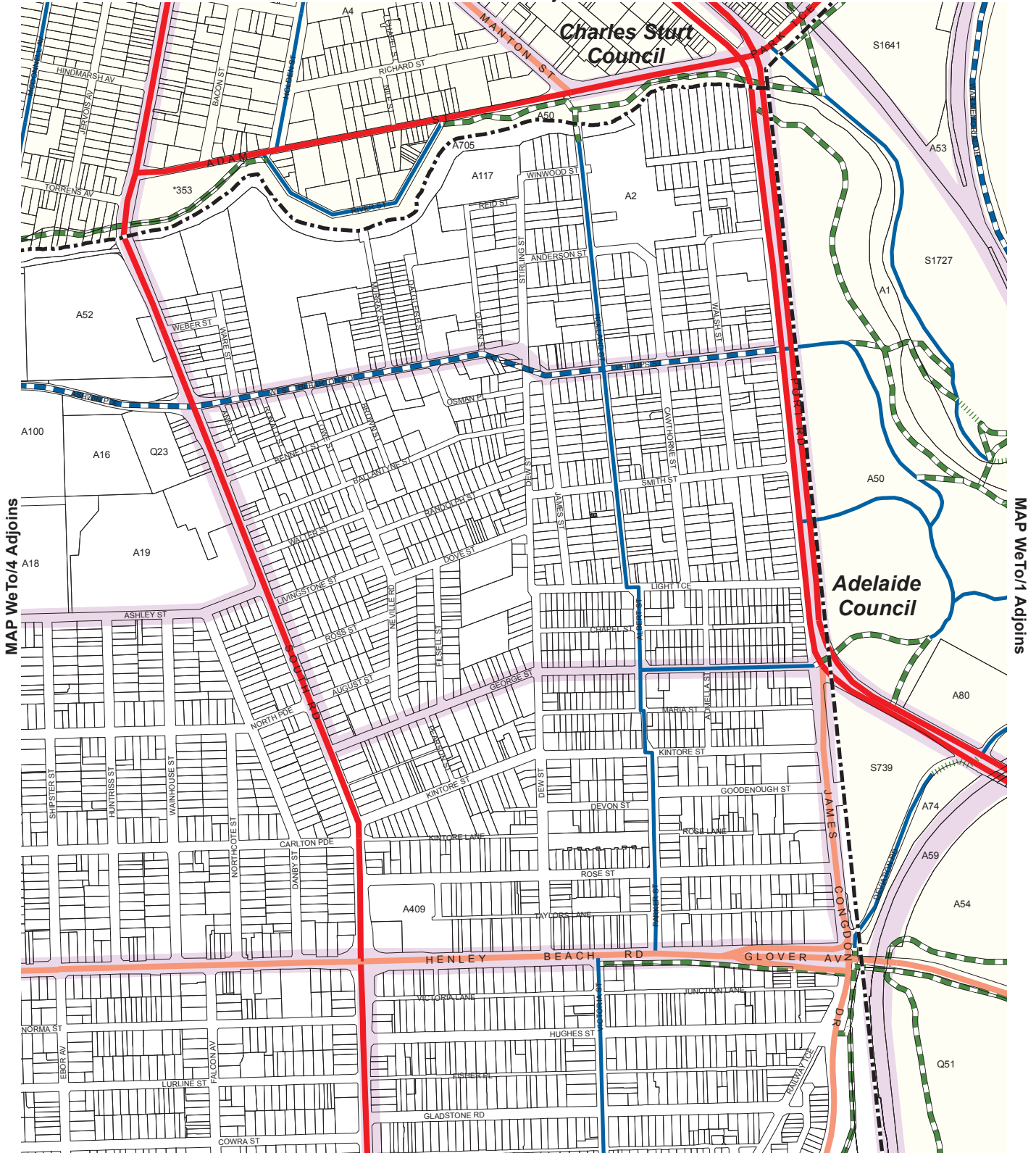


- S School
- L Public Library
- P Post Office
- H Hospital
- Railways
- Local Reserves
- Development Plan Boundary

# Location Map WeTo/5

MAP WeTo/1 Adjoins

Charles Sturt Council



MAP WeTo/4 Adjoins

MAP WeTo/1 Adjoins

MAP WeTo/9 Adjoins

Adelaide Council

- Primary Arterial Roads
- Secondary Arterial Roads
- Bikedirect Network**
- Main Road - Bike Lane
- Main Road
- Secondary Road - Bike Lane
- Secondary Road
- Off Road Sealed Path
- Off Road Track
- Public Transport
- Development Plan Boundary

0 500m



# Overlay Map WeTo/5 TRANSPORT

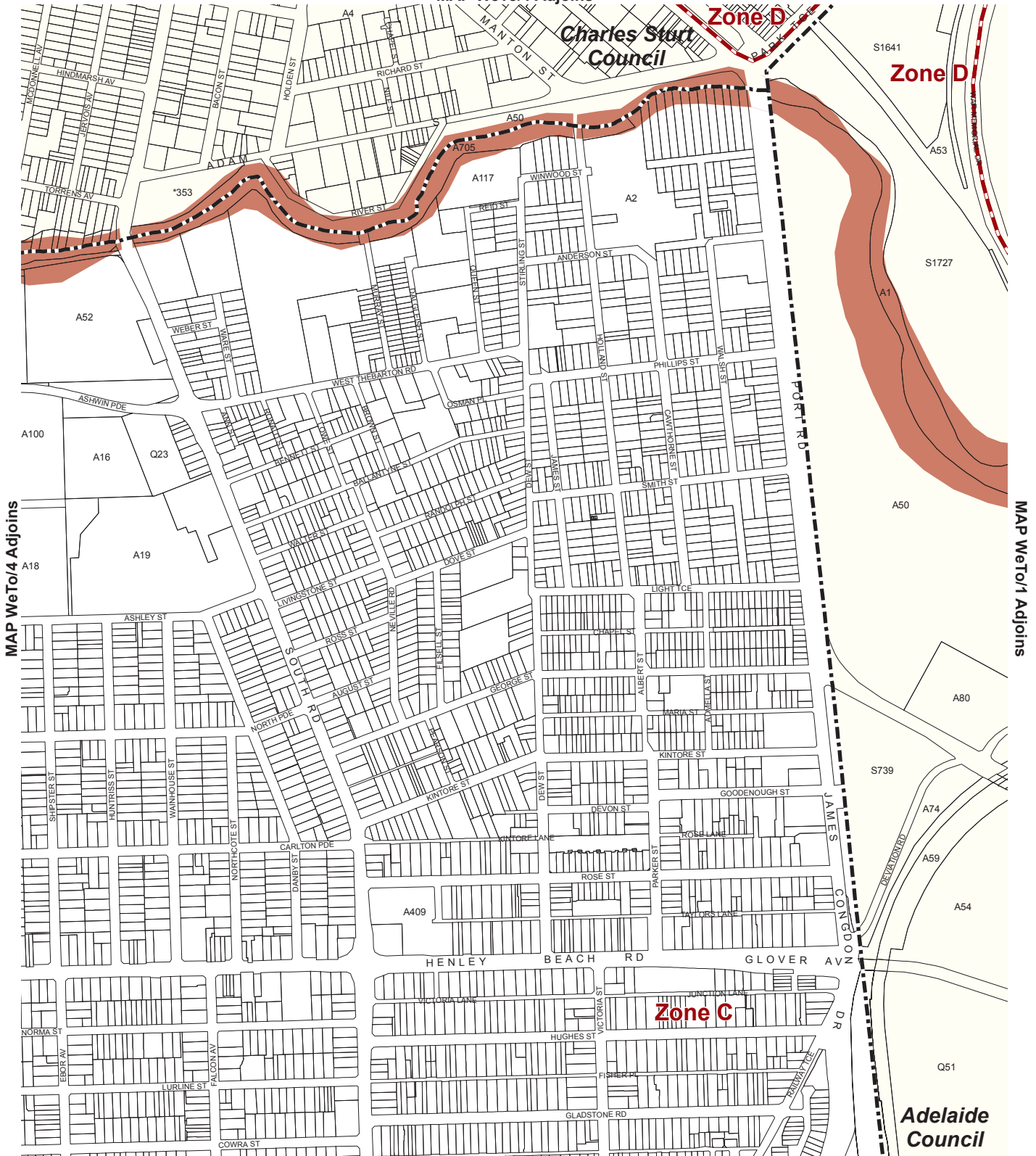
MAP WeTo/1 Adjoins

Charles Sturt Council

Zone D

Zone D

Adelaide Council



- Airport Building Heights**
- Zone C** All Structures Exceeding 15 metres above existing ground level
- Zone D** All Structures Exceeding 45 metres above existing ground level



# Overlay Map WeTo/5

## DEVELOPMENT CONSTRAINTS

- Airport Building Heights
- Flood Hazard
- Development Plan Boundary

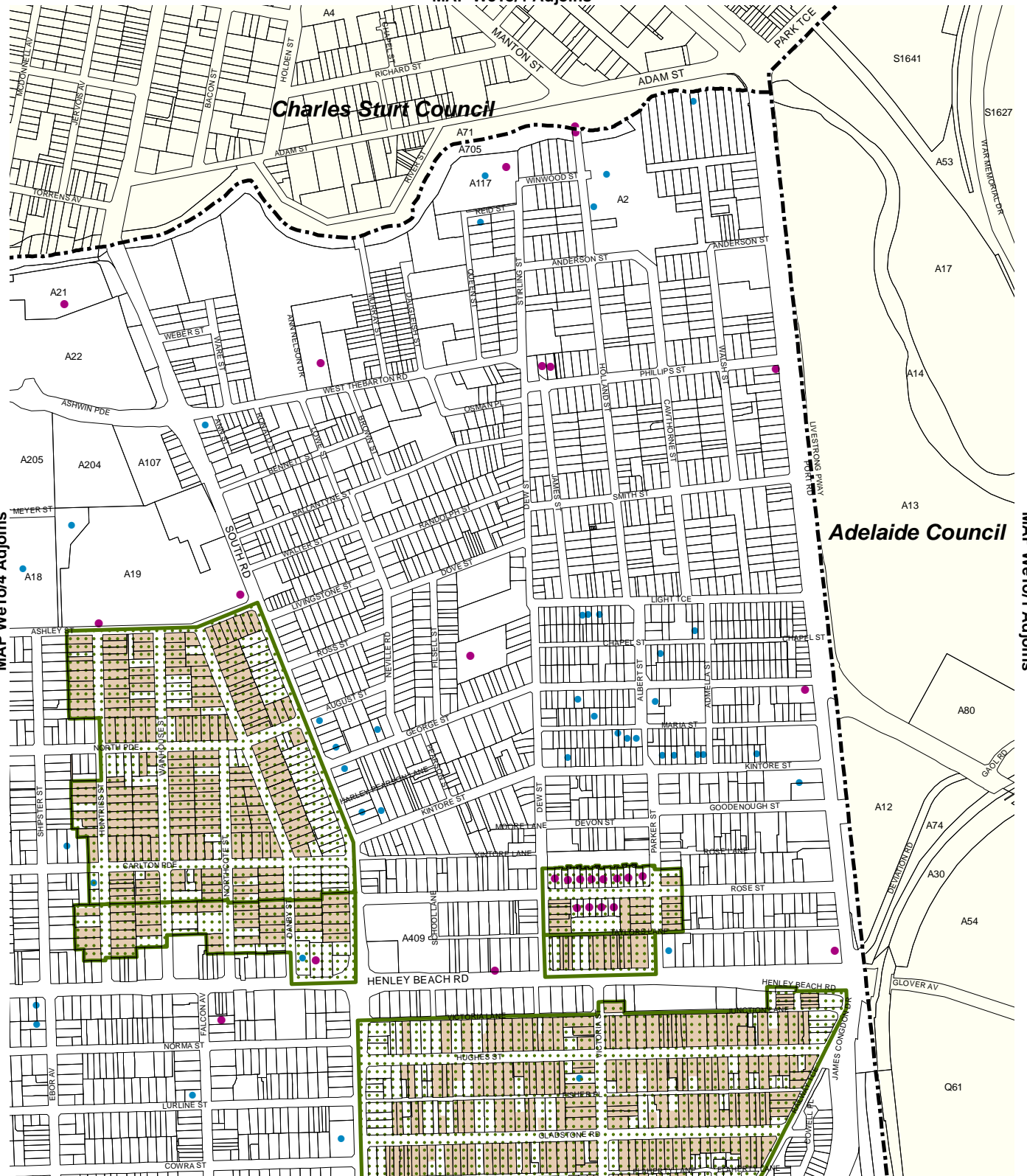
MAP WeTo/1 Adjoins

Charles Sturt Council

Adelaide Council

MAP WeTo/1 Adjoins

MAP WeTo/4 Adjoins



MAP WeTo/9 Adjoins

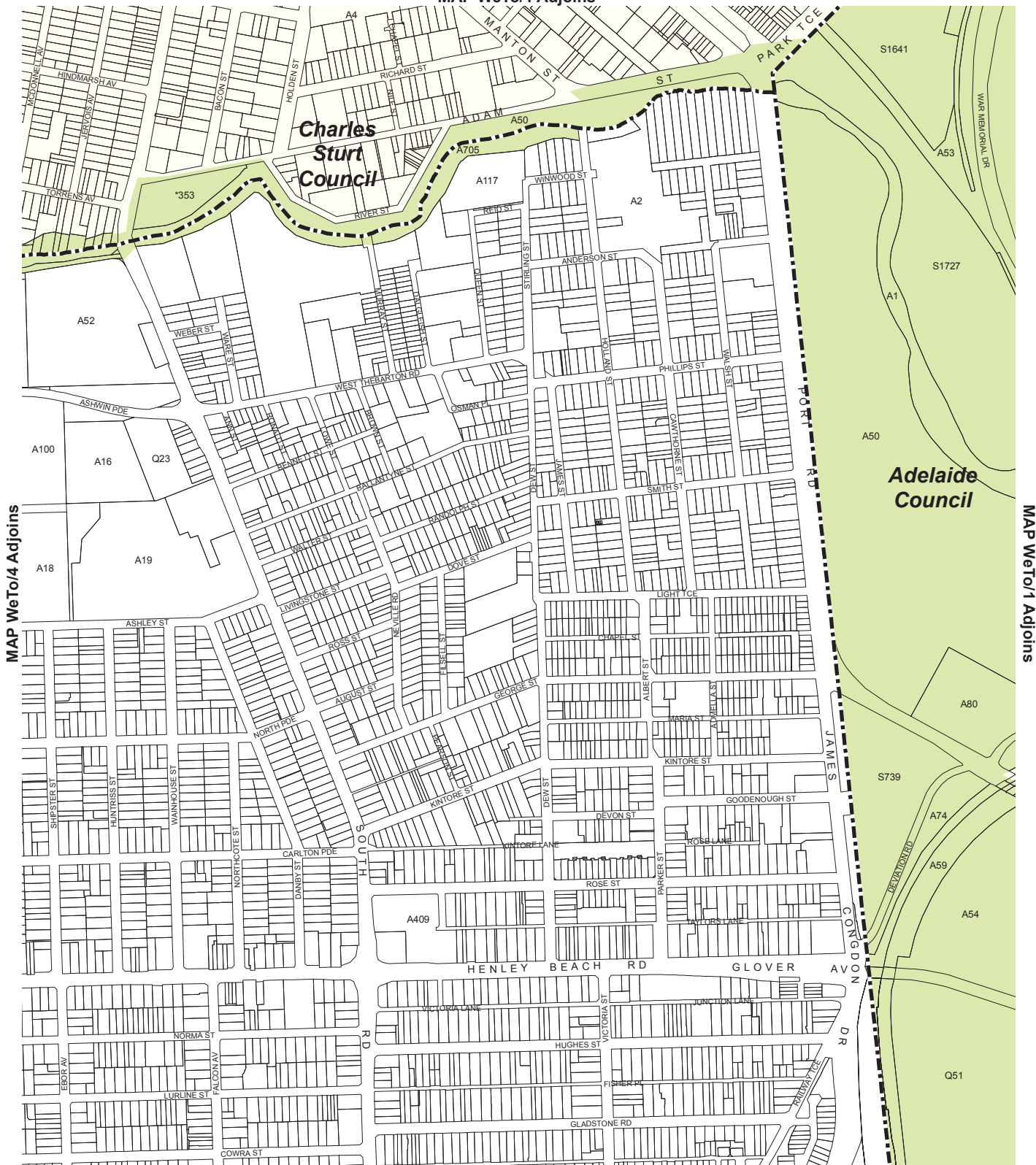
Heritage points are indicative only.  
For further information on State and Local Heritage Places and Contributory Items please refer to the relevant tables within this document.



- Local heritage place
- State heritage place
- Contributory Item
- Historic Conservation Area
- Development Plan Boundary

# Overlay Map WeTo/5 HERITAGE

MAP WeTo/1 Adjoins



MAP WeTo/4 Adjoins

MAP WeTo/1 Adjoins

MAP WeTo/9 Adjoins



# Overlay Map WeTo/5

## NATURAL RESOURCES

- MOSS Study Area
- Development Plan Boundary



MAP WeTo/1 Adjoins

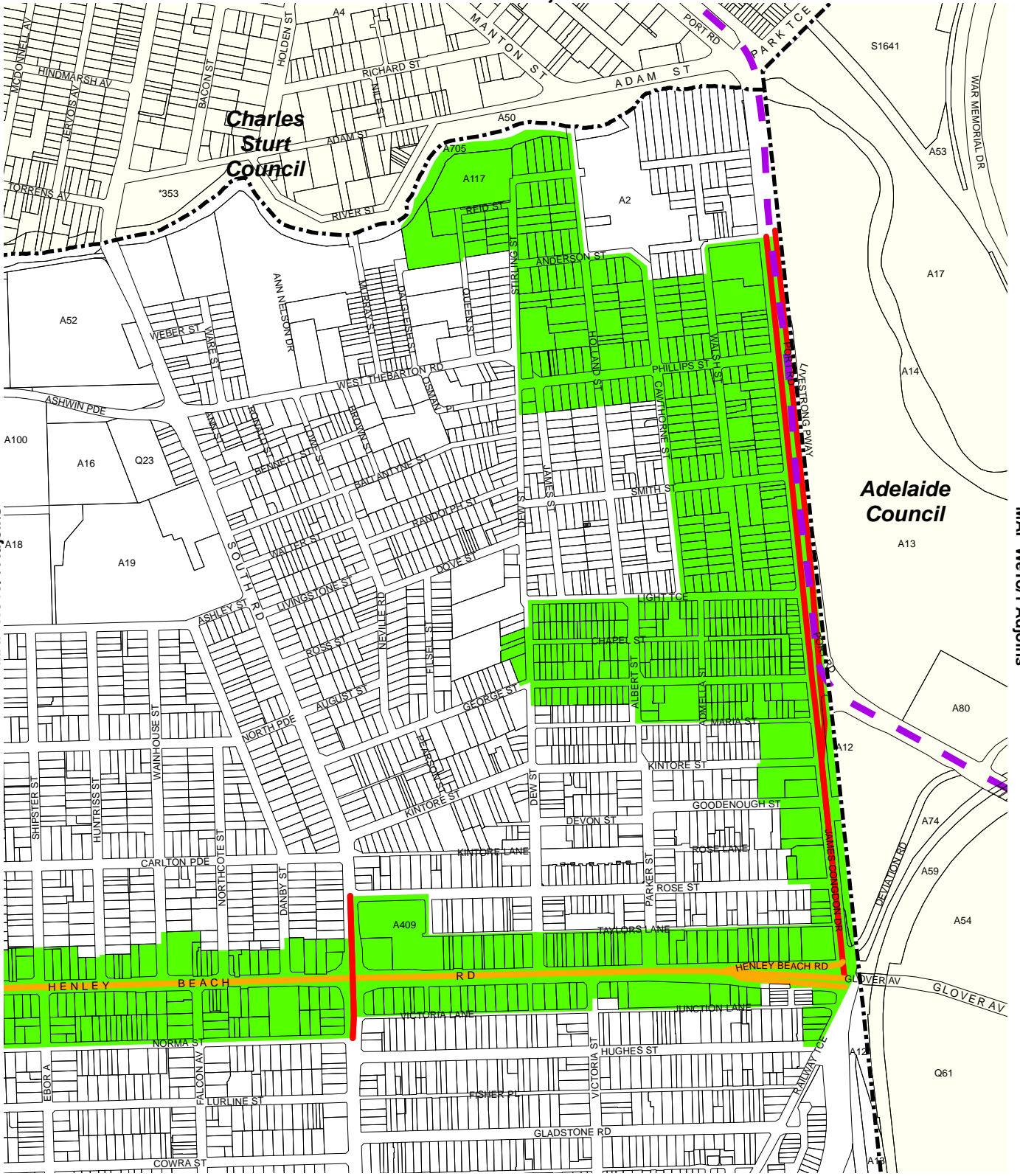
Charles Sturt Council






Adelaide Council

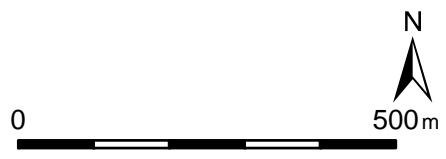
MAP WeTo/1 Adjoins

MAP WeTo/4 Adjoins

MAP WeTo/9 Adjoins



-  Tram Line
-  Designated Road: type A road
-  Designated Road: type B road
-  Noise and Air Emissions Designated Area
-  Development Plan Boundary



# Overlay Map WeTo/5

## NOISE AND AIR EMISSIONS

MAP WeTo/1 Adjoins

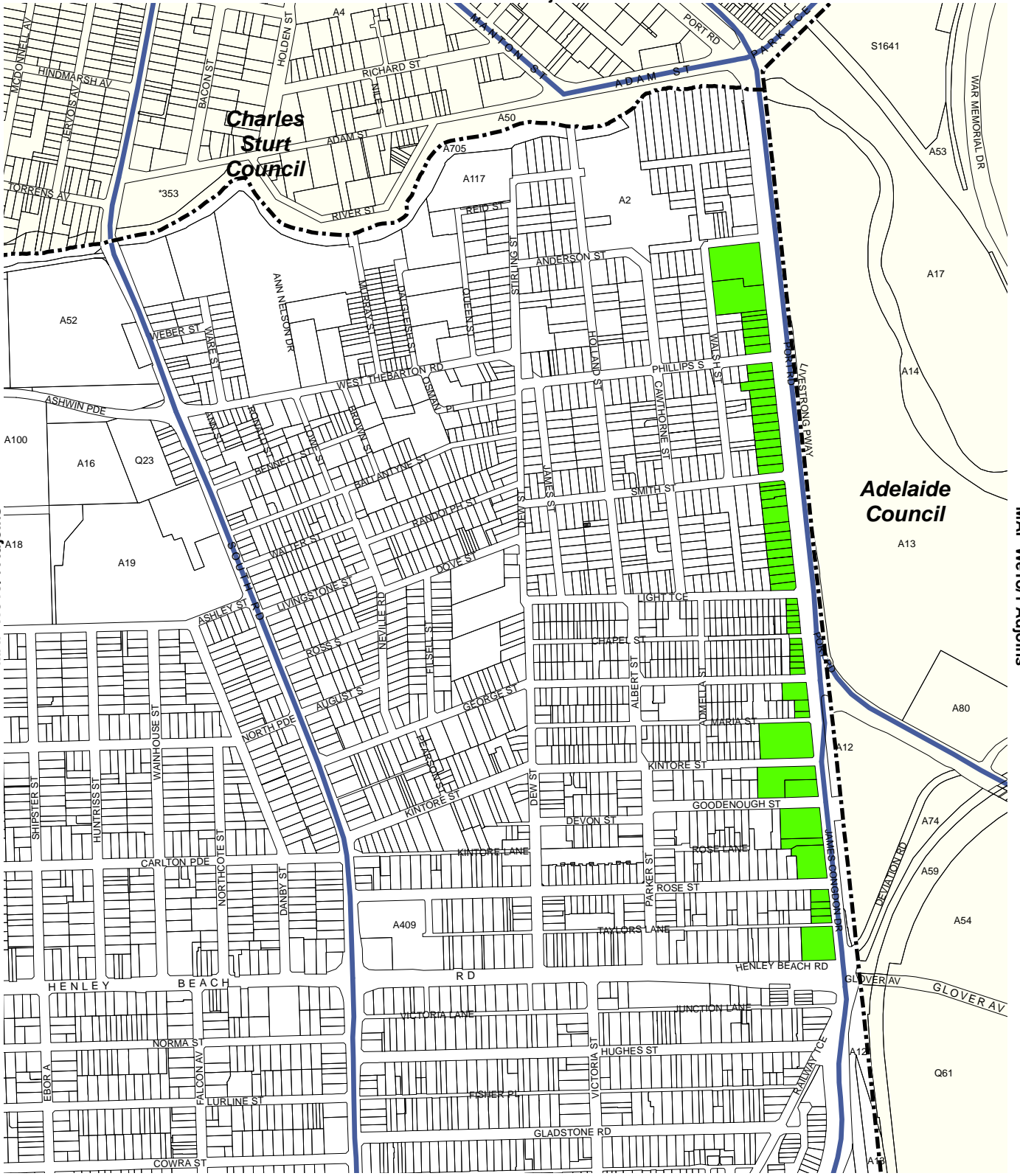
Charles Sturt Council




Adelaide Council

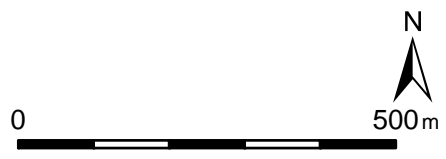
MAP WeTo/1 Adjoins

MAP WeTo/4 Adjoins

MAP WeTo/9 Adjoins



-  Strategic Transport Route
-  Strategic Transport Routes Designated Area
-  Development Plan Boundary



# Overlay Map WeTo/5

## STRATEGIC TRANSPORT ROUTES

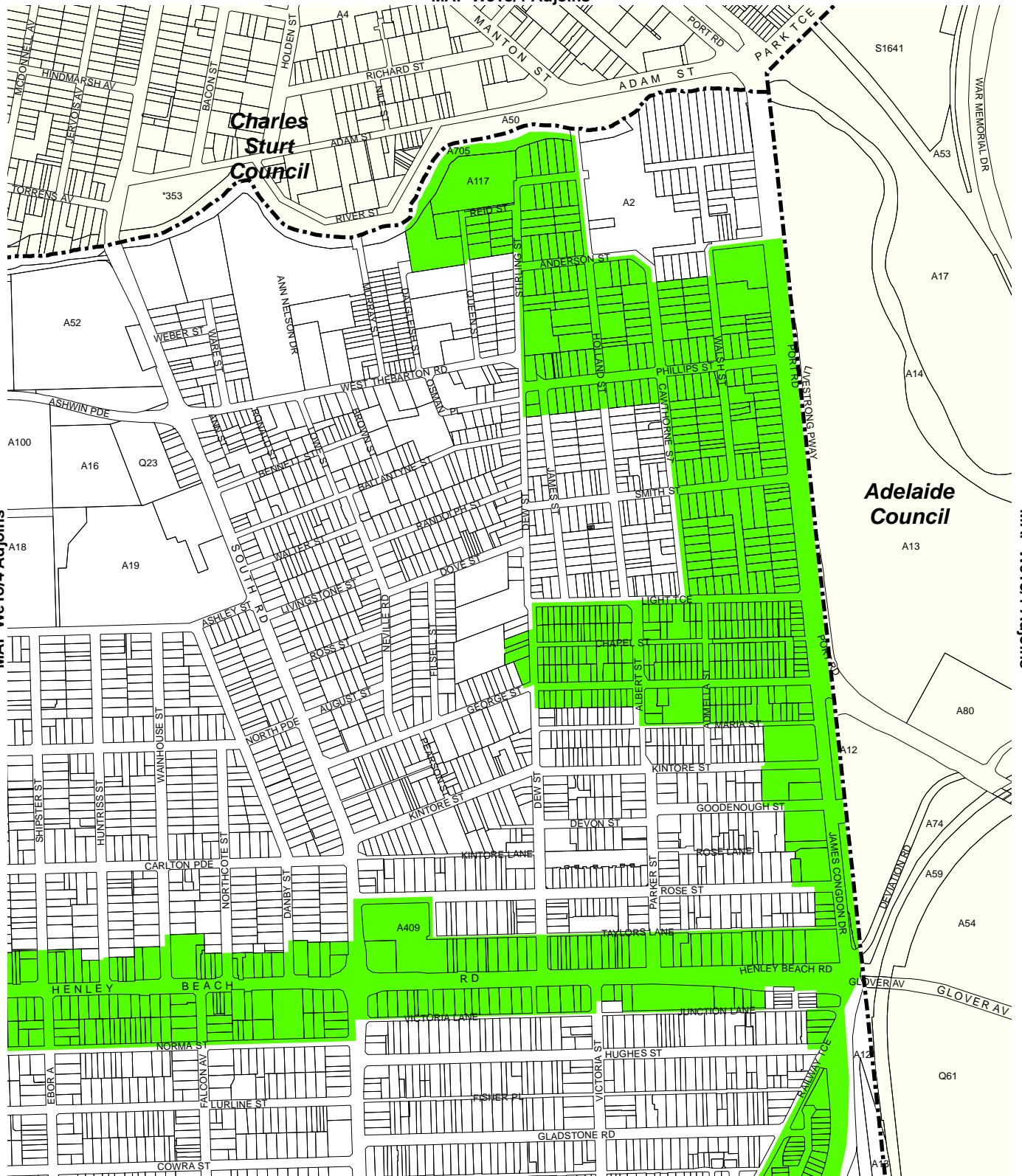
MAP WeTo/1 Adjoins

Charles Sturt Council

Adelaide Council

MAP WeTo/1 Adjoins

MAP WeTo/4 Adjoins



MAP WeTo/9 Adjoins



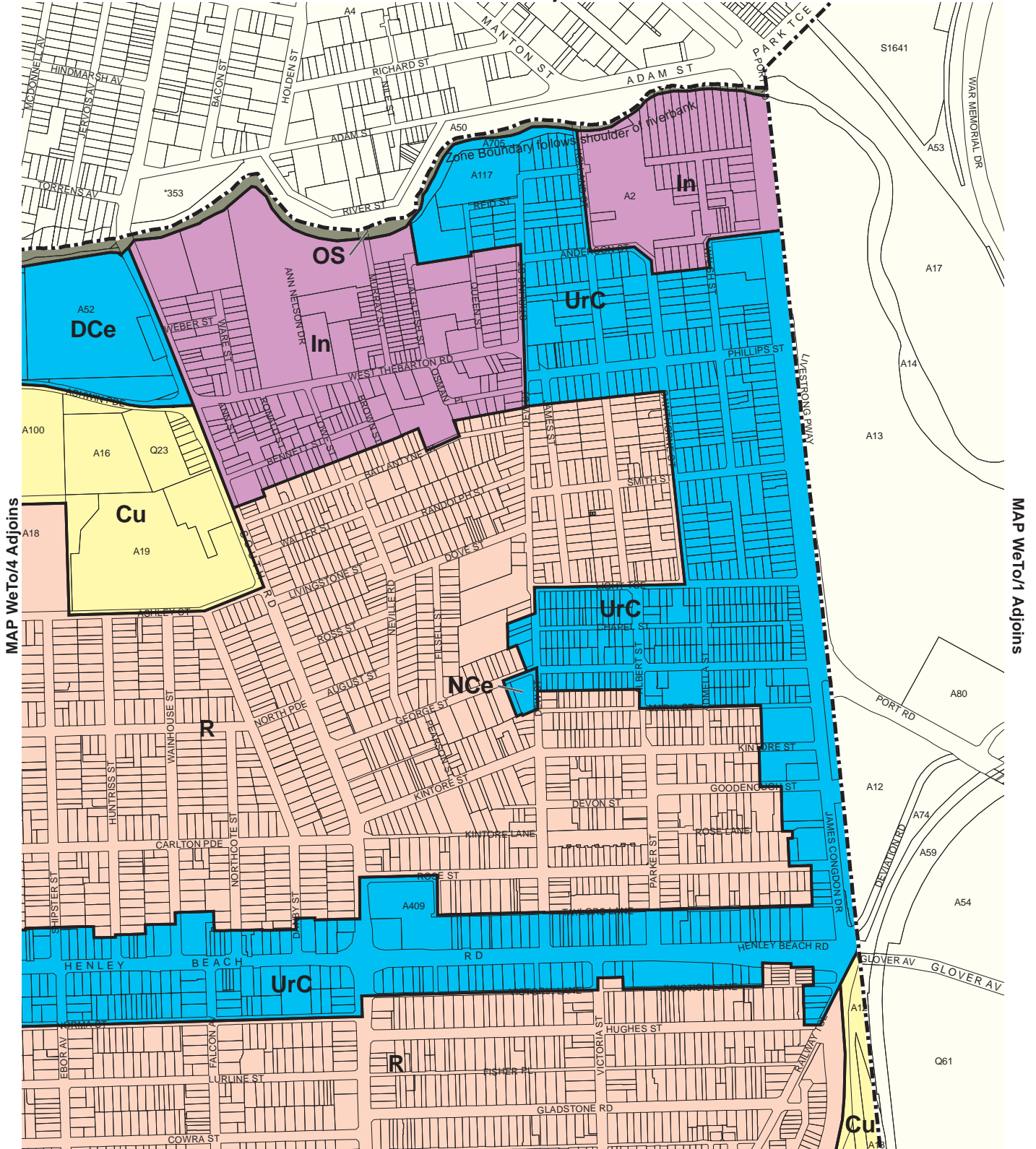
0 500m

# Overlay Map WeTo/5

## AFFORDABLE HOUSING

- Affordable Housing Designated Area
- Development Plan Boundary

MAP WeTo/1 Adjoins



MAP WeTo/9 Adjoins

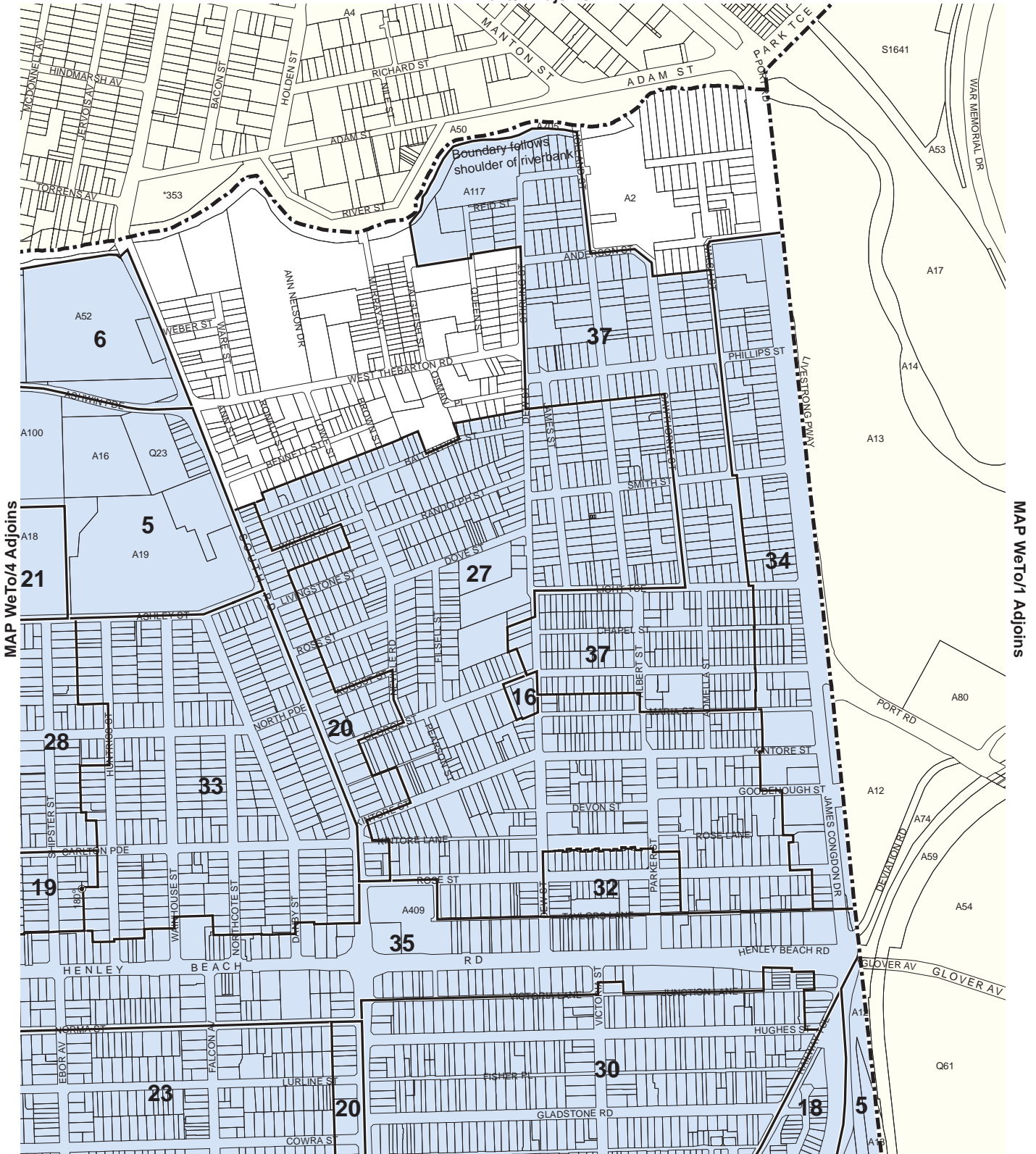
Lamberts Conformal Conic Projection, GDA94

- Zones**
- Cu** Community
  - DCe** District Centre
  - In** Industry
  - NCe** Neighbourhood Centre
  - OS** Open Space
  - R** Residential
  - UrC** Urban Corridor
  - Zone Boundary**
  - Development Plan Boundary**



# Zone Map WeTo/5

MAP WeTo/1 Adjoins



MAP WeTo/9 Adjoins

Lamberts Conformal Conic Projection, GDA94

**Policy Area**

- 16 Thebarton
- 18 Medium Density
- 19 Medium Density
- 20 Low Density
- 21 Low Density
- 23 Cowandilla / Mile End West Character
- 27 Thebarton Character
- 28 Torrensville Character
- 30 Mile End Conservation
- 32 Rose Street Conservation
- 33 Torrensville East Conservation
- 34 Boulevard
- 35 High Street
- 37 Business

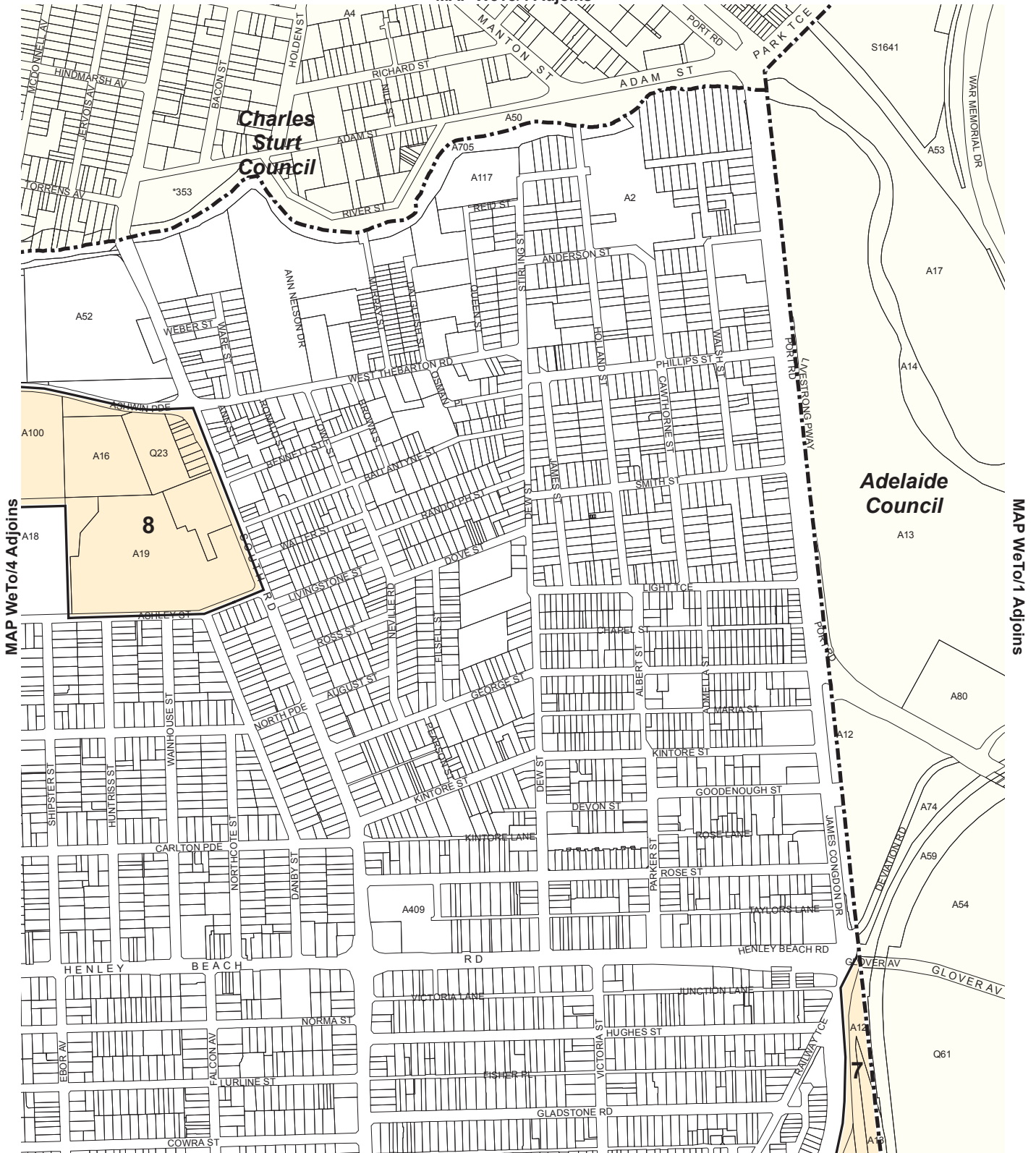
- 5 Recreation
- 6 Brickworks

- Policy Area Boundary
- Development Plan Boundary



# Policy Area Map WeTo/5

MAP WeTo/1 Adjoins



Charles Sturt Council

Adelaide Council

MAP WeTo/4 Adjoins

MAP WeTo/1 Adjoins

MAP WeTo/9 Adjoins

Lamberts Conformal Conic Projection, GDA94

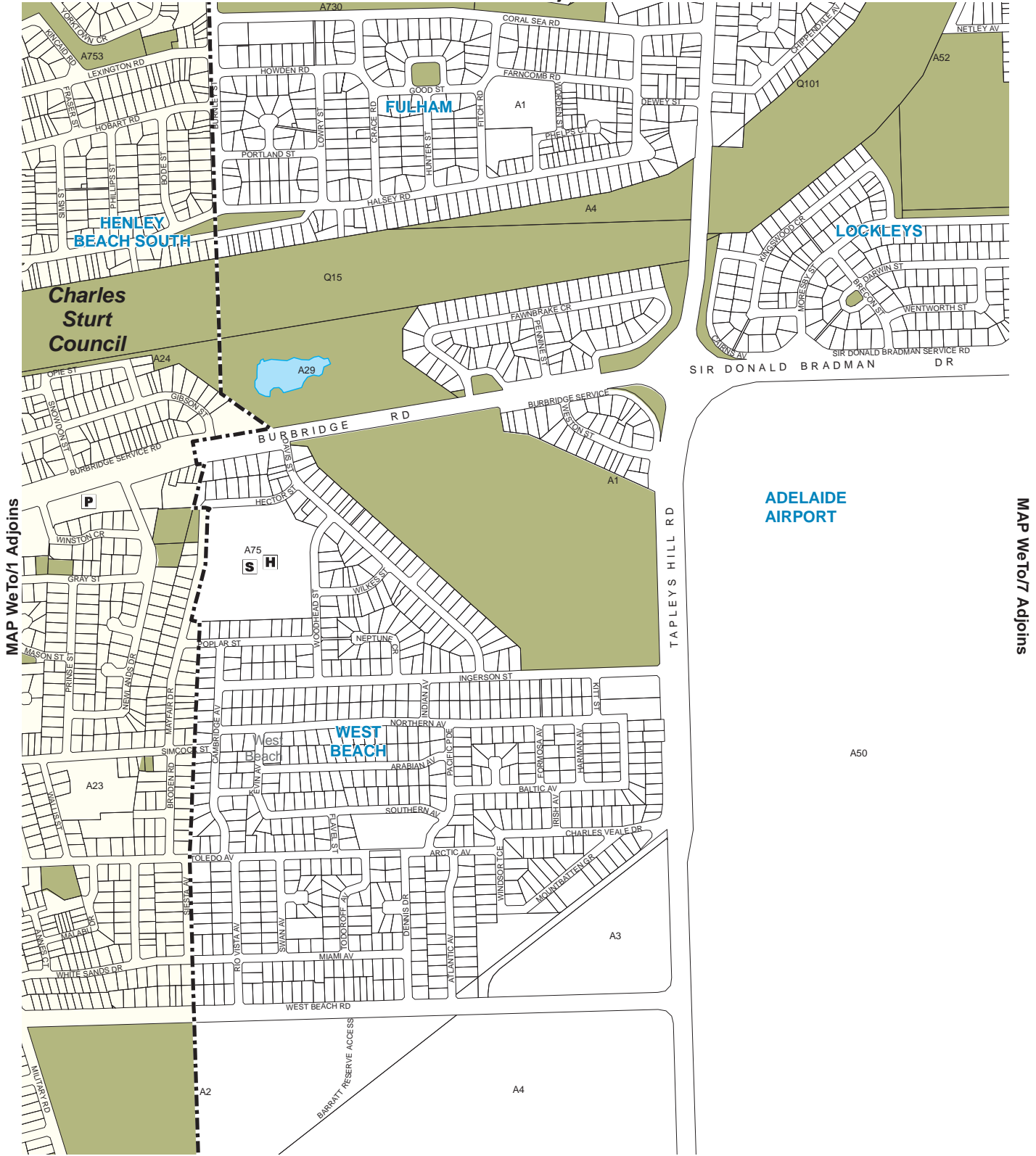
- Precinct
- 7 Mile End
- 8 Open Space



# Precinct Map WeTo/5

- Precinct Boundary
- Development Plan Boundary

MAP WeTo/2 Adjoins



MAP WeTo/1 Adjoins

MAP WeTo/7 Adjoins

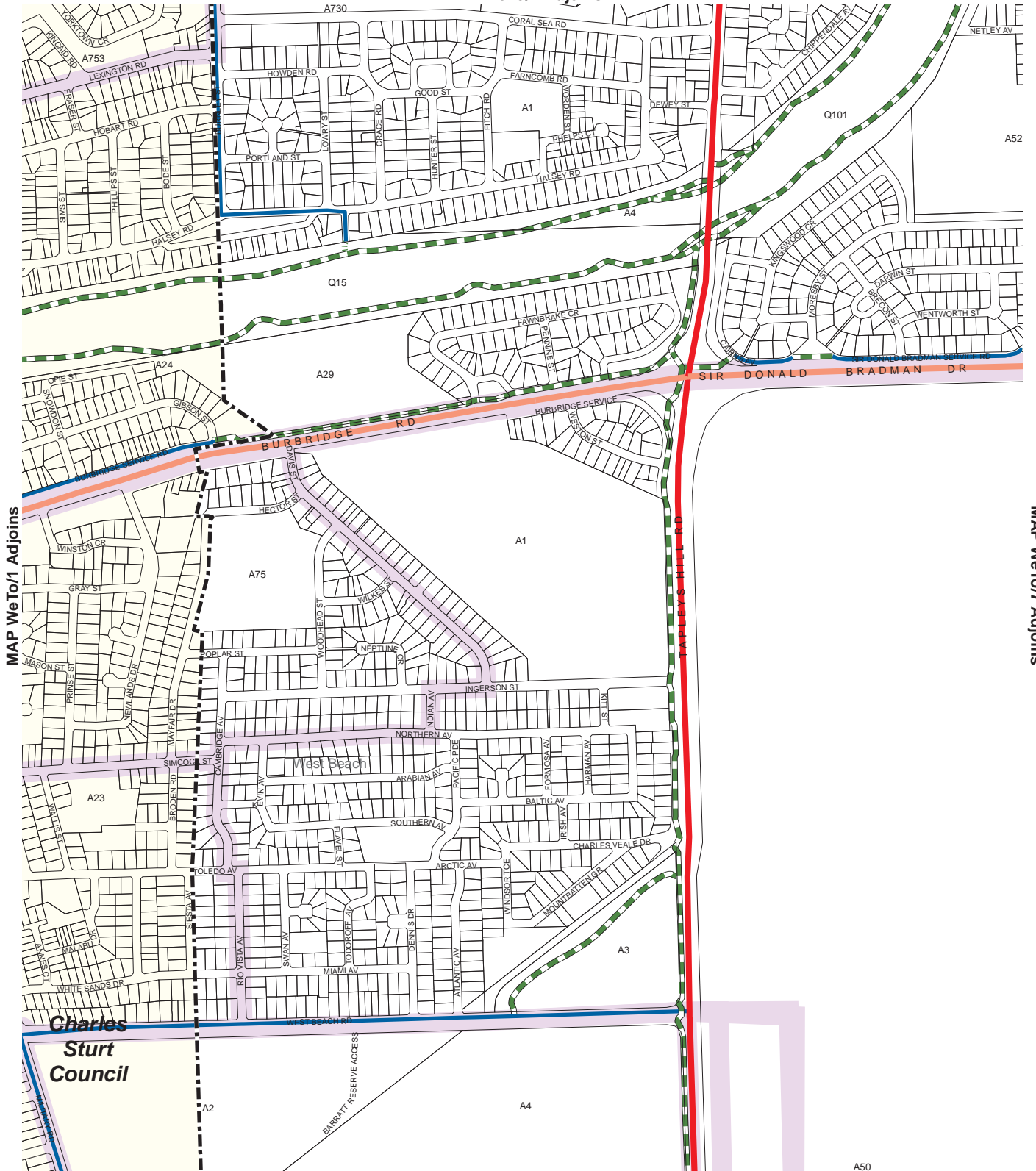
MAP WeTo/10 Adjoins



- S** School
- P** Post Office
- H** Other Health Services
- Local Reserves
- Waterbodies
- Development Plan Boundary

# Location Map WeTo/6

MAP WeTo/2 Adjoins



MAP WeTo/1 Adjoins

MAP WeTo/7 Adjoins

MAP WeTo/10 Adjoins

- Primary Arterial Roads
- Secondary Arterial Roads
- Bikedirect Network**
- Main Road - Bike Lane
- Main Road
- Secondary Road
- - - Off Road Sealed Path
- | | | | | Off Road Track
- Public Transport
- Development Plan Boundary

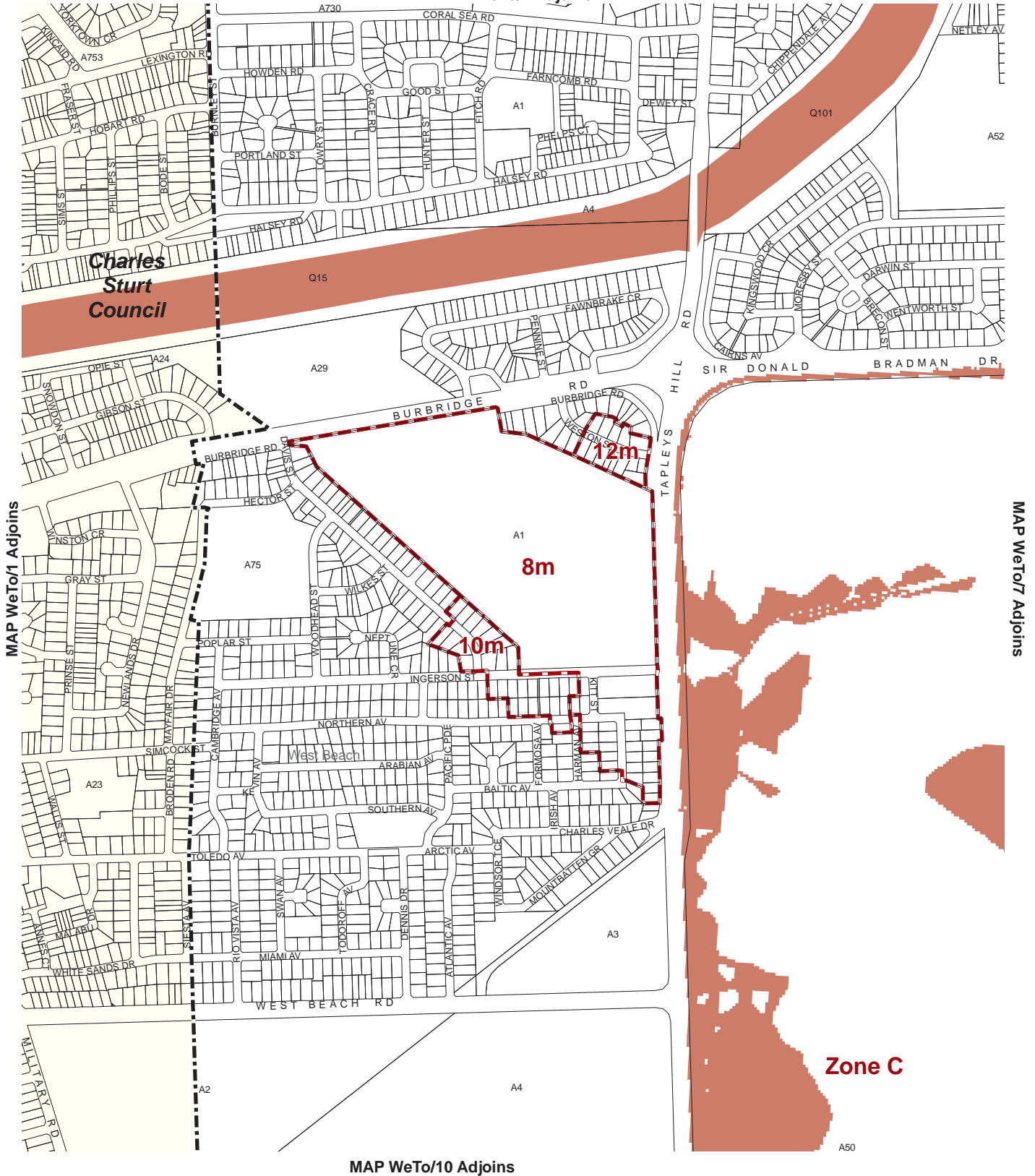


# Overlay Map WeTo/6

## TRANSPORT



MAP WeTo/2 Adjoins



**Airport Building Heights**  
**4m - 14m** All Structures restricted to height identified on maps  
 (above existing ground level, measured from the top of the nearest roadside curb)  
**Zone C** All Structures Exceeding 15 metres above existing ground level



- Airport Building Heights
- Flood Hazard
- Development Plan Boundary

# Overlay Map WeTo/6 DEVELOPMENT CONSTRAINTS

MAP WeTo/2 Adjoins



MAP WeTo/10 Adjoins

MAP WeTo/1 Adjoins

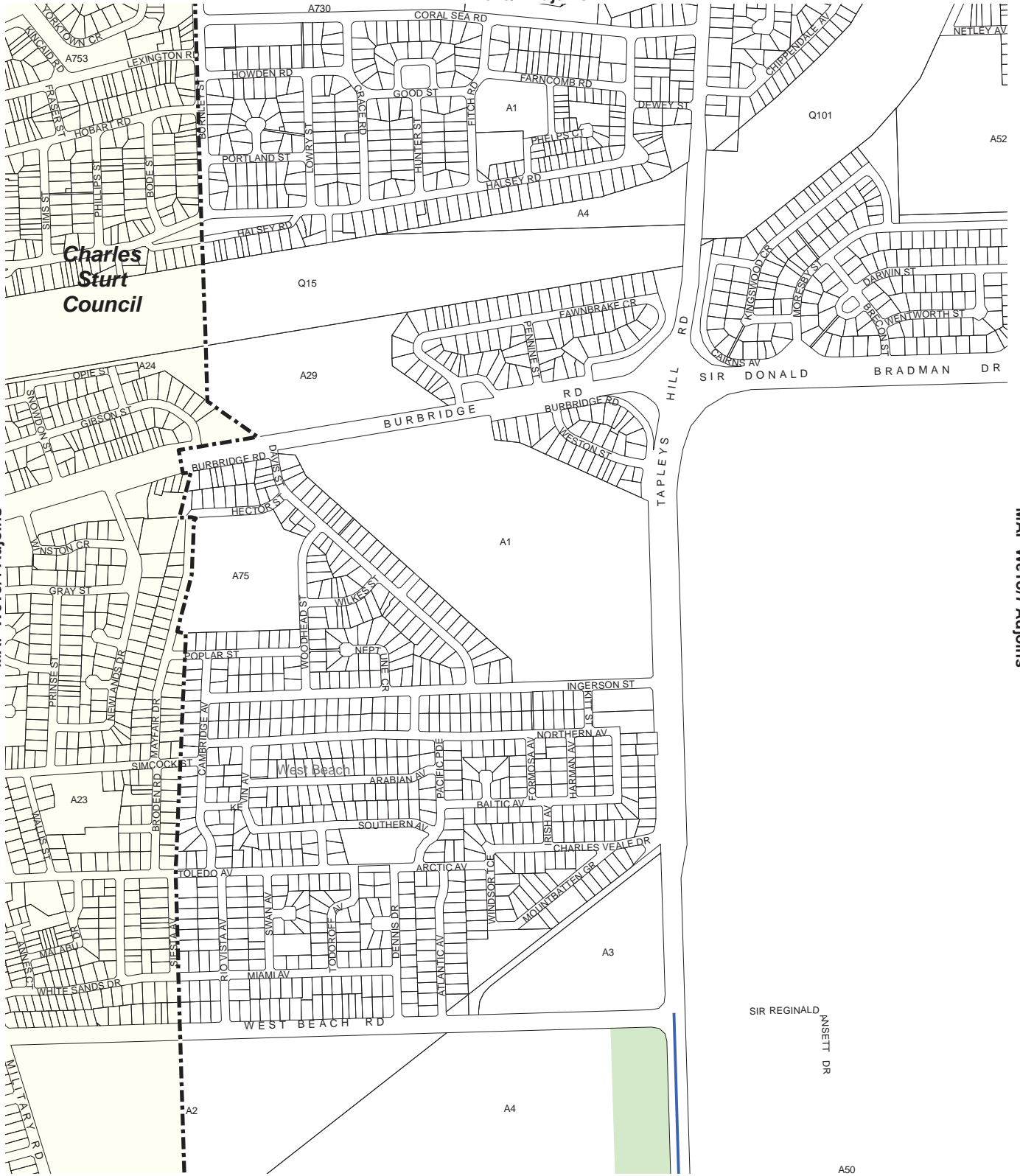
MAP WeTo/7 Adjoins



# Overlay Map WeTo/6 NATURAL RESOURCES

- MOSS Study Area
- Development Plan Boundary

MAP WeTo/2 Adjoins



MAP WeTo/1 Adjoins

MAP WeTo/7 Adjoins

MAP WeTo/10 Adjoins

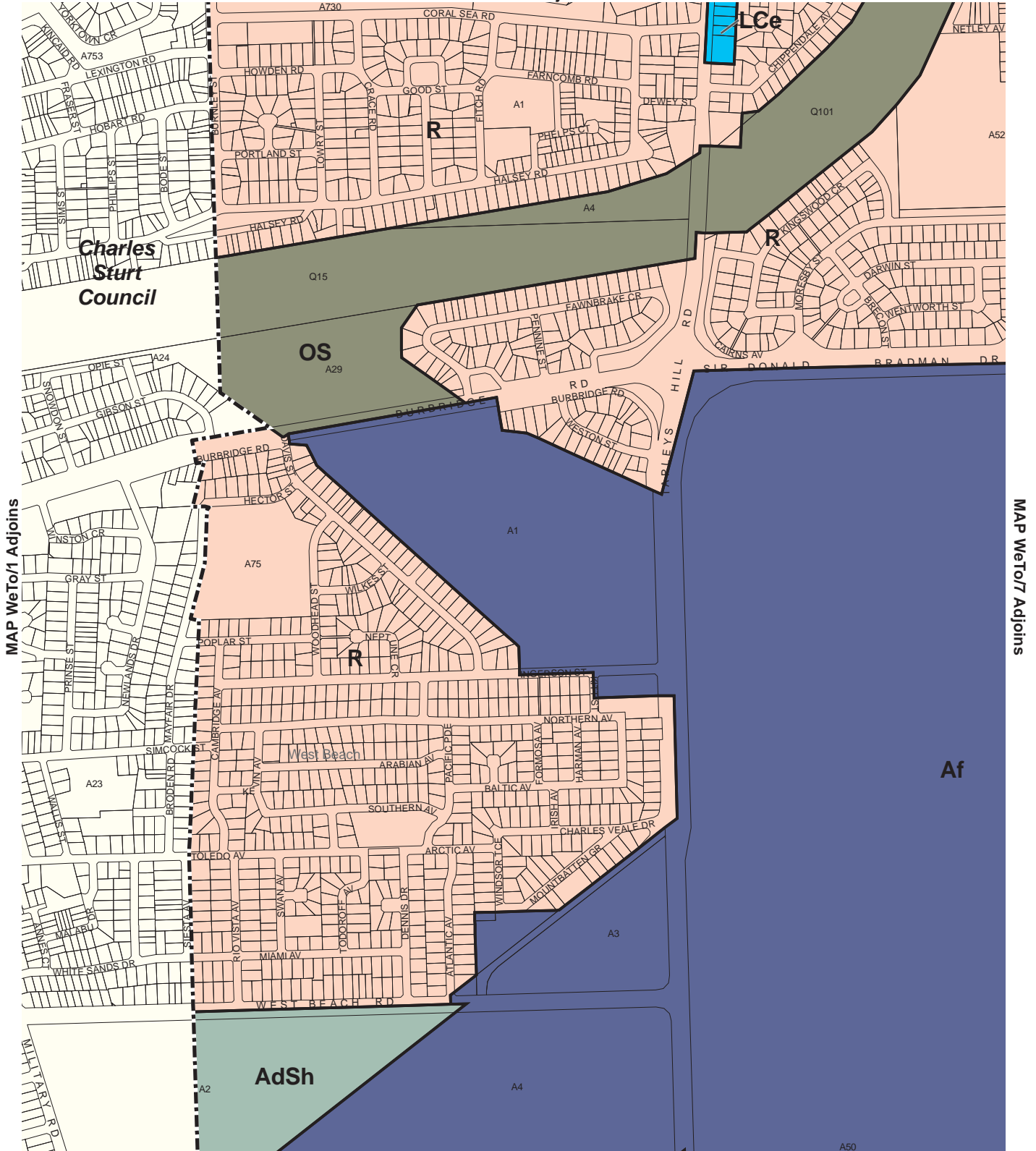


# Overlay Map WeTo/6

## STRATEGIC TRANSPORT ROUTES

- Strategic Transport Route
- Designated Area
- Development Plan Boundary

MAP WeTo/2 Adjoins



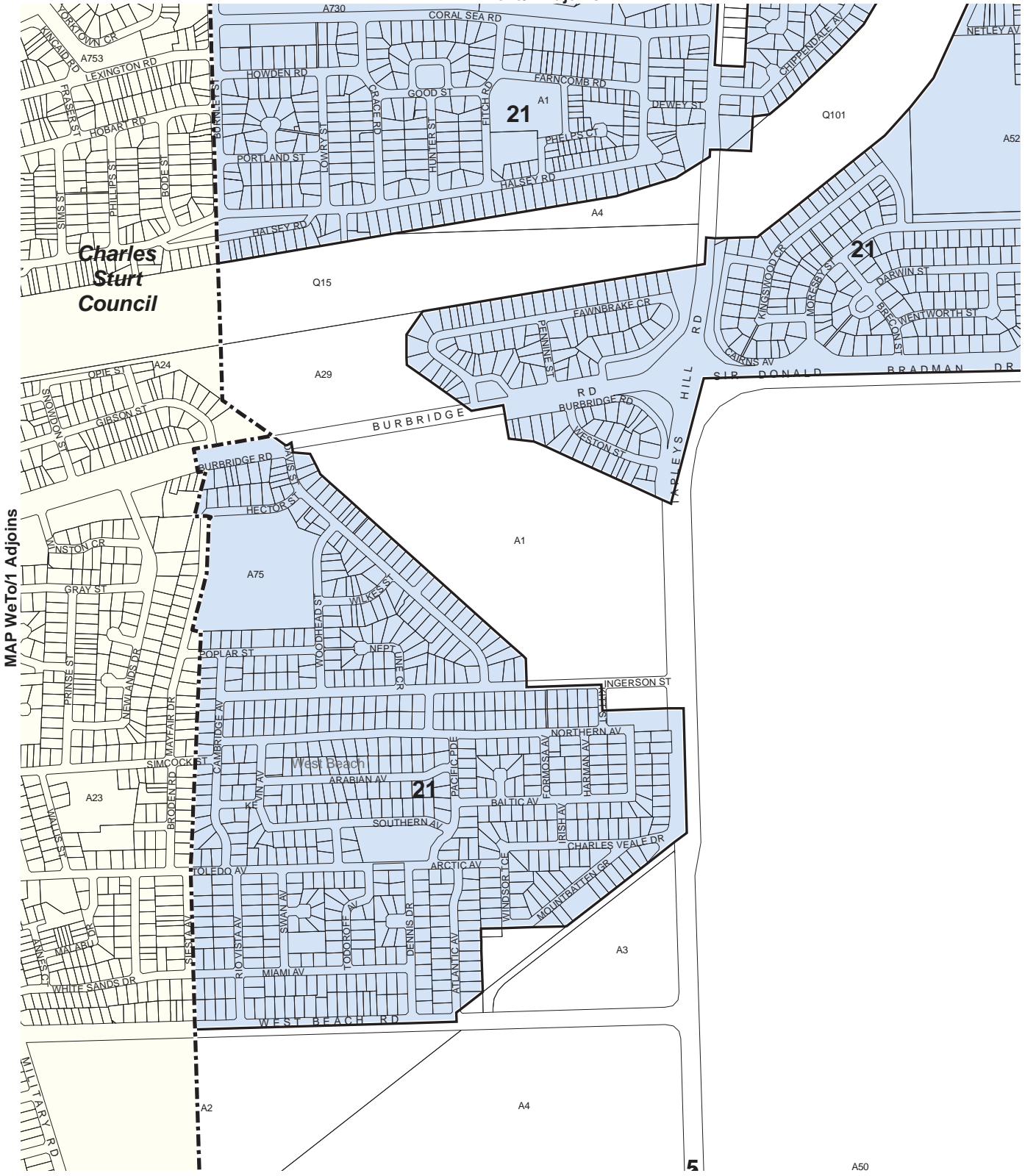
Lamberts Conformal Conic Projection, GDA94



- Zones**
- AdSh Adelaide Shores
  - Af Airfield
  - LCe Local Centre
  - OS Open Space
  - R Residential
  - Zone Boundary
  - Development Plan Boundary

# Zone Map WeTo/6

MAP WeTo/2 Adjoins



Charles Sturt Council

MAP WeTo/1 Adjoins

MAP WeTo/7 Adjoins

MAP WeTo/10 Adjoins

Lamberts Conformal Conic Projection, GDA94

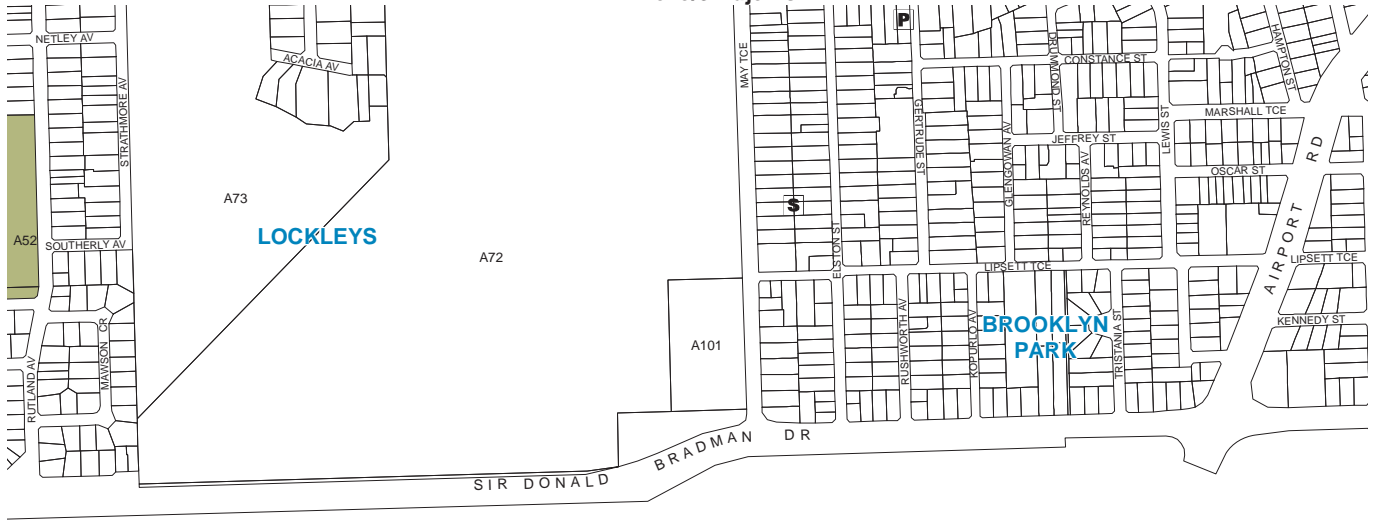
Policy Area  
21 Low Density



# Policy Area Map WeTo/6

- Policy Area Boundary
- Development Plan Boundary

MAP WeTo/3 Adjoins



MAP WeTo/6 Adjoins

MAP WeTo/8 Adjoins

ADELAIDE AIRPORT

A50



MAP WeTo/11 Adjoins



# Location Map WeTo/7

- School
- Post Office
- Local Reserves

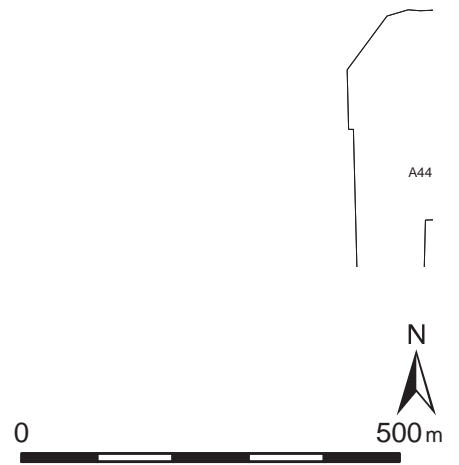
MAP WeTo/3 Adjoins



MAP WeTo/6 Adjoins

MAP WeTo/8 Adjoins

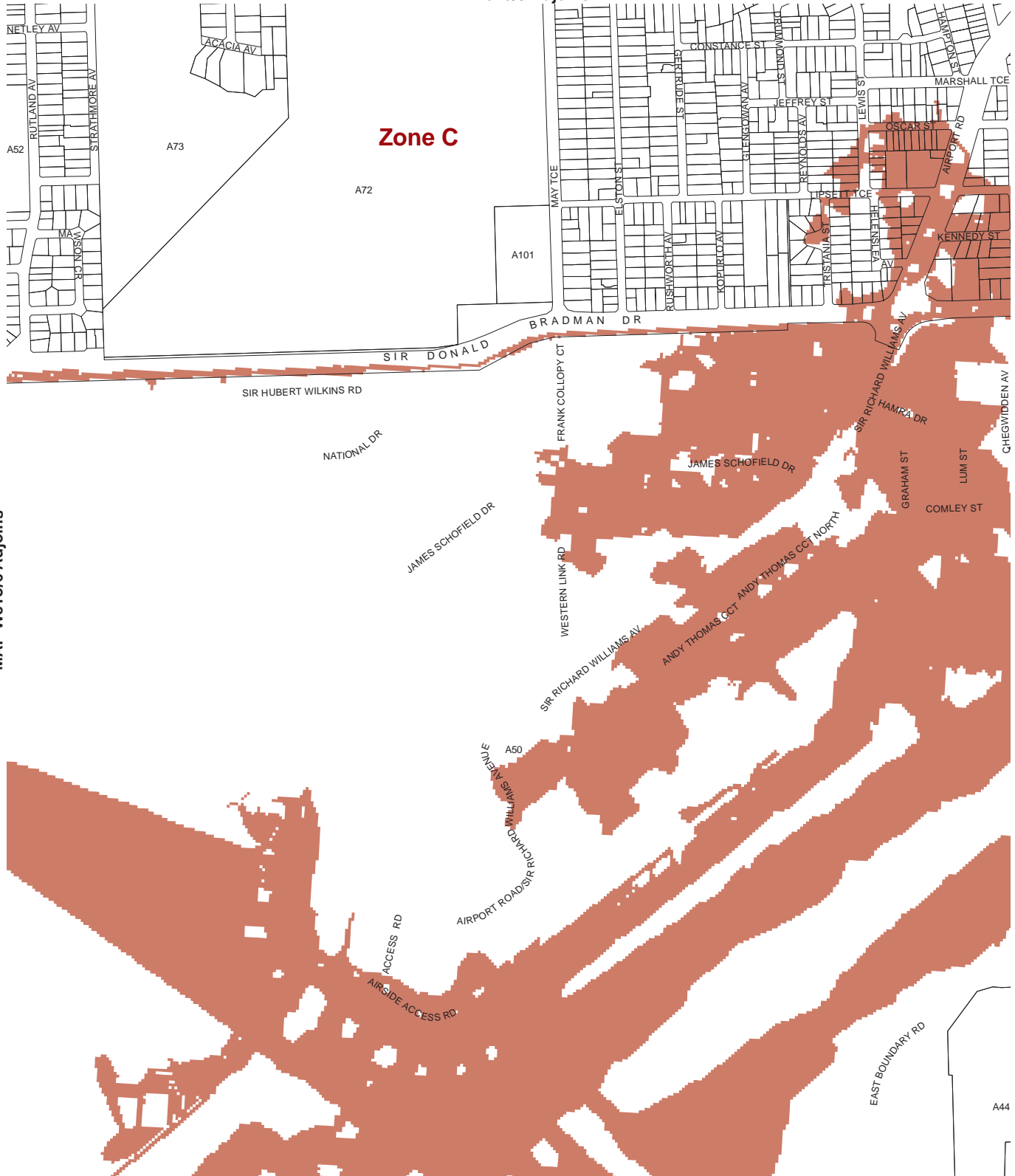
MAP WeTo/11 Adjoins



- Secondary Arterial Roads
- Bikedirect Network
- Main Road - Bike Lane
- Main Road
- Secondary Road - Bike Lane
- Secondary Road
- Off Road Sealed Path
- Public Transport

# Overlay Map WeTo/7 TRANSPORT

MAP WeTo/3 Adjoins



**Airport Building Heights**  
**Zone C** All Structures Exceeding 15 metres above existing ground level

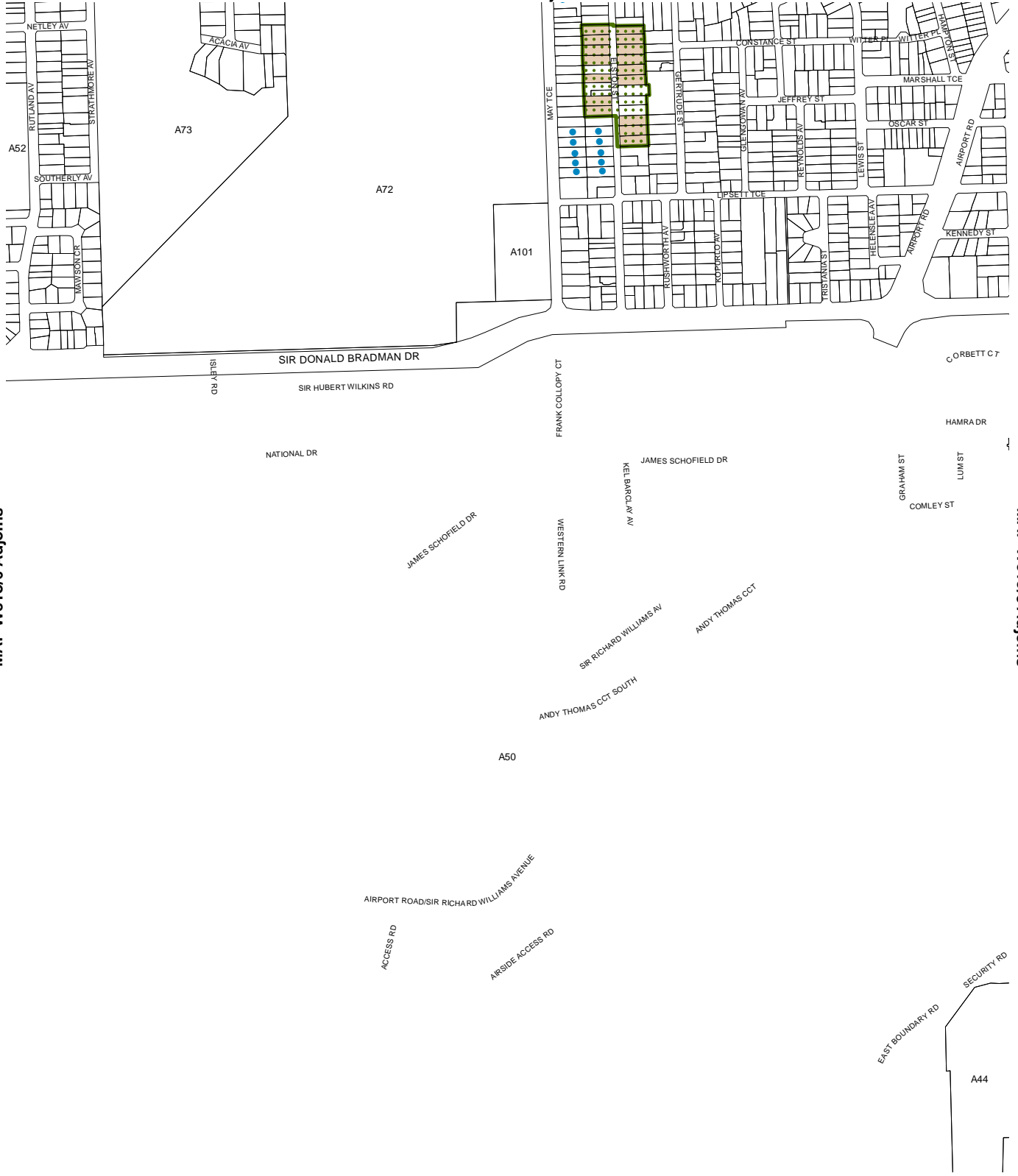


# Overlay Map WeTo/7 DEVELOPMENT CONSTRAINTS

- Airport Building Heights
- Flood Hazard



MAP WeTo/3 Adjoins



MAP WeTo/6 Adjoins

MAP WeTo/8 Adjoins

MAP WeTo/11 Adjoins

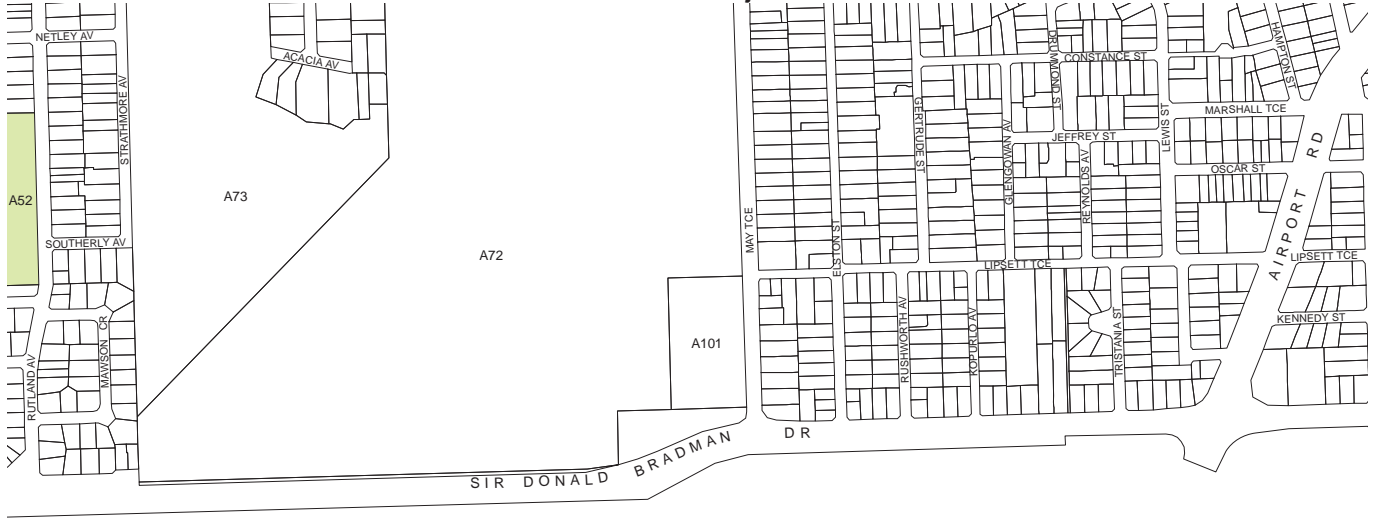
Heritage points are indicative only. For further information on State and Local Heritage Places and Contributory Items please refer to the relevant tables within this document.



- Local heritage place
- State heritage place
- Contributory Item
- Historic Conservation Area
- Development Plan Boundary

# Overlay Map WeTo/7 HERITAGE

MAP WeTo/3 Adjoins

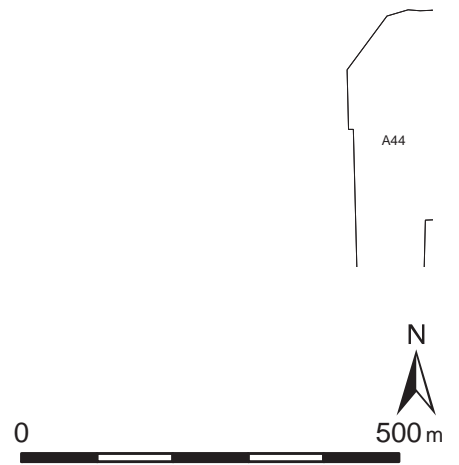


MAP WeTo/6 Adjoins

MAP WeTo/8 Adjoins

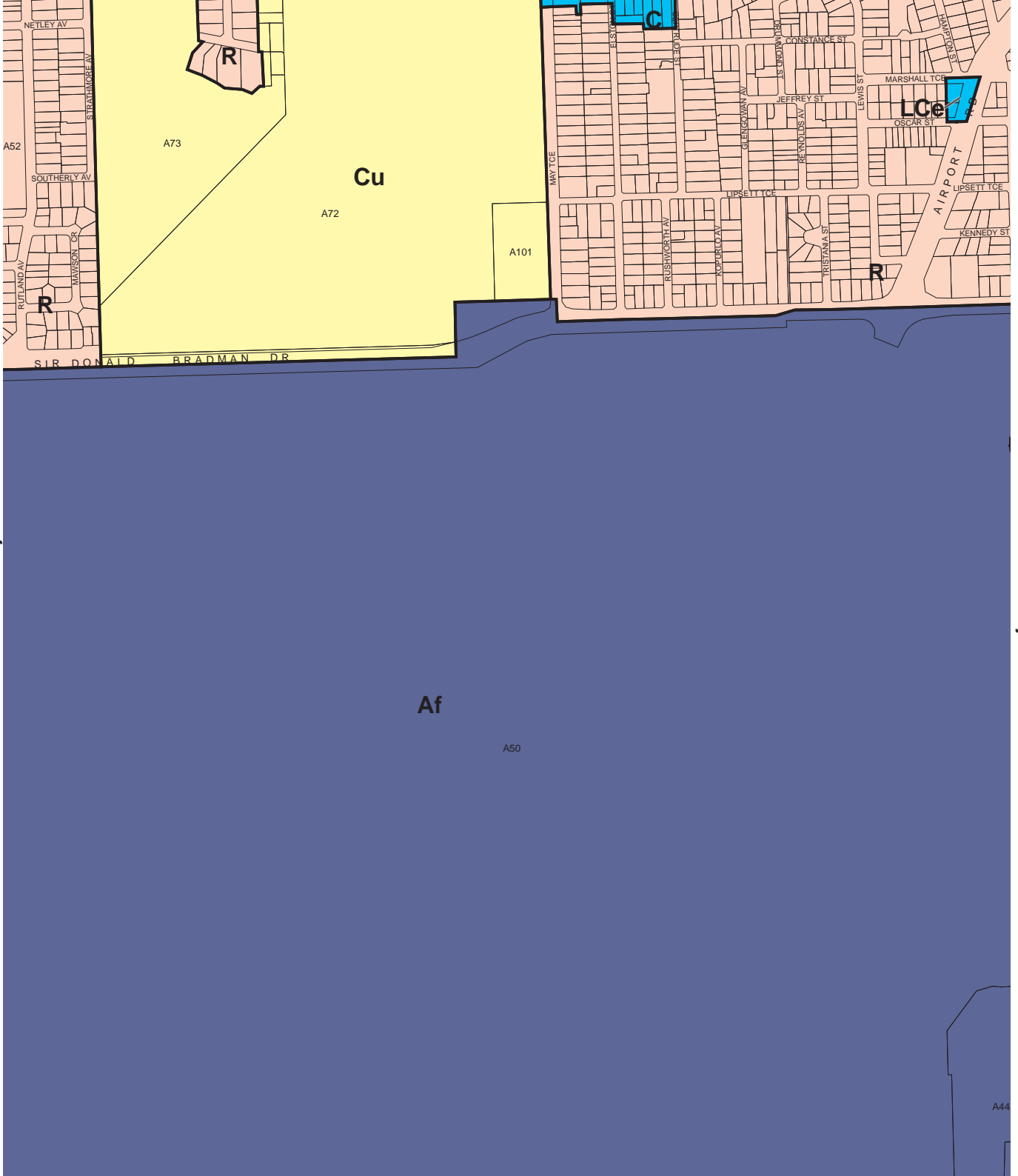
A50

MAP WeTo/11 Adjoins



# Overlay Map WeTo/7 NATURAL RESOURCES

MAP WeTo/3 Adjoins



MAP WeTo/6 Adjoins

MAP WeTo/8 Adjoins

MAP WeTo/11 Adjoins

Lamberts Conformal Conic Projection, GDA94

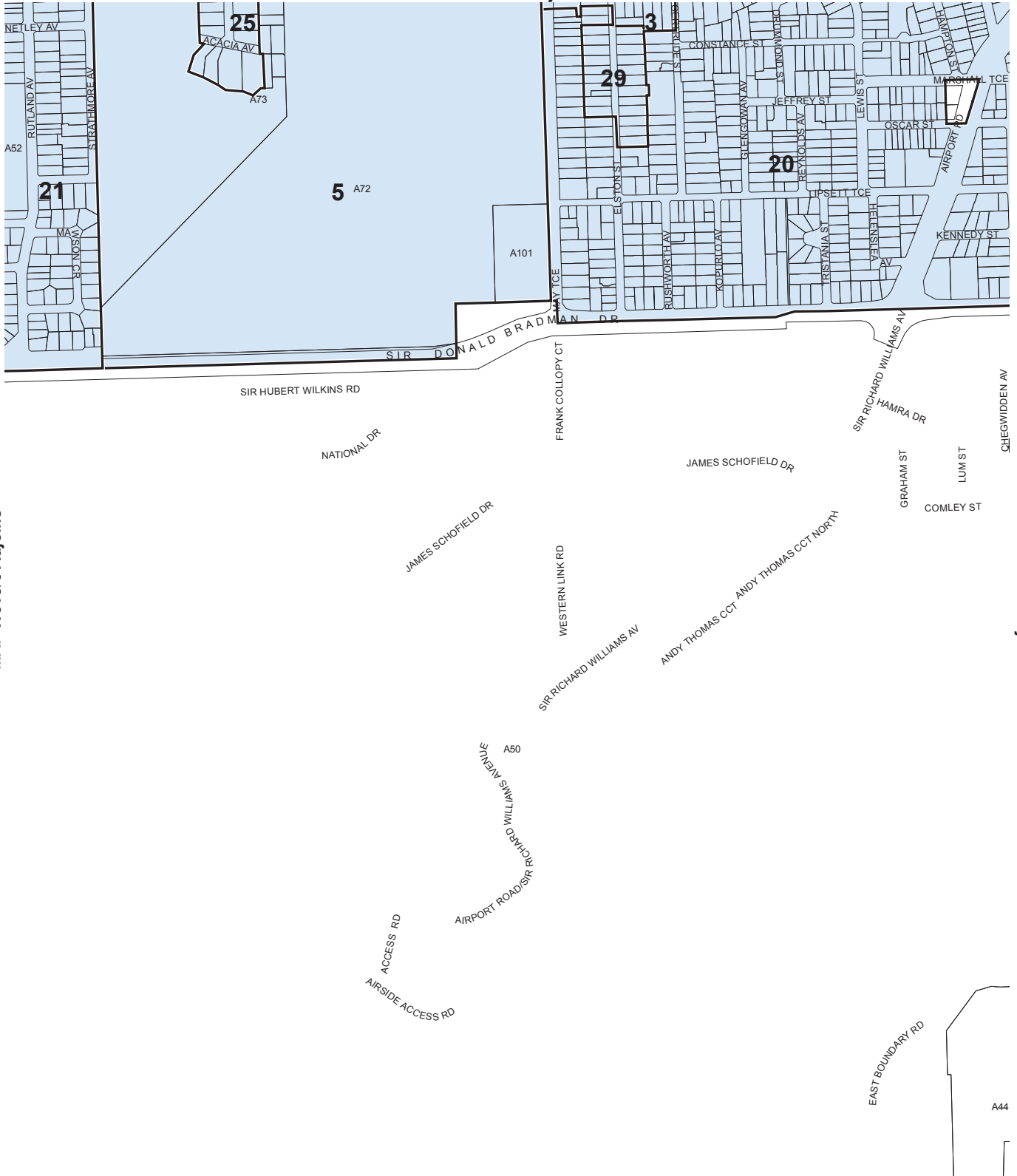


Zones

- Af Airfield
- C Commercial
- Cu Community
- LCe Local Centre
- R Residential
- Zone Boundary

# Zone Map WeTo/7

MAP WeTo/3 Adjoins



MAP WeTo/6 Adjoins

MAP WeTo/8 Adjoins

MAP WeTo/11 Adjoins

Lamberts Conformal Conic Projection, GDA94

Policy Area

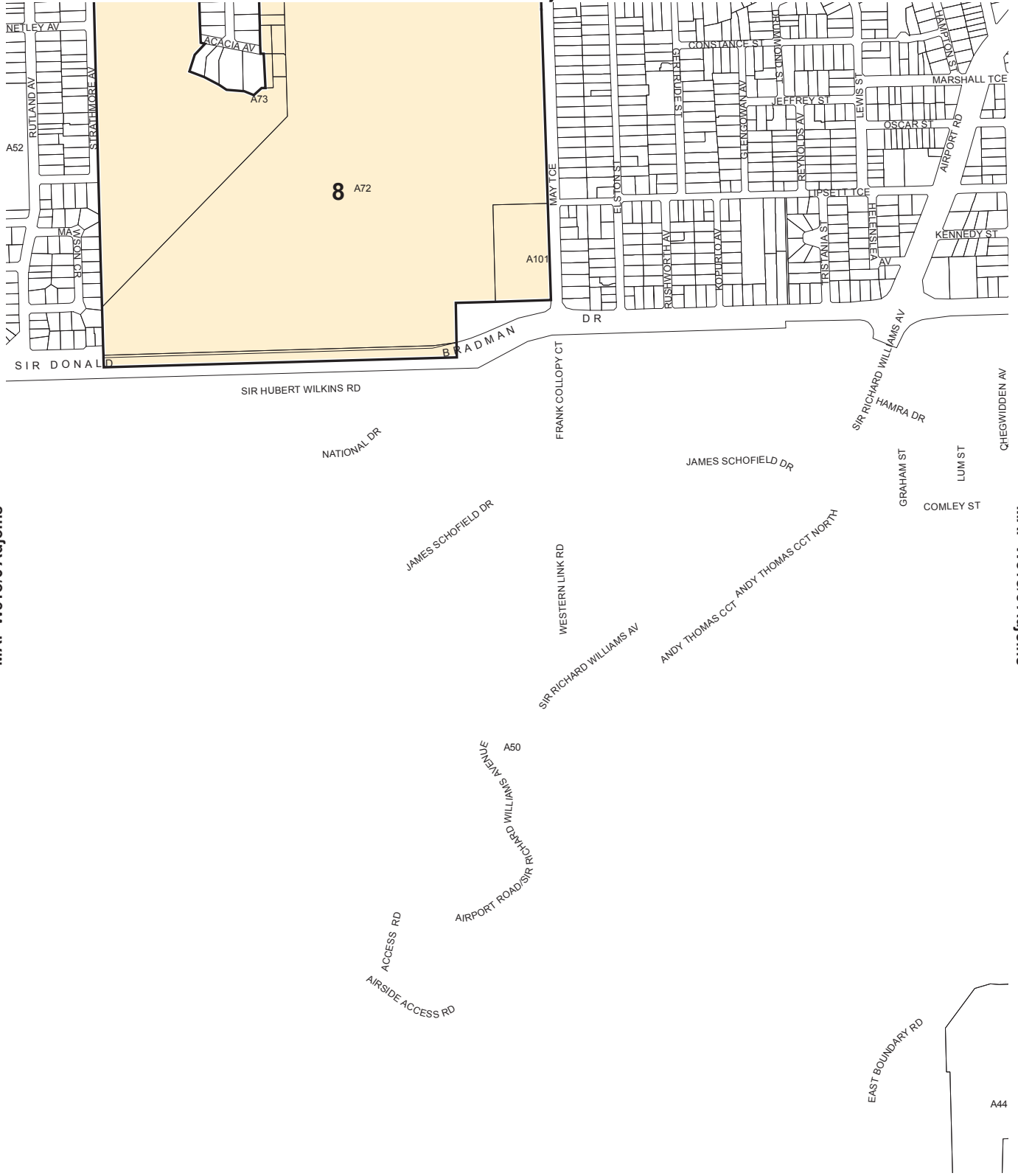
- 20 Low Density
- 21 Low Density
- 25 Lockleys Character
- 29 Elston Street Conservation
- 3 Local Commercial
- 5 Recreation



# Policy Area Map WeTo/7

 Policy Area Boundary

MAP WeTo/3 Adjoins



MAP WeTo/6 Adjoins

MAP WeTo/8 Adjoins

MAP WeTo/11 Adjoins

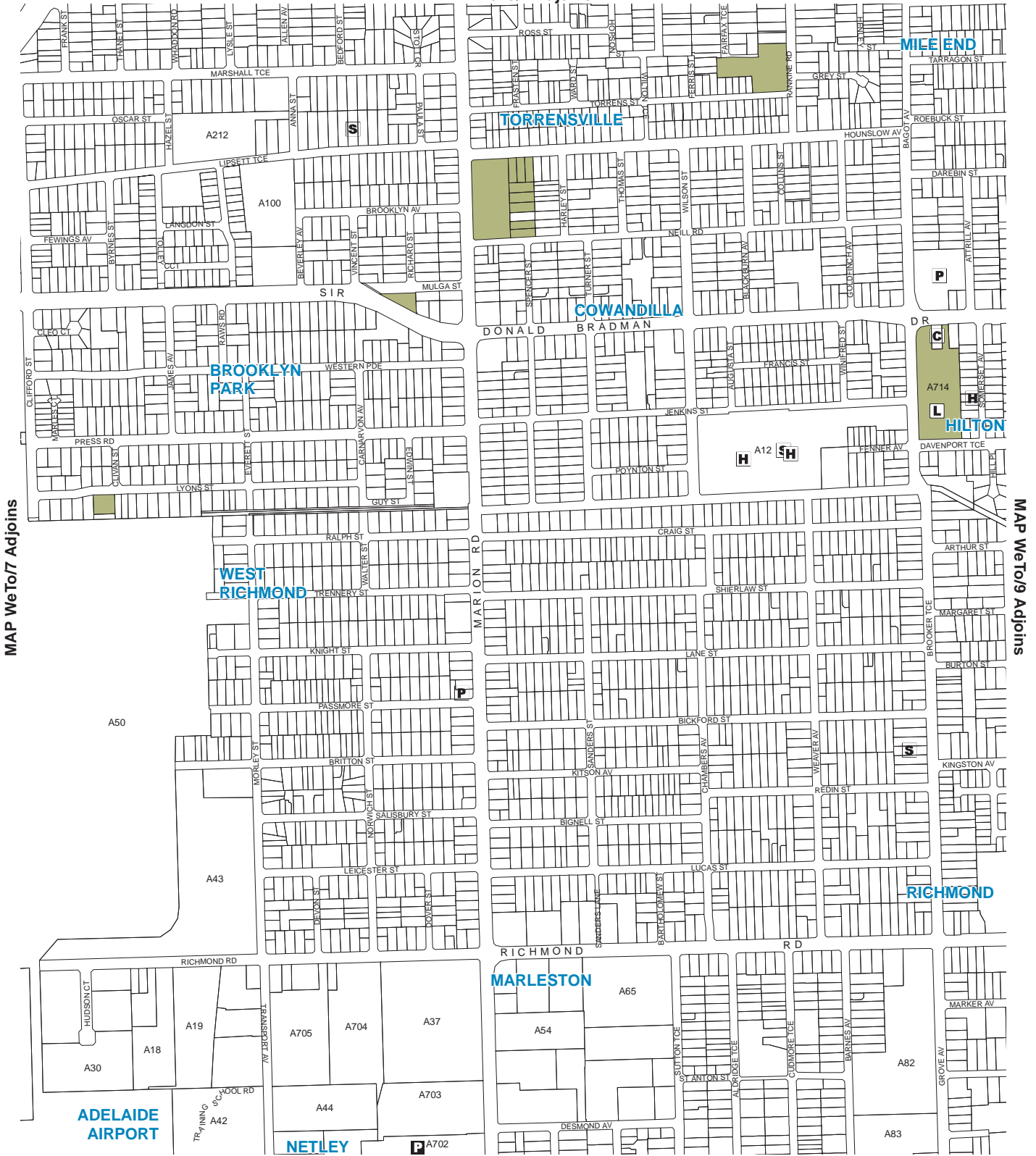
Lamberts Conformal Conic Projection, GDA94

**Precinct**  
**8** Open Space

 Precinct Boundary

# Precinct Map WeTo/7








MAP WeTo/4 Adjoins



MAP WeTo/7 Adjoins

MAP WeTo/9 Adjoins

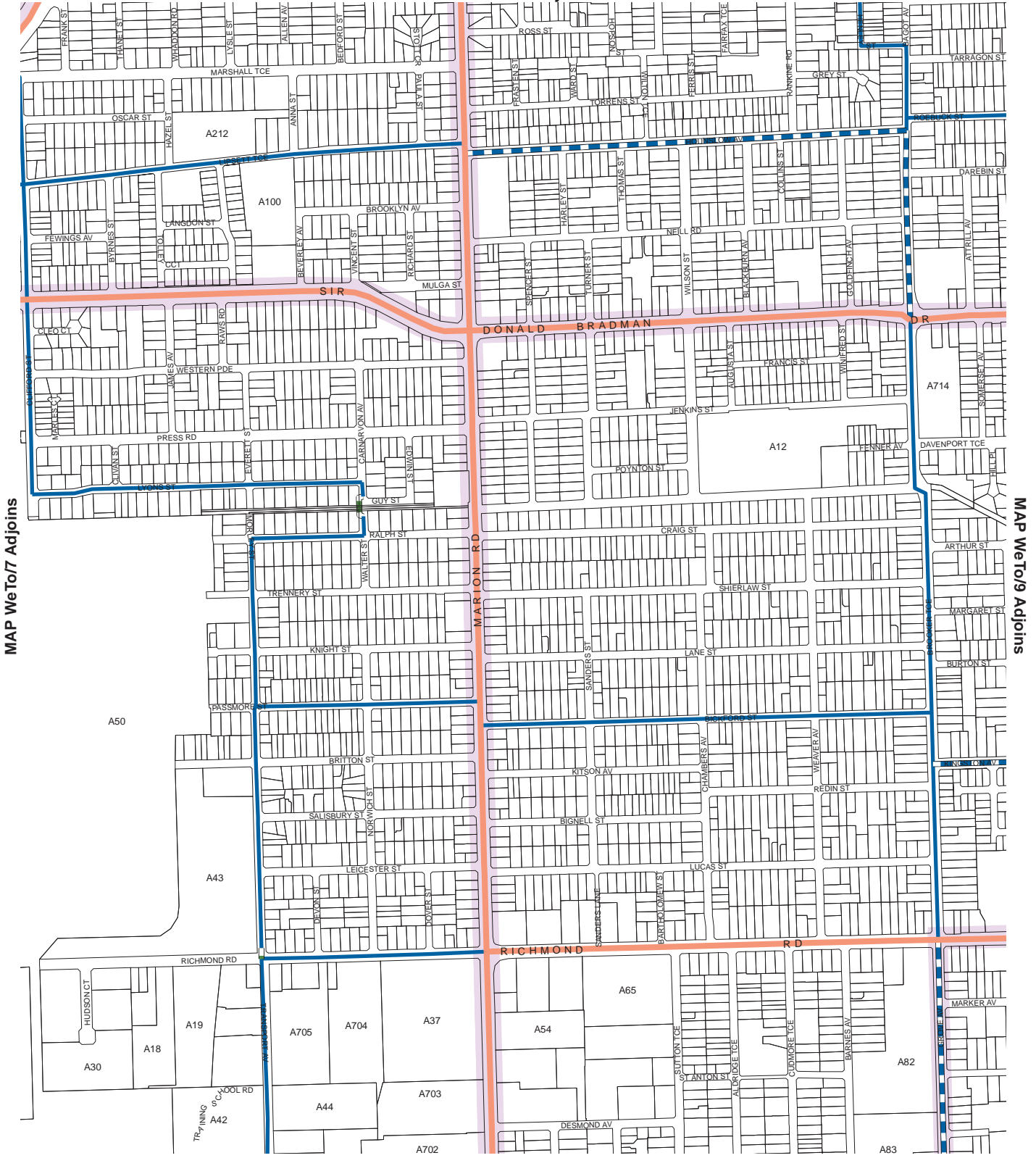
MAP WeTo/12 Adjoins

-  School
-  Public Library
-  Council Office
-  Post Office
-  Other Health Services
-  Police Station
-  Local Reserves



# Location Map WeTo/8









MAP WeTo/4 Adjoins



MAP WeTo/7 Adjoins

MAP WeTo/9 Adjoins

MAP WeTo/12 Adjoins

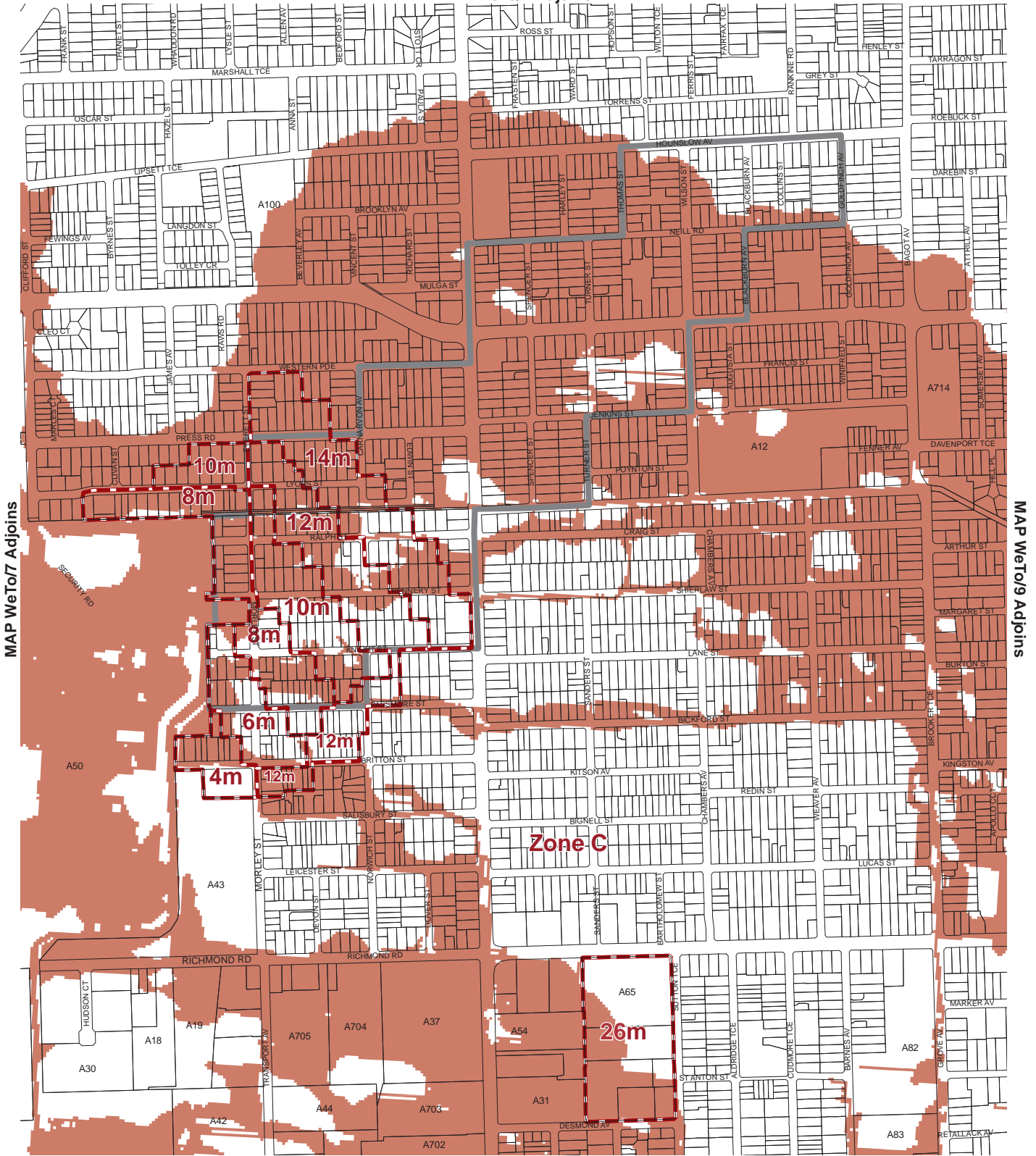
-  Secondary Arterial Roads
-  Bikedirect Network
-  Main Road - Bike Lane
-  Main Road
-  Secondary Road - Bike Lane
-  Secondary Road
-  Off Road Sealed Path
-  Public Transport



# Overlay Map WeTo/8

## TRANSPORT

MAP WeTo/4 Adjoins






MAP WeTo/12 Adjoins

**Airport Building Heights**

**4m - 26m** All Structures restricted to height identified on maps (above existing ground level, measured from the top of the nearest roadside curb)

**Zone C** All Structures Exceeding 15 metres above existing ground level



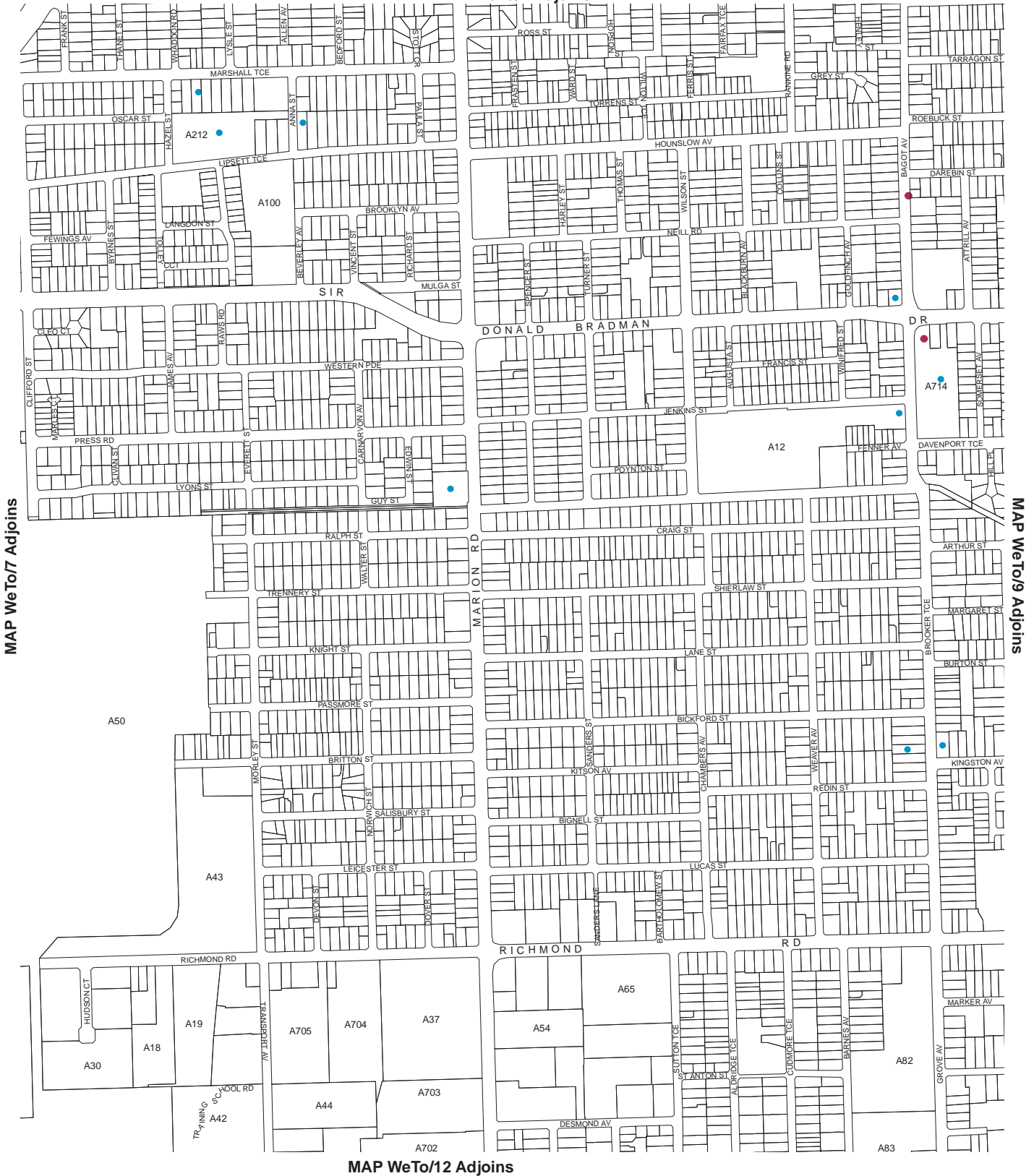
-  Airport Building Heights
-  Areas affected by aircraft noise
-  Flood Hazard

# Overlay Map WeTo/8

## DEVELOPMENT CONSTRAINTS



MAP WeTo/4 Adjoins



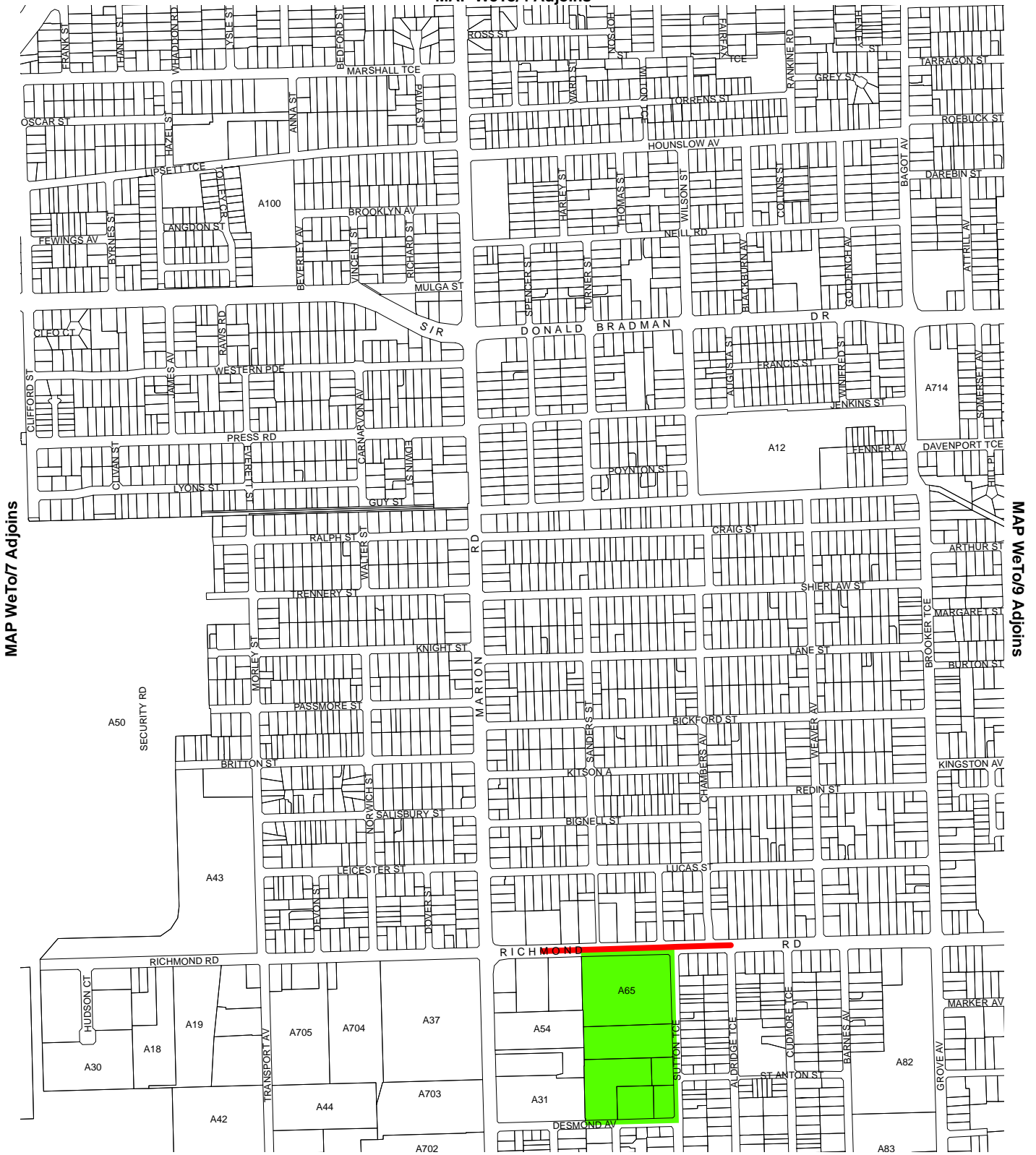
Heritage points are indicative only.  
 For further information on State and Local Heritage Places and Contributory  
 Items please refer to the relevant tables within this document.



# Overlay Map WeTo/8 HERITAGE

- State heritage place
- Local heritage place

MAP WeTo/4 Adjoins



MAP WeTo/7 Adjoins

MAP WeTo/9 Adjoins

MAP WeTo/12 Adjoins

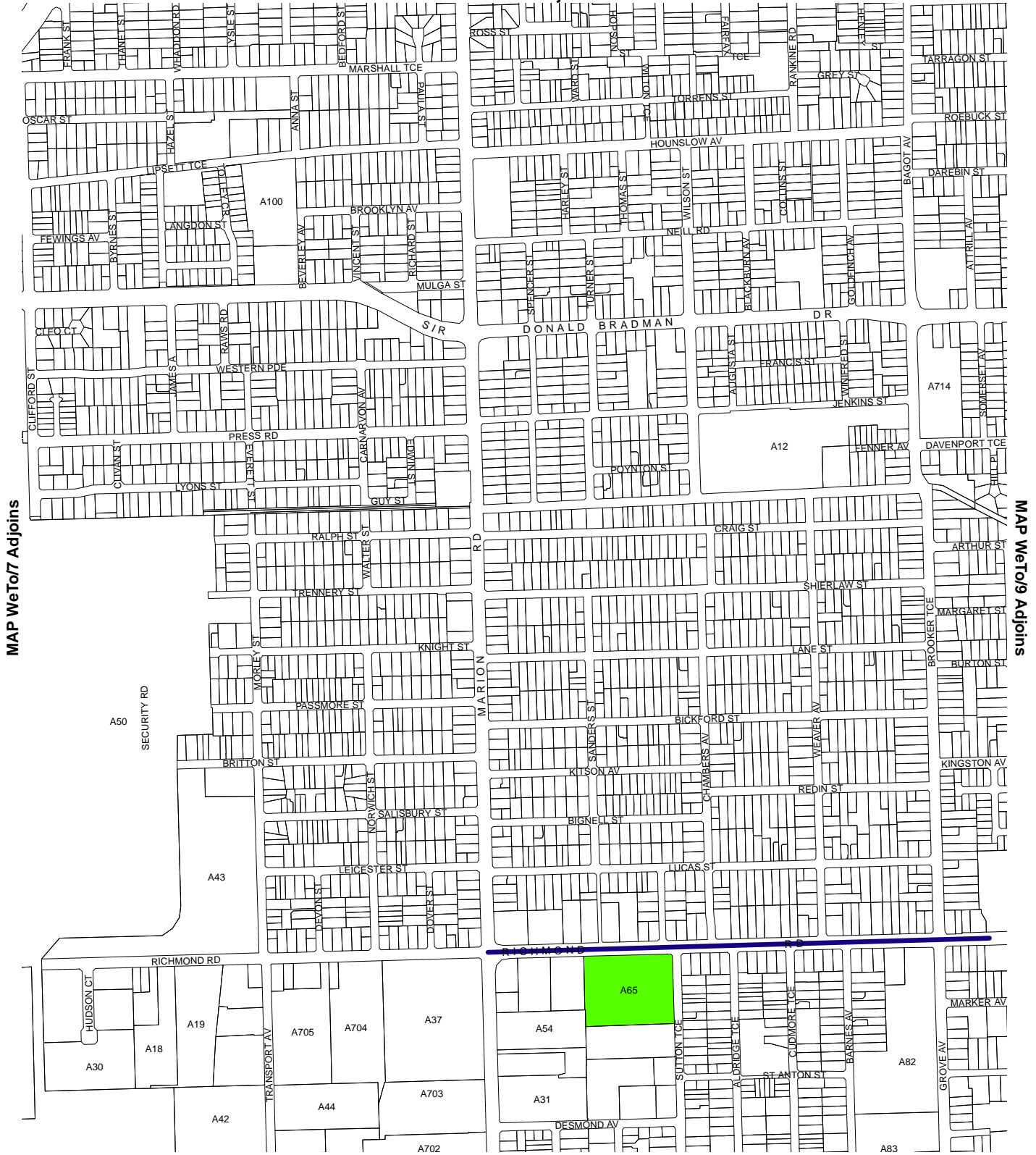
- Designated Road: type A road
- Noise and Air Emissions Designated Area



# Overlay Map WeTo/8

## NOISE AND AIR EMISSIONS



MAP WeTo/4 Adjoins



MAP WeTo/7 Adjoins

MAP WeTo/9 Adjoins

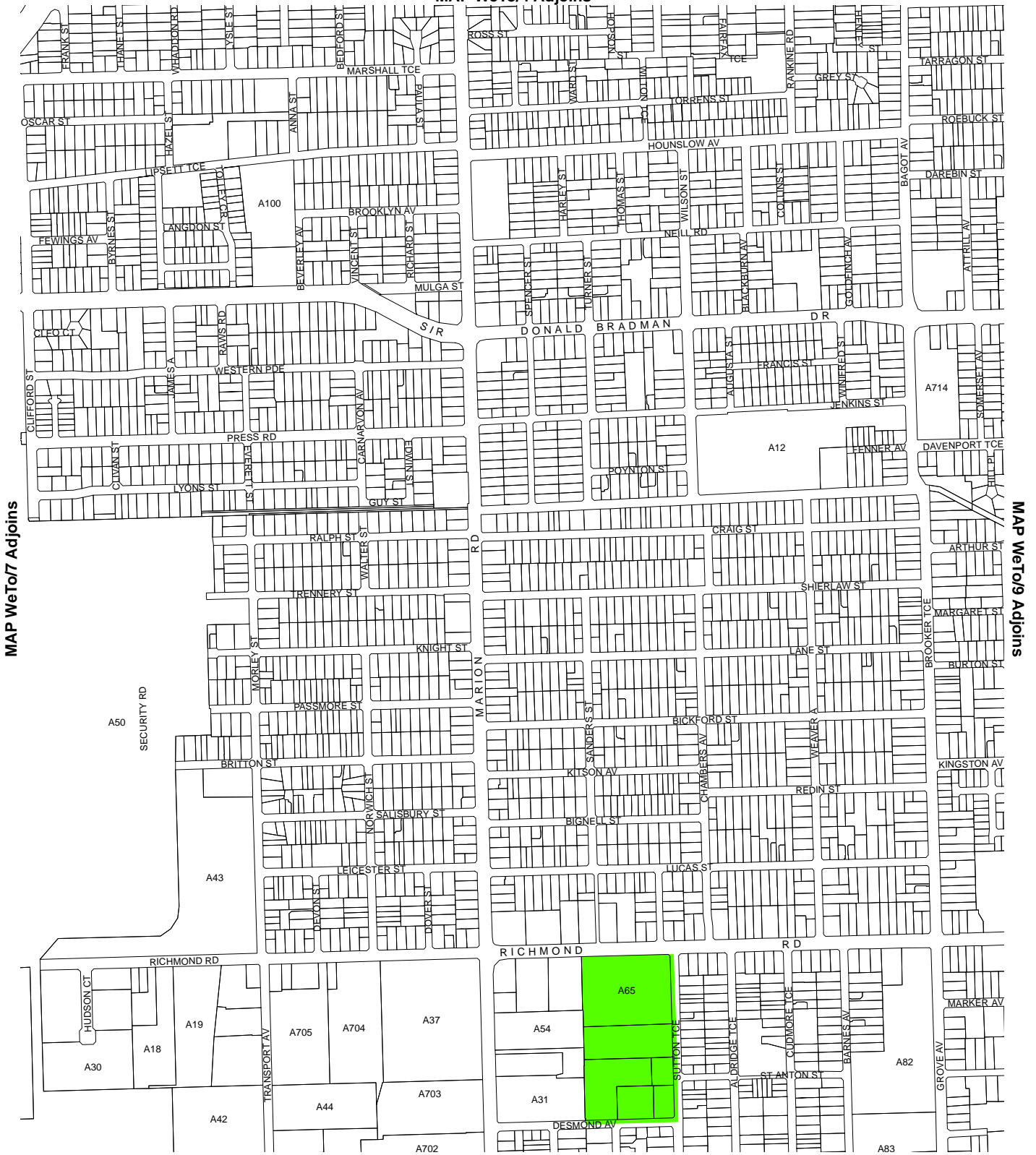
MAP WeTo/12 Adjoins

-  Strategic Transport Route
-  Strategic Transport Routes Designated Area

# Overlay Map WeTo/8

## STRATEGIC TRANSPORT ROUTES

MAP WeTo/4 Adjoins



MAP WeTo/7 Adjoins

MAP WeTo/9 Adjoins

MAP WeTo/12 Adjoins

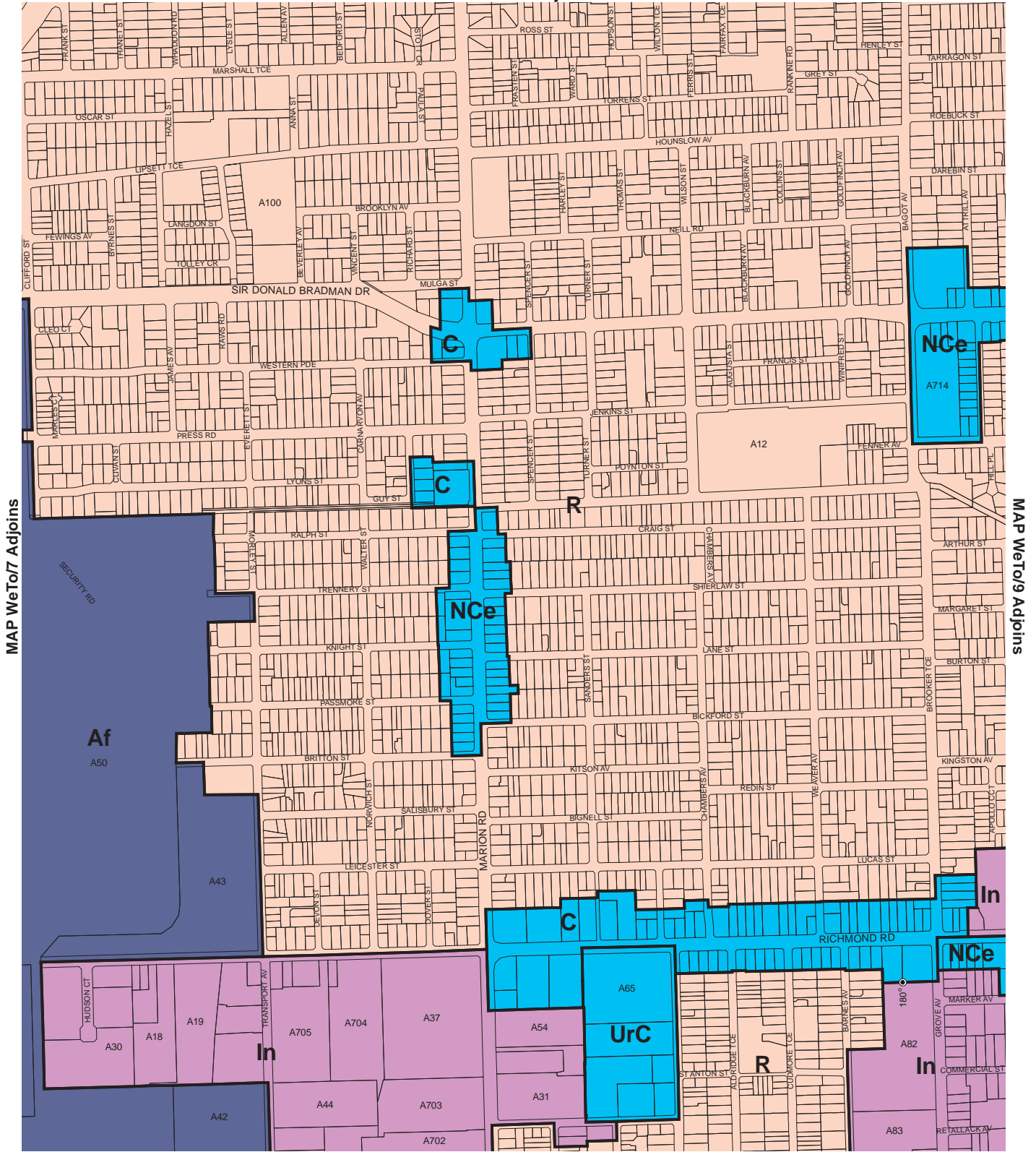


# Overlay Map WeTo/8

## AFFORDABLE HOUSING

Affordable Housing Designated Area

MAP WeTo/4 Adjoins



MAP WeTo/12 Adjoins

Lamberts Conformal Conic Projection, GDA94

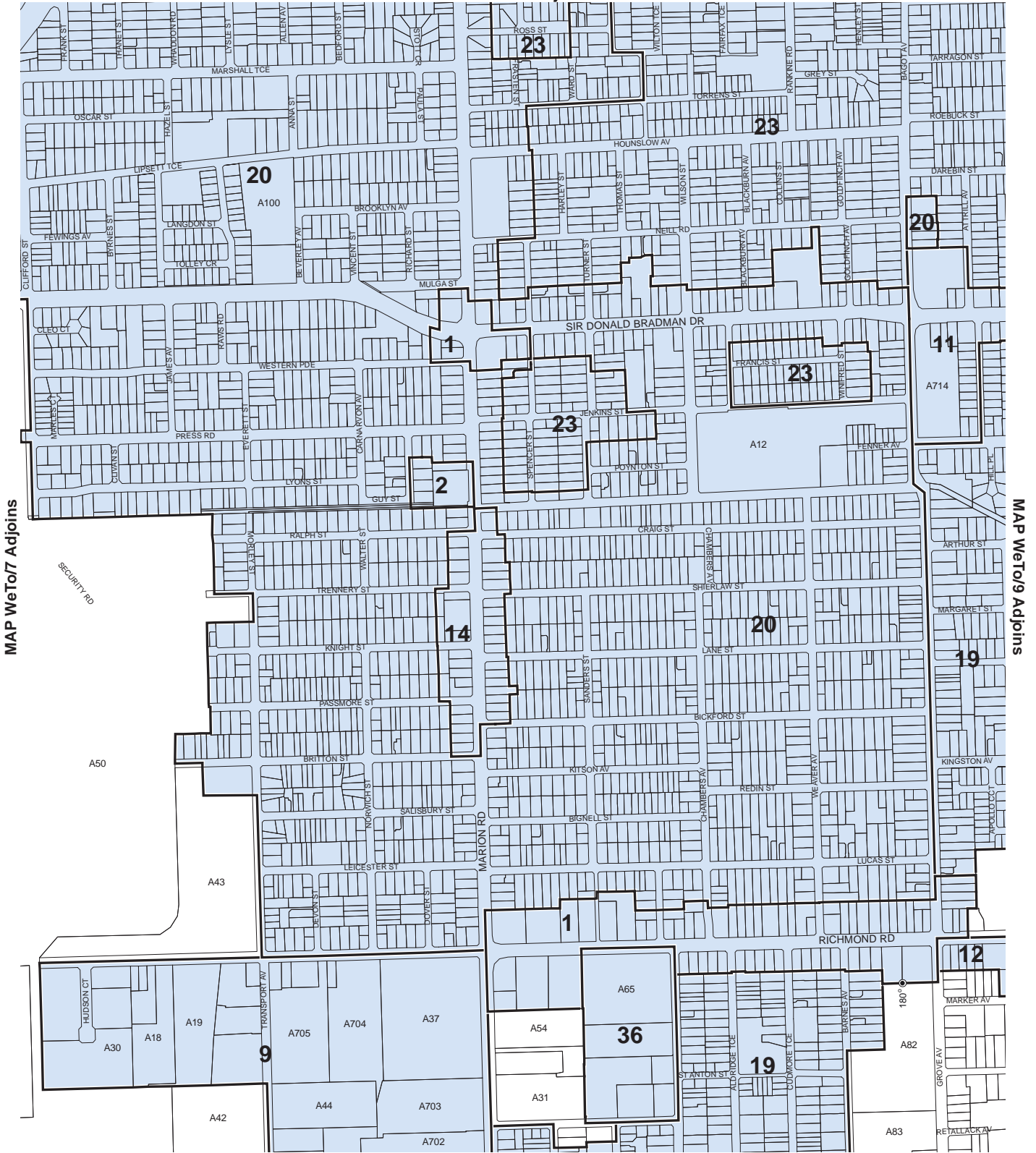


Zones

- Af Airfield
- C Commercial
- In Industry
- NCe Neighbourhood Centre
- R Residential
- UrC Urban Corridor
- Zone Boundary

# Zone Map WeTo/8

MAP WeTo/4 Adjoins



Lamberts Conformal Conic Projection, GDA94

**Policy Area**

- 1 Arterial Roads
- 11 Hilton
- 12 Marlestone
- 14 Richmond
- 19 Medium Density
- 2 District Commercial
- 20 Low Density
- 23 Cowandilla / Mile End West Character
- 36 Transit Living
- 9 Netley



# Policy Area Map WeTo/8

Policy Area Boundary

MAP WeTo/4 Adjoins



MAP WeTo/7 Adjoins

MAP WeTo/9 Adjoins

MAP WeTo/12 Adjoins

Lamberts Conformal Conic Projection, GDA94

Precinct

- 1 Intersection
- 2 Richmond Road







# Precinct Map WeTo/8

Precinct Boundary

MAP WeTo/5 Adjoins

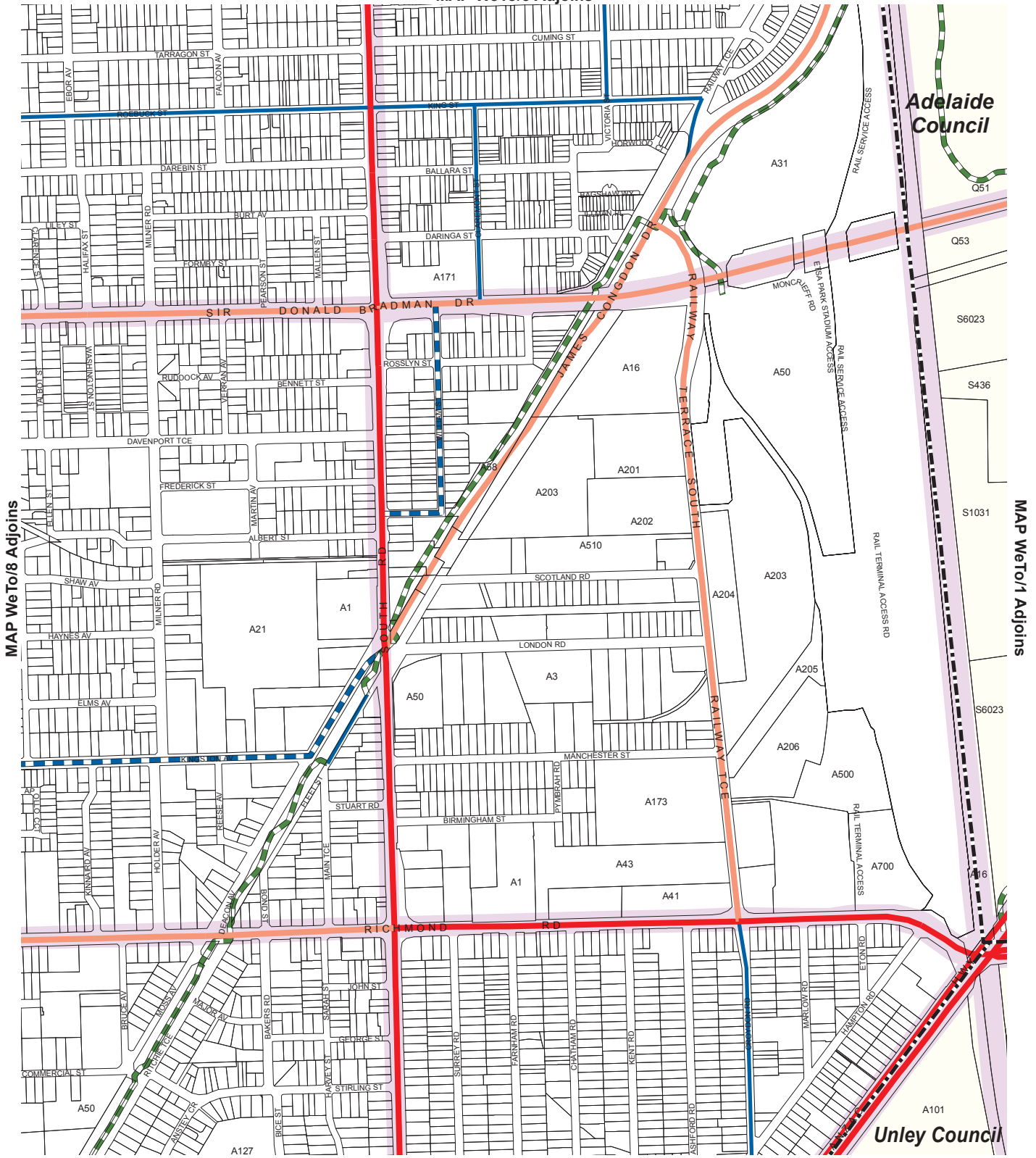


# Location Map WeTo/9

-  School
-  Railways
-  Local Reserves
-  Development Plan Boundary



MAP WeTo/5 Adjoins



MAP WeTo/8 Adjoins

Adelaide Council

MAP WeTo/1 Adjoins

Unley Council

MAP WeTo/13 Adjoins

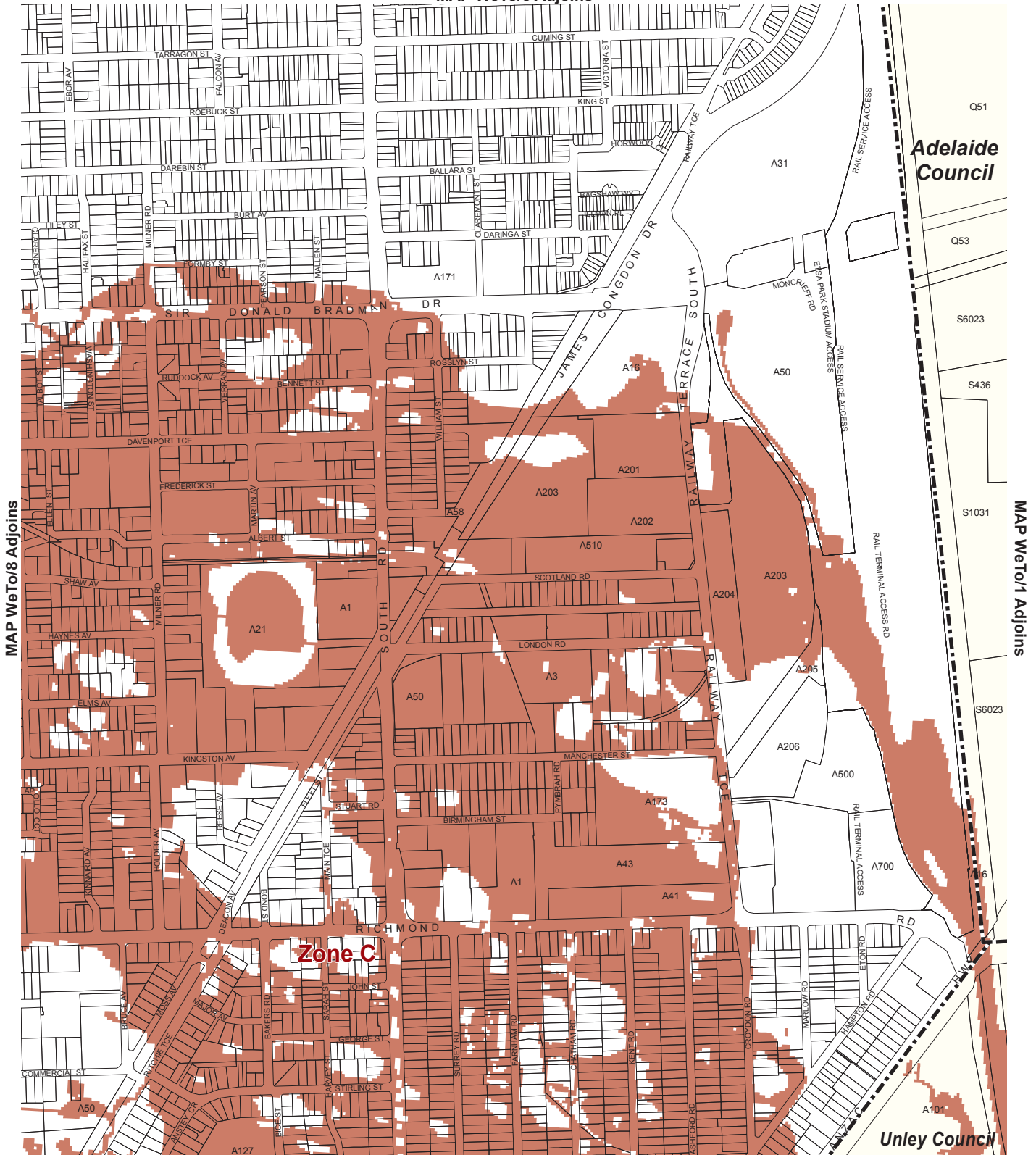
- Primary Arterial Roads
- Secondary Arterial Roads
- Bikedirect Network**
- Main Road - Bike Lane
- Main Road
- Secondary Road - Bike Lane
- Secondary Road
- Off Road Sealed Path
- Public Transport
- Development Plan Boundary



# Overlay Map WeTo/9

## TRANSPORT

MAP WeTo/5 Adjoins



**Airport Building Heights**  
**Zone C** All Structures Exceeding 15 metres above existing ground level

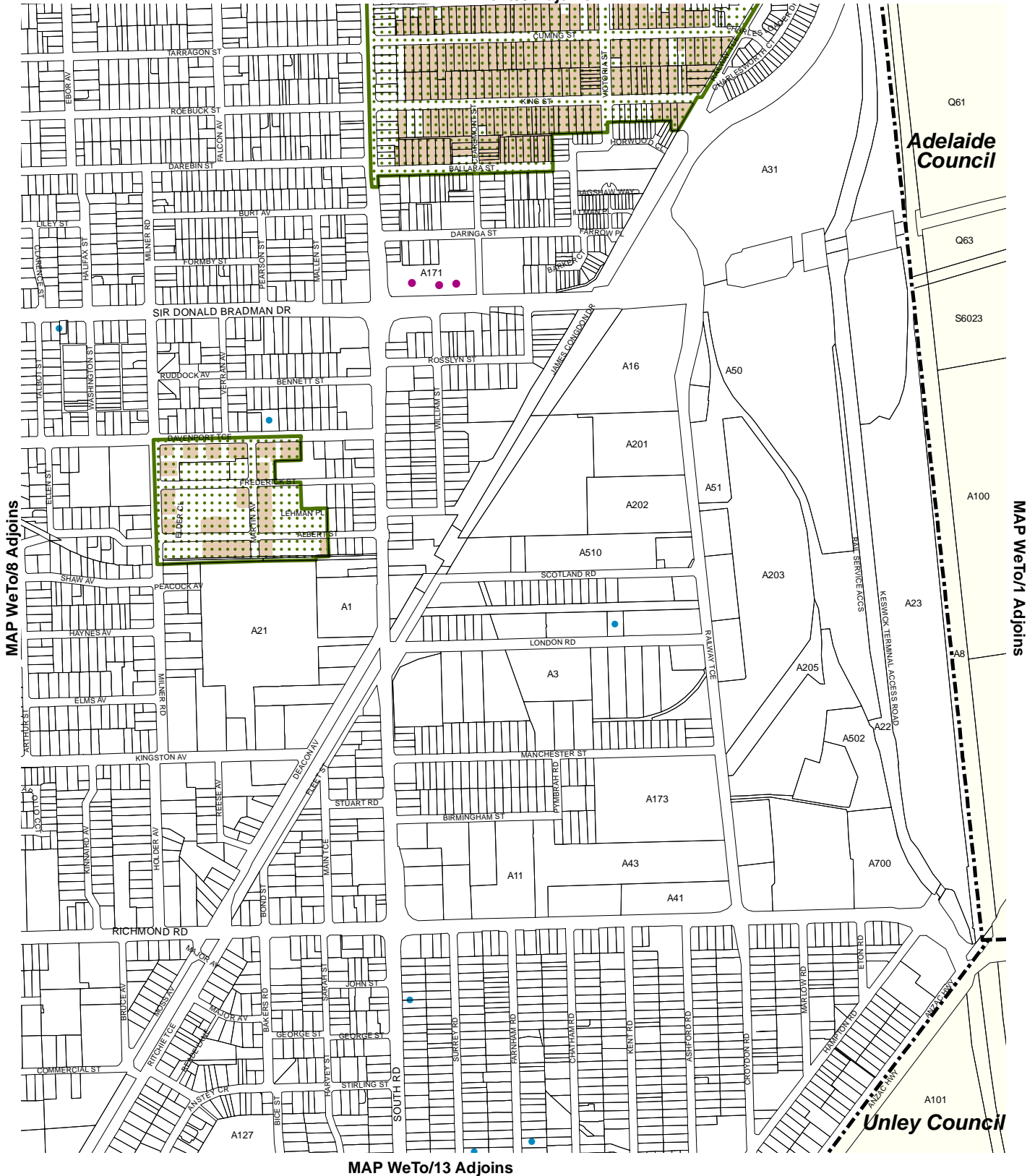


- Airport Building Heights
- Flood Hazard
- Development Plan Boundary

# Overlay Map WeTo/9

## DEVELOPMENT CONSTRAINTS

MAP WeTo/5 Adjoins



Heritage points are indicative only.  
 For further information on State and Local Heritage Places and Contributory Items please refer to the relevant tables within this document.

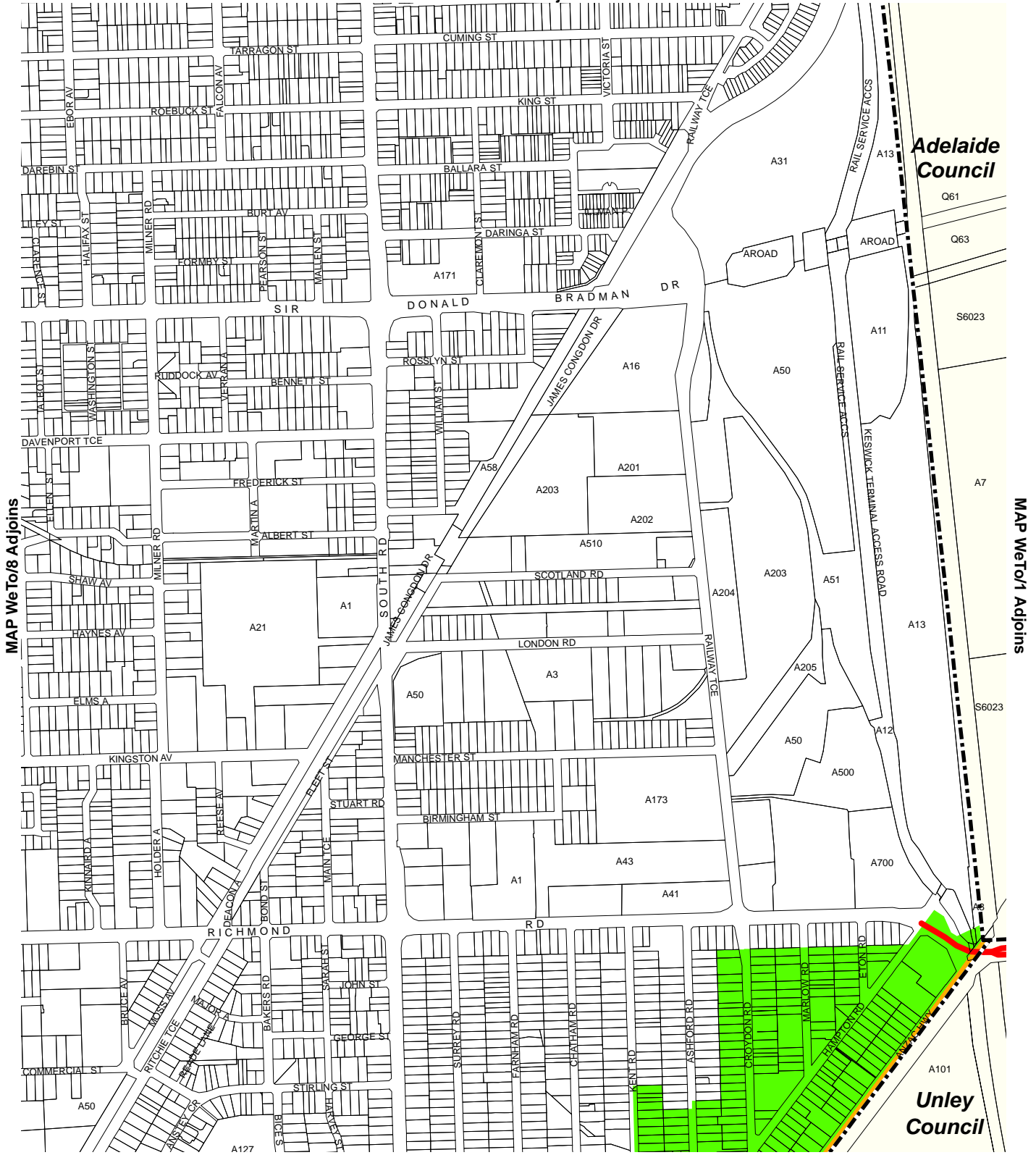


- Local heritage place
- State heritage place
- Contributory Item
- Historic Conservation Area
- Development Plan Boundary

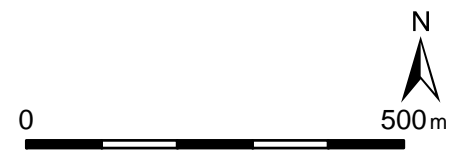
# Overlay Map WeTo/9

## HERITAGE

MAP WeTo/5 Adjoins



MAP WeTo/13 Adjoins

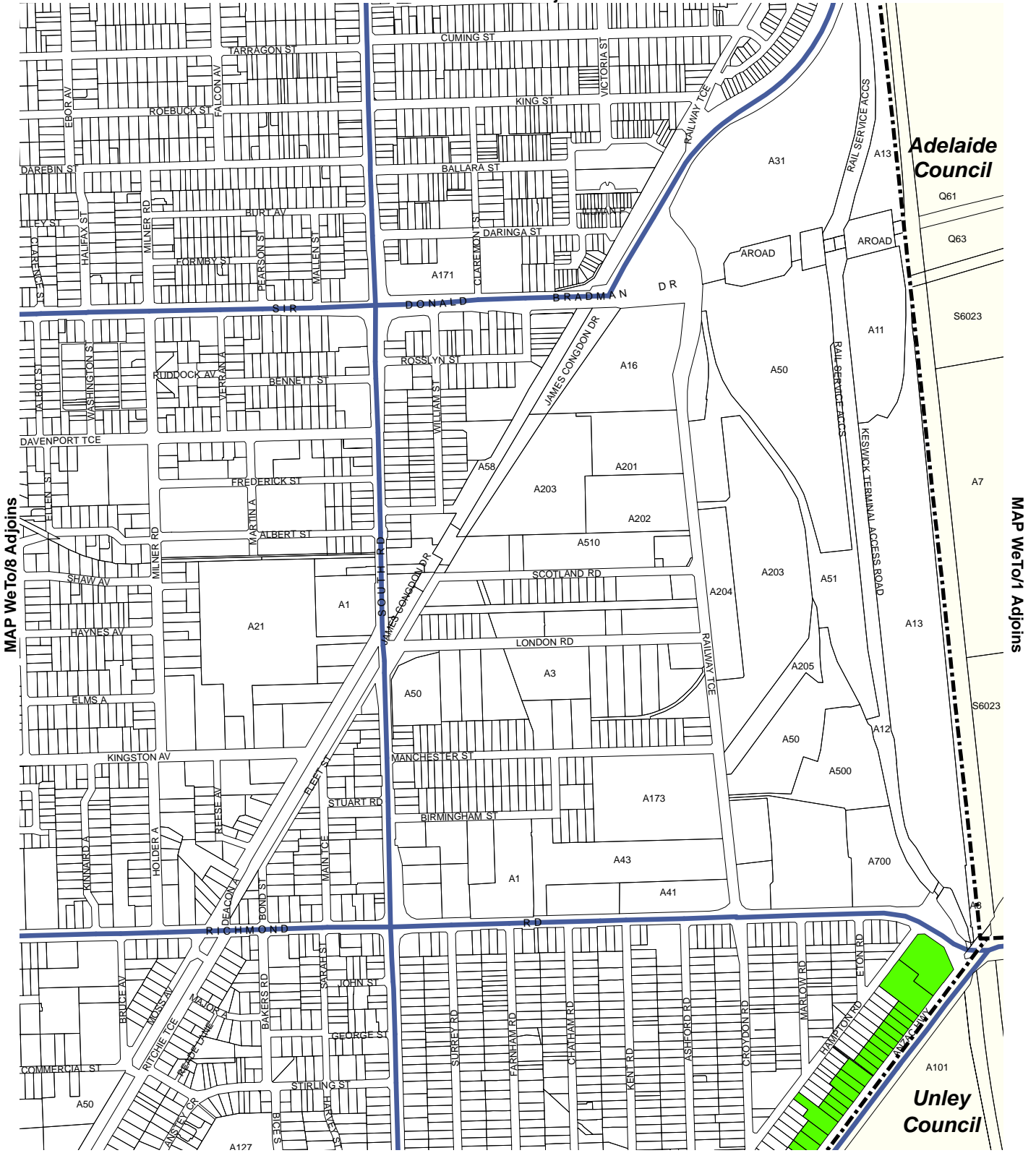


- Designated Road: type A road
- Designated Road: type B road
- Noise and Air Emissions Designated Area
- Development Plan Boundary

# Overlay Map WeTo/9

## NOISE AND AIR EMISSIONS

MAP WeTo/5 Adjoins

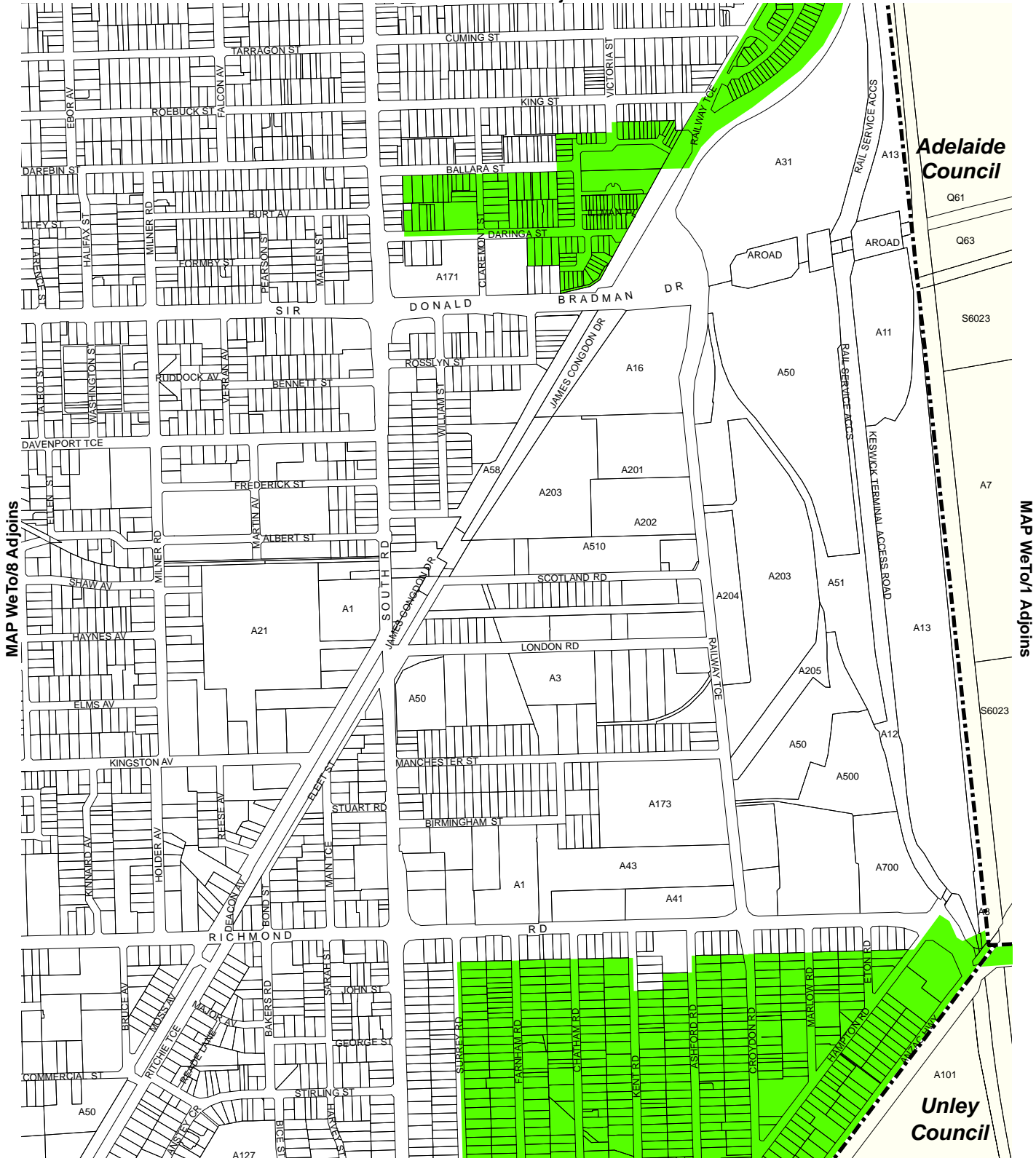


- Strategic Transport Route
- Strategic Transport Routes Designated Area
- Development Plan Boundary

# Overlay Map WeTo/9

## STRATEGIC TRANSPORT ROUTES

MAP WeTo/5 Adjoins



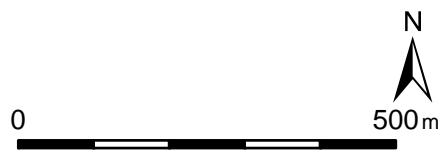
Adelaide Council

Unley Council

MAP WeTo/1 Adjoins

MAP WeTo/8 Adjoins

MAP WeTo/13 Adjoins

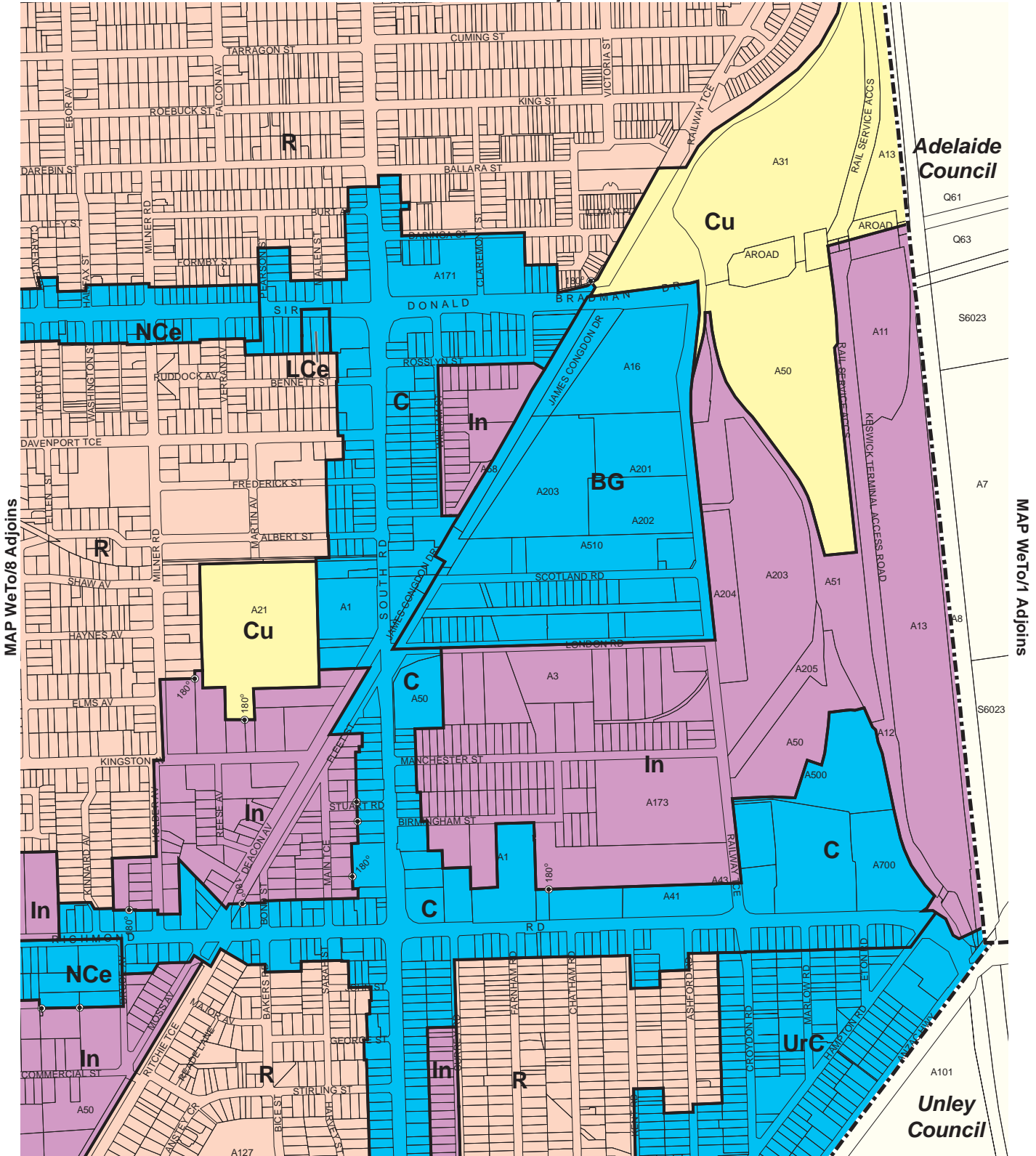


# Overlay Map WeTo/9

## AFFORDABLE HOUSING



- Affordable Housing Designated Area
- Development Plan Boundary

MAP WeTo/5 Adjoins



MAP WeTo/13 Adjoins

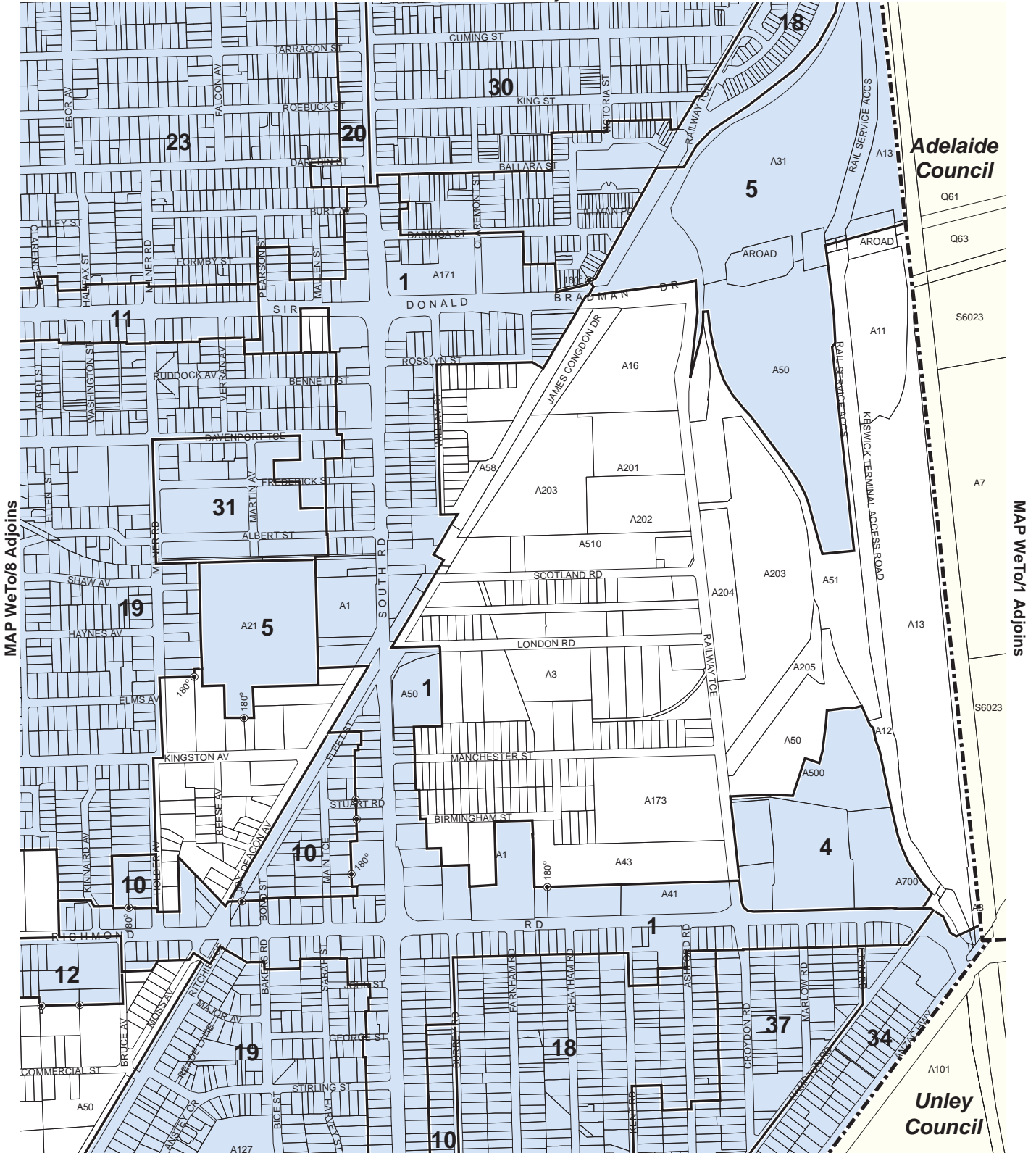
Lamberts Conformal Conic Projection, GDA94

- Zones**
- BG** Bulky Goods
  - C** Commercial
  - Cu** Community
  - In** Industry
  - LCe** Local Centre
  - NCe** Neighbourhood Centre
  - R** Residential
  - UrC** Urban Corridor
  -  Zone Boundary
  -  Development Plan Boundary



# Zone Map WeTo/9

MAP WeTo/5 Adjoins



MAP WeTo/13 Adjoins

Lamberts Conformal Conic Projection, GDA94

**Policy Area**

- 1 Arterial Roads
- 10 Mixed Use
- 11 Hilton
- 12 Marlestone
- 18 Medium Density
- 19 Medium Density
- 20 Low Density
- 23 Cowandilla / Mile End West Character
- 30 Mile End Conservation
- 31 Richmond Conservation
- 34 Boulevard
- 37 Business
- 4 Office Park
- 5 Recreation

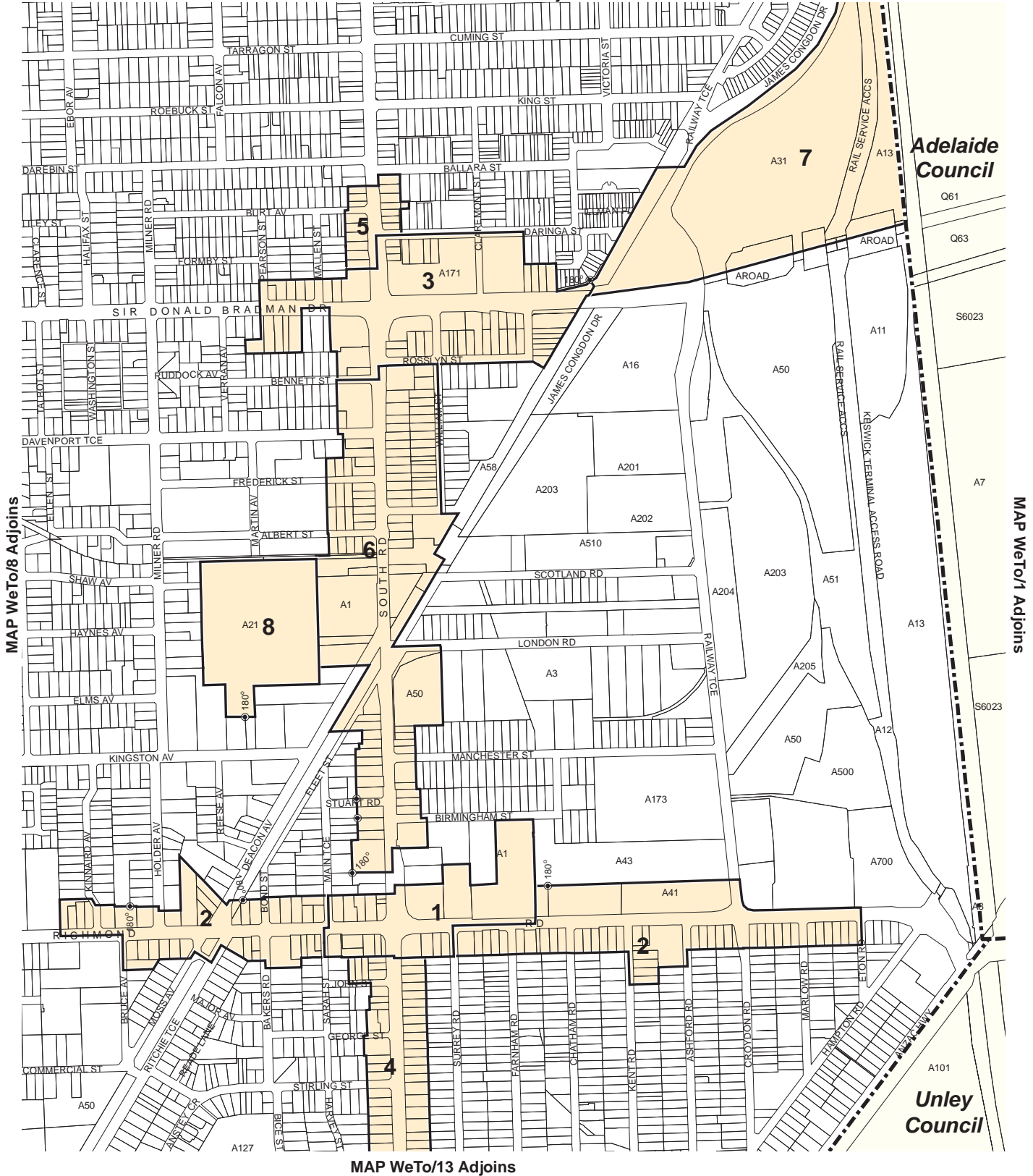
- Policy Area Boundary
- Development Plan Boundary



# Policy Area Map WeTo/9





MAP WeTo/5 Adjoins



Lamberts Conformal Conic Projection, GDA94

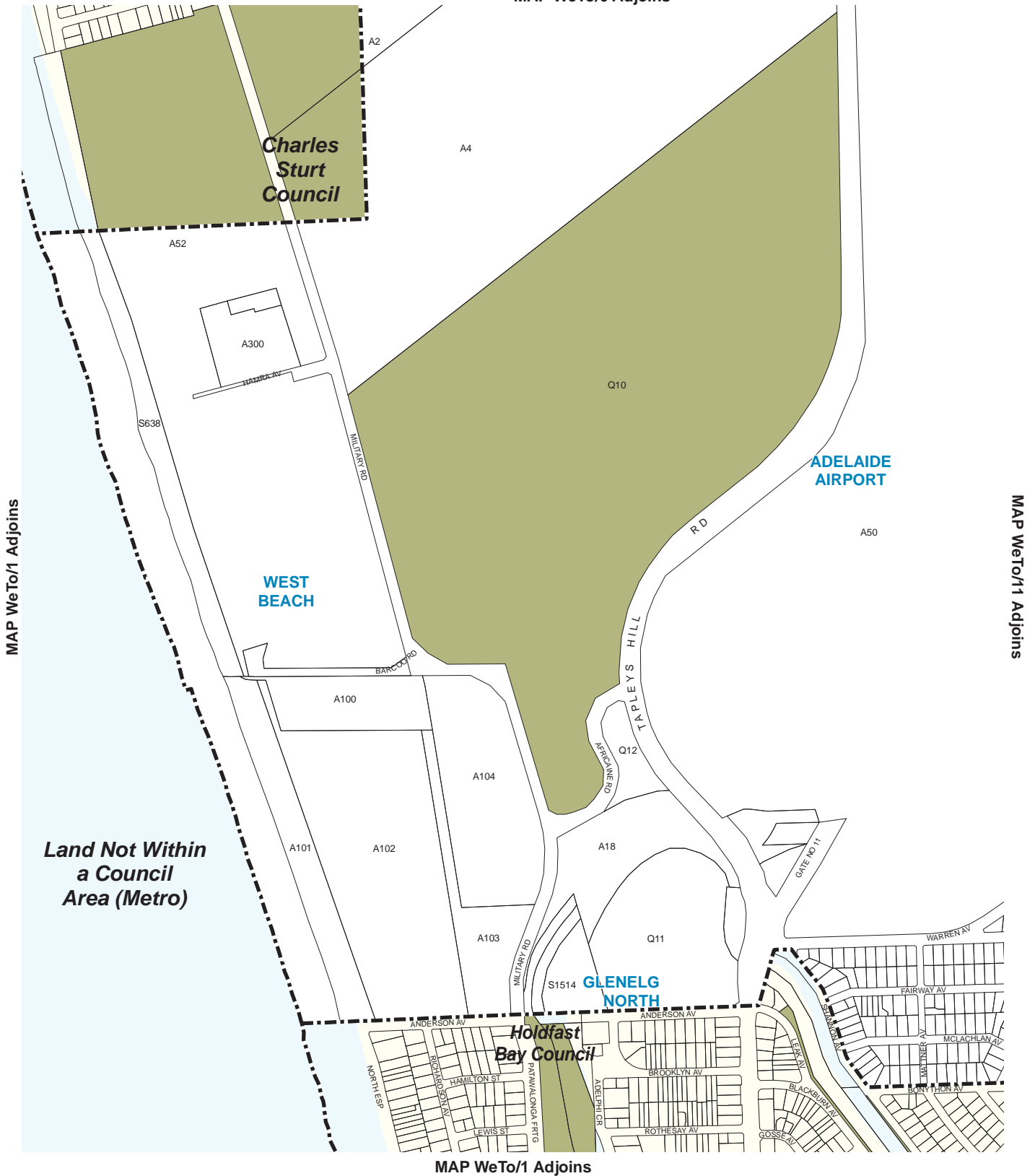
**Precinct**

- 1 Intersection
- 2 Richmond Road
- 3 Sir Donald Bradman Drive (Mile End)
- 4 South Road (Keswick)
- 5 South Road (Mile End)
- 6 South Road (Mile End South)
- 7 Mile End
- 8 Open Space

-  Precinct Boundary
-  Development Plan Boundary

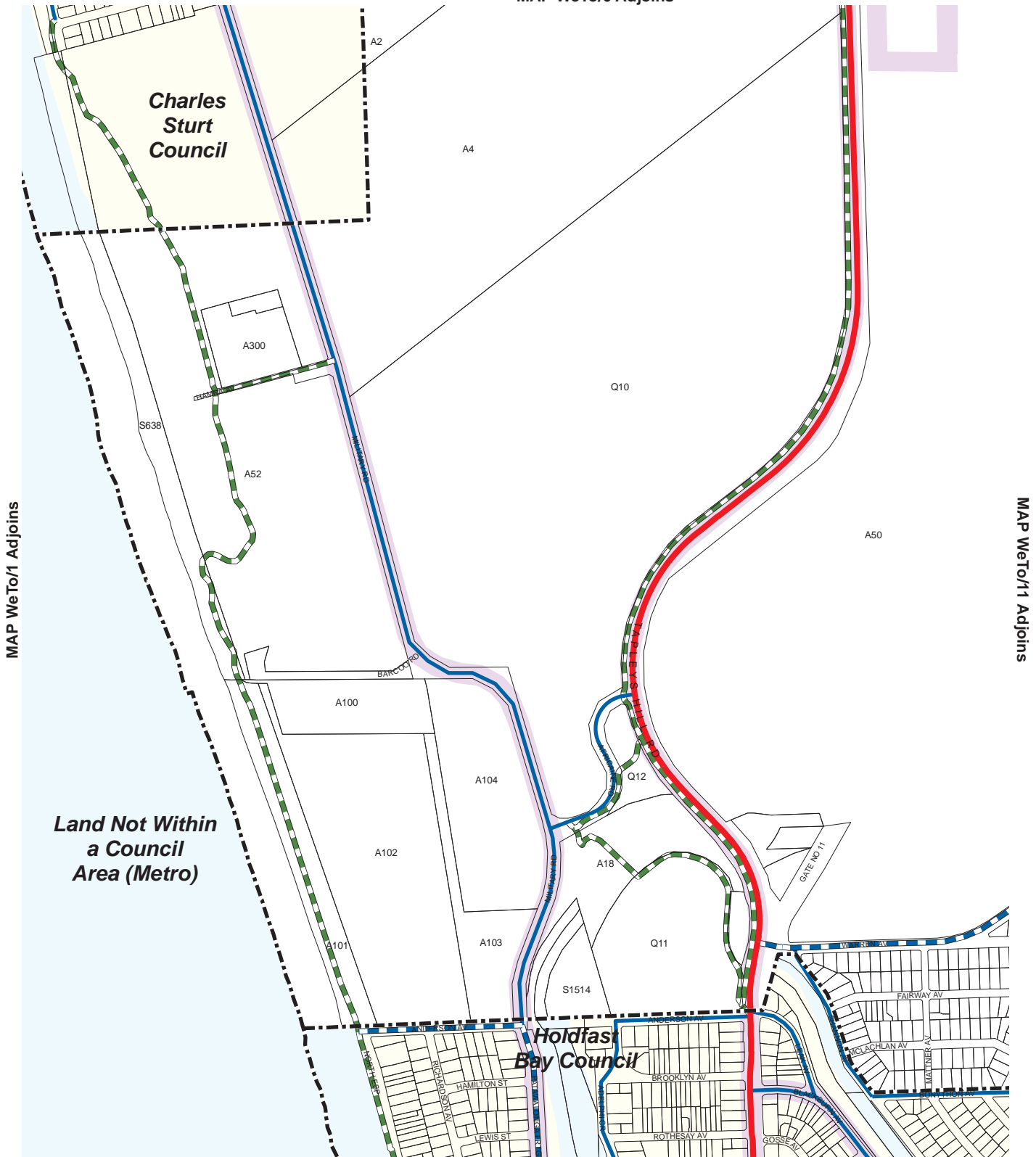


# Precinct Map WeTo/9



# Location Map WeTo/10

- Local Reserves
- Development Plan Boundary



MAP WeTo/1 Adjoins

MAP WeTo/1 Adjoins

Land Not Within a Council Area (Metro)

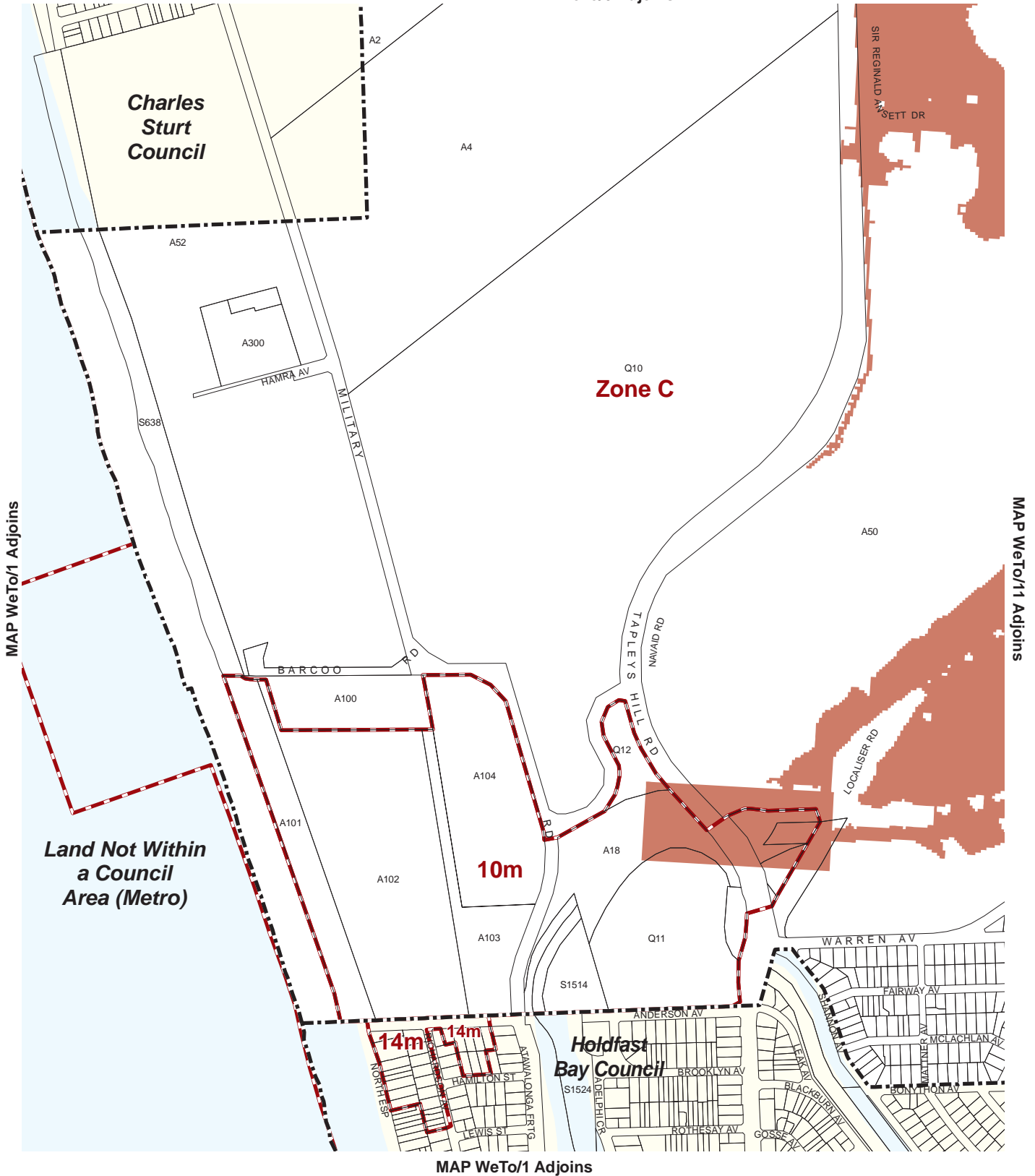
MAP WeTo/1 Adjoins

- Primary Arterial Roads
- Bikedirect Network
- Main Road - Bike Lane
- Main Road
- Secondary Road - Bike Lane
- Secondary Road
- Off Road Sealed Path
- Public Transport
- Development Plan Boundary



# Overlay Map WeTo/10




## TRANSPORT



**Airport Building Heights**  
**10m - 14m** All Structures restricted to height identified on maps  
 (above existing ground level, measured from the top of the nearest roadside curb)  
**Zone C** All Structures Exceeding 15 metres above existing ground level



# Overlay Map WeTo/10 DEVELOPMENT CONSTRAINTS

-  Airport Building Heights
-  Flood Hazard
-  Development Plan Boundary



MAP WeTo/1 Adjoins

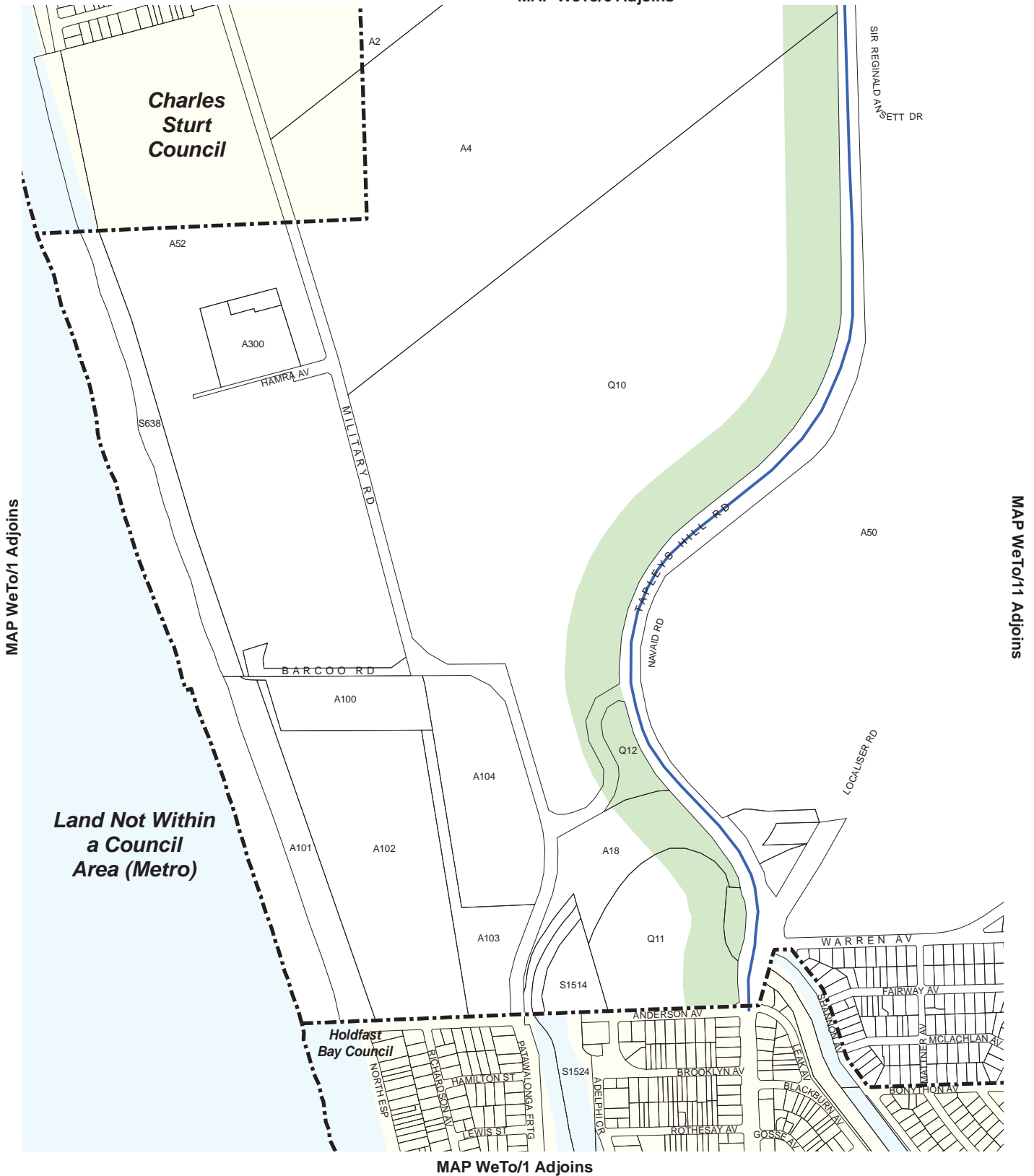


# Overlay Map WeTo/10

## NATURAL RESOURCES

- MOSS Study Area
- Development Plan Boundary

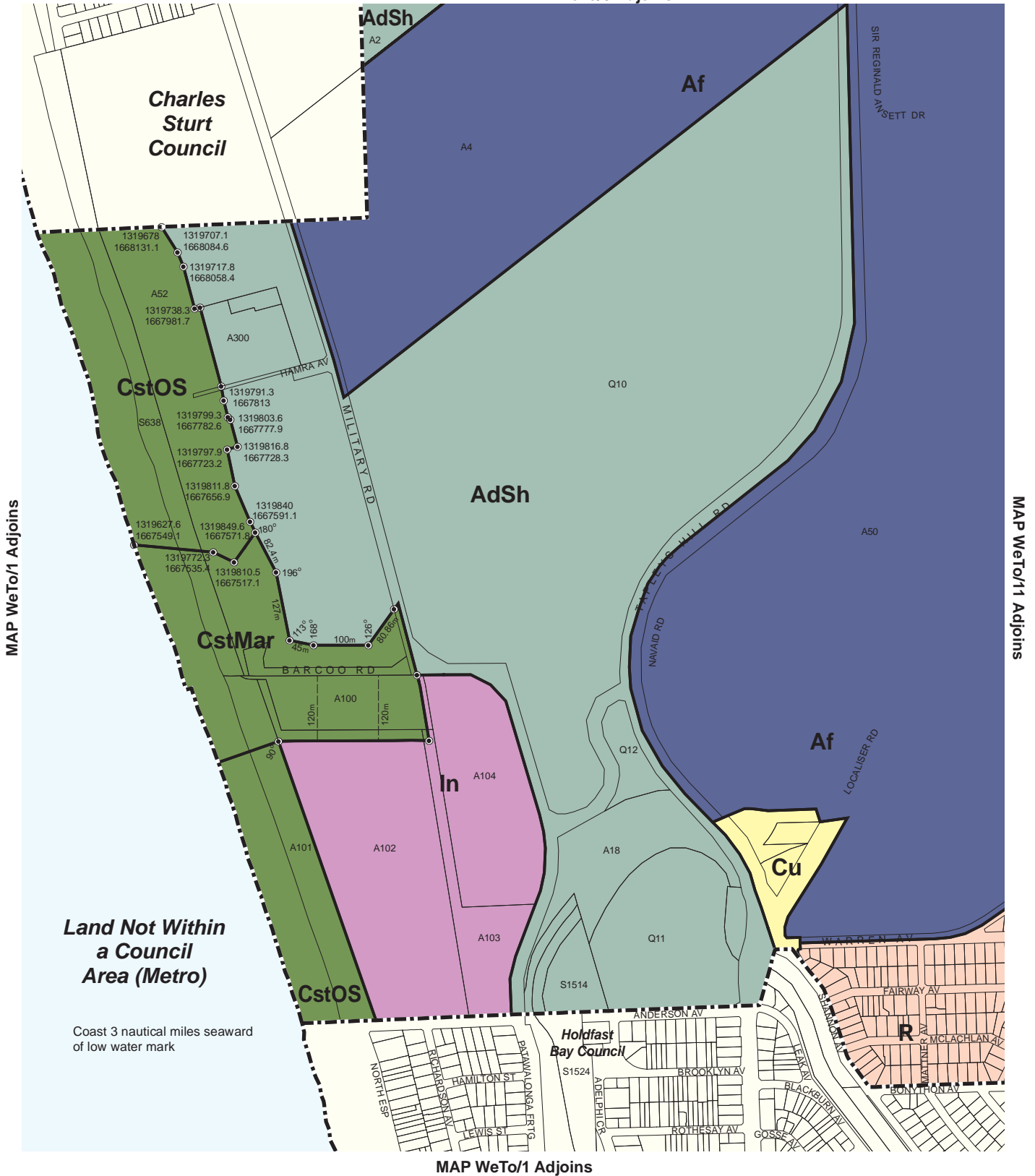
MAP WeTo/6 Adjoins



# Overlay Map WeTo/10

## STRATEGIC TRANSPORT ROUTES

- Strategic Transport Route
- Designated Area
- Development Plan Boundary



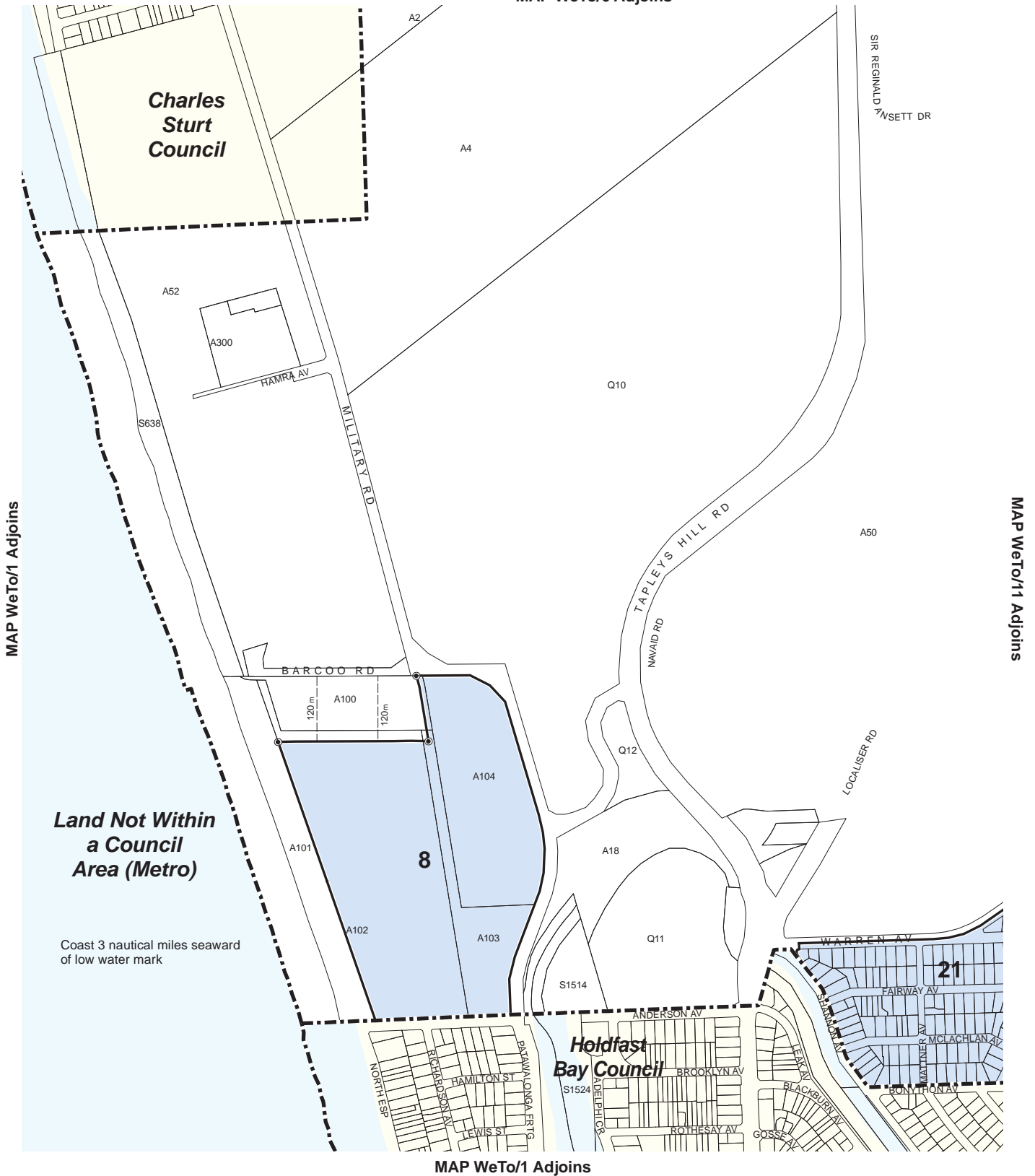
Lamberts Conformal Conic Projection, GDA94

**Zones**

- AdSh Adelaide Shores
- Af Airfield
- CstMar Coastal Marina
- CstOS Coastal Open Space
- Cu Community
- In Industry
- R Residential
- Zone Boundary
- Development Plan Boundary



# Zone Map WeTo/10



MAP WeTo/1 Adjoins

Lamberts Conformal Conic Projection, GDA94

**Policy Area**

- 21 Low Density
- 8 Infrastructure



# Policy Area Map WeTo/10

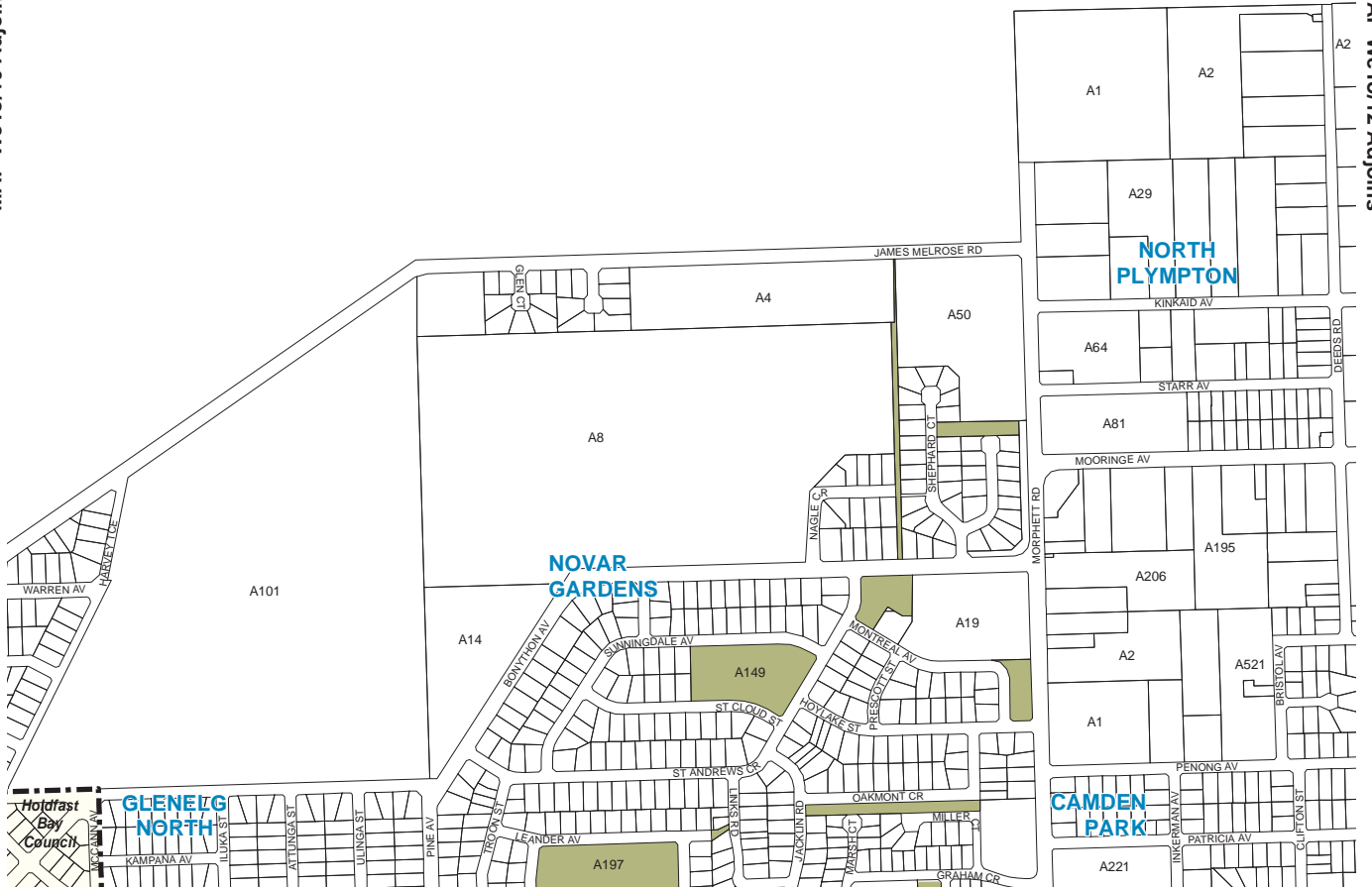
- Policy Area Boundary
- Development Plan Boundary



A50

MAP WeTo/10 Adjoins

MAP WeTo/12 Adjoins



MAP WeTo/14 Adjoins



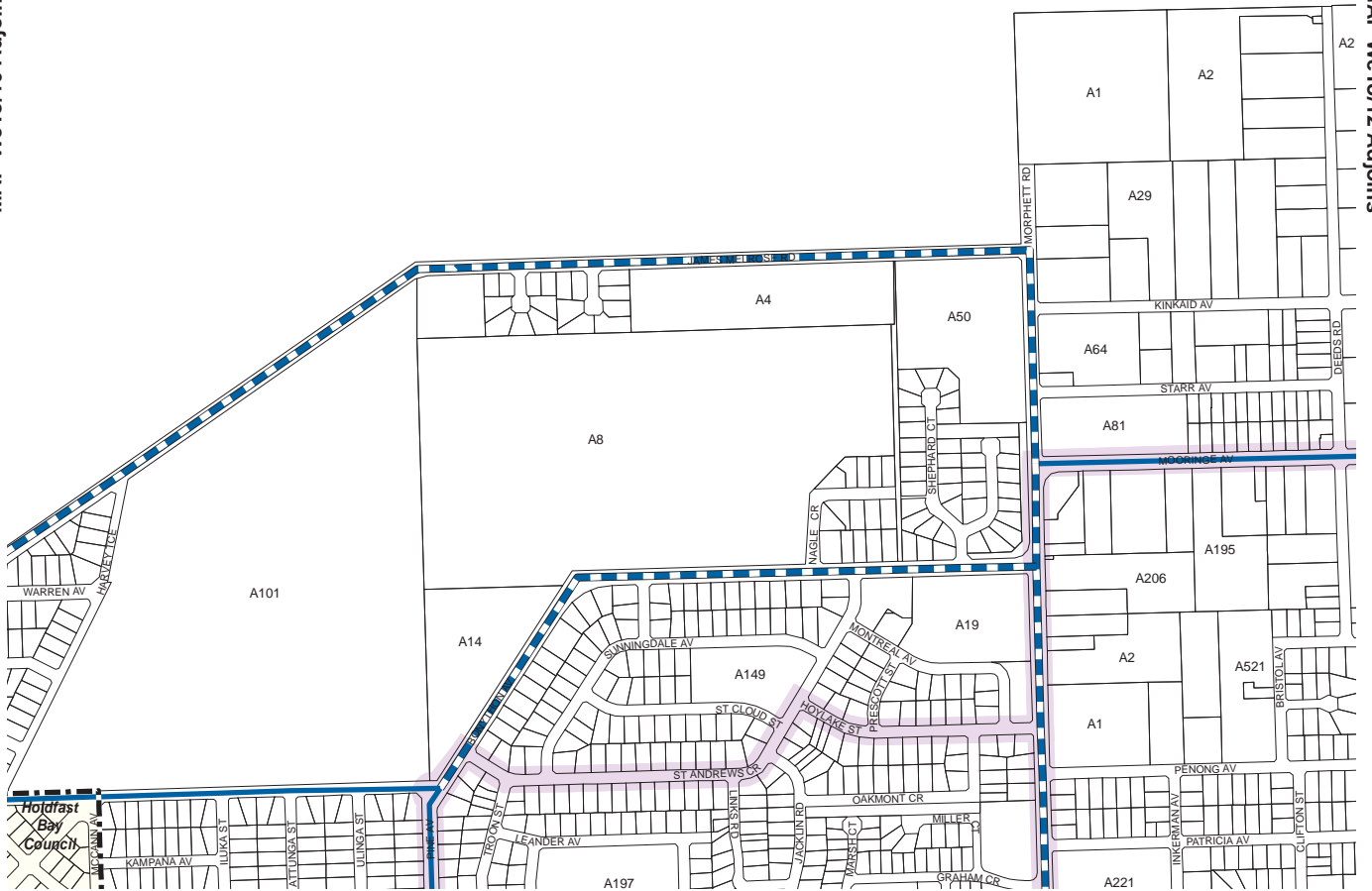
# Location Map WeTo/11

- Local Reserves
- Development Plan Boundary

A50

MAP WeTo/10 Adjoins

MAP WeTo/12 Adjoins



MAP WeTo/14 Adjoins

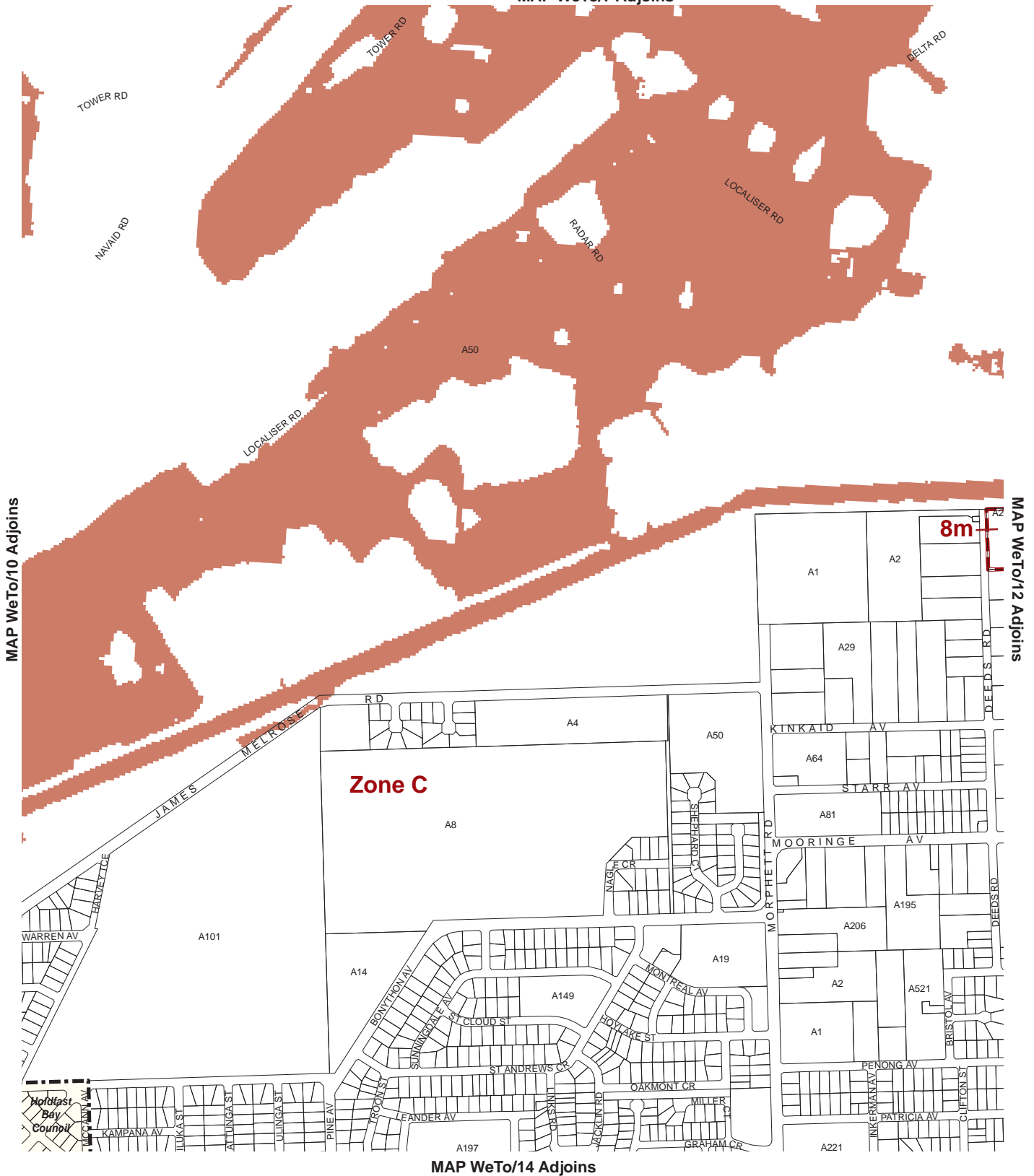


- Bikedirect Network
- Secondary Road - Bike Lane
- Secondary Road
- Public Transport
- Development Plan Boundary

# Overlay Map WeTo/11

## TRANSPORT

MAP WeTo/7 Adjoins

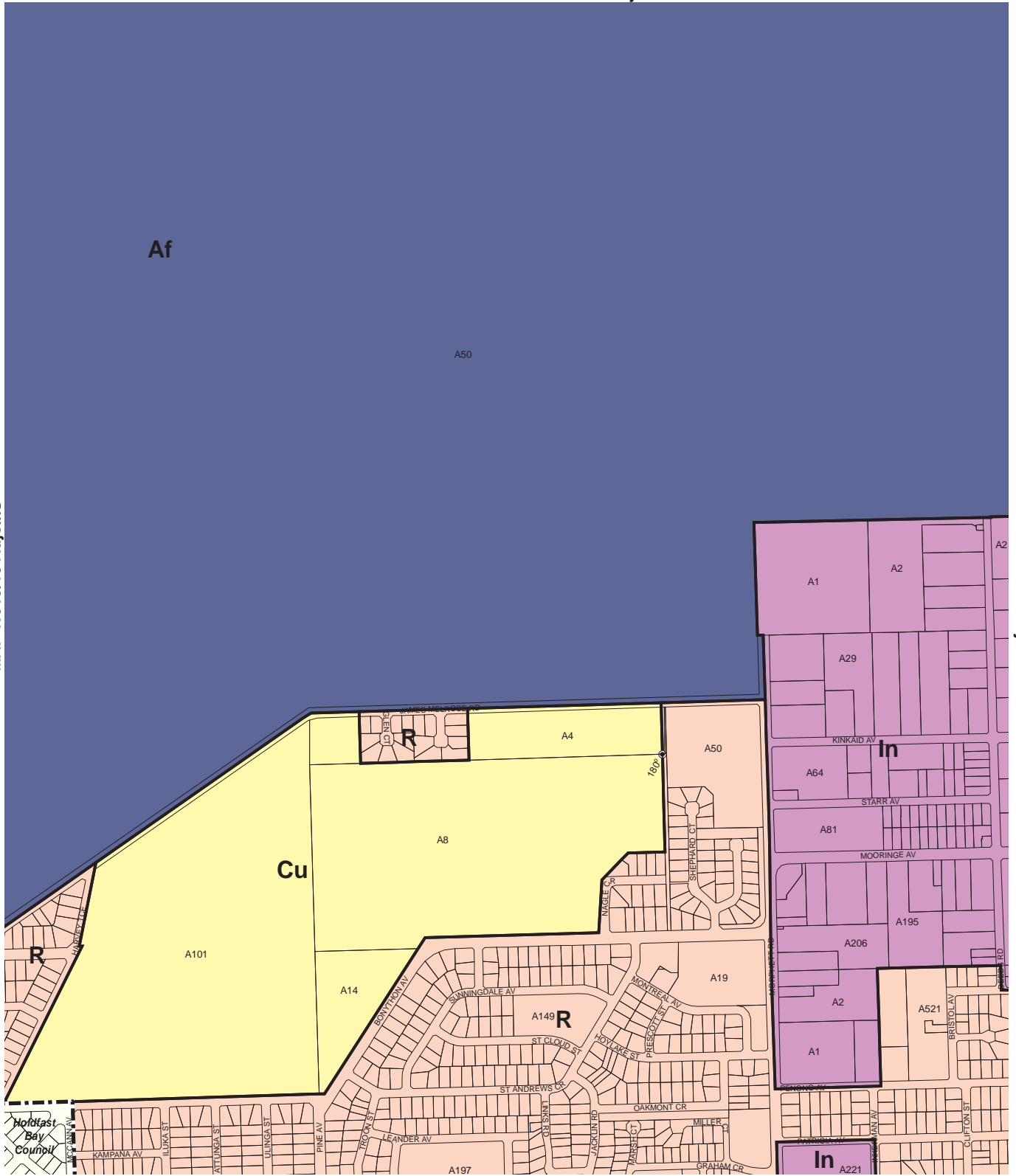


**Airport Building Heights**  
**8m - 14m** All Structures restricted to height identified on maps  
 (above existing ground level, measured from the top of the nearest roadside curb)  
**Zone C** All Structures Exceeding 15 metres above existing ground level



# Overlay Map WeTo/11 DEVELOPMENT CONSTRAINTS

- Airport Building Heights
- Flood Hazard
- Development Plan Boundary



MAP WeTo/14 Adjoins

Lamberts Conformal Conic Projection, GDA94

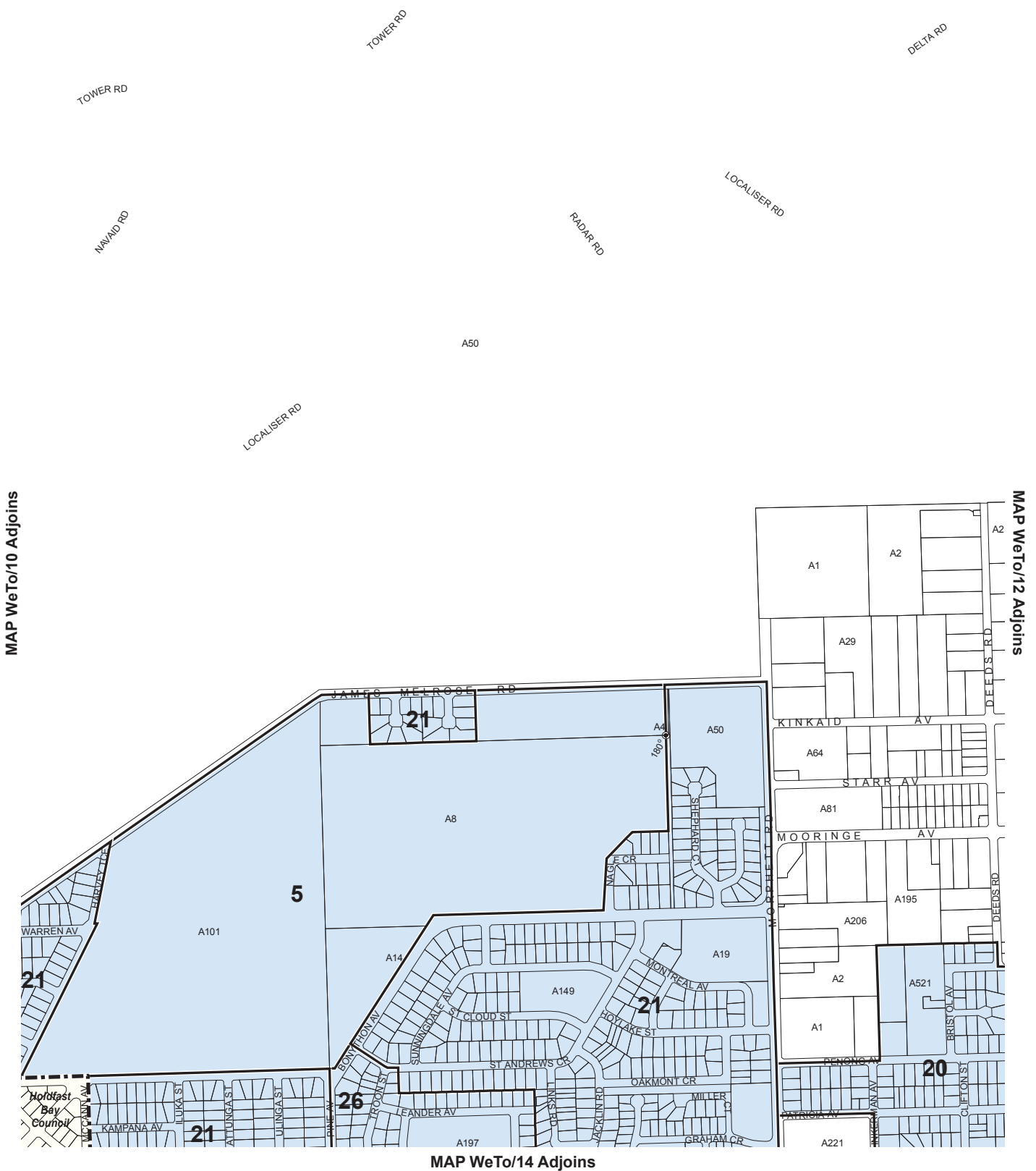


**Zones**

- Af Airfield
- Cu Community
- In Industry
- R Residential
- Zone Boundary
- Development Plan Boundary

# Zone Map WeTo/11

# MAP WeTo/7 Adjoins



Lamberts Conformal Conic Projection, GDA94

### Policy Area

- 20 Low Density
- 21 Low Density
- 26 Novar Gardens Character
- 5 Recreation



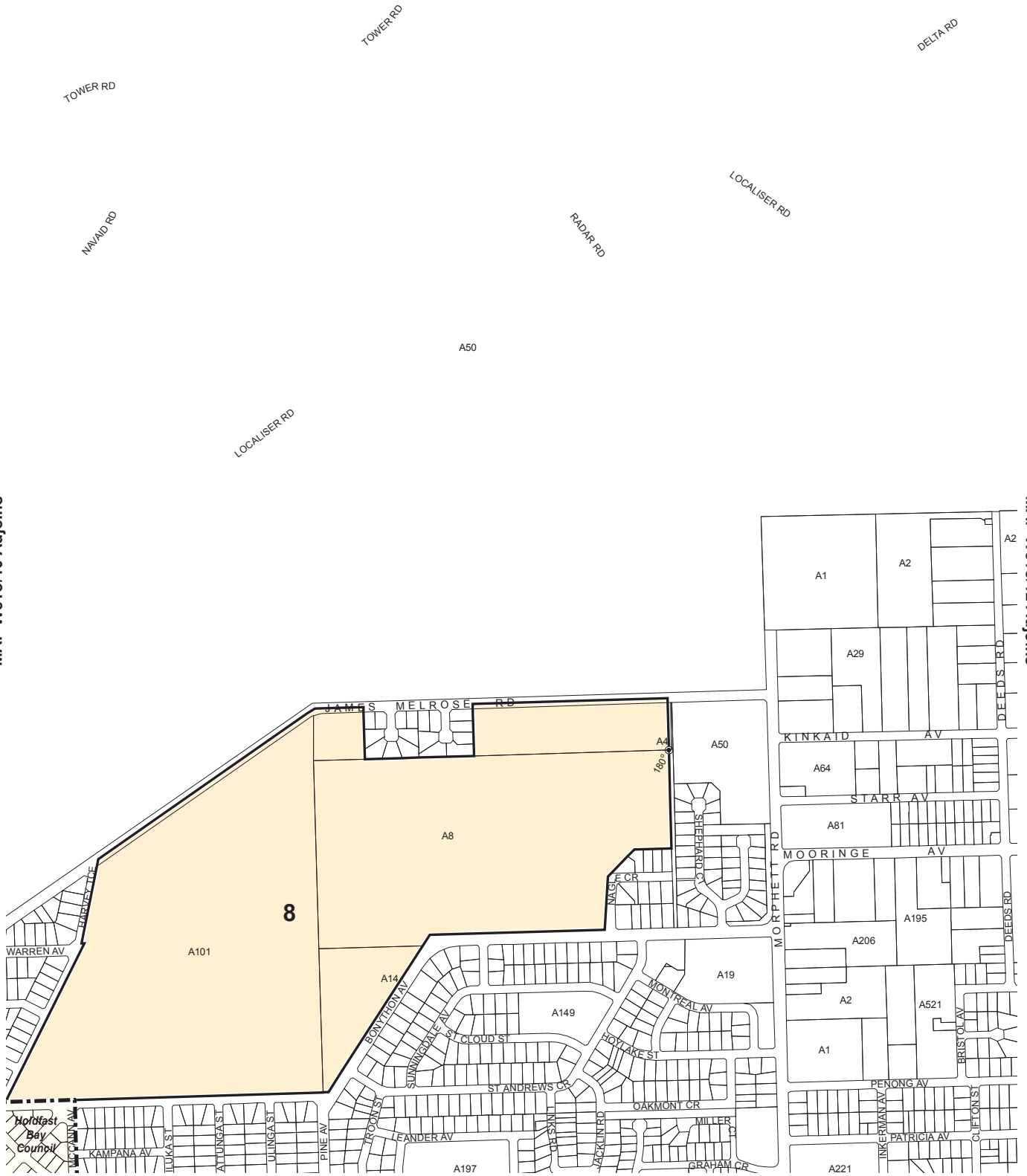
# Policy Area Map WeTo/11

- Policy Area Boundary
- Development Plan Boundary

# MAP WeTo/7 Adjoins

MAP WeTo/10 Adjoins

MAP WeTo/12 Adjoins



MAP WeTo/14 Adjoins

Lamberts Conformal Conic Projection, GDA94

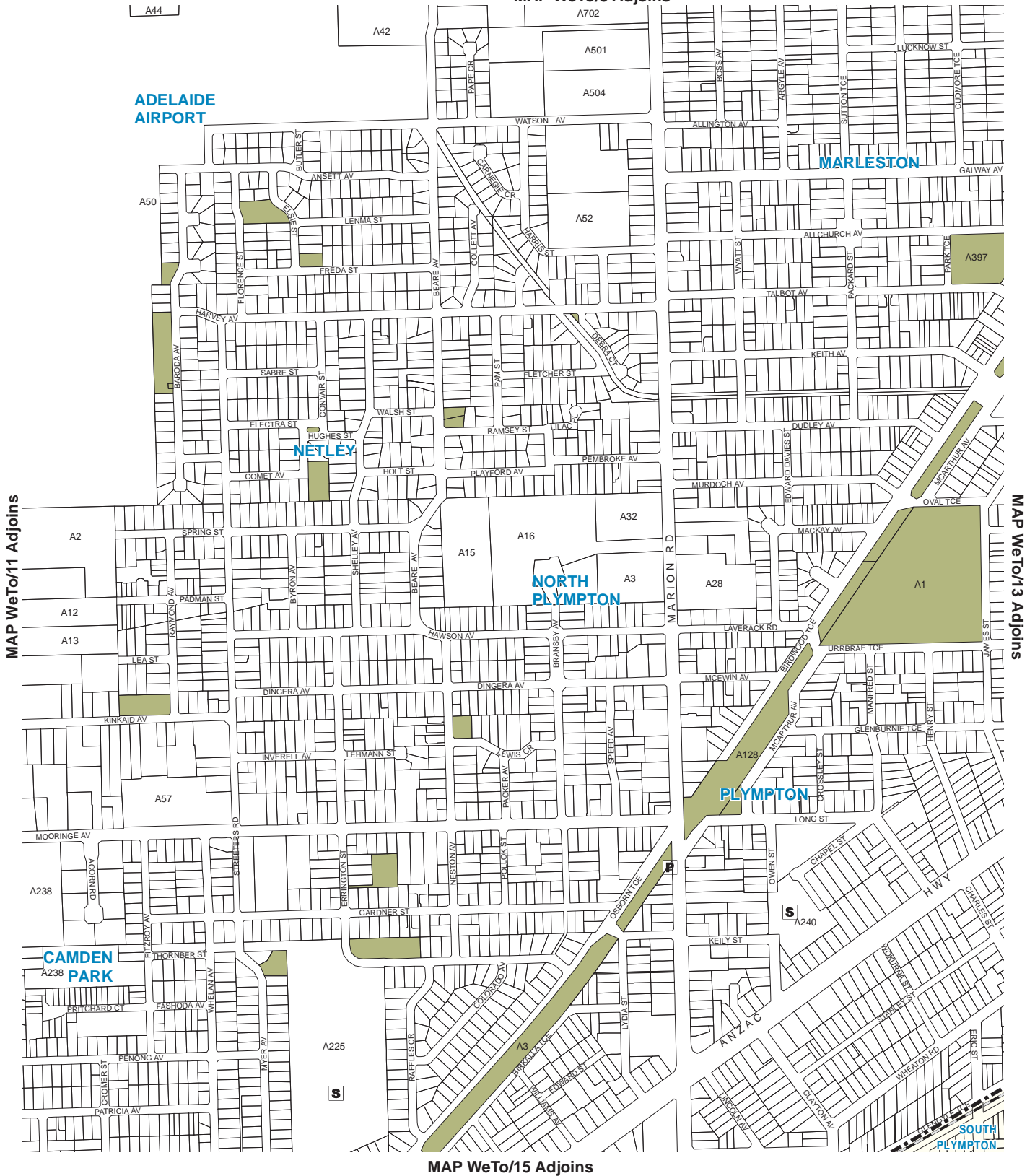
**Precinct**  
8 Open Space



# Precinct Map WeTo/11

- Precinct Boundary
- Development Plan Boundary

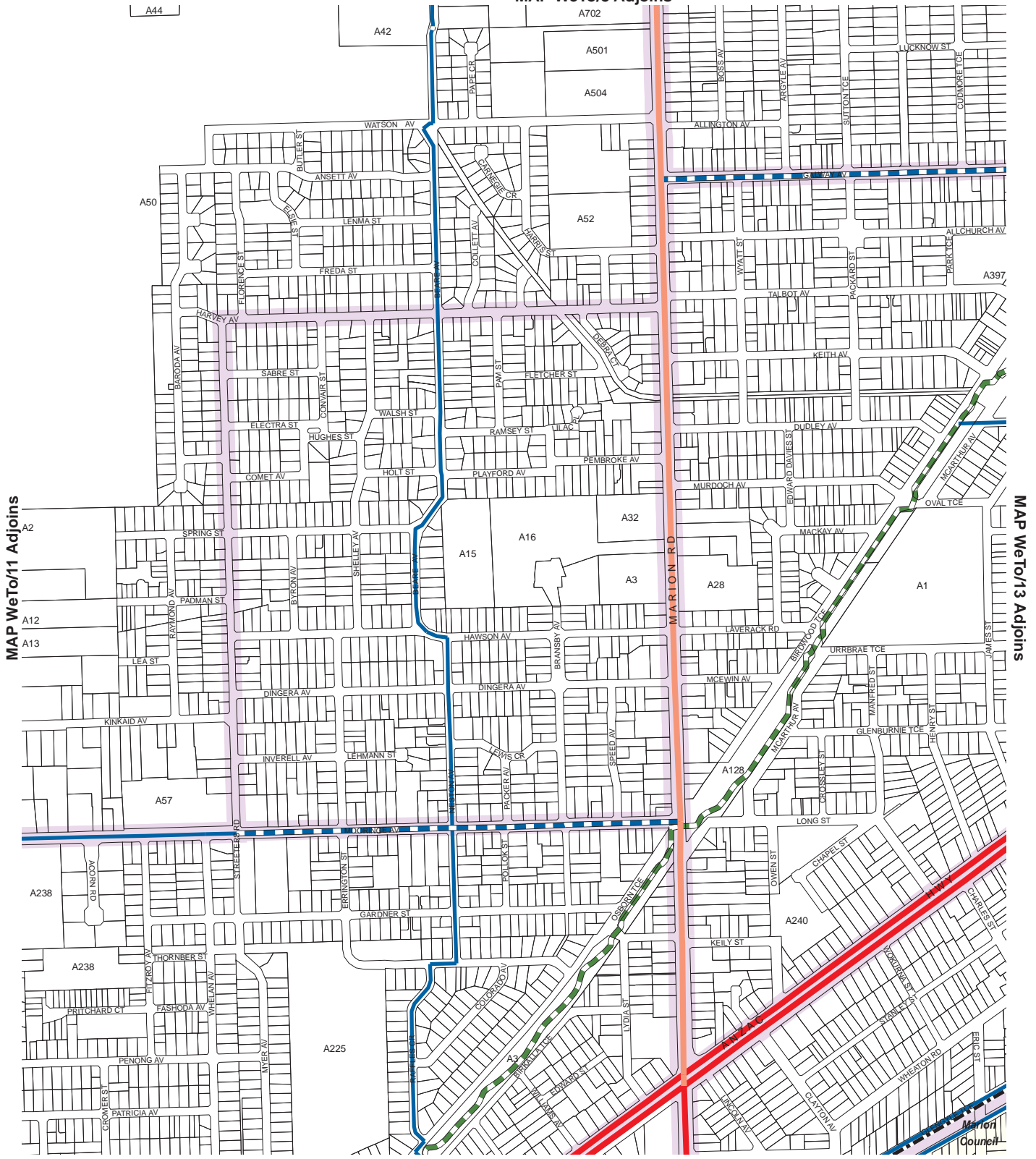
MAP WeTo/8 Adjoins



# Location Map WeTo/12

- S** School
- P** Post Office
- Railways
- Local Reserves
- Development Plan Boundary

MAP WeTo/8 Adjoins



MAP WeTo/11 Adjoins

MAP WeTo/13 Adjoins

MAP WeTo/15 Adjoins

- Primary Arterial Roads
- Secondary Arterial Roads
- Bikedirect Network
- Main Road - Bike Lane
- Main Road
- Secondary Road - Bike Lane
- Secondary Road
- Off Road Sealed Path
- Public Transport
- Development Plan Boundary

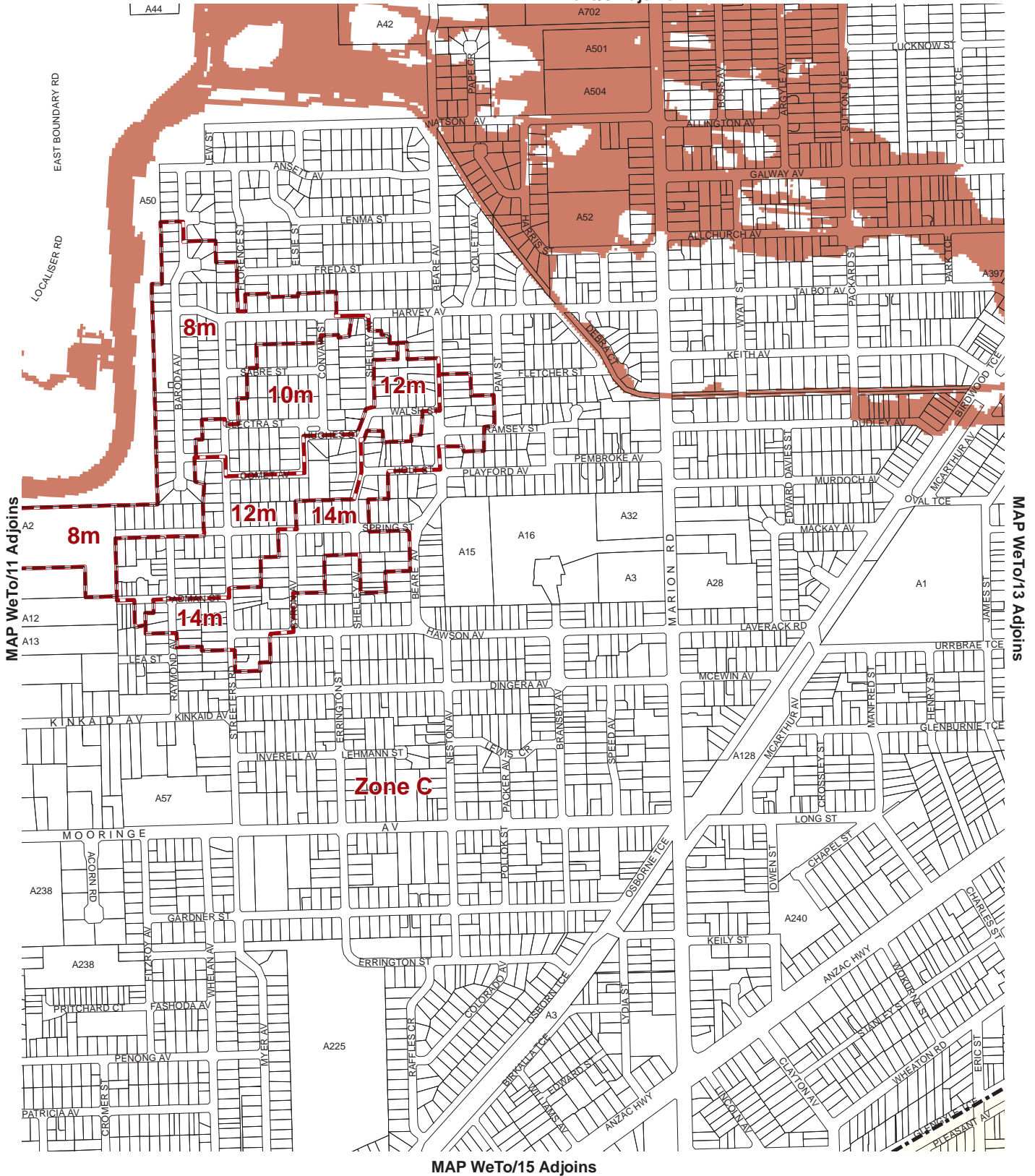


# Overlay Map WeTo/12

## TRANSPORT



MAP WeTo/8 Adjoins



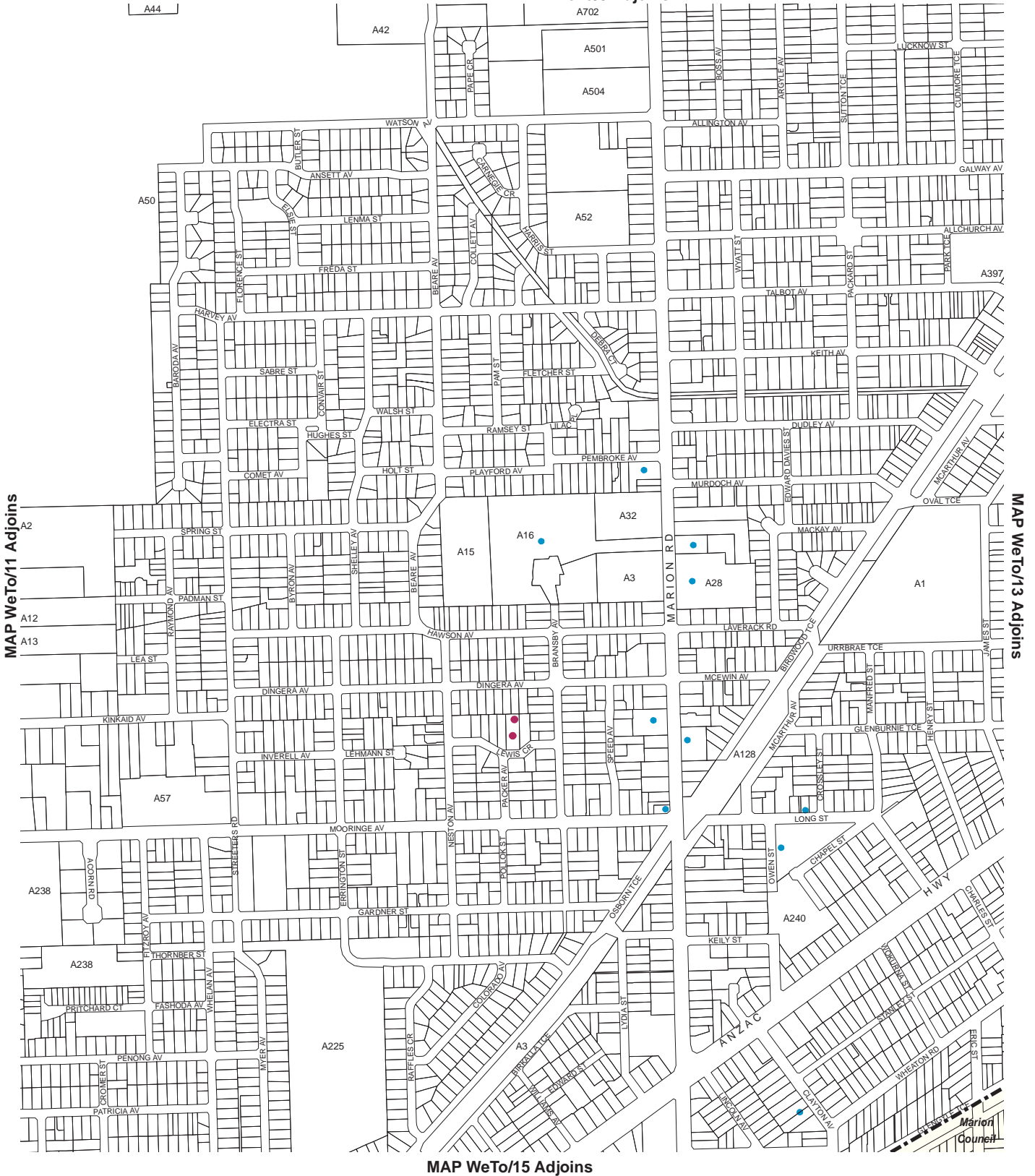
**Airport Building Heights**  
 8m - 14m All Structures restricted to height identified on maps  
 (above existing ground level, measured from the top of the nearest roadside curb)  
**Zone C** All Structures Exceeding 15 metres above existing ground level



# Overlay Map WeTo/12 DEVELOPMENT CONSTRAINTS

- Airport Building Heights
- Flood Hazard
- Development Plan Boundary

MAP WeTo/8 Adjoins



Heritage points are indicative only.  
 For further information on State and Local Heritage Places and Contributory  
 Items please refer to the relevant tables within this document.

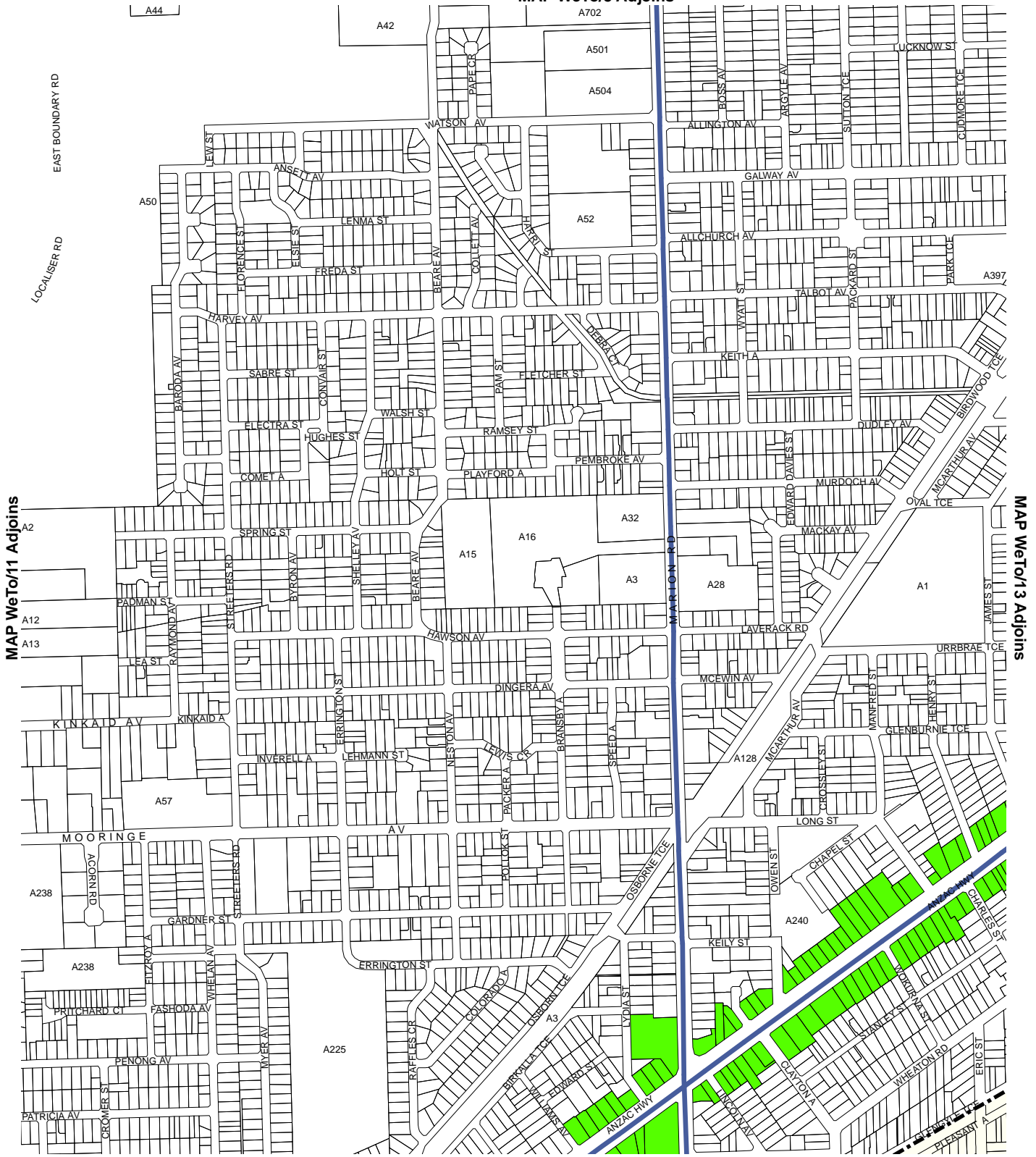


# Overlay Map WeTo/12 HERITAGE

- State heritage place
- Local heritage place
- Development Plan Boundary



MAP WeTo/8 Adjoins

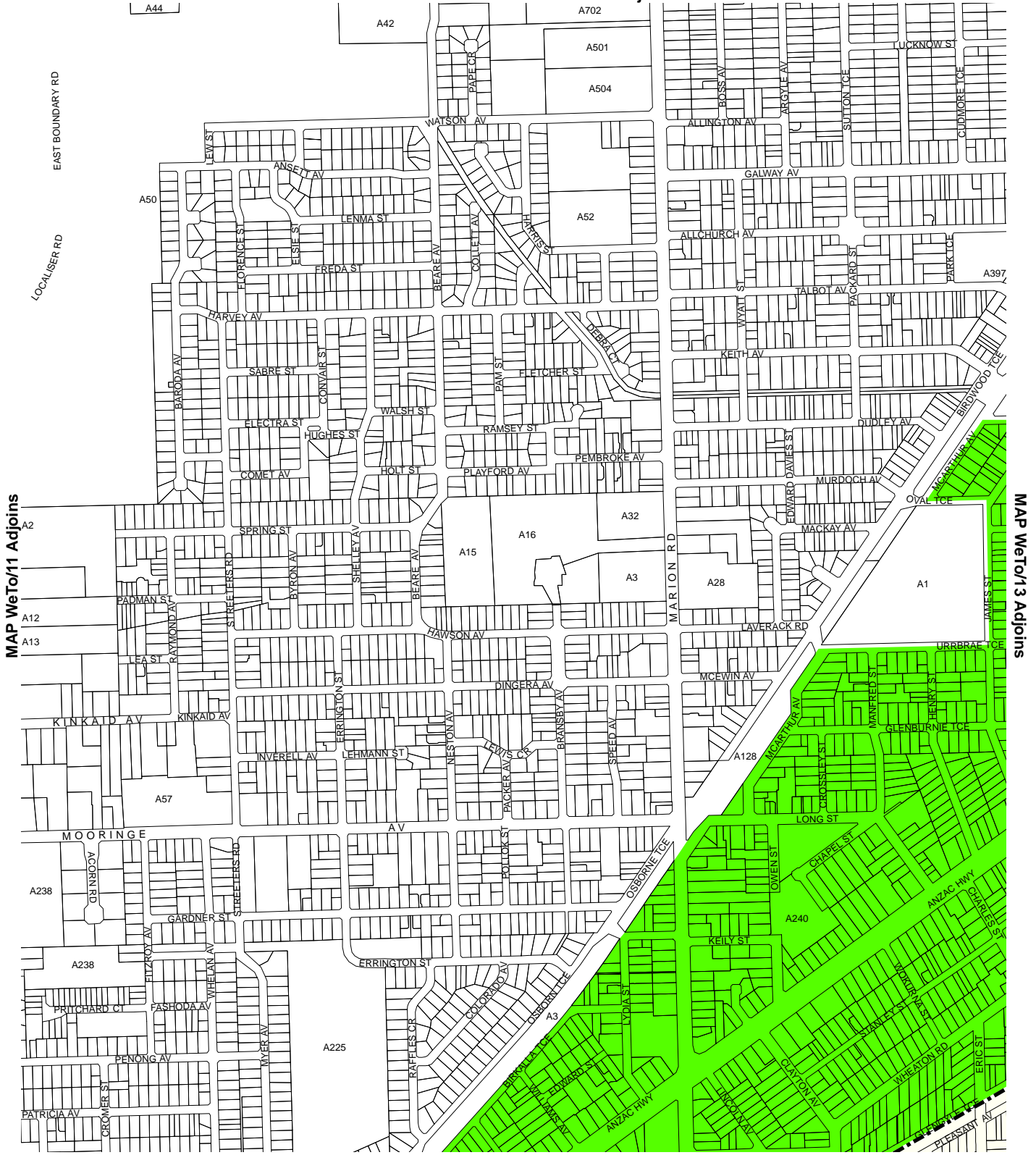


# Overlay Map WeTo/12

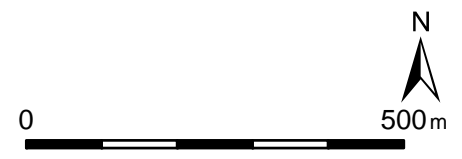
## STRATEGIC TRANSPORT ROUTES

- Strategic Transport Route
- Strategic Transport Routes Designated Area
- Development Plan Boundary

MAP WeTo/8 Adjoins



MAP WeTo/15 Adjoins



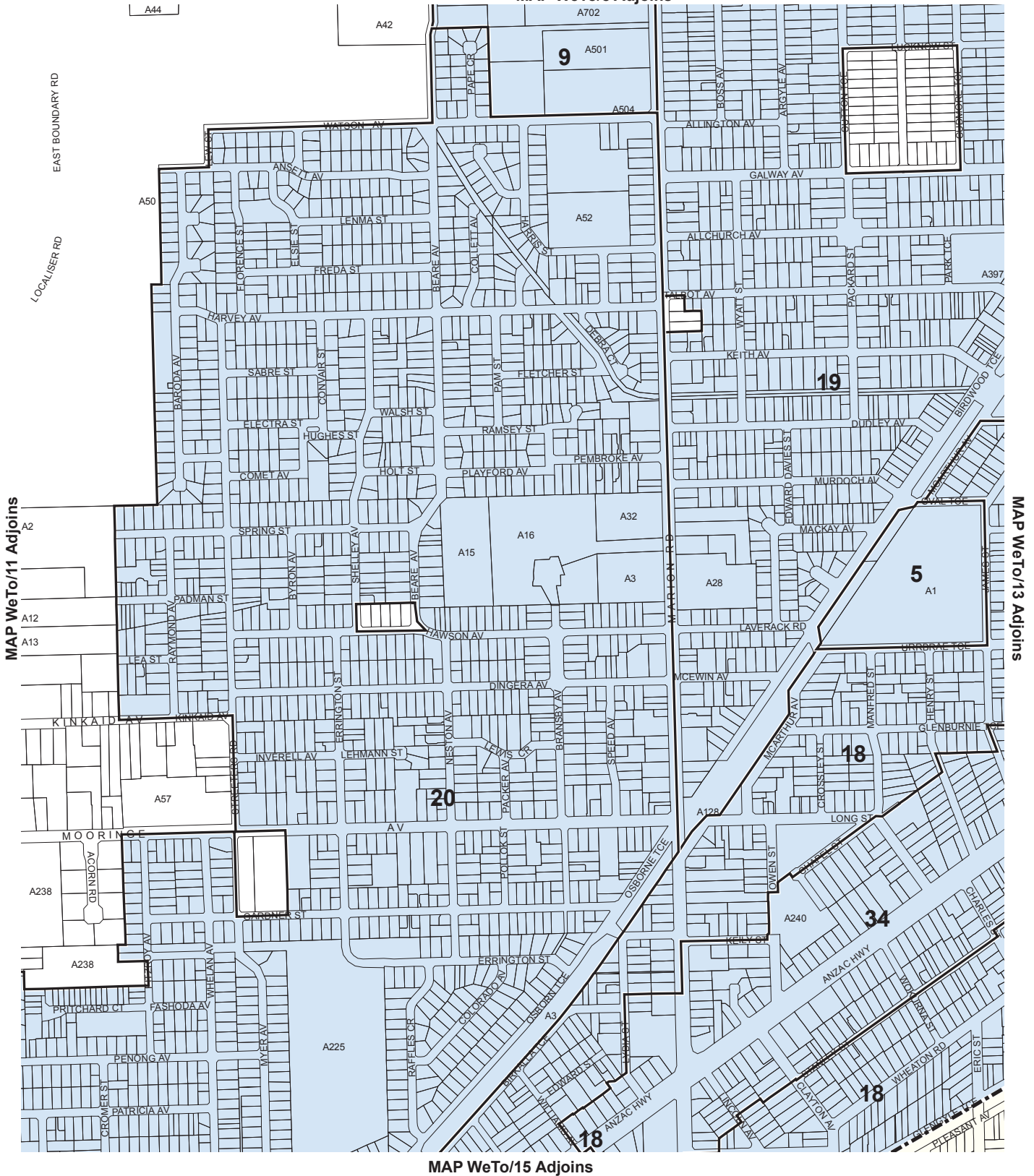
# Overlay Map WeTo/12

## AFFORDABLE HOUSING

- Affordable Housing Designated Area
- Development Plan Boundary



MAP WeTo/8 Adjoins



Lamberts Conformal Conic Projection, GDA94

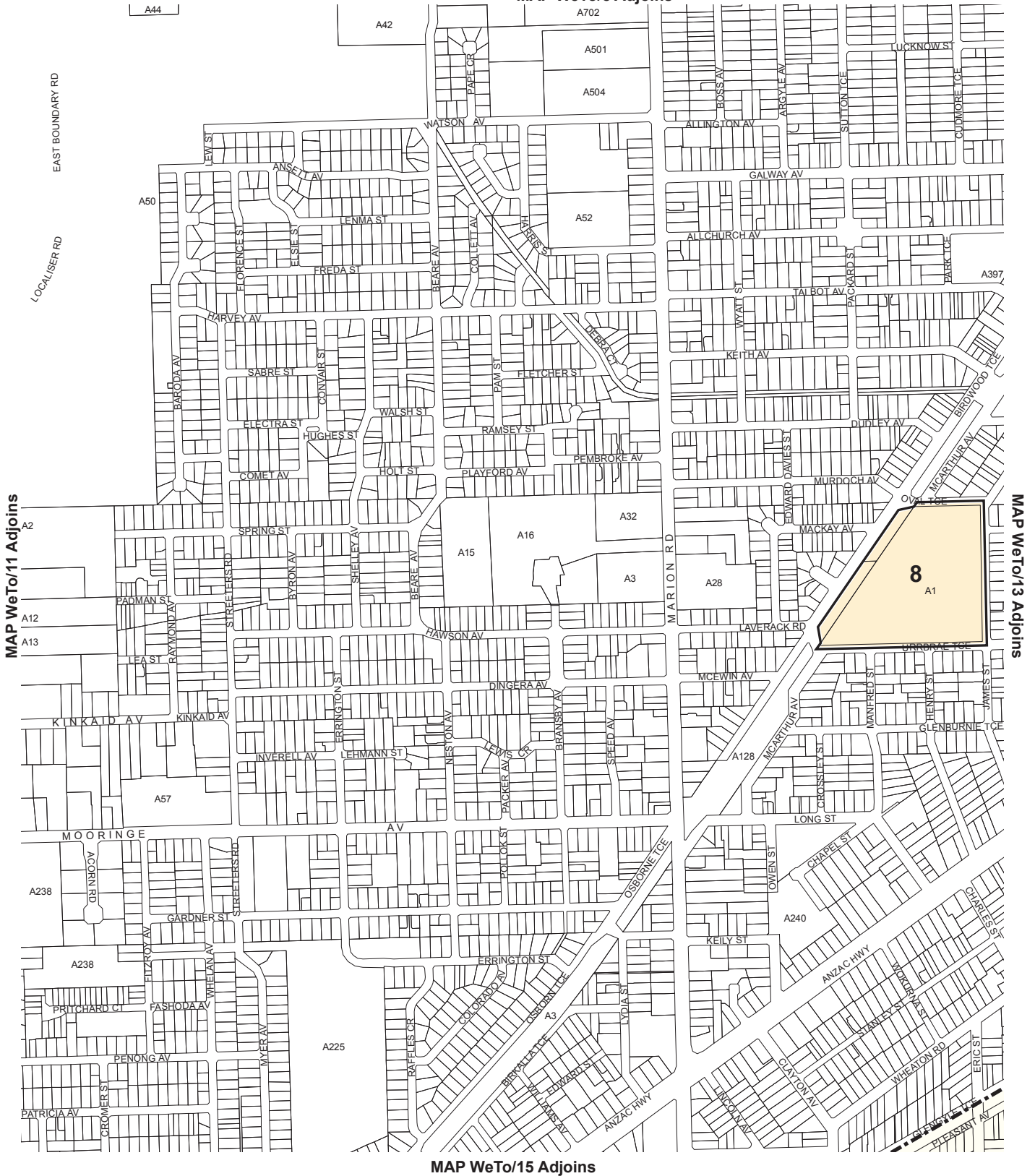
- Policy Area**
- 18 Medium Density
  - 19 Medium Density
  - 20 Low Density
  - 34 Boulevard
  - 5 Recreation
  - 9 Netley



# Policy Area Map WeTo/12

- Policy Area Boundary
- Development Plan Boundary

MAP WeTo/8 Adjoins



# Precinct Map WeTo/12

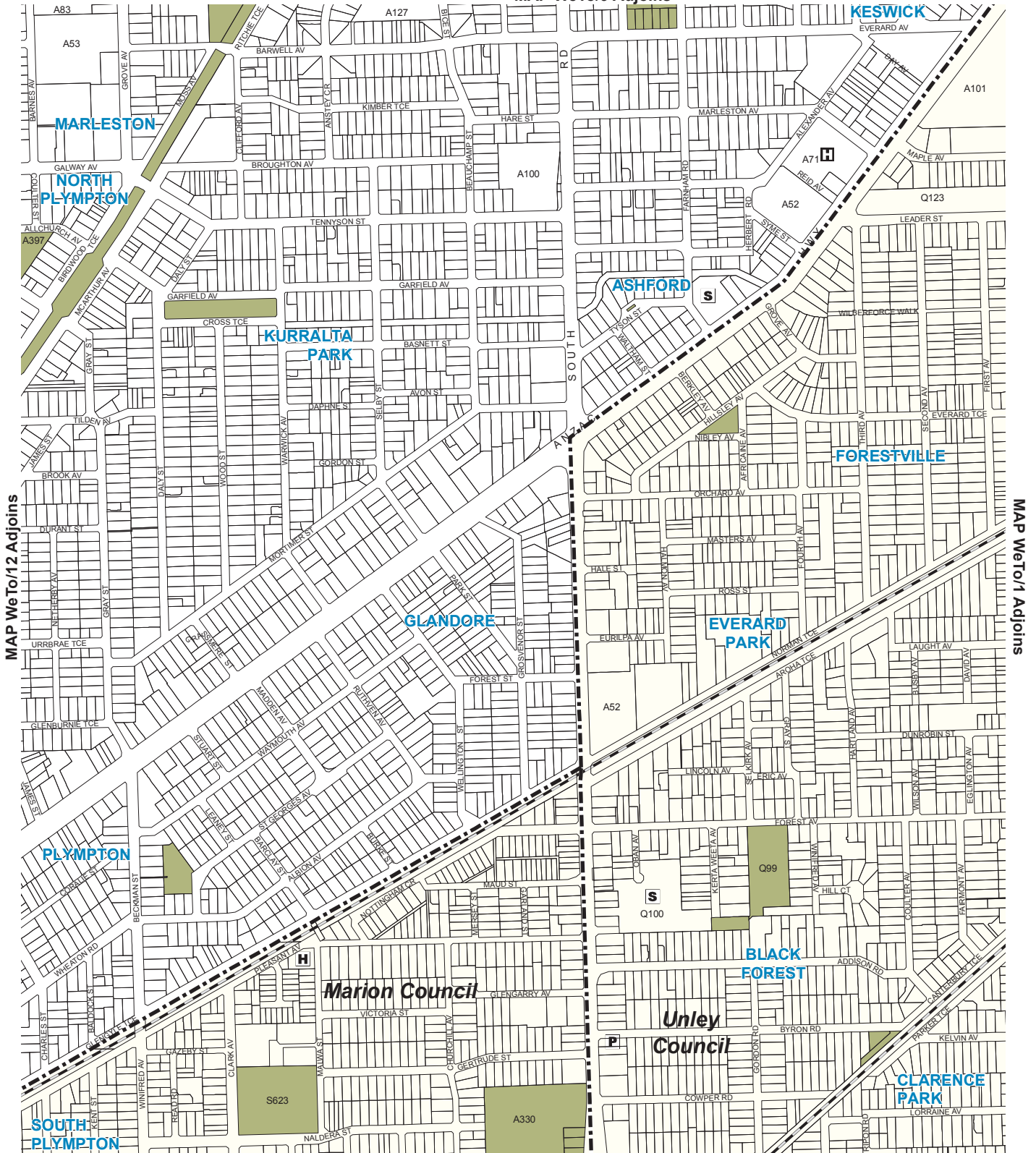
Lamberts Conformal Conic Projection, GDA94

**Precinct**  
8 Open Space

- Precinct Boundary
- Development Plan Boundary



MAP WeTo/9 Adjoins



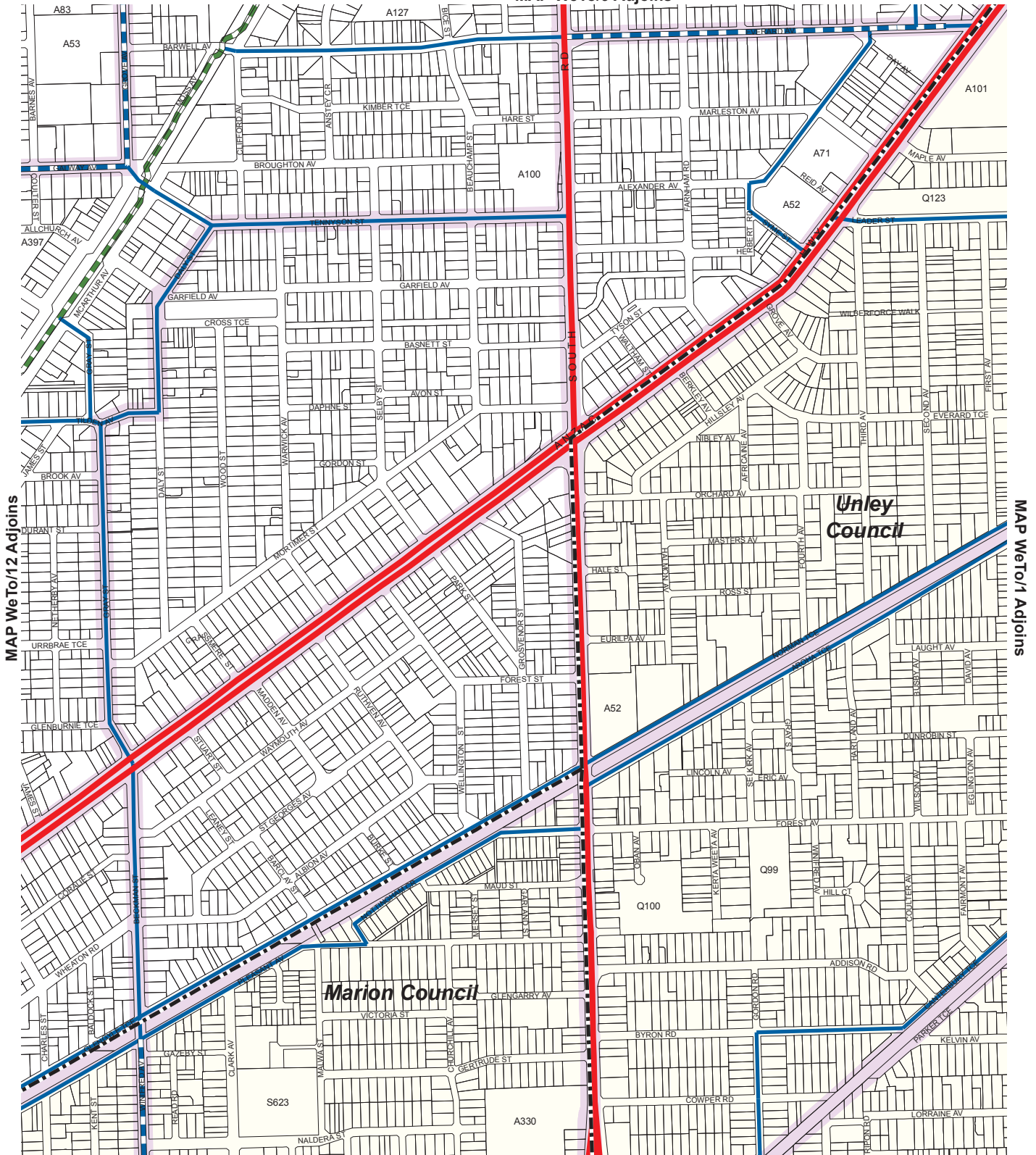
MAP WeTo/1 Adjoins



- S** School
- P** Post Office
- H** Other Health Services
- H** Hospital
- Railways
- Local Reserves
- - - - - Development Plan Boundary

# Location Map WeTo/13

MAP WeTo/9 Adjoins



MAP WeTo/1 Adjoins

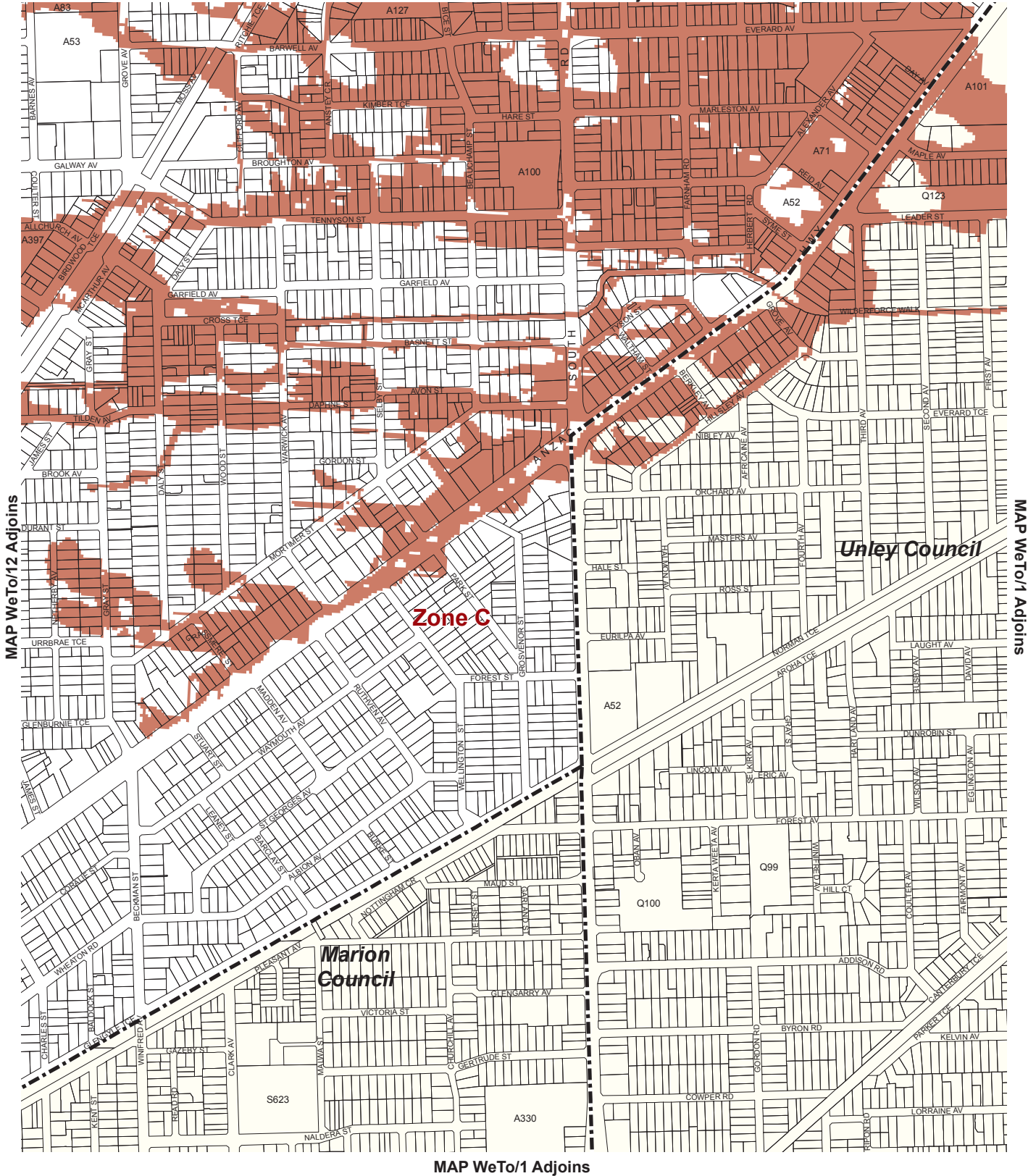
- Primary Arterial Roads
- Bikedirect Network**
- Main Road - Bike Lane
- Main Road
- Secondary Road - Bike Lane
- Secondary Road
- Off Road Sealed Path
- Public Transport
- Development Plan Boundary



# Overlay Map WeTo/13

## TRANSPORT

MAP WeTo/9 Adjoins



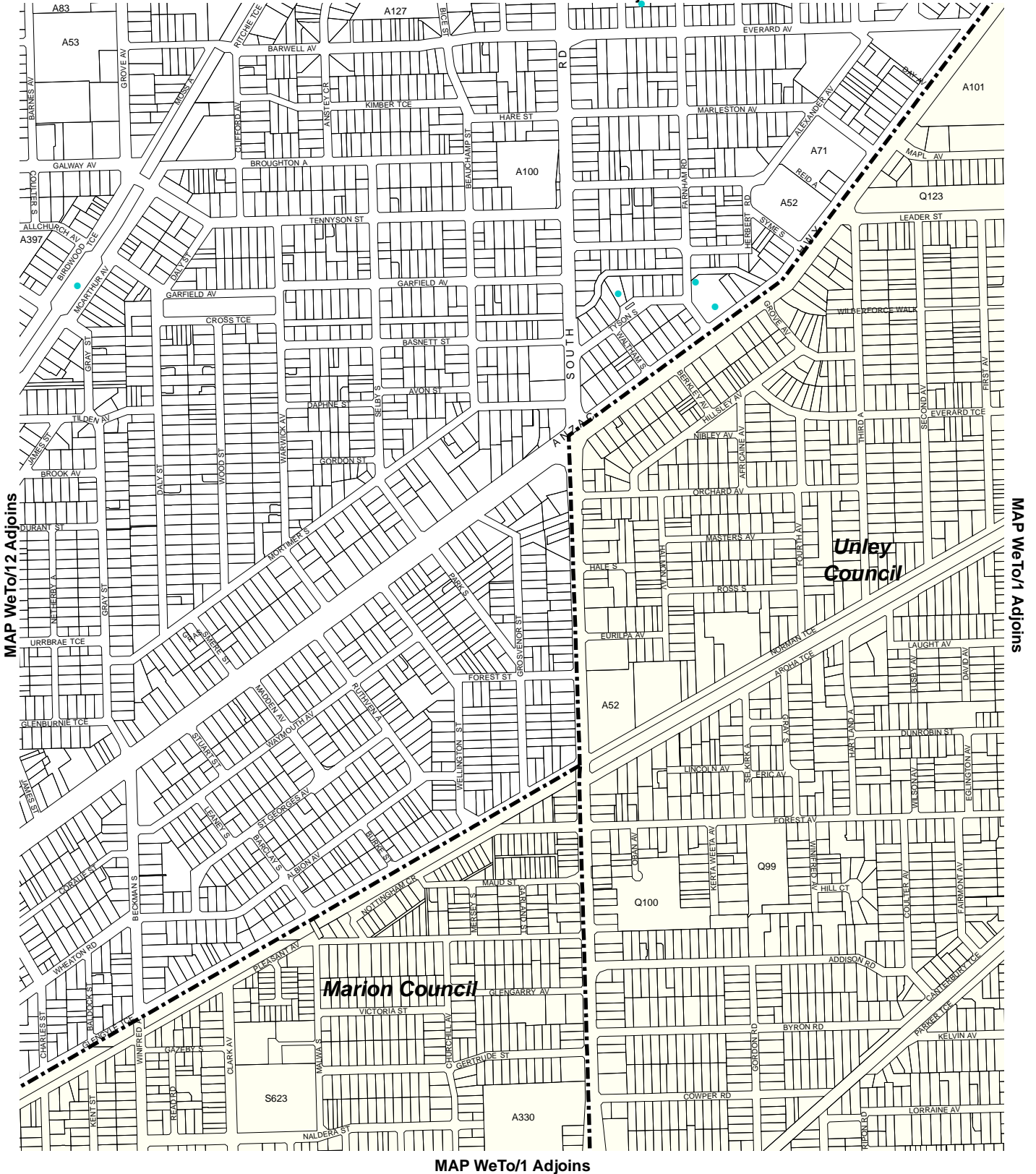
**Airport Building Heights**  
**Zone C** All Structures Exceeding 15 metres above existing ground level



# Overlay Map WeTo/13 DEVELOPMENT CONSTRAINTS

- Airport Building Heights
- Flood Hazard
- Development Plan Boundary

MAP WeTo/9 Adjoins



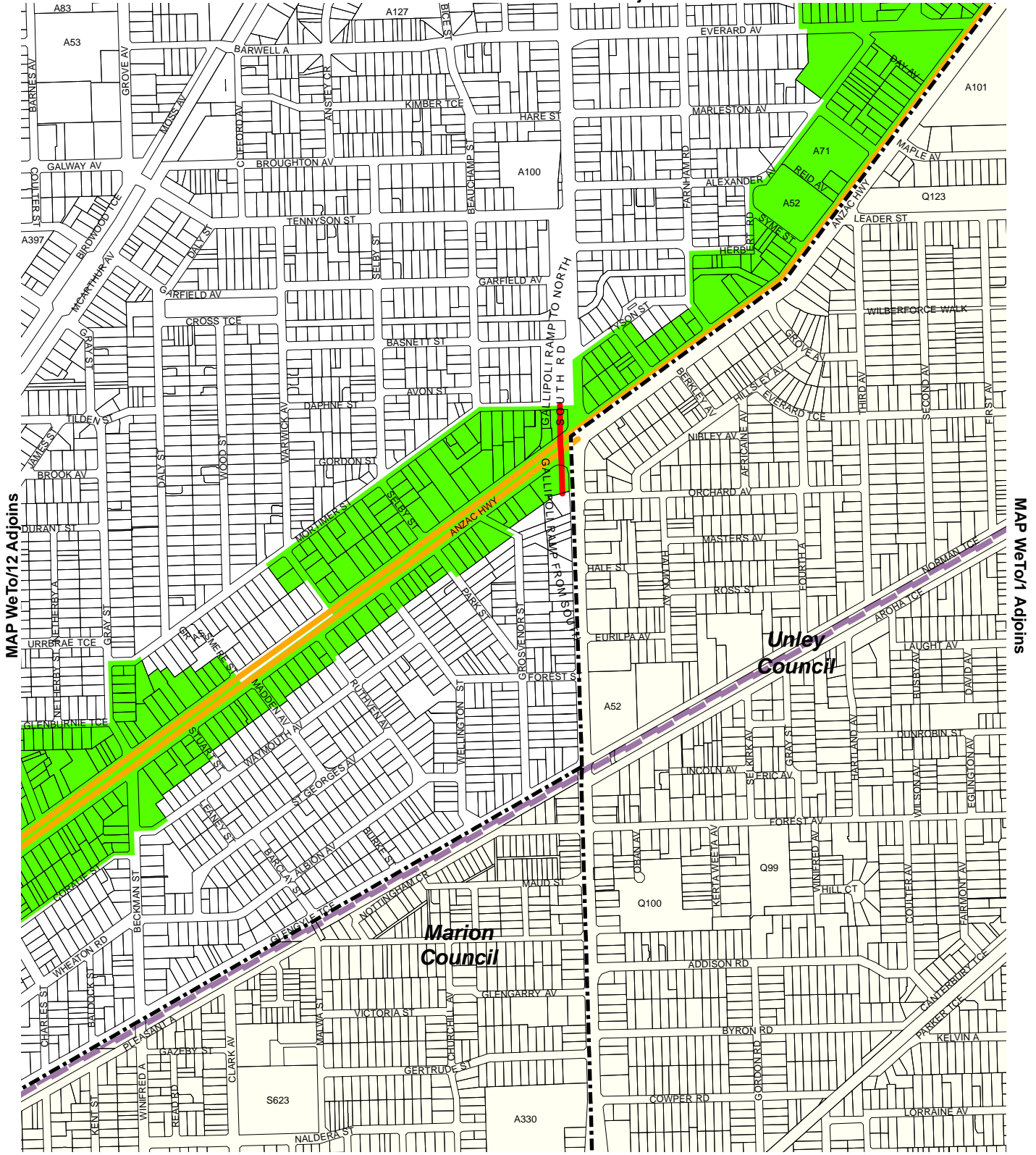
Heritage points are indicative only.  
 For further information on State and Local Heritage Places and Contributory  
 Items please refer to the relevant tables within this document.








# Overlay Map WeTo/13 HERITAGE

- Local heritage place
- Development Plan Boundary

MAP WeTo/9 Adjoins



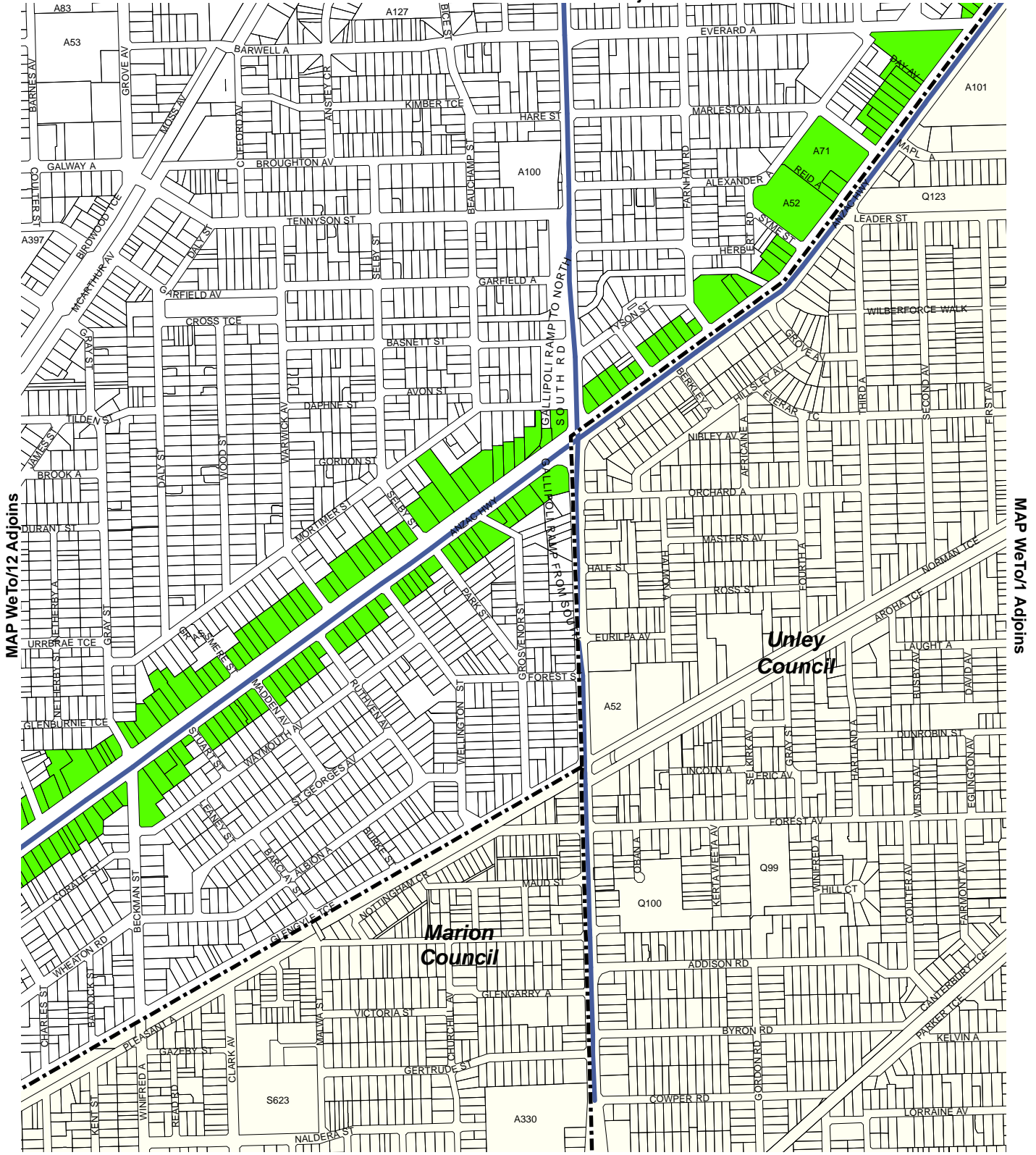
MAP WeTo/1 Adjoins

-  Tram Line
-  Designated Road: type A road
-  Designated Road: type B road
-  Noise and Air Emissions Designated Area
-  Development Plan Boundary

# Overlay Map WeTo/13

## NOISE AND AIR EMISSIONS

MAP WeTo/9 Adjoins

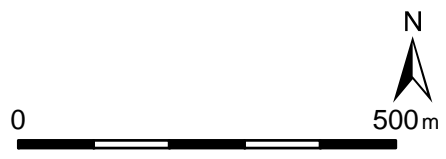


MAP WeTo/1 Adjoins

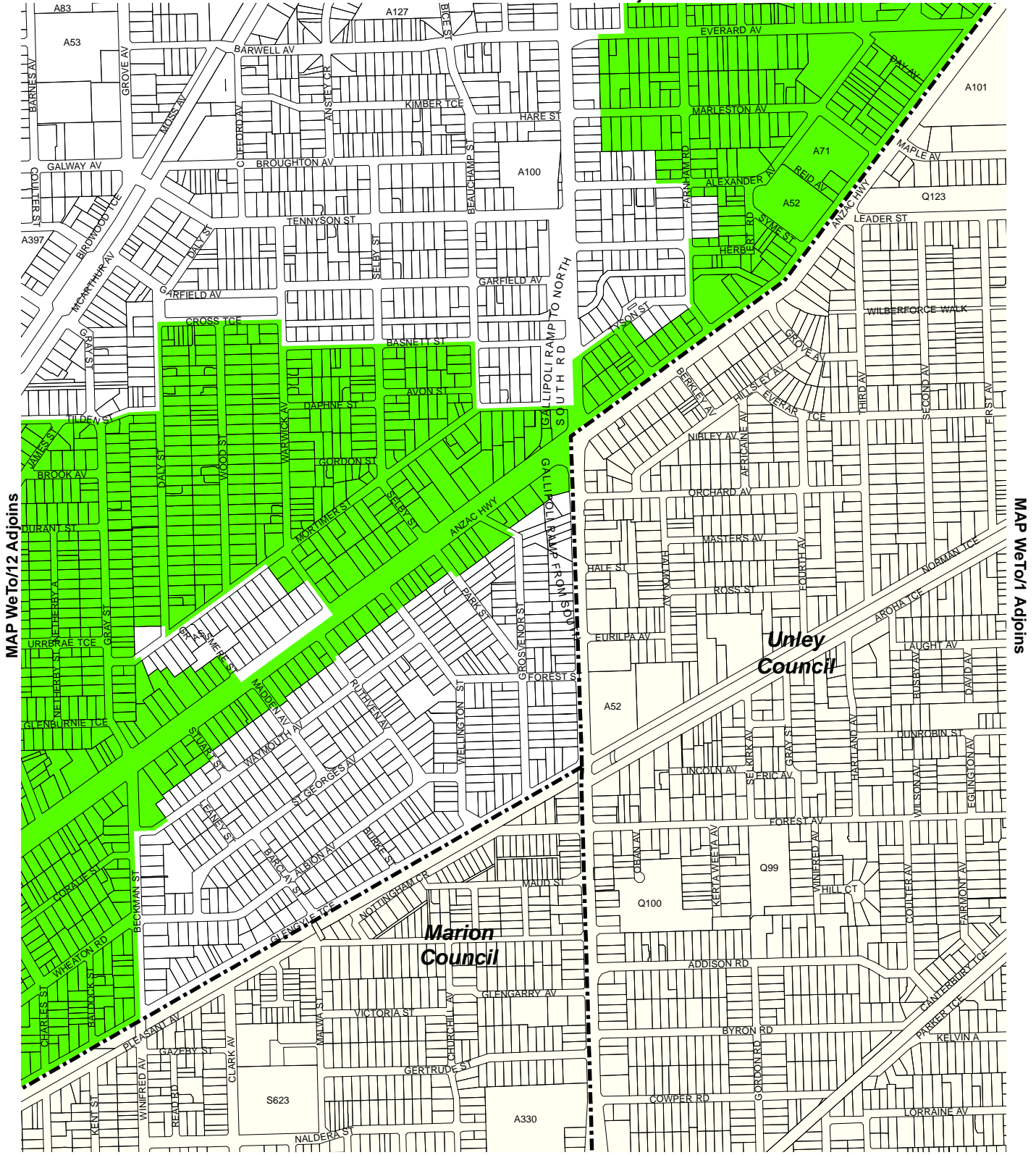
- Strategic Transport Route
- Strategic Transport Routes Designated Area
- Development Plan Boundary

# Overlay Map WeTo/13

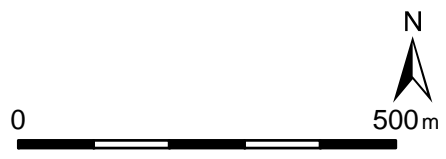
## STRATEGIC TRANSPORT ROUTES



MAP WeTo/9 Adjoins



MAP WeTo/1 Adjoins

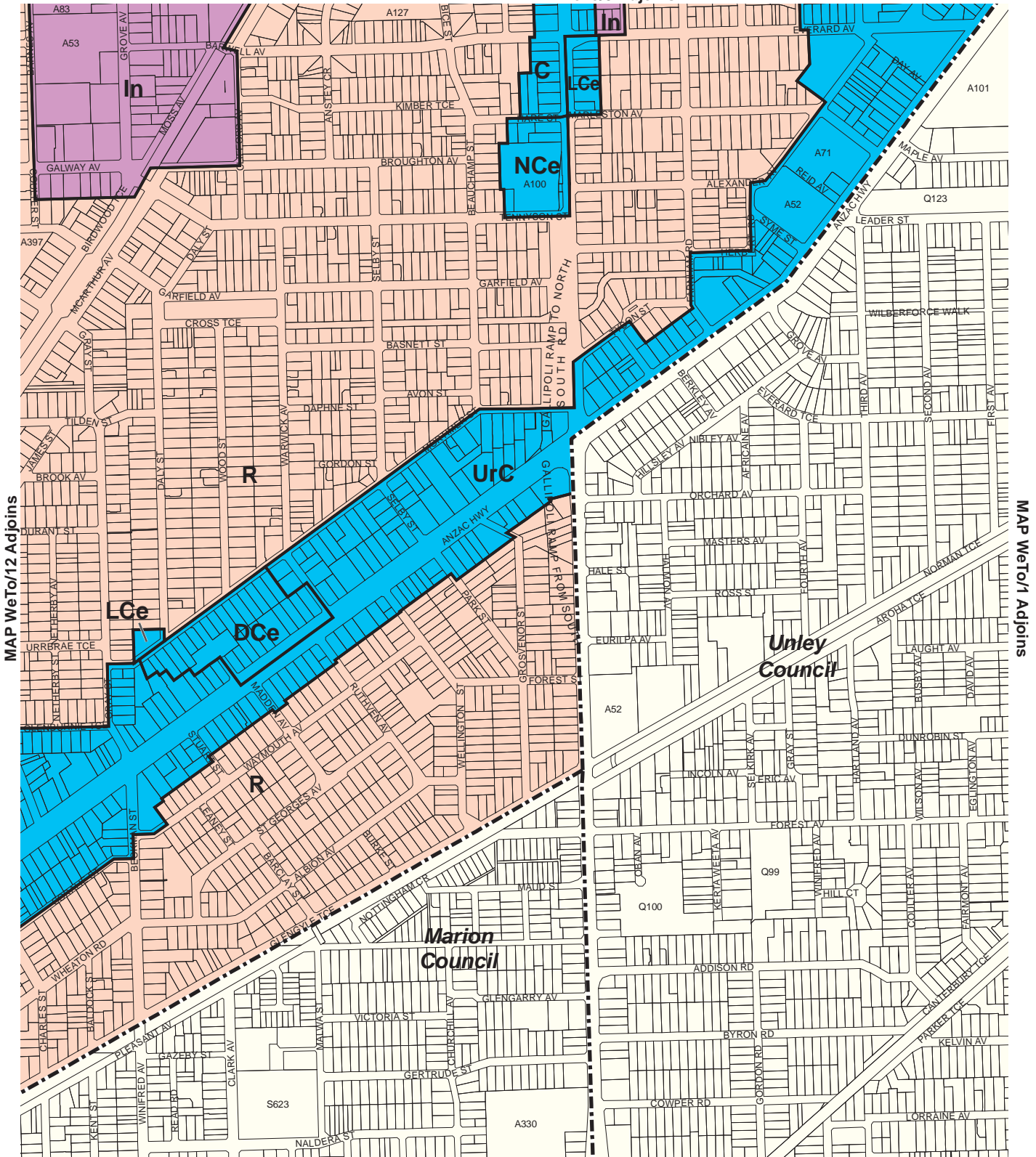


# Overlay Map WeTo/13

## AFFORDABLE HOUSING

- Affordable Housing Designated Area
- Development Plan Boundary

MAP WeTo/9 Adjoins



MAP WeTo/1 Adjoins

Lamberts Conformal Conic Projection, GDA94

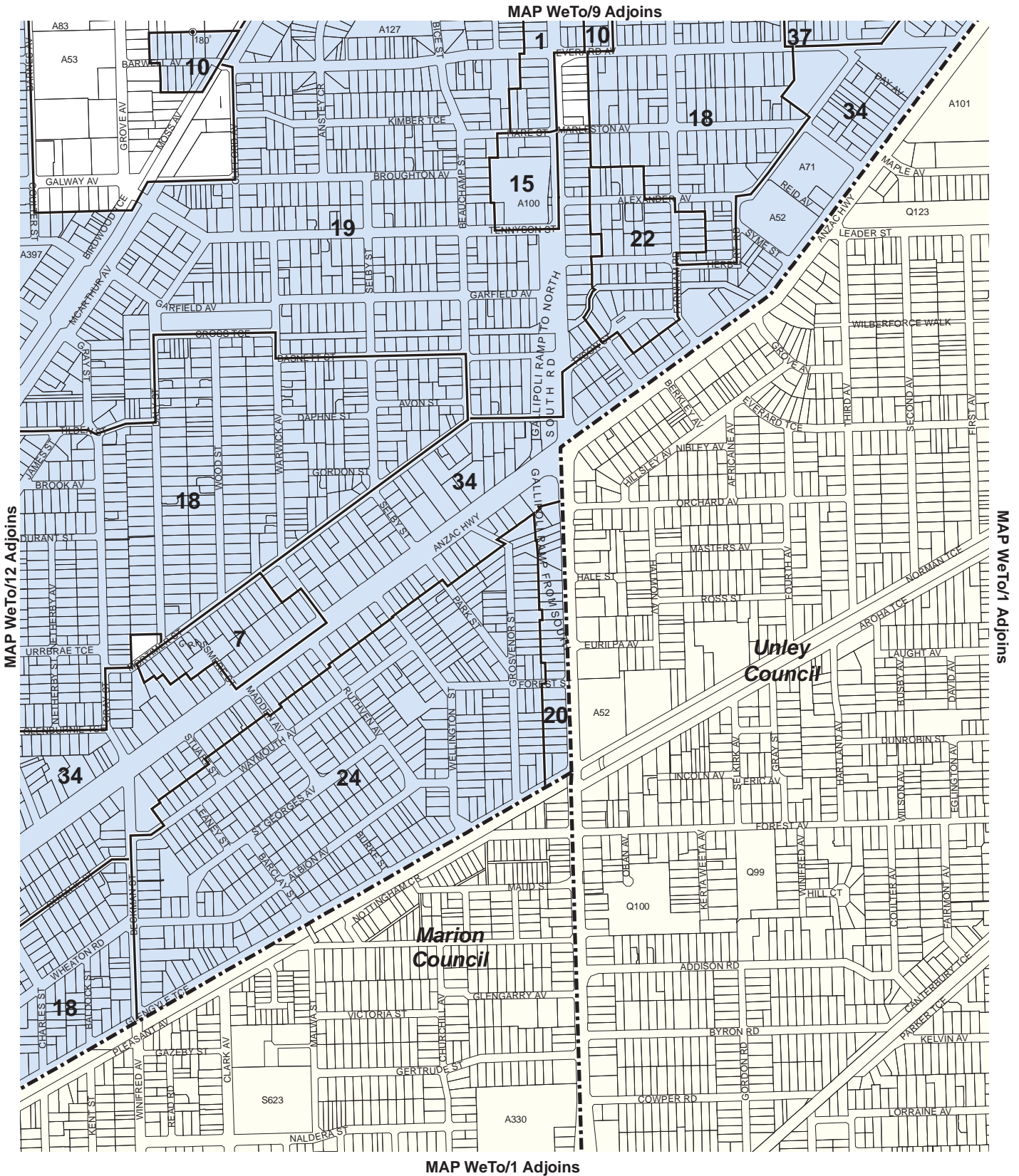
Zones

- Commercial
- District Centre
- Industry
- Local Centre
- Neighbourhood Centre
- Residential
- Urban Corridor
- Zone Boundary
- Development Plan Boundary



# Zone Map WeTo/13





Lamberts Conformal Conic Projection, GDA94

**Policy Area**

- 1 Arterial Roads
- 10 Mixed Use
- 15 Tennyson Street
- 18 Medium Density
- 19 Medium Density
- 20 Low Density
- 22 Ashford Character
- 24 Glandore Character
- 34 Boulevard
- 37 Business
- 7 Kurralta Park

- Policy Area Boundary
- Development Plan Boundary



# Policy Area Map WeTo/13

MAP WeTo/9 Adjoins



MAP WeTo/12 Adjoins

MAP WeTo/1 Adjoins

MAP WeTo/1 Adjoins

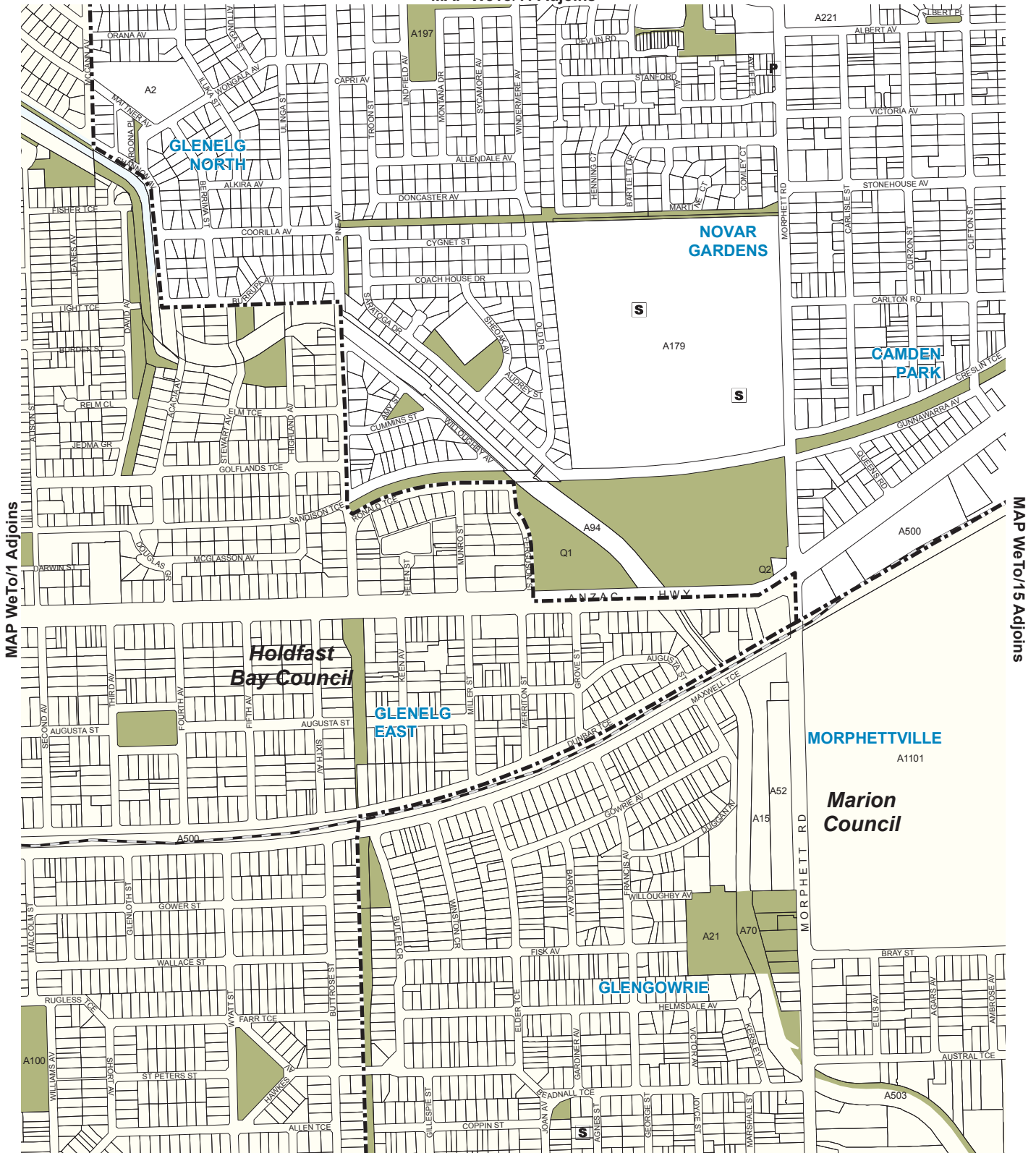
Lamberts Conformal Conic Projection, GDA94  
**Precinct**  
 4 South Road (Keswick)



# Precinct Map WeTo/13

- Precinct Boundary
- Development Plan Boundary

MAP WeTo/11 Adjoins



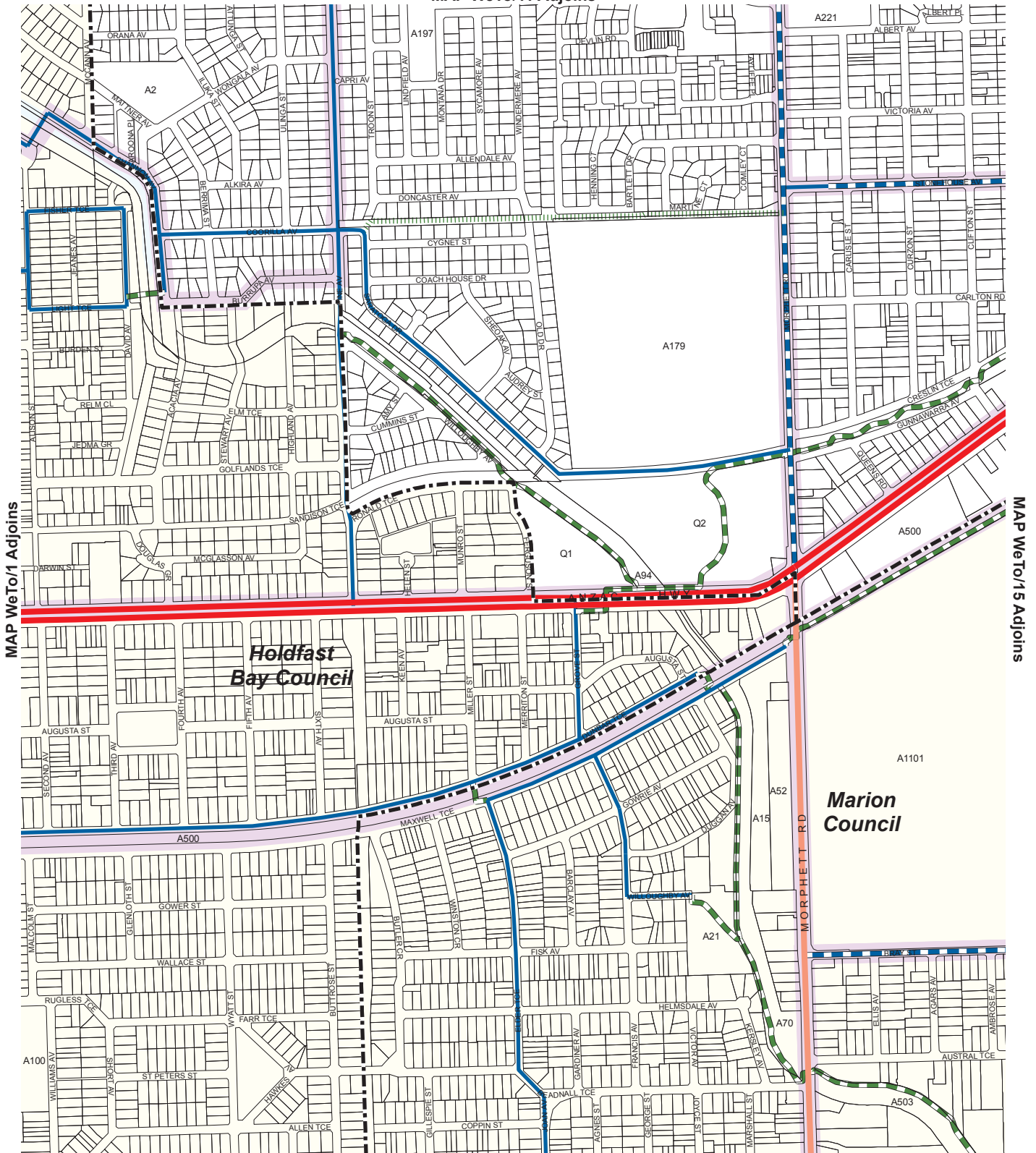
MAP WeTo/1 Adjoins



- S** School
- P** Post Office
- Railways
- Local Reserves
- Development Plan Boundary

# Location Map WeTo/14

MAP WeTo/11 Adjoins



MAP WeTo/1 Adjoins

MAP WeTo/15 Adjoins

MAP WeTo/1 Adjoins

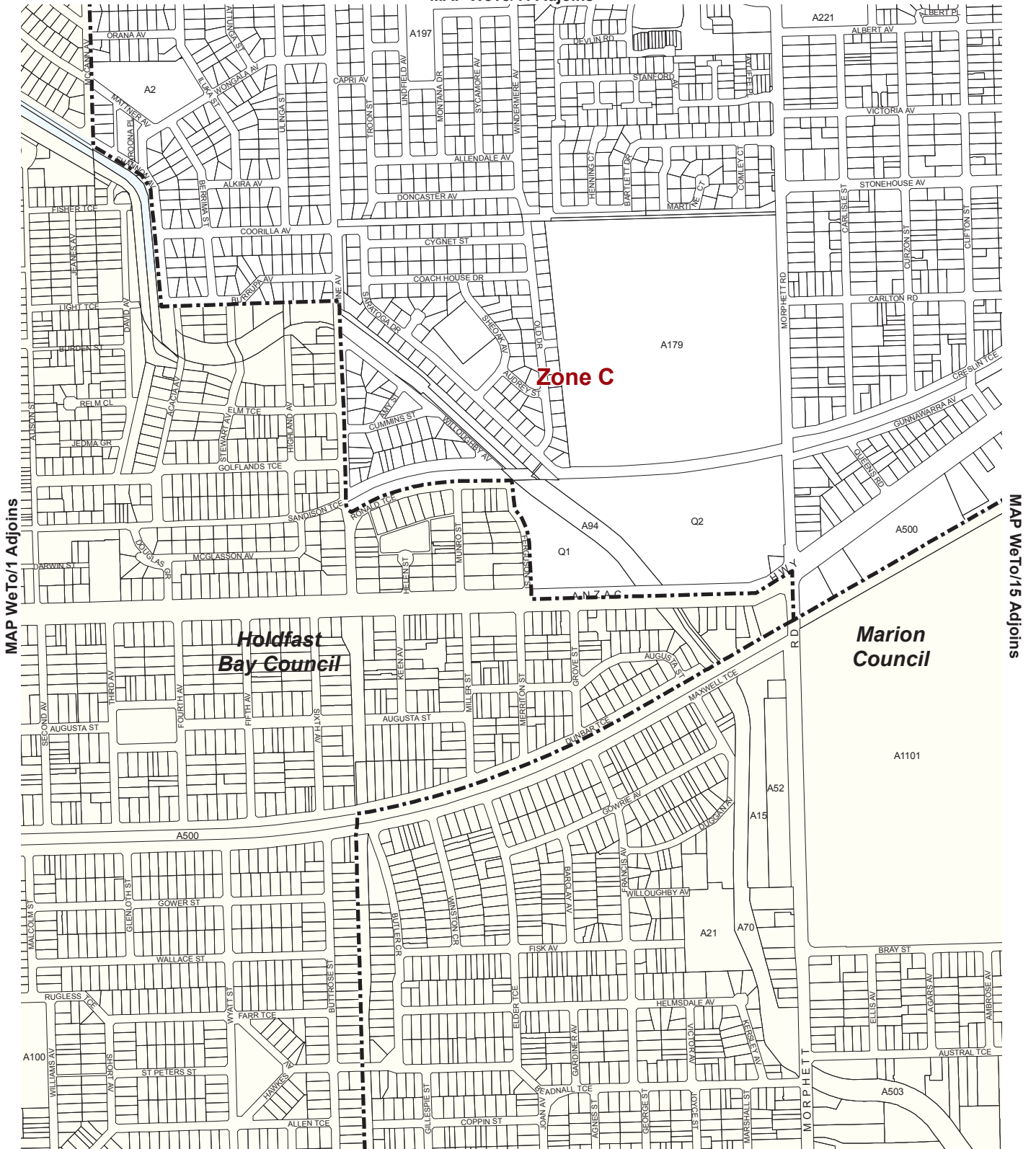
- Primary Arterial Roads
- Secondary Arterial Roads
- Bikedirect Network**
- Main Road - Bike Lane
- Main Road
- Secondary Road - Bike Lane
- Secondary Road
- Off Road Sealed Path
- Off Road Track
- Public Transport
- Development Plan Boundary



# Overlay Map WeTo/14

## TRANSPORT

MAP WeTo/11 Adjoins



MAP WeTo/1 Adjoins

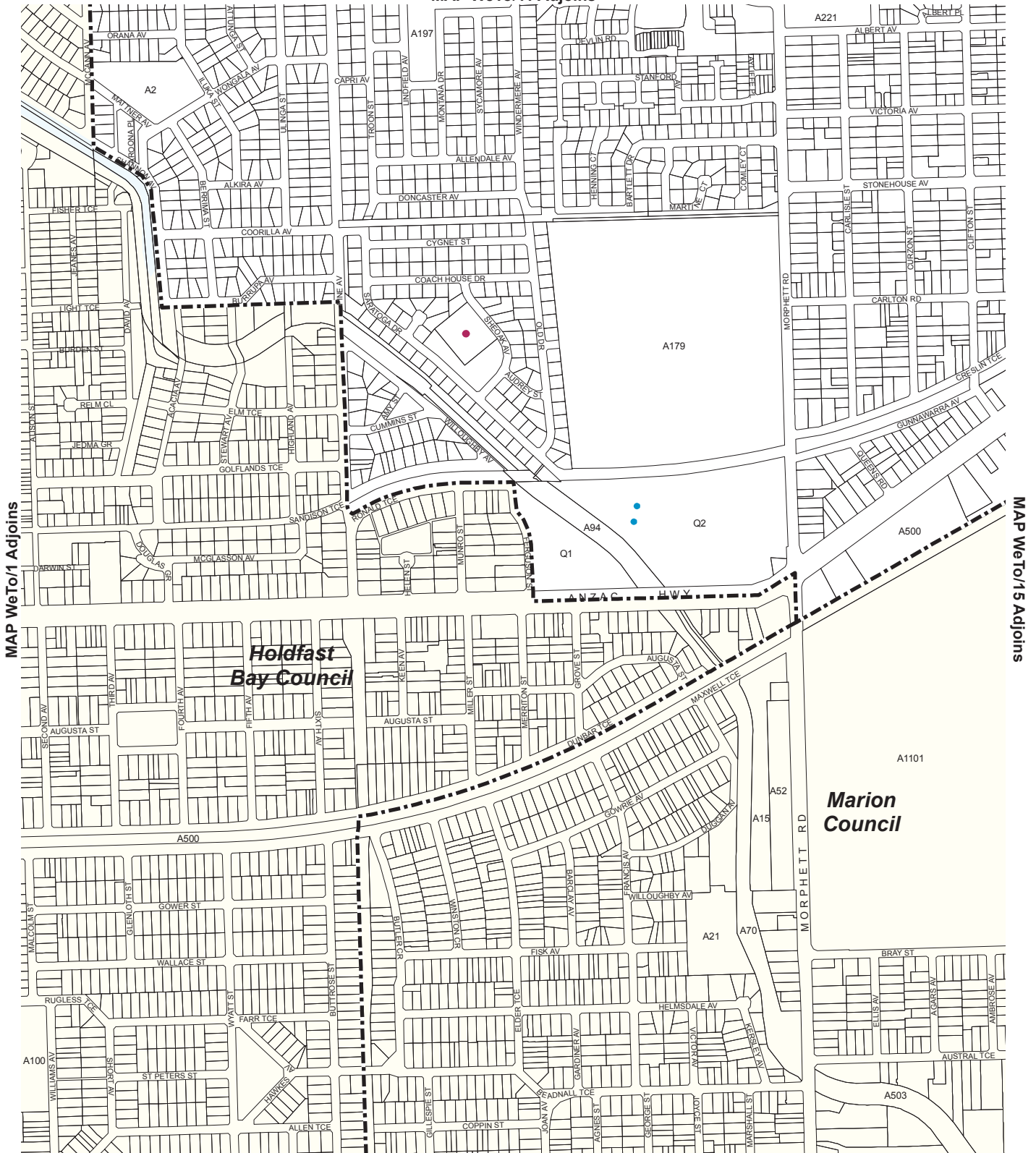
**Airport Building Heights**  
**Zone C** All Structures Exceeding 15 metres above existing ground level



# Overlay Map WeTo/14 DEVELOPMENT CONSTRAINTS

- Airport Building Heights
- Development Plan Boundary

MAP WeTo/11 Adjoins



Heritage points are indicative only.  
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 Items please refer to the relevant tables within this document.

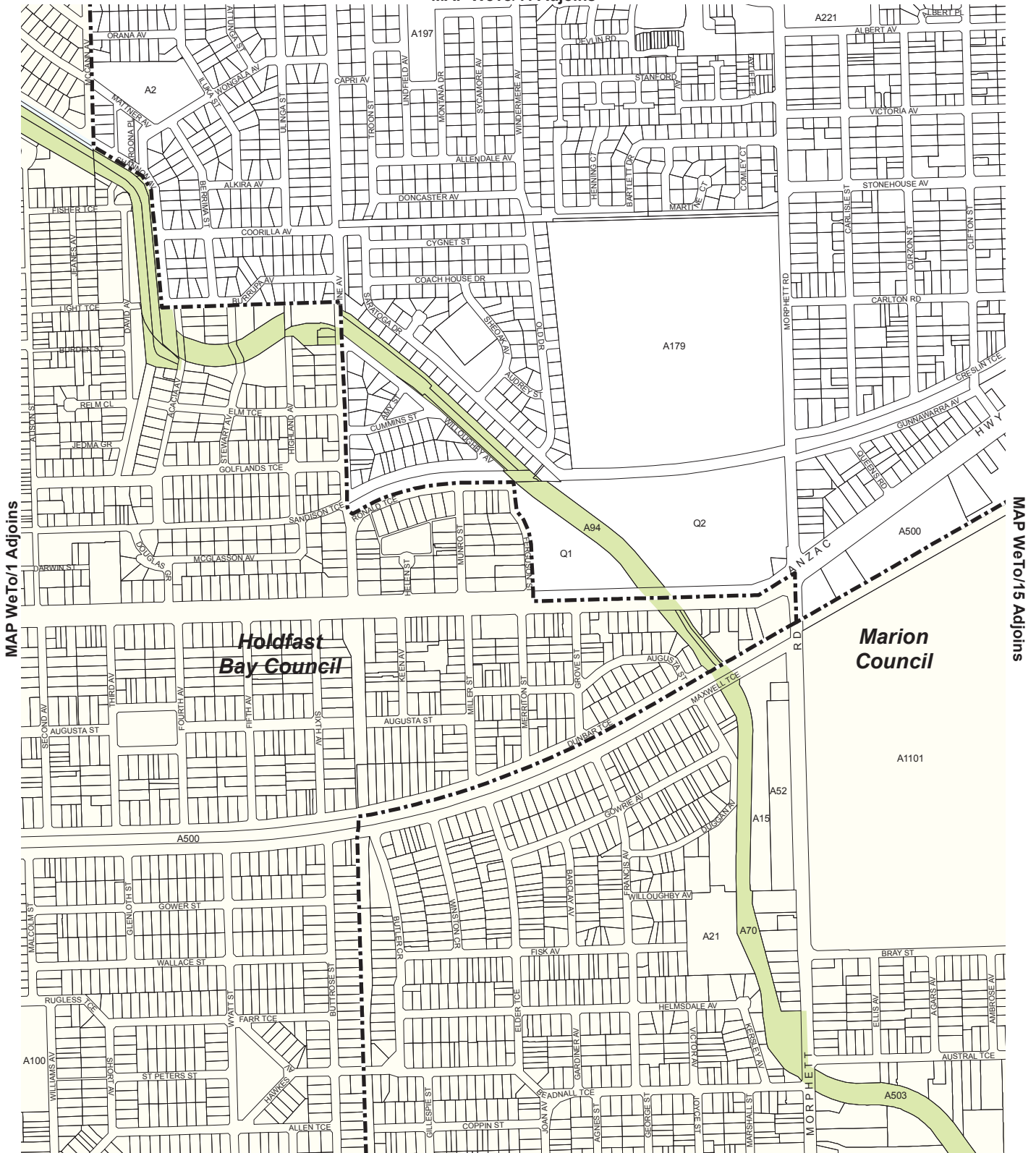


- State heritage place
- Local heritage place
- Development Plan Boundary

# Overlay Map WeTo/14

## HERITAGE

MAP WeTo/11 Adjoins



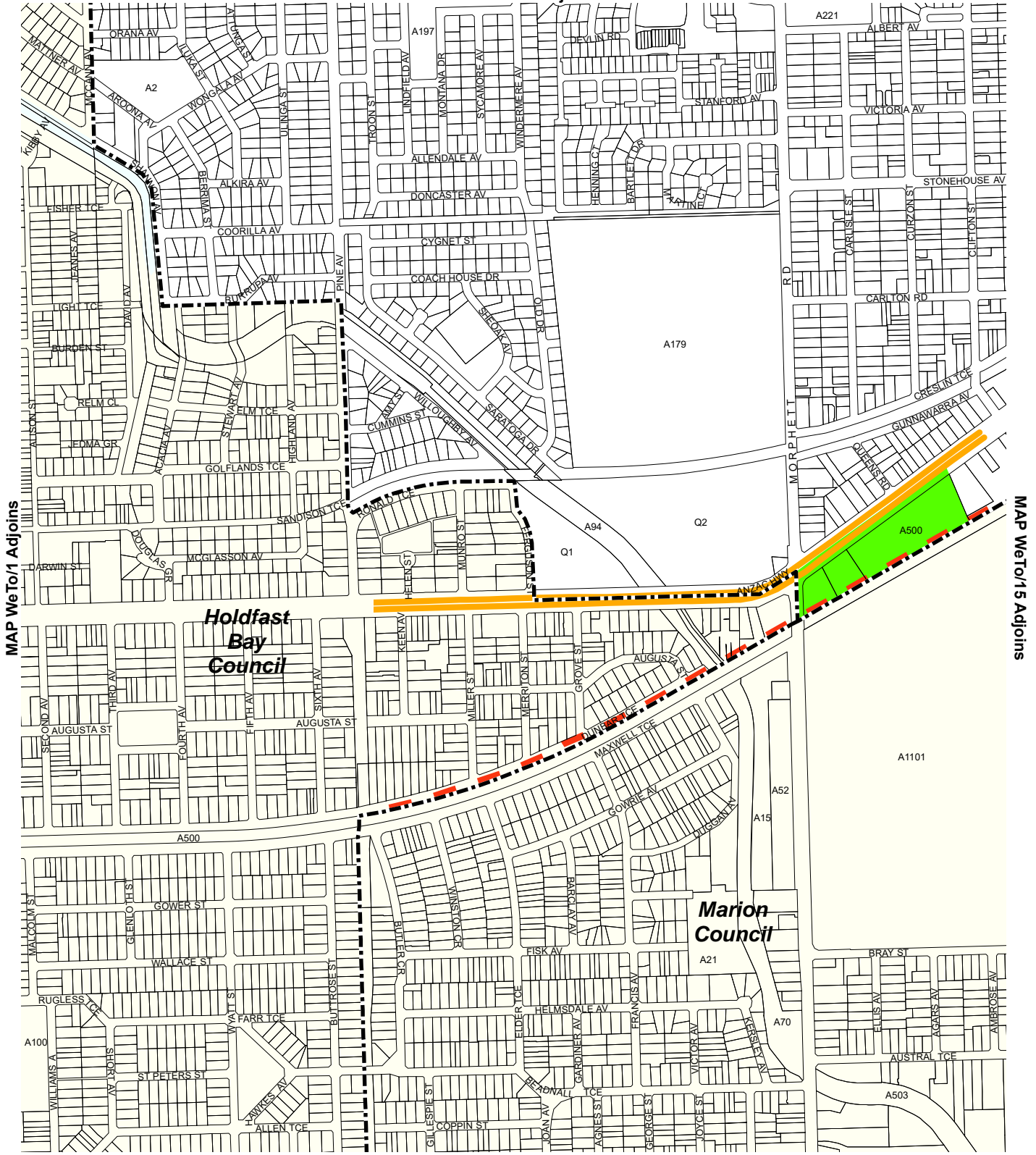
MAP WeTo/1 Adjoins



# Overlay Map WeTo/14 NATURAL RESOURCES

- MOSS Study Area
- Development Plan Boundary

MAP WeTo/11 Adjoins



MAP WeTo/1 Adjoins

MAP WeTo/15 Adjoins

**Holdfast Bay Council**

**Marion Council**

MAP WeTo/1 Adjoins

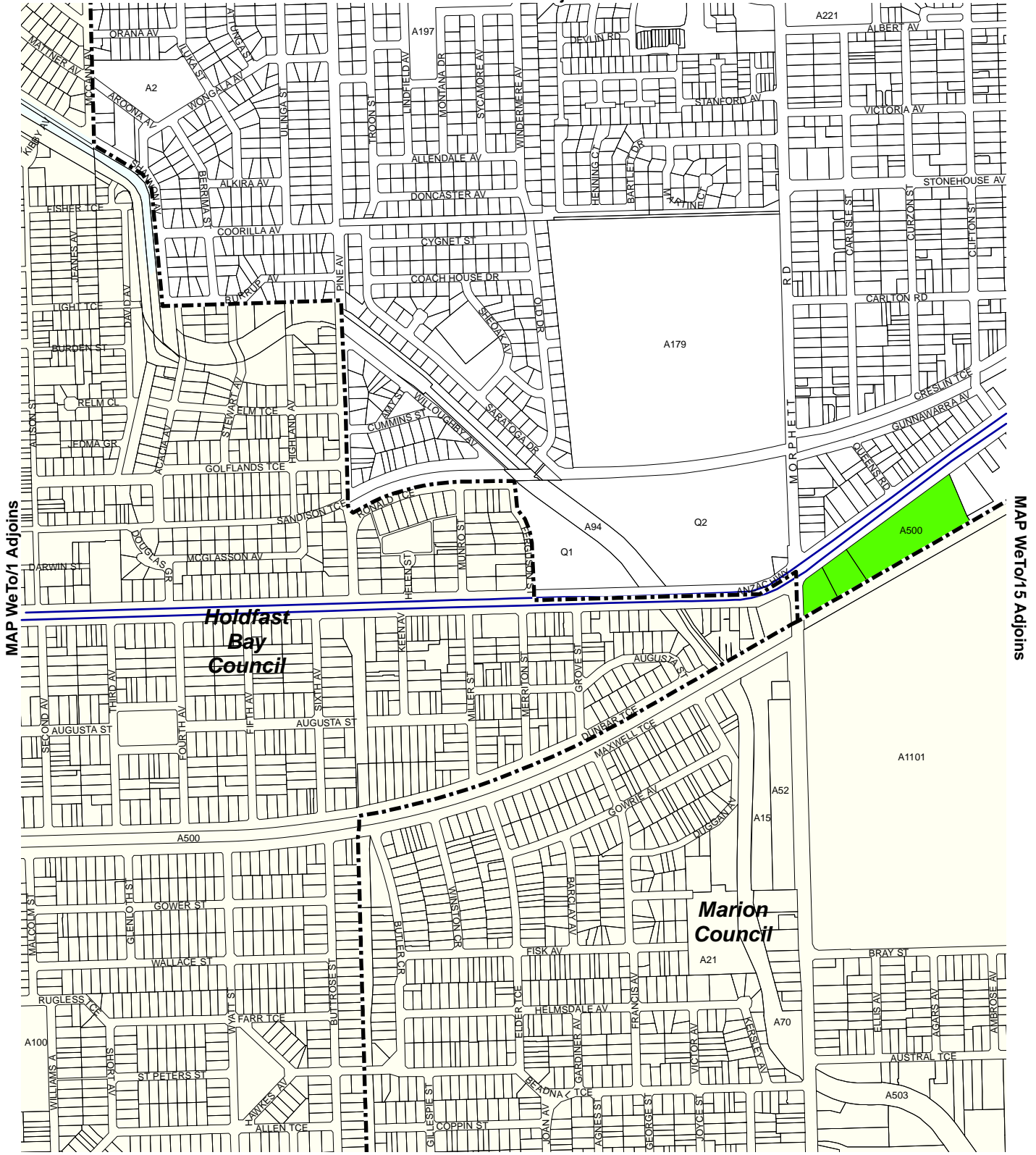
- - - - - Tram Line
- Designated Road: type B road
- Noise and Air Emissions Designated Area
- Development Plan Boundary



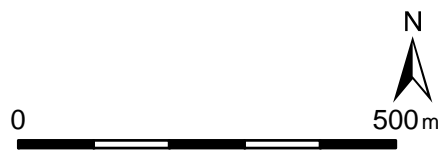
# Overlay Map WeTo/14 NOISE AND AIR EMISSIONS



MAP WeTo/11 Adjoins



MAP WeTo/1 Adjoins



# Overlay Map WeTo/14

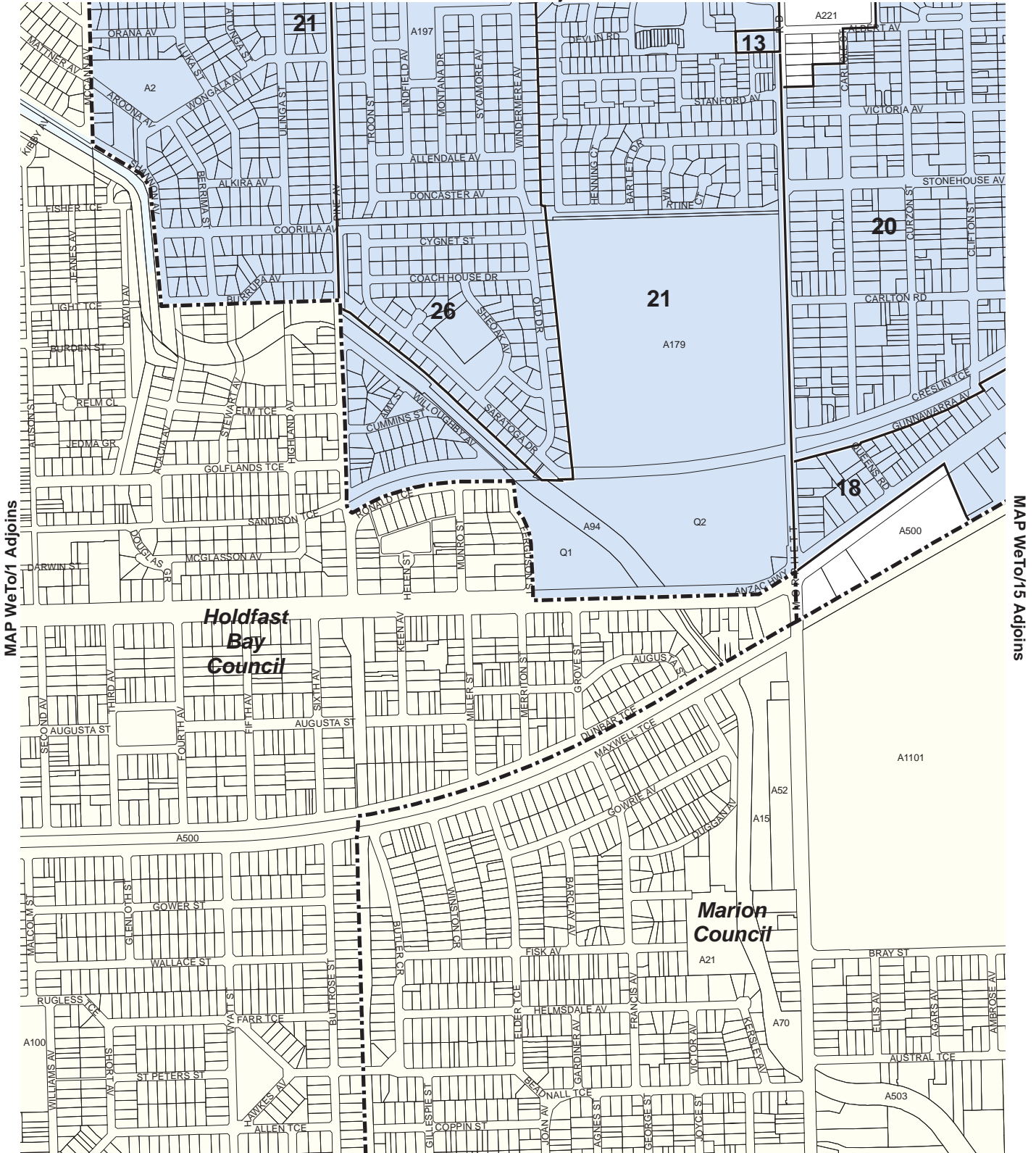
## STRATEGIC TRANSPORT ROUTES

- Strategic Transport Route
- Strategic Transport Routes Designated Area
- Development Plan Boundary





MAP WeTo/11 Adjoins



Lamberts Conformal Conic Projection, GDA94

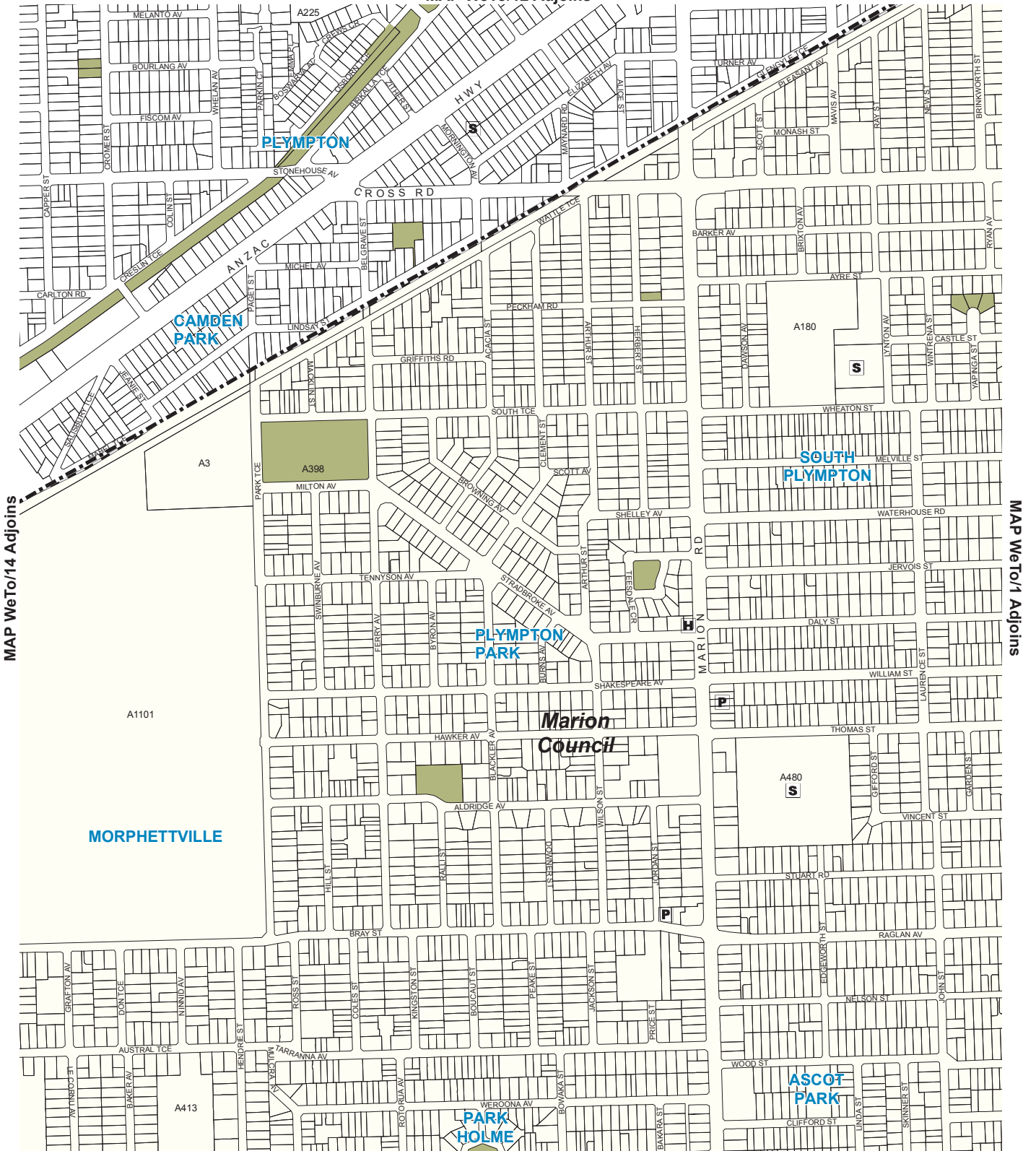
- Policy Area**
- 13 Novar Gardens
  - 18 Medium Density
  - 20 Low Density
  - 21 Low Density
  - 26 Novar Gardens Character



# Policy Area Map WeTo/14

- Policy Area Boundary
- Development Plan Boundary

MAP WeTo/12 Adjoins



MAP WeTo/14 Adjoins

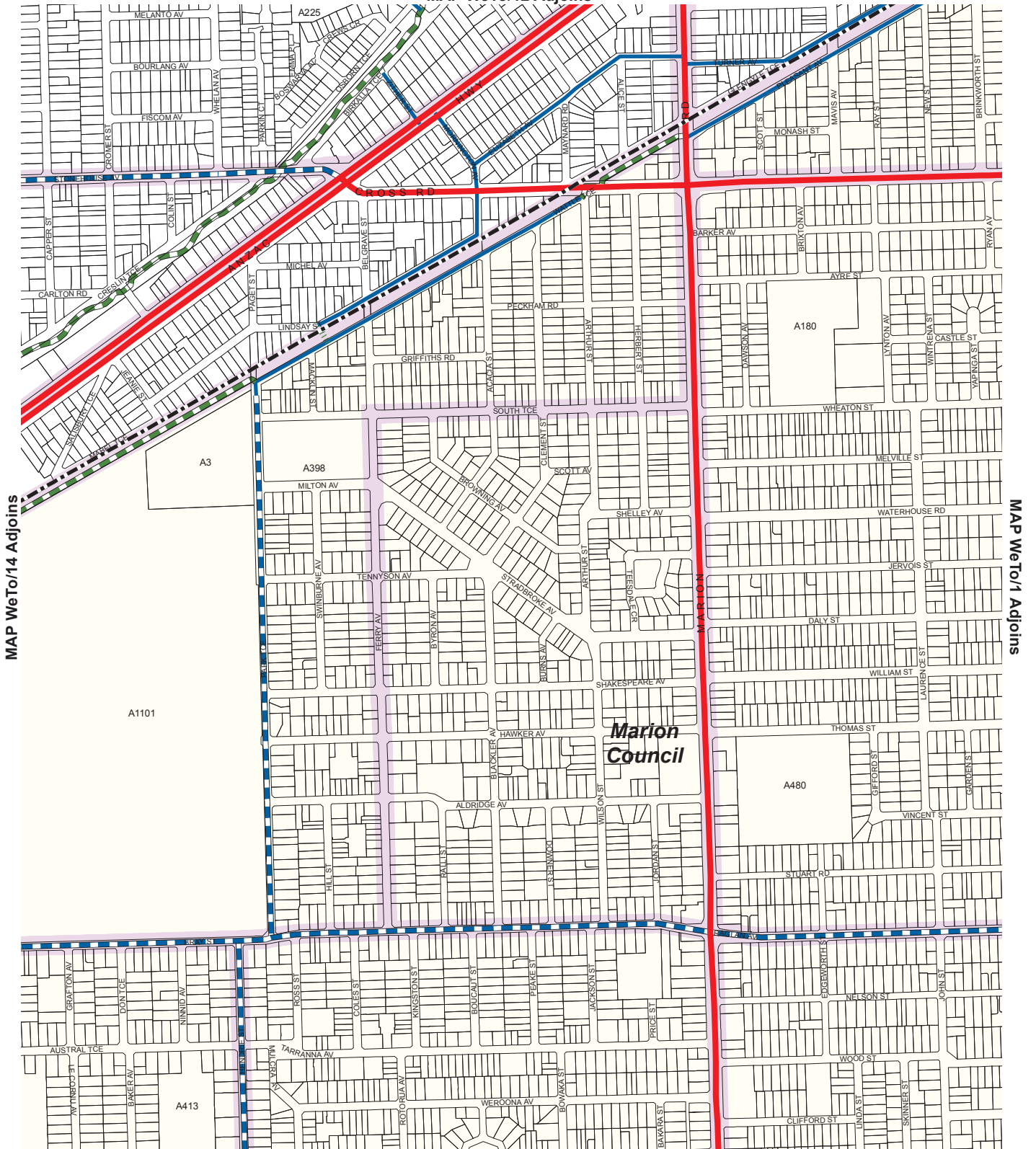
MAP WeTo/1 Adjoins

MAP WeTo/1 Adjoins

- S School
- P Post Office
- H Other Health Services
- Railways
- Local Reserves
- Development Plan Boundary

# Location Map WeTo/15

MAP WeTo/12 Adjoins



MAP WeTo/14 Adjoins

MAP WeTo/1 Adjoins

Marion Council

MAP WeTo/1 Adjoins

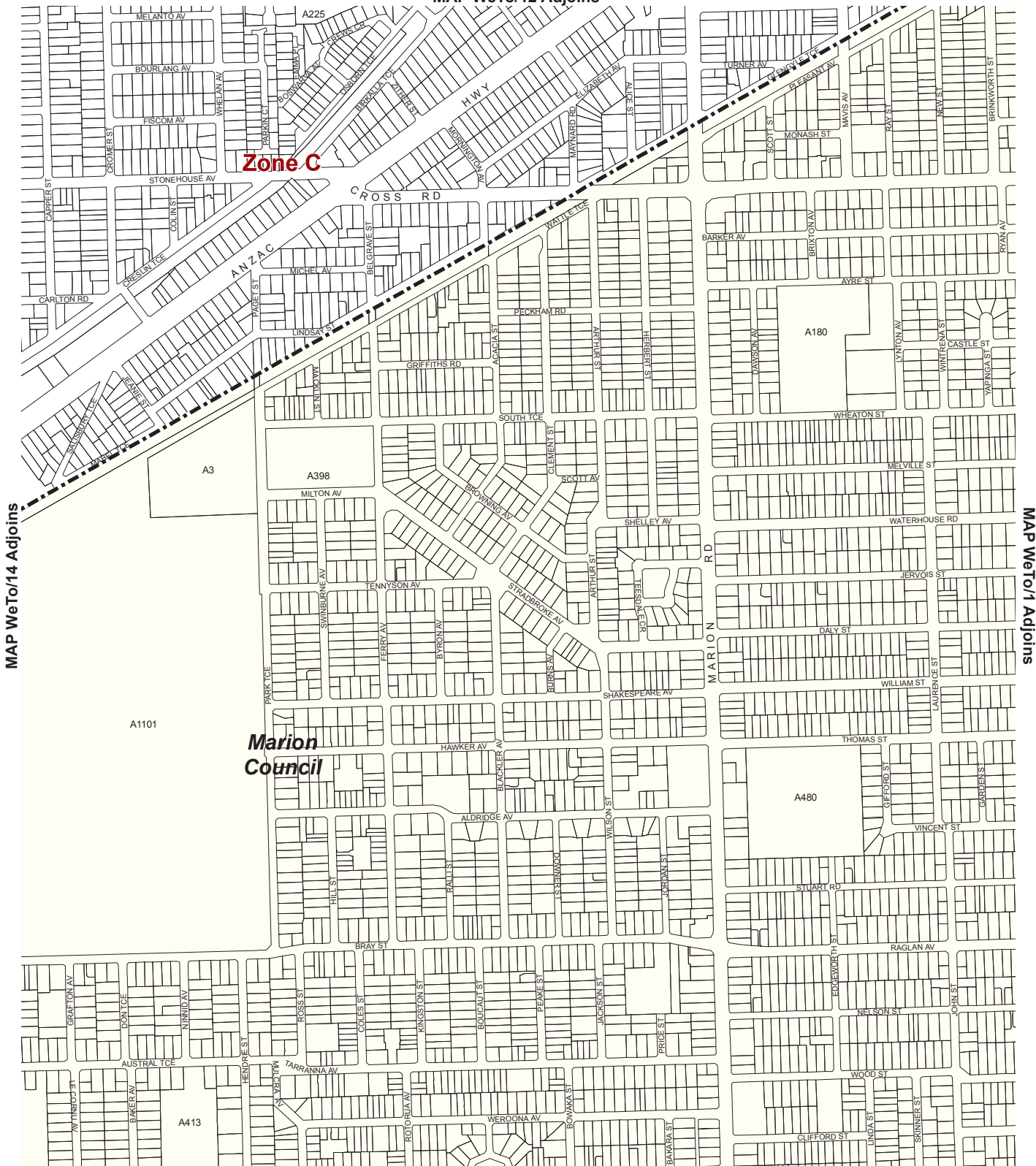
- Primary Arterial Roads
- Bikedirect Network**
- Main Road - Bike Lane
- Main Road
- Secondary Road - Bike Lane
- Secondary Road
- Off Road Sealed Path
- Off Road Track
- Public Transport
- Development Plan Boundary



# Overlay Map WeTo/15

## TRANSPORT



MAP WeTo/12 Adjoins



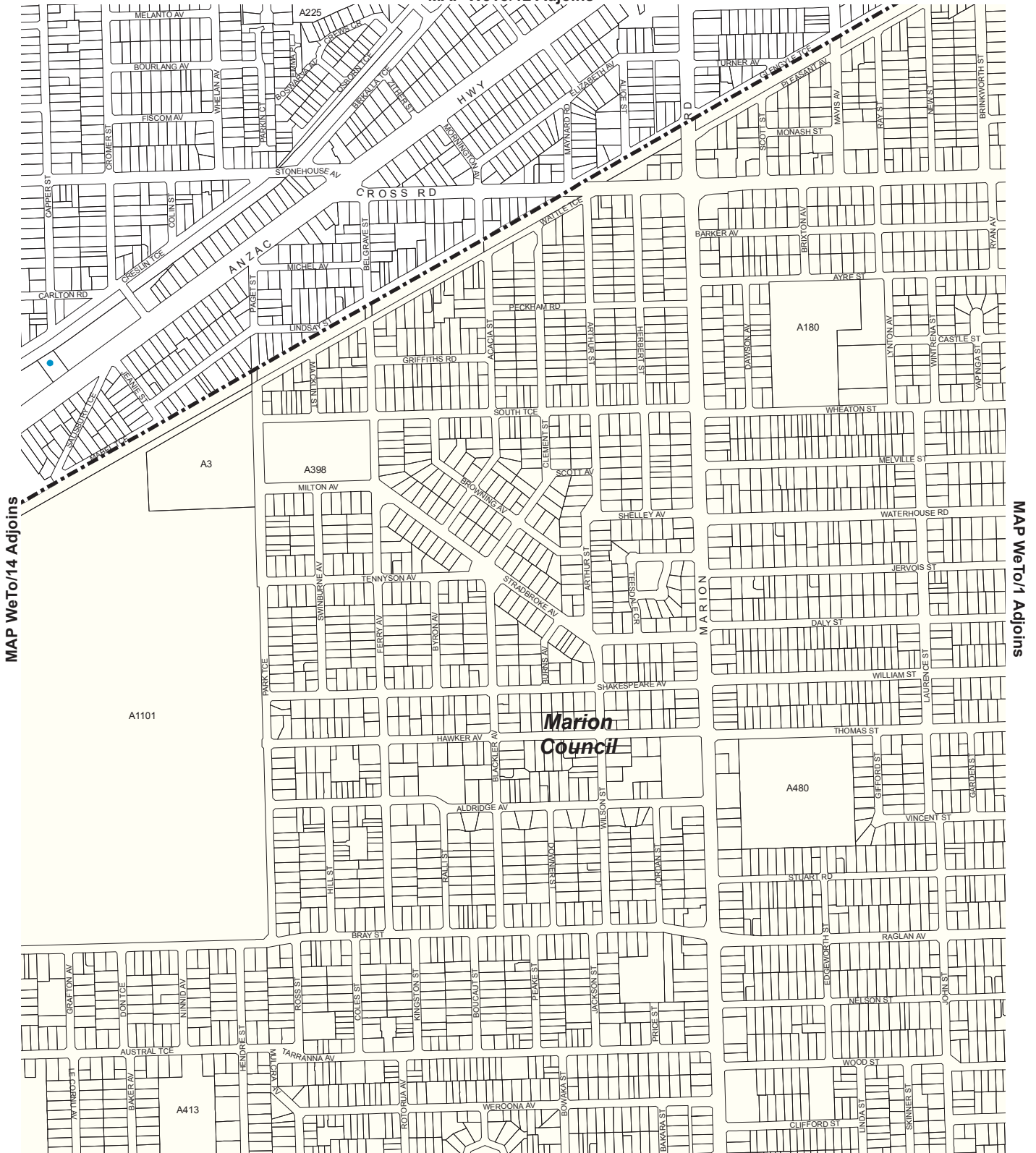
**Airport Building Heights**  
**Zone C** All Structures Exceeding 15 metres above existing ground level



# Overlay Map WeTo/15 DEVELOPMENT CONSTRAINTS

-  Airport Building Heights
-  Development Plan Boundary

MAP WeTo/12 Adjoins



Heritage points are indicative only.  
 For further information on State and Local Heritage Places and Contributory  
 Items please refer to the relevant tables within this document.

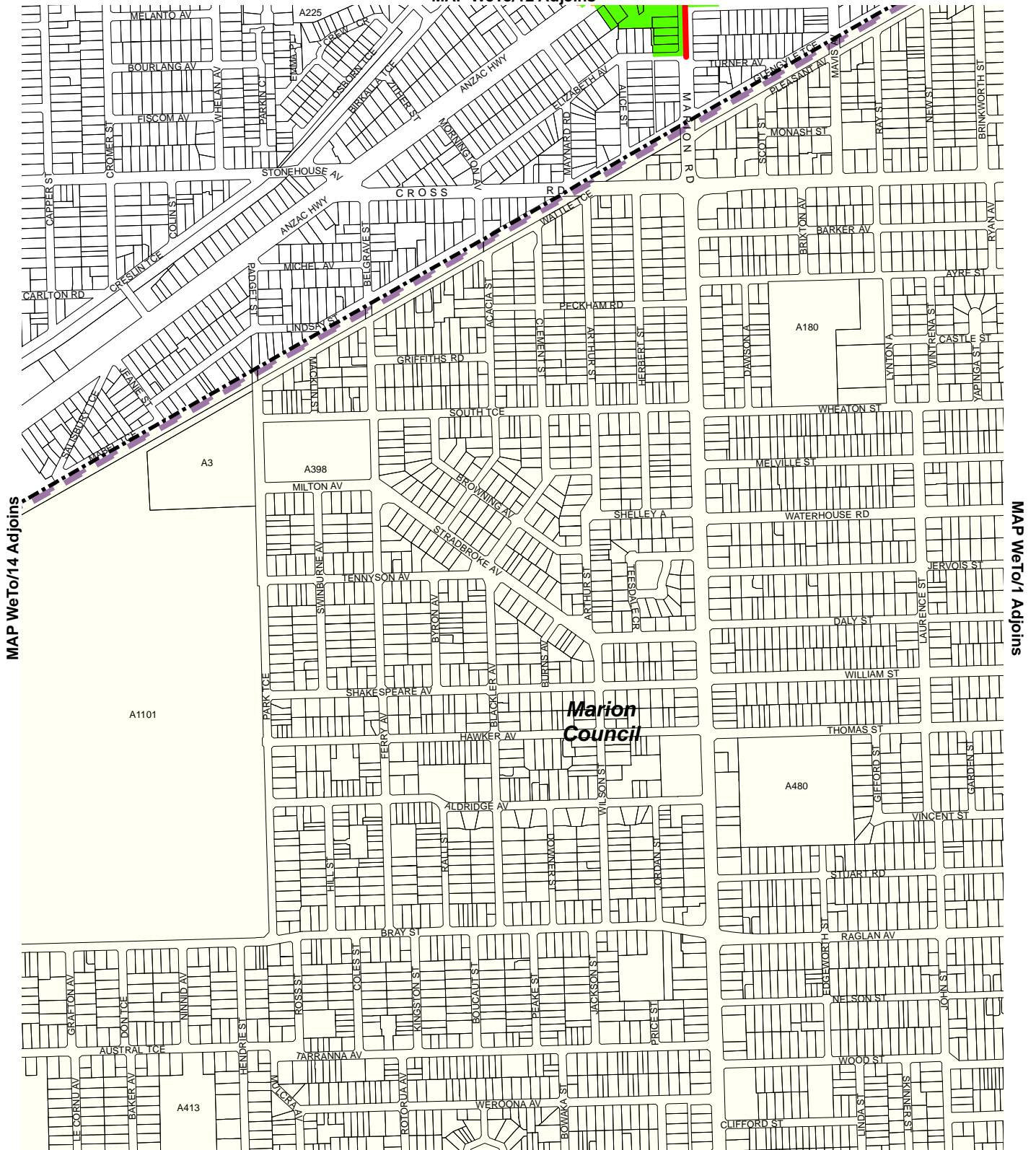


# Overlay Map WeTo/15 HERITAGE





- Local heritage place
- Development Plan Boundary



MAP WeTo/12 Adjoins



MAP WeTo/1 Adjoins

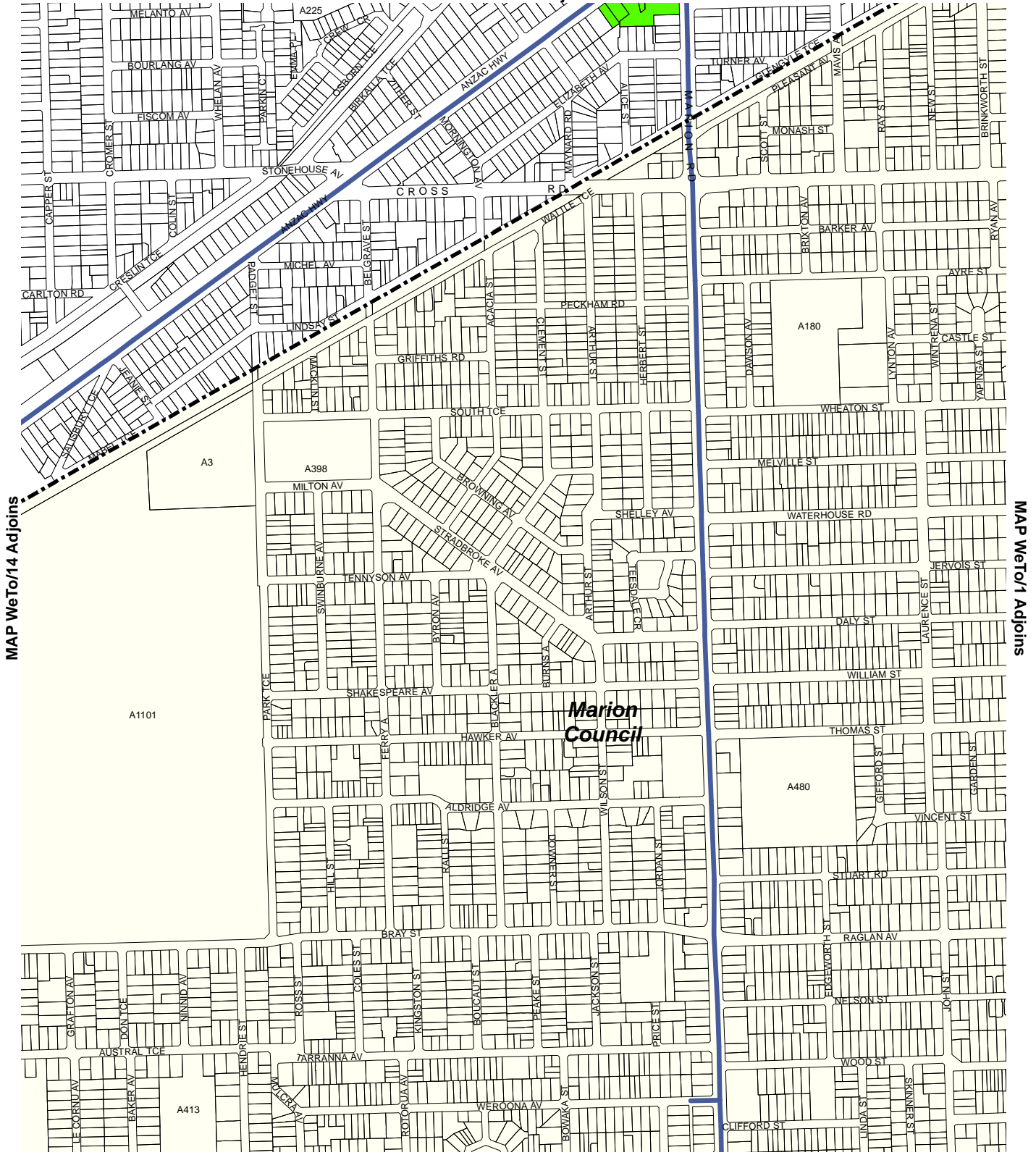
-  Tram Line
-  Designated Road: type A road
-  Noise and Air Emissions Designated Area
-  Development Plan Boundary

# Overlay Map WeTo/15

## NOISE AND AIR EMISSIONS



MAP WeTo/12 Adjoins



MAP WeTo/14 Adjoins

MAP WeTo/1 Adjoins

MAP WeTo/1 Adjoins

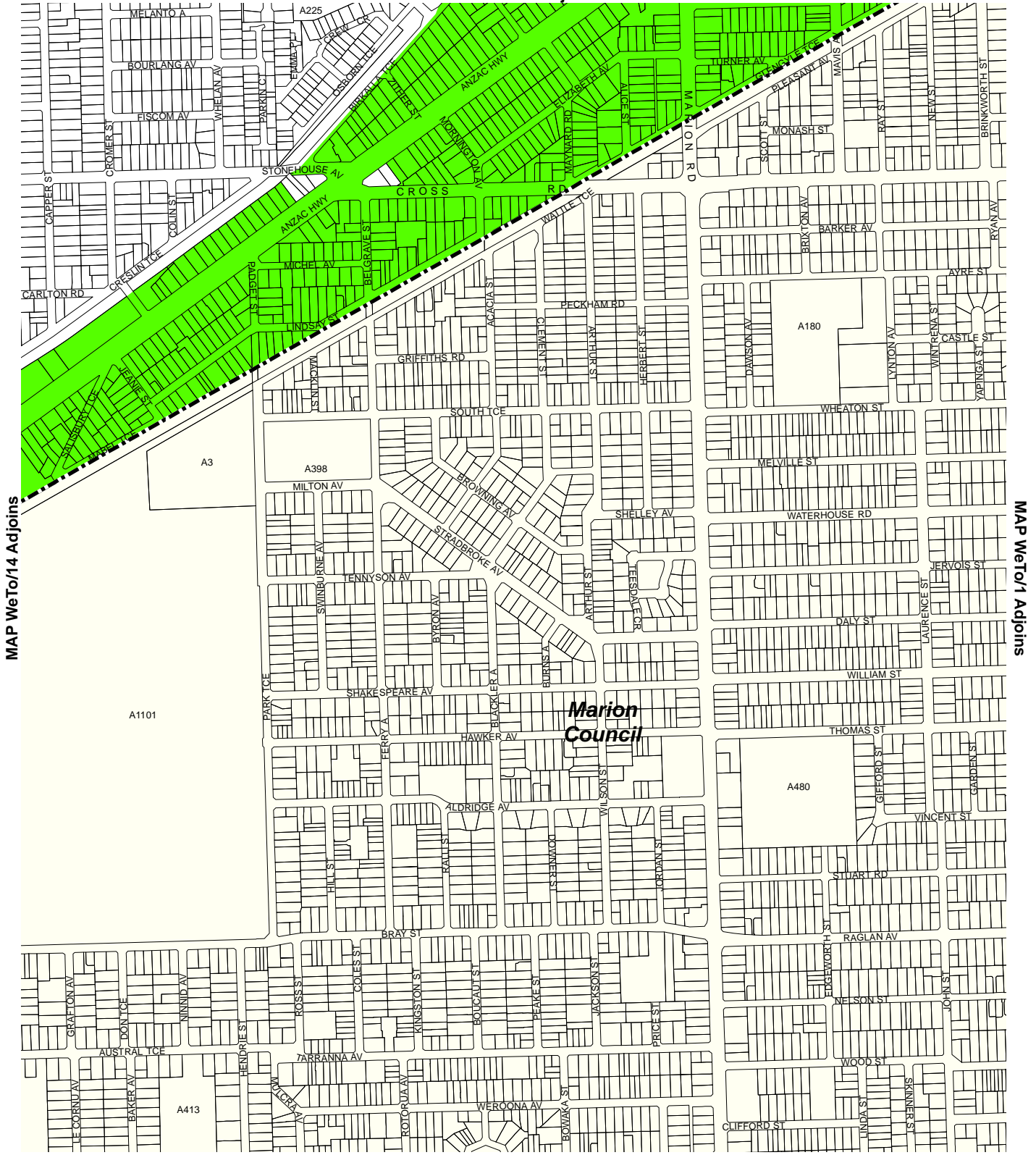
- Strategic Transport Route
- Strategic Transport Routes Designated Area
- Development Plan Boundary

# Overlay Map WeTo/15

## STRATEGIC TRANSPORT ROUTES

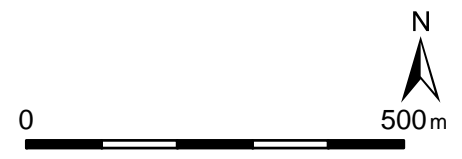


MAP WeTo/12 Adjoins



MAP WeTo/1 Adjoins

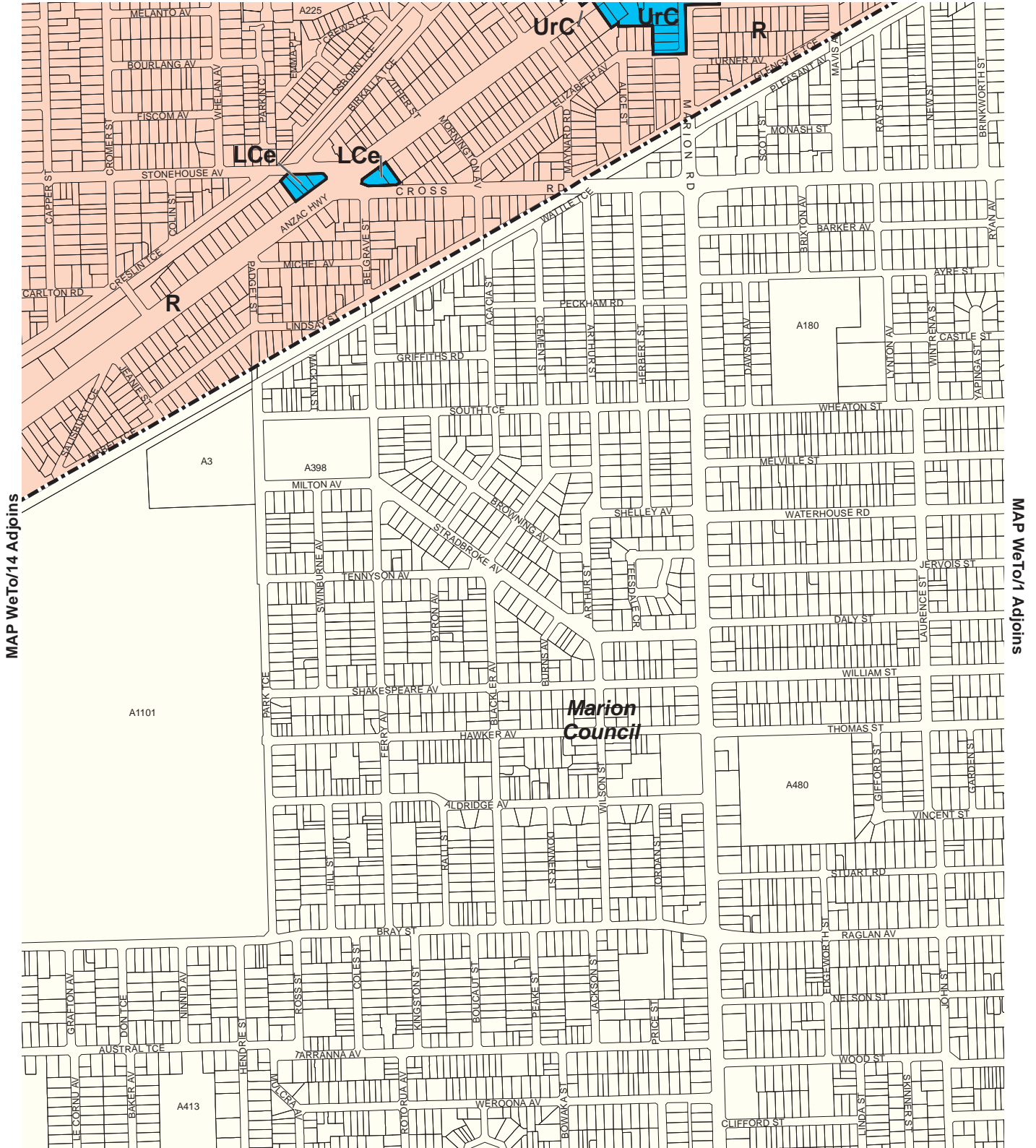
- Affordable Housing Designated Area
- Development Plan Boundary



# Overlay Map WeTo/15

## AFFORDABLE HOUSING

MAP WeTo/12 Adjoins



Lamberts Conformal Conic Projection, GDA94

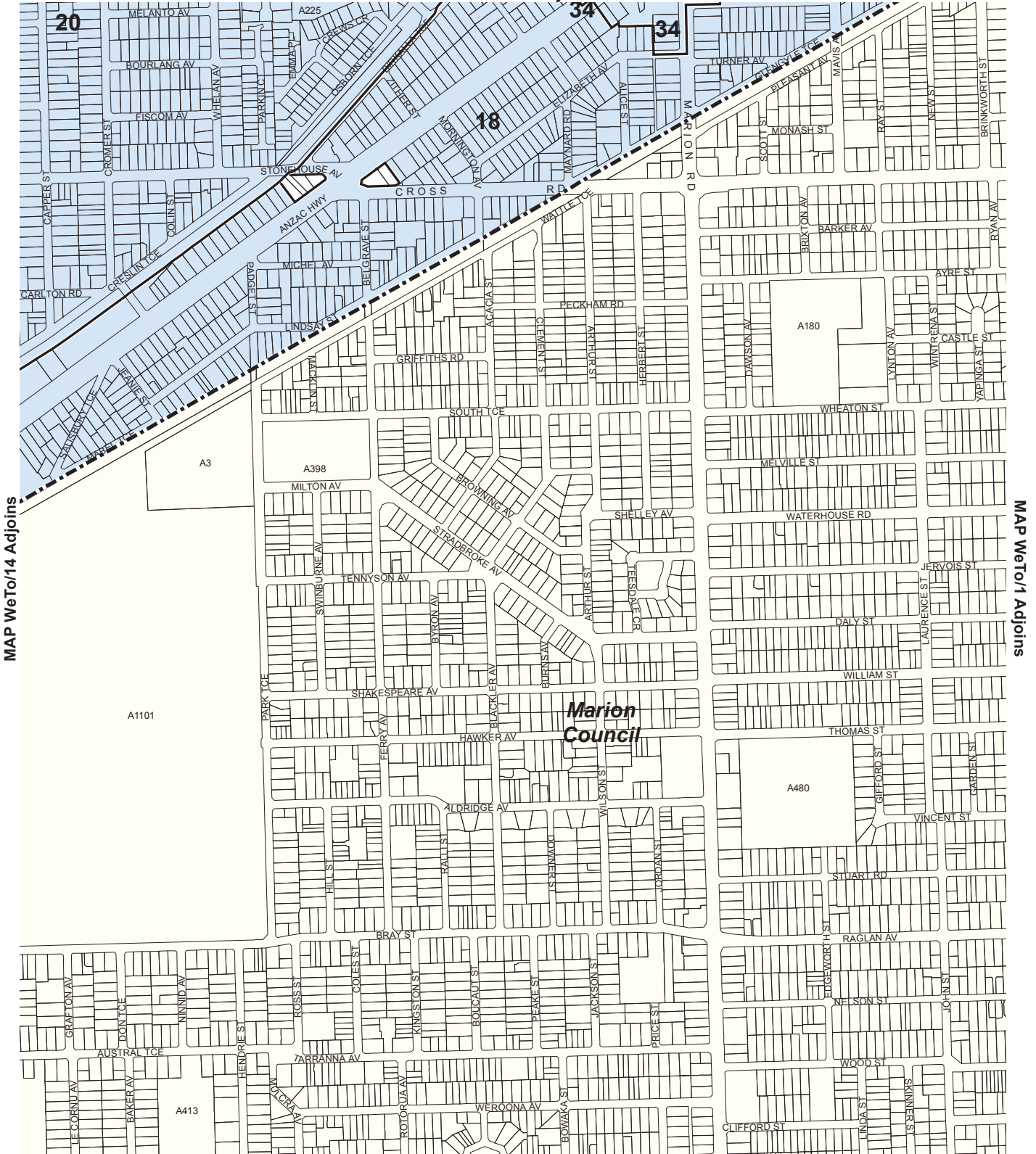


Zones

- LCe Local Centre
- R Residential
- UrC Urban Corridor
- Zone Boundary
- Development Plan Boundary

# Zone Map WeTo/15

MAP WeTo/12 Adjoins



Lamberts Conformal Conic Projection, GDA94

- Policy Area**
- 18 Medium Density
  - 20 Low Density
  - 34 Boulevard



# Policy Area Map WeTo/15

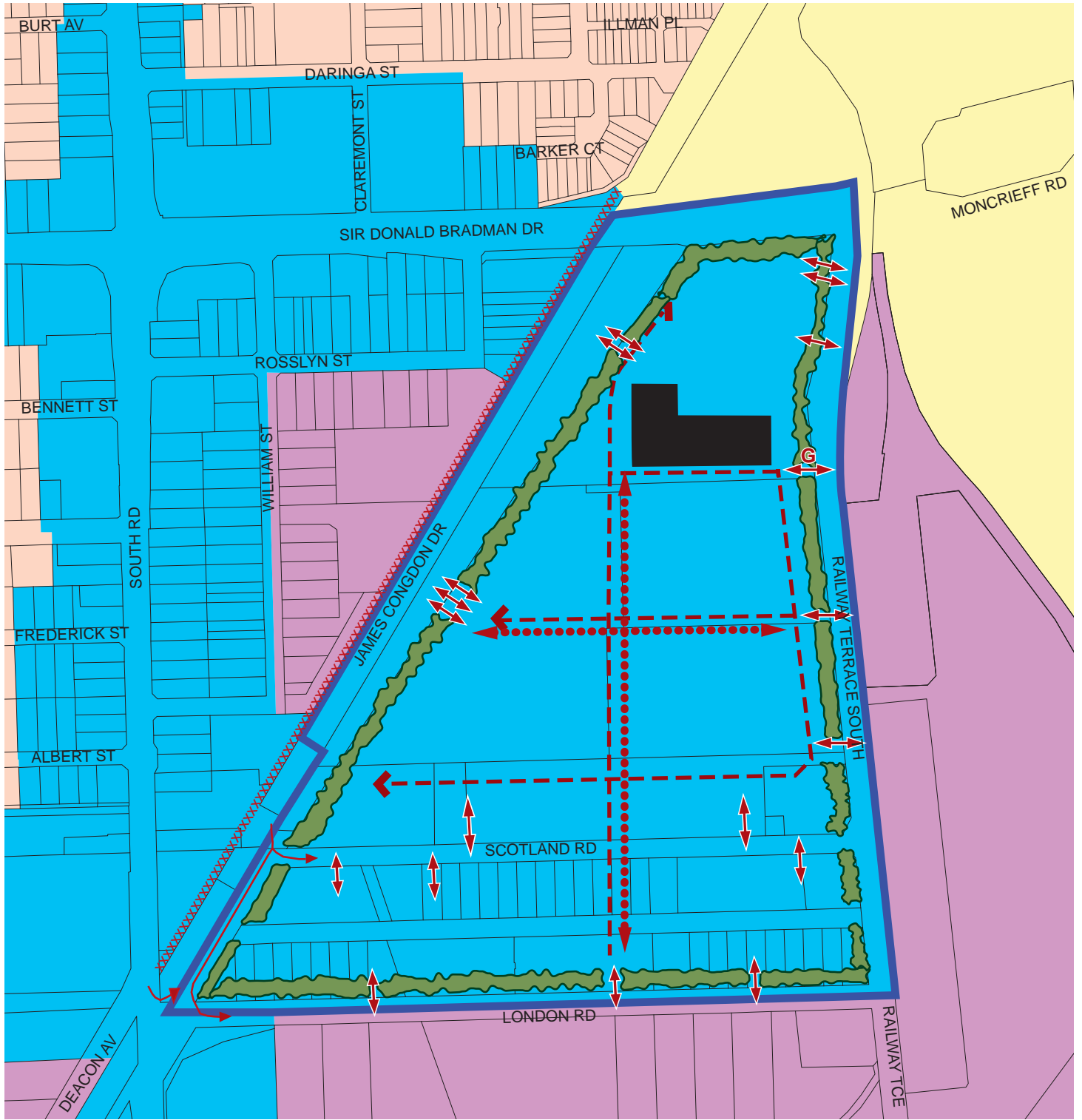
- Policy Area Boundary
- Development Plan Boundary



# Concept Plan Maps







- ▬ Concept Area Boundary
- ▬ Proposed Landscaping Buffer & Footpath
- ▬ Existing Building
- xxxxxxx No Direct Access to Western Bypass from Western Side.
- ↔ All Direction Access Point
- ↔↔ Left In/Left Out Access Point
- ↔↔↔ Left In/Left Out /Right In Access Point
- ↔<sup>G</sup> Goods Only
- Major Designated Sheltered Pedestrian Route
- - - Internal Road Network
- Commercial    ■ Residential
- Industrial    ■ Community Facilities



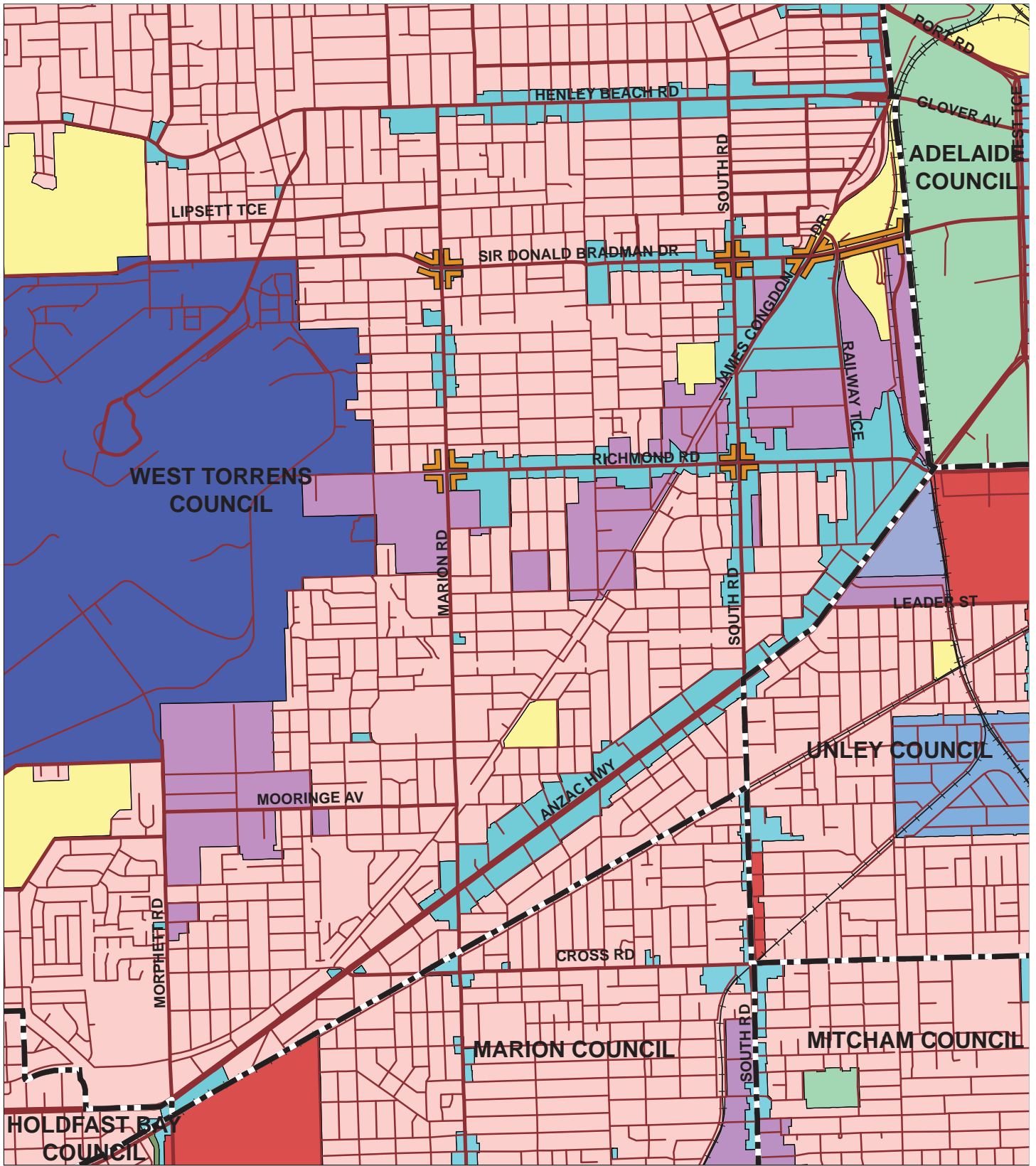
1:4,500



MILE END SOUTH

# Concept Plan Map WeTo/1

## MILE END SOUTH BULKY GOODS

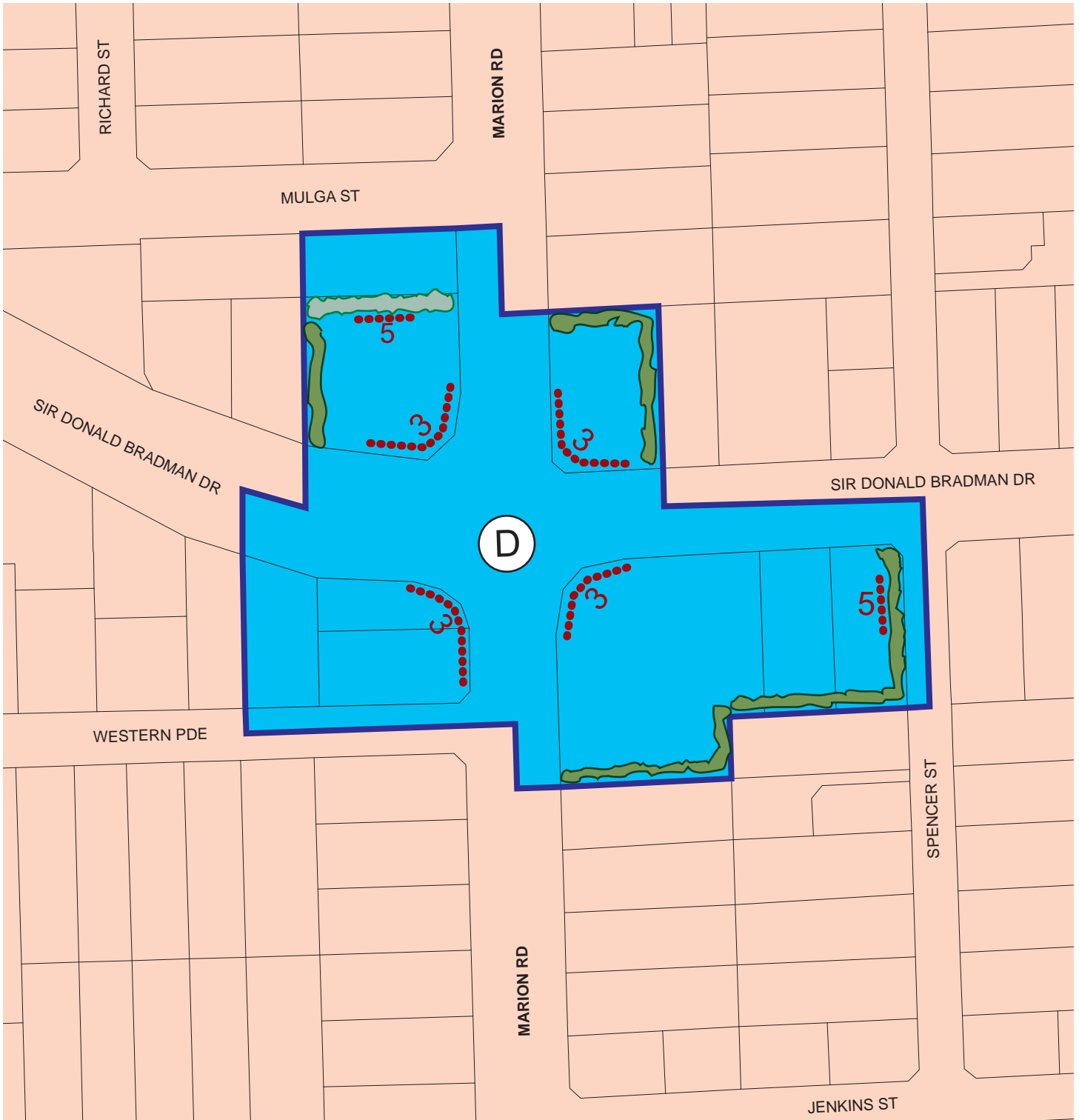


-  Prominent Development Sites
-  Primary Roads
-  Local Roads
-  Railways
-  Development Plan Boundary



# Concept Plan Map WeTo/2

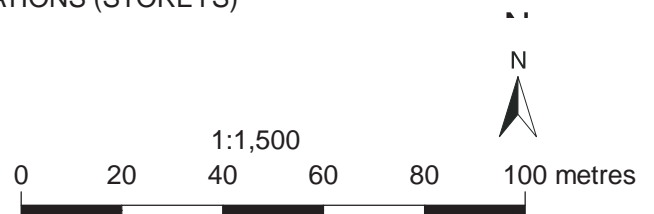
## PROMINENT DEVELOPMENT SITES



- Concept Plan Boundary
- Extensive Landscaping
- Intensive Landscaping
- 3m Road Setback (metres)
- Commercial
- Residential

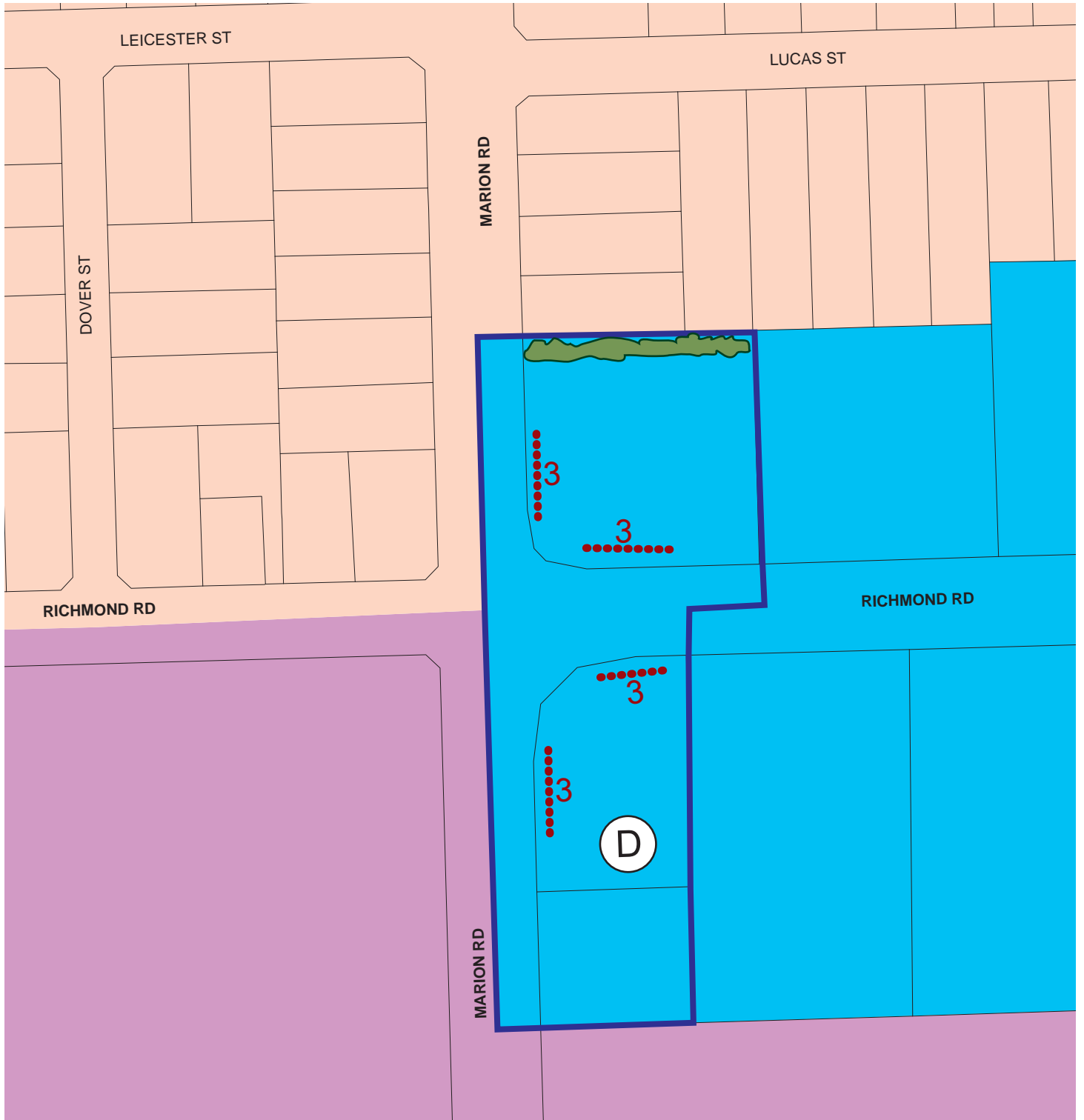
HEIGHT LIMITATIONS (STOREYS)







Ⓧ 2-3



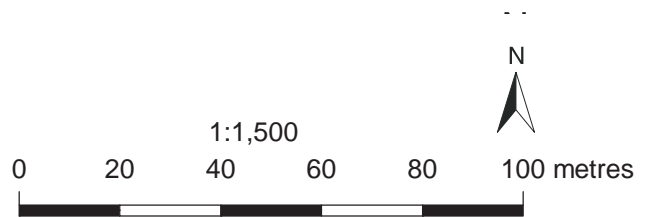
# Concept Plan Map WeTo/3a

## SIR DONALD BRADMAN DRIVE & MARION ROAD INTERSECTION COMMERCIAL



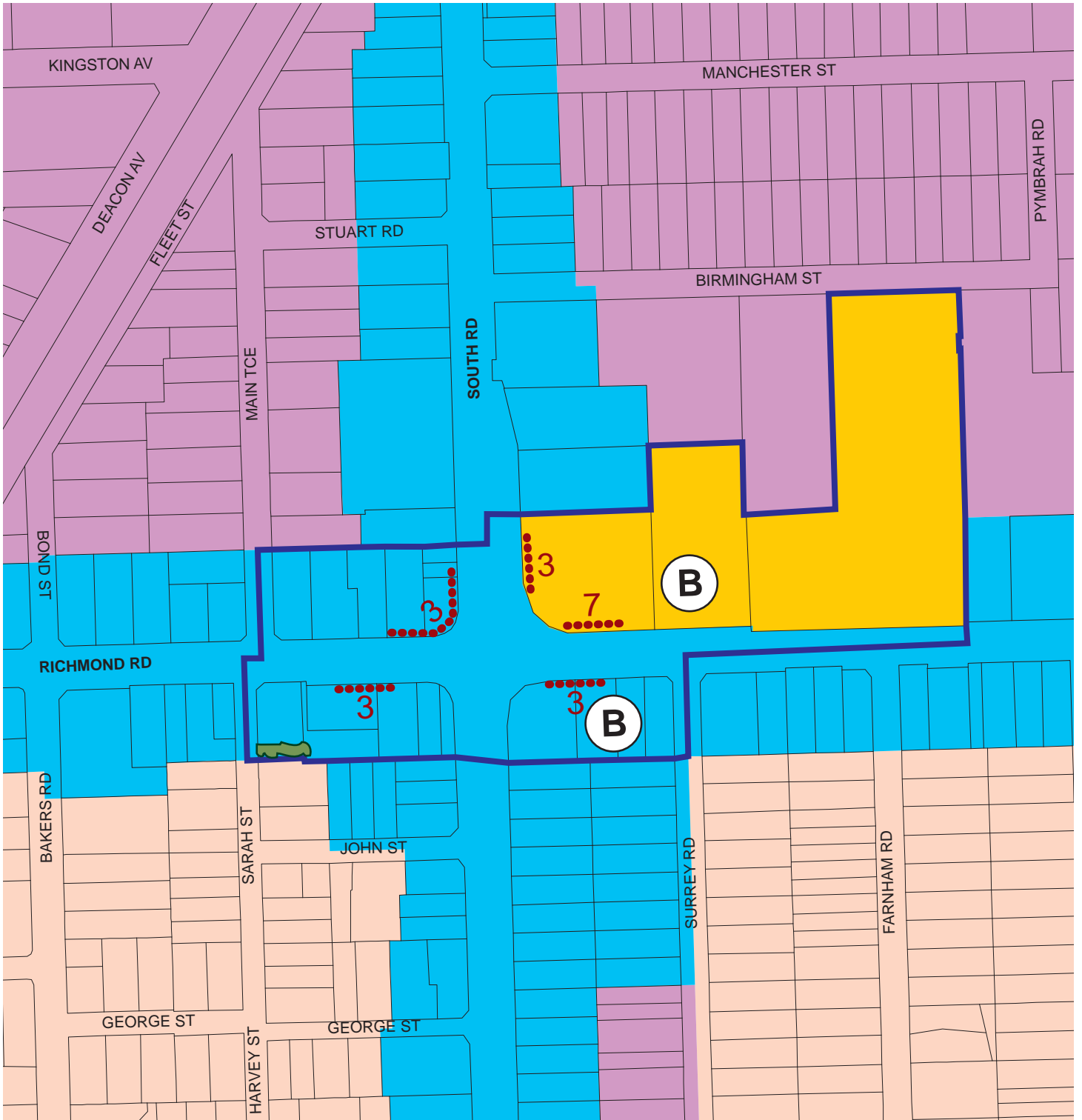
-  Concept Plan Boundary
-  Intensive Landscaping
-  Road Setback (metres)
-  Commercial
-  Industrial
-  Residential






HEIGHT LIMITATIONS (STOREYS)  
 Ⓧ 2-3

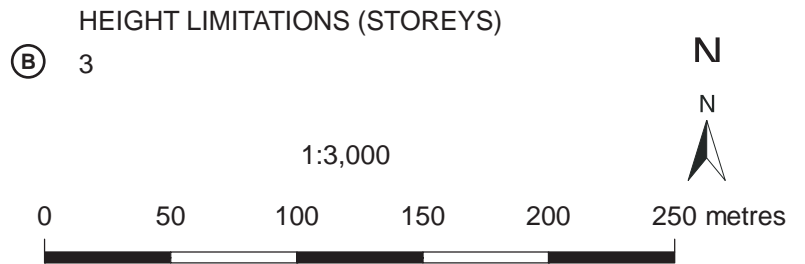


# Concept Plan Map WeTo/3b

## MARION ROAD & RICHMOND ROAD INTERSECTION COMMERCIAL

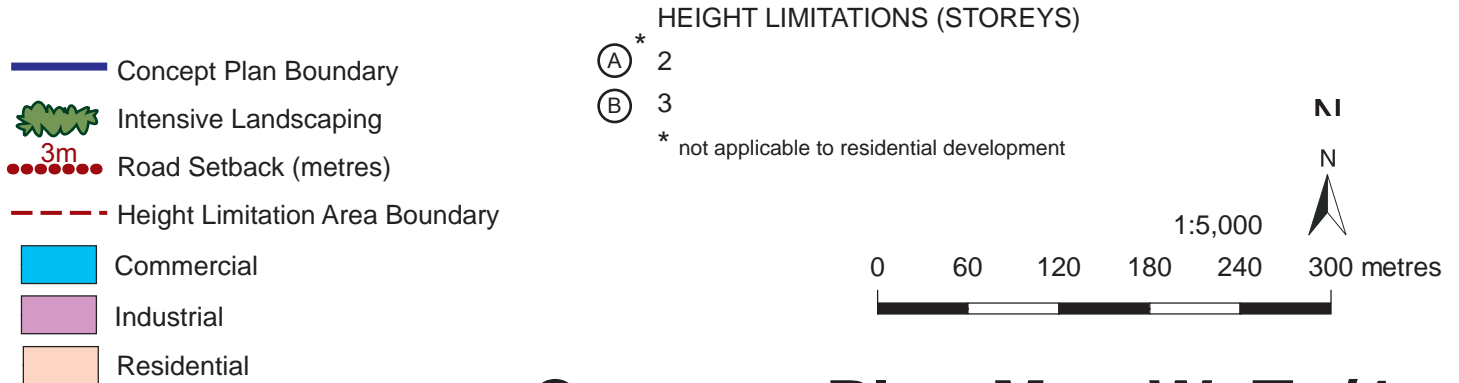
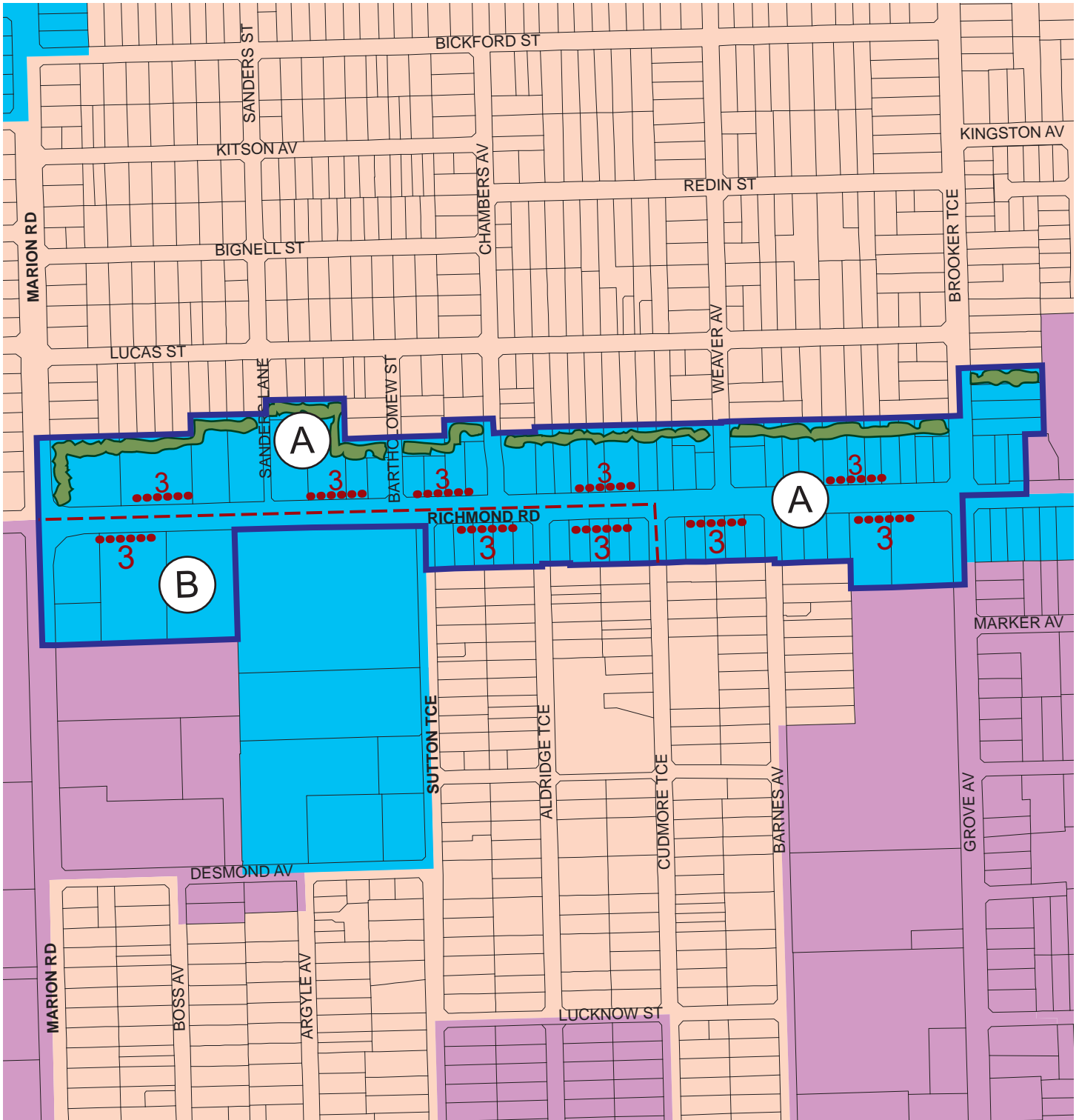


-  Concept Plan Boundary
-  Major Integrated Mixed Use Development
-  Intensive Landscaping
-  Road Setback (metres)
-  Commercial
-  Industrial
-  Residential



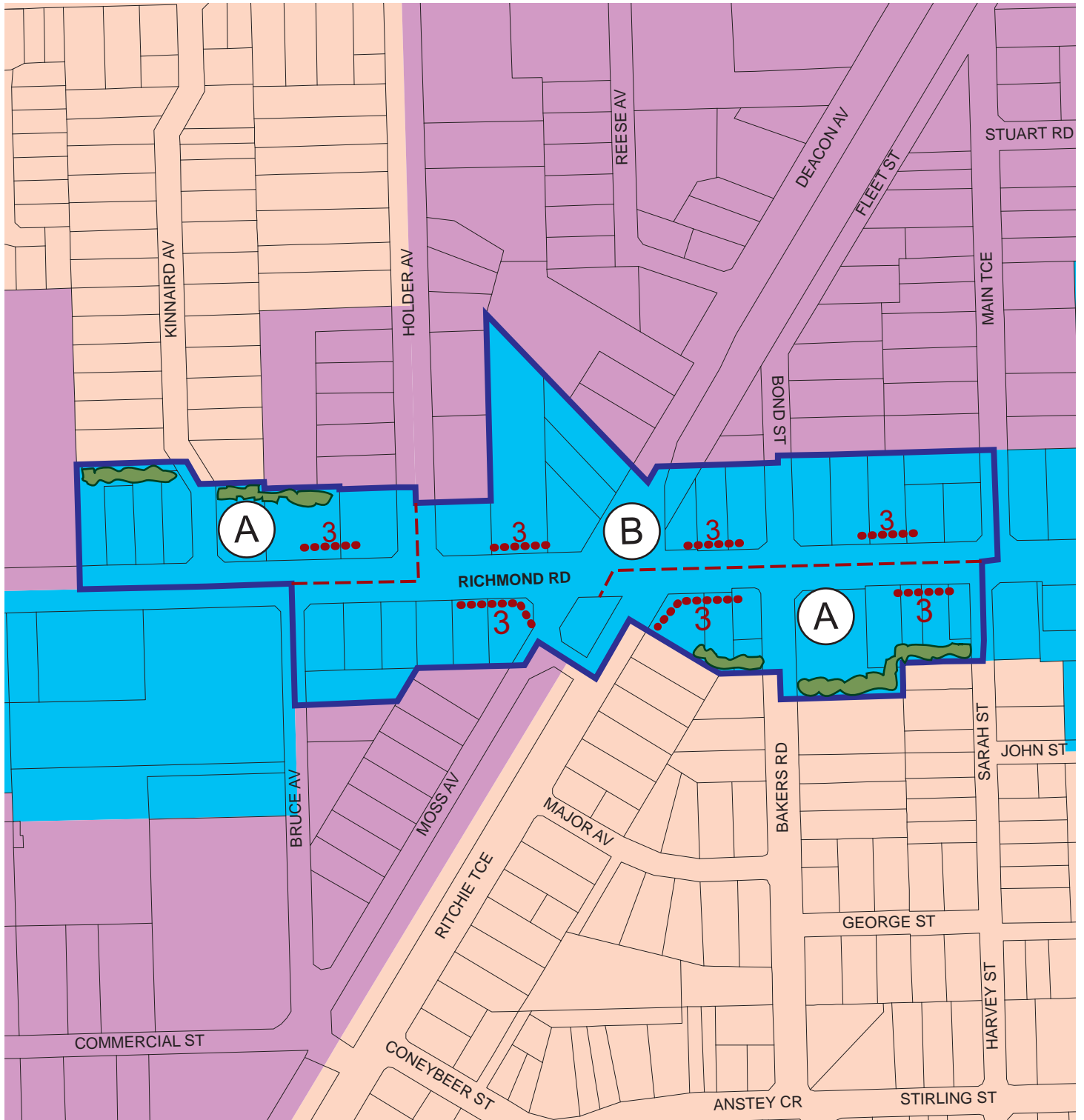
# Concept Plan Map/3c

## RICHMOND ROAD & SOUTH ROAD INTERSECTION COMMERCIAL



# Concept Plan Map WeTo/4a

## RICHMOND ROAD (MARLESTON) COMMERCIAL



-  Concept Plan Boundary
-  Intensive Landscaping
-  Road Setback (metres)
-  Height Limitation Area Boundary
-  Commercial
-  Industrial
-  Residential

**HEIGHT LIMITATIONS (STOREYS)**

- (A) \* 2
- (B) 3
- \* not applicable to residential development

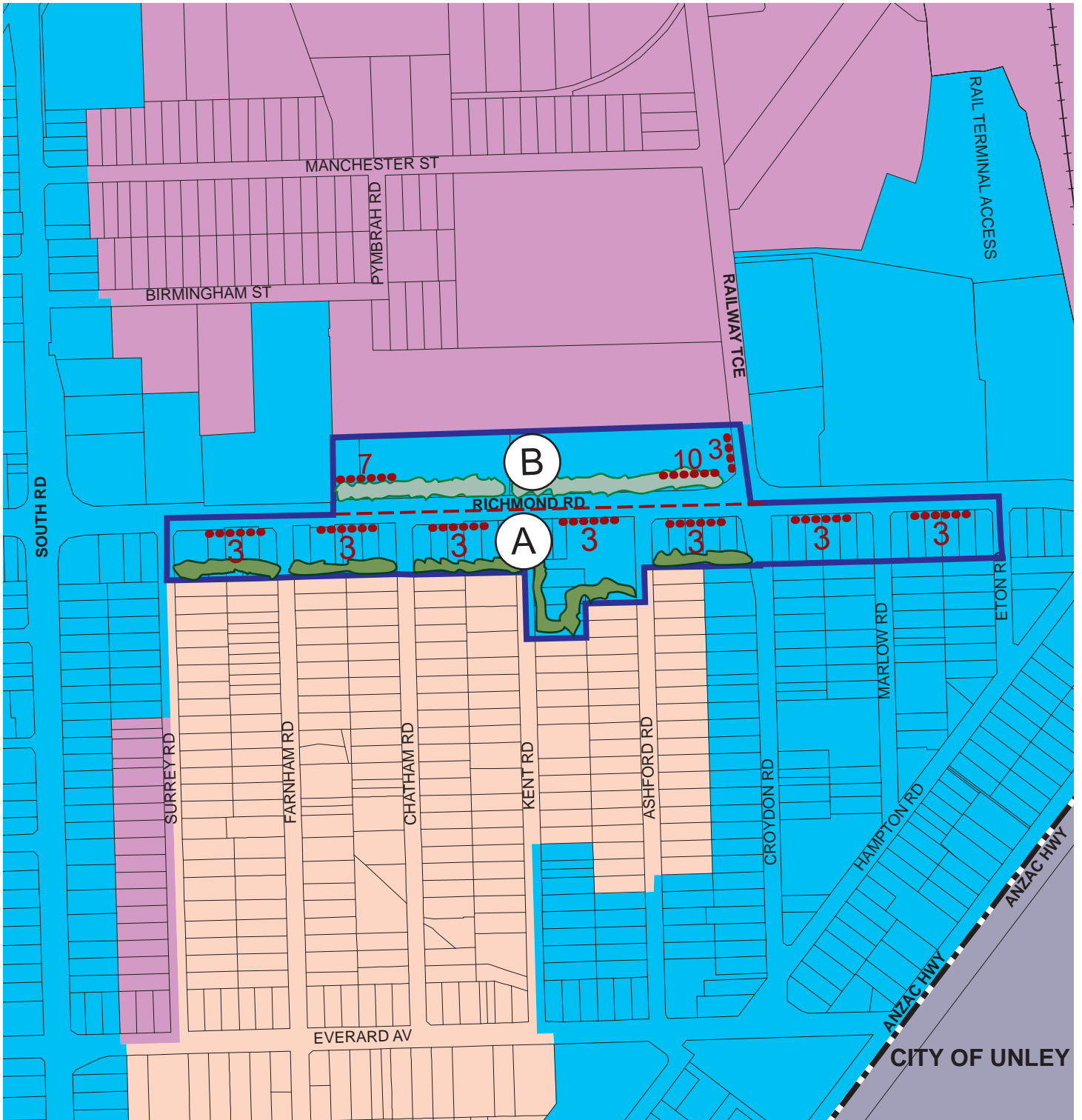


1:3,000



# Concept Plan Map WeTo/4b

## RICHMOND ROAD (RICHMOND) COMMERCIAL



- Concept Plan Boundary
- Extensive Landscaping
- Intensive Landscaping
- Road Setback (metres)
- Height Limitation Area Boundary
- Commercial
- Industrial
- Residential
- Development Plan Boundary

HEIGHT LIMITATIONS (STOREYS)

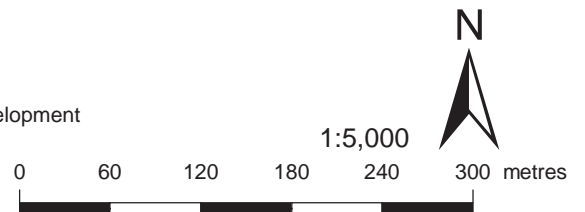
(A) \*

2

(B)

3

\* not applicable to residential development

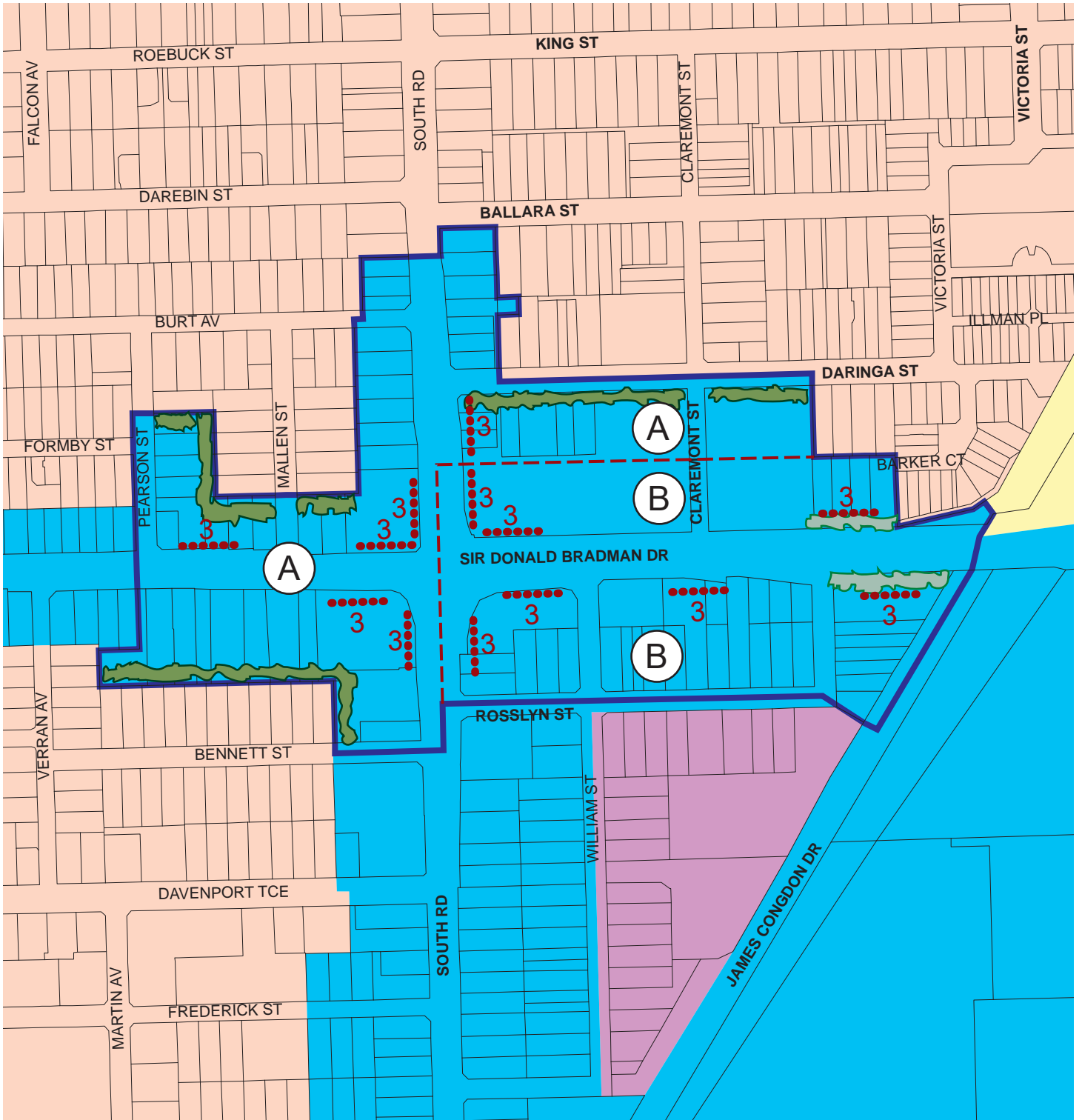



# Concept Plan Map WeTo/4c

## RICHMOND ROAD

### (KESWICK/MILE END SOUTH) COMMERCIAL

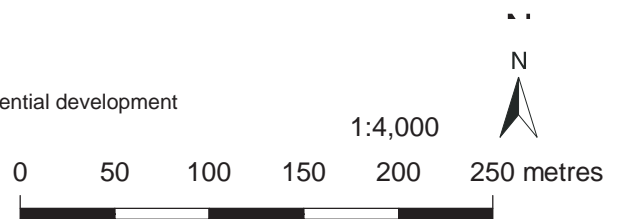




-  Concept Plan Boundary
-  Extensive Landscaping
-  Intensive Landscaping
-  3m Road Setback (metres)
-  Height Limitation Area Boundary
-  Commercial
-  Community Facilities
-  Industrial
-  Residential

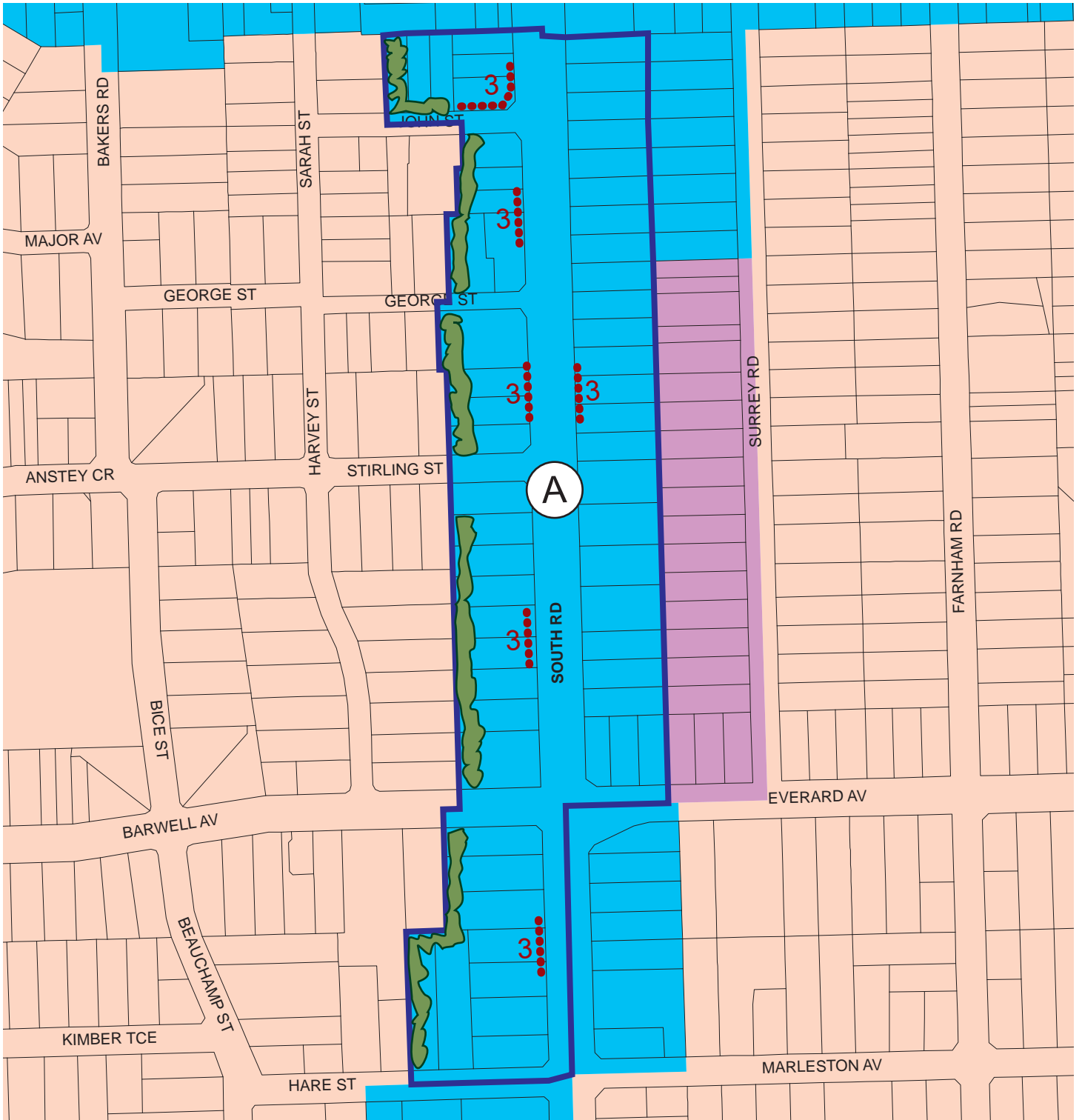
HEIGHT LIMITATIONS (STOREYS)







- (A) \* 2
- (B) 3
- \* not applicable to residential development



# Concept Plan Map WeTo/5

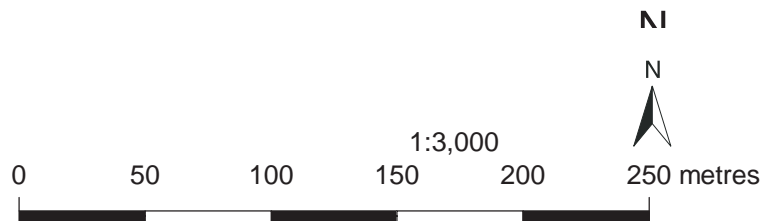
## SIR DONALD BRADMAN DRIVE (MILE END) COMMERCIAL



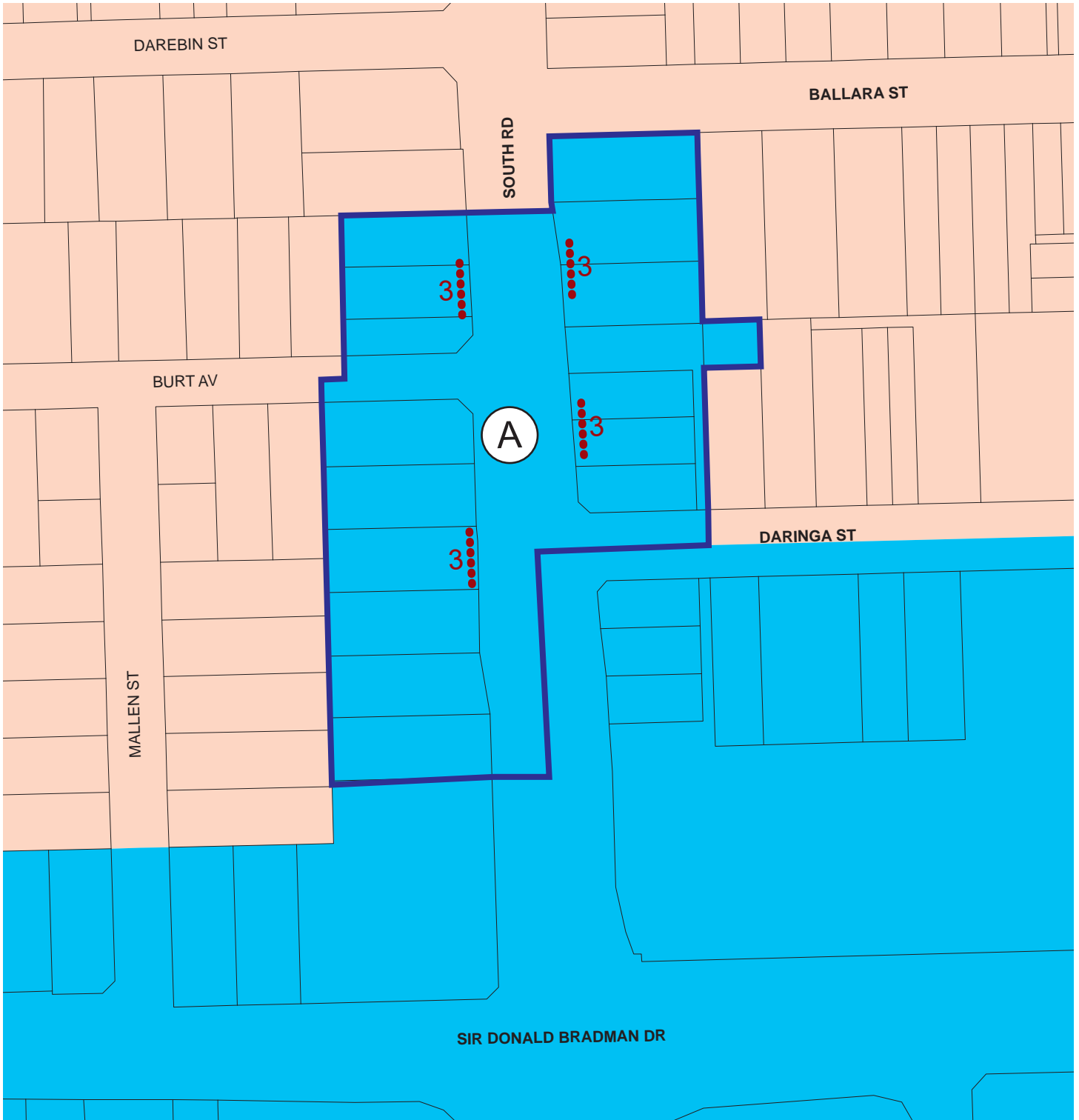
-  Concept Plan Boundary
-  Screen Landscaping
-  Road Setback (metres)
-  Commercial
-  Industrial
-  Residential

HEIGHT LIMITATIONS (STOREYS)

(A) 2



# Concept Plan Map WeTo/6 SOUTH ROAD (KESWICK) COMMERCIAL



— Concept Plan Boundary

•••••<sup>3m</sup> Road Setback (metres)

Commercial

Residential

HEIGHT LIMITATIONS (STOREYS)

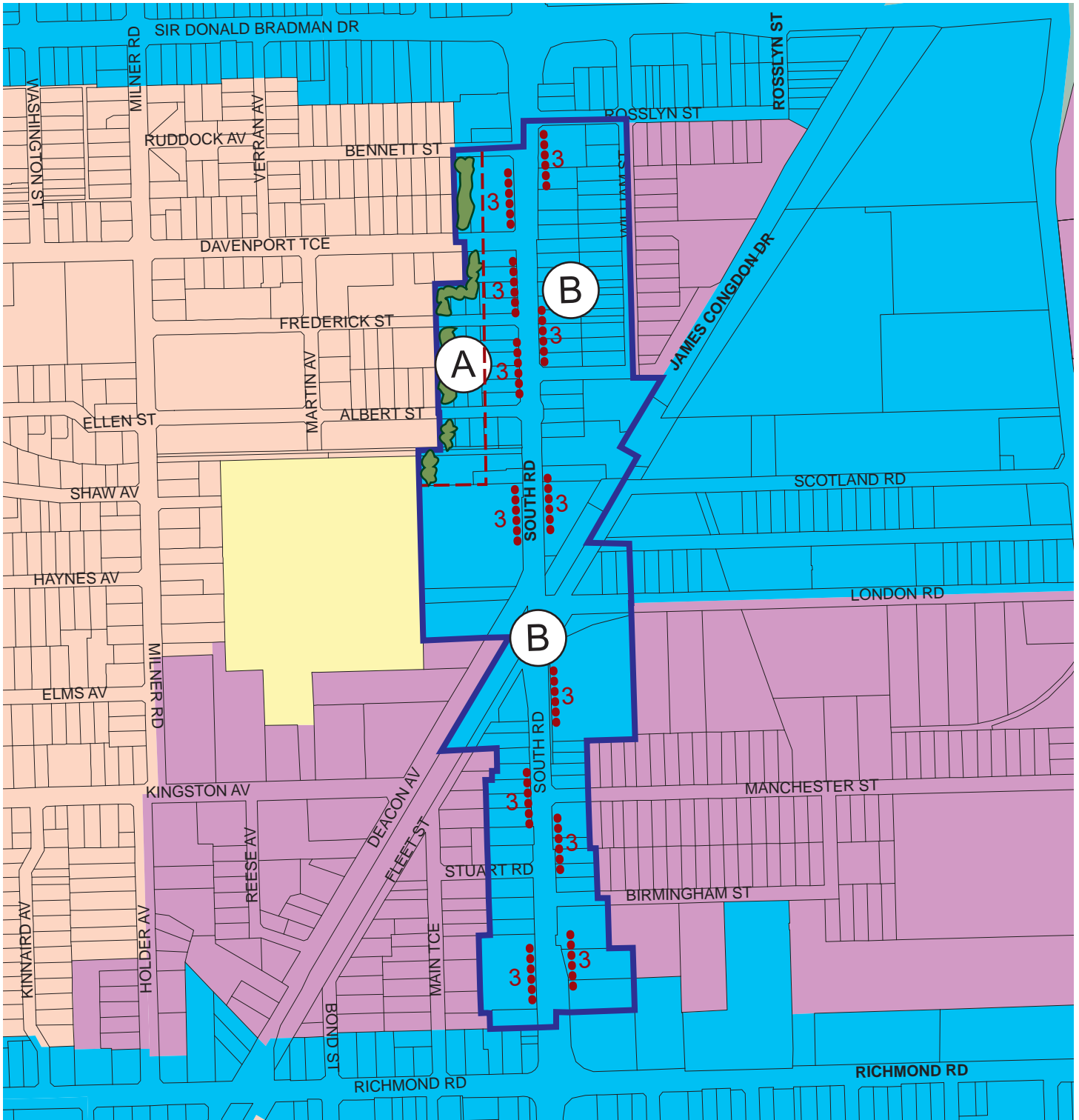
(A) 2

1:1,500



# Concept Plan Map WeTo/7

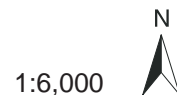
## SOUTH ROAD (MILE END) COMMERCIAL



HEIGHT LIMITATIONS (STOREYS)

- (A) 2
- (B) 3

- Concept Plan Boundary
- Screen Landscaping
- 3m Road Setback (metres)
- Height Limitation Area Boundary
- Commercial
- Community Facilities
- Industrial
- Residential

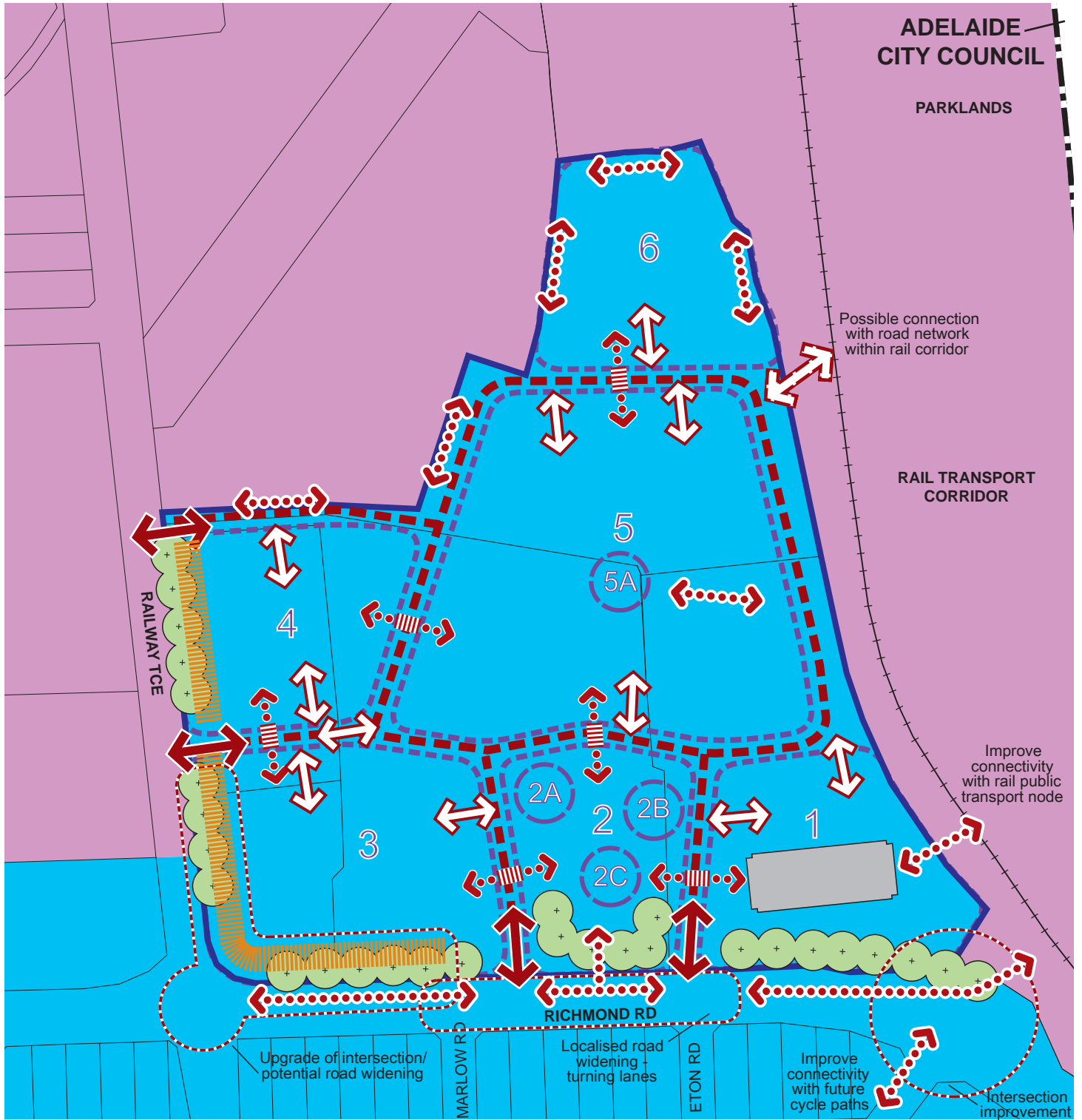


1:6,000



# Concept Plan Map WeTo/8

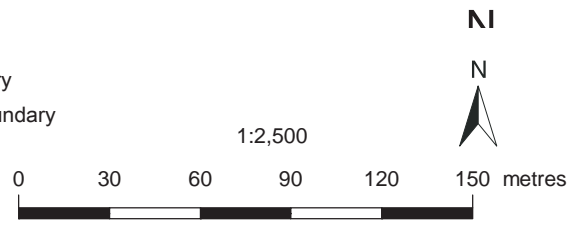
## SOUTH ROAD (MILE END SOUTH) COMMERCIAL



- Primary Access to Public Roads
- Internal Site access
- Primary circulation route
- Pedestrian / Cycle movement
- Pedestrian Crossing
- Built form to address public realm & redefine grid pattern
- Landscaping / Street trees

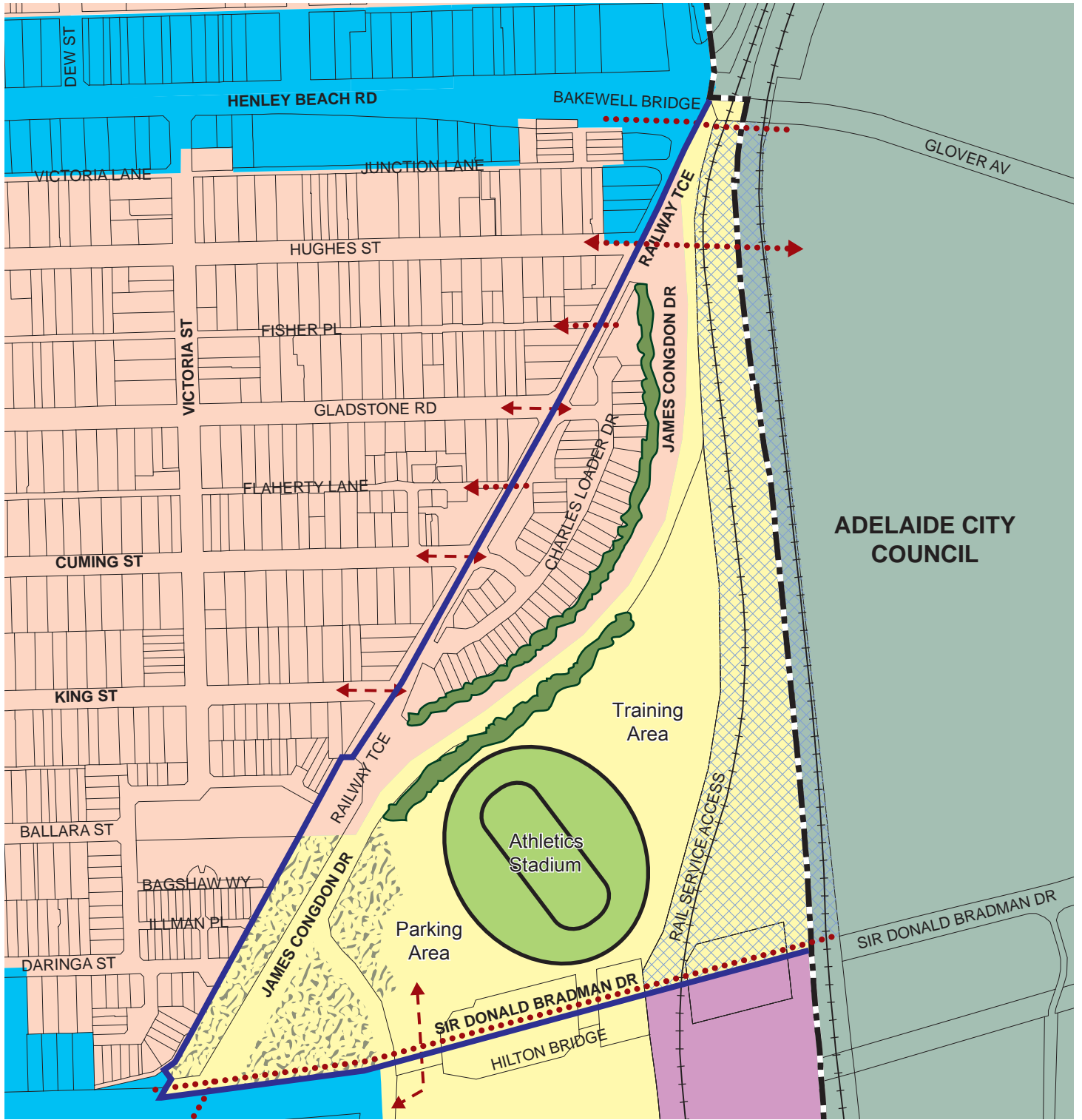
- Commercial
- Industrial
- Concept Plan Boundary
- Development Plan Boundary

- 1 Existing Office Development
- 2 Landscape and Amenities
- 2A Child Care / Recreational
- 2B Retail / Service Uses
- 2C Stormwater Detention
- 3 Office and Commercial - including Retail Showroom to corner
- 4 Office and Commercial
- 5 Office Development
- 5A Stormwater Detention
- 6 Deck Carparking















# Concept Plan Map WeTo/9

## OFFICE PARK COMMERCIAL



ADELAIDE CITY COUNCIL

-  Concept Plan Boundary
-  Open Space
-  Rail Transport Corridor
-  Landscape Buffer
-  Proposed Pedestrian / Cycle Route
-  Proposed Access Point (two way)
-  Development Plan Boundary

-  Commercial
-  Community Facilities
-  Industrial
-  Recreation
-  Residential

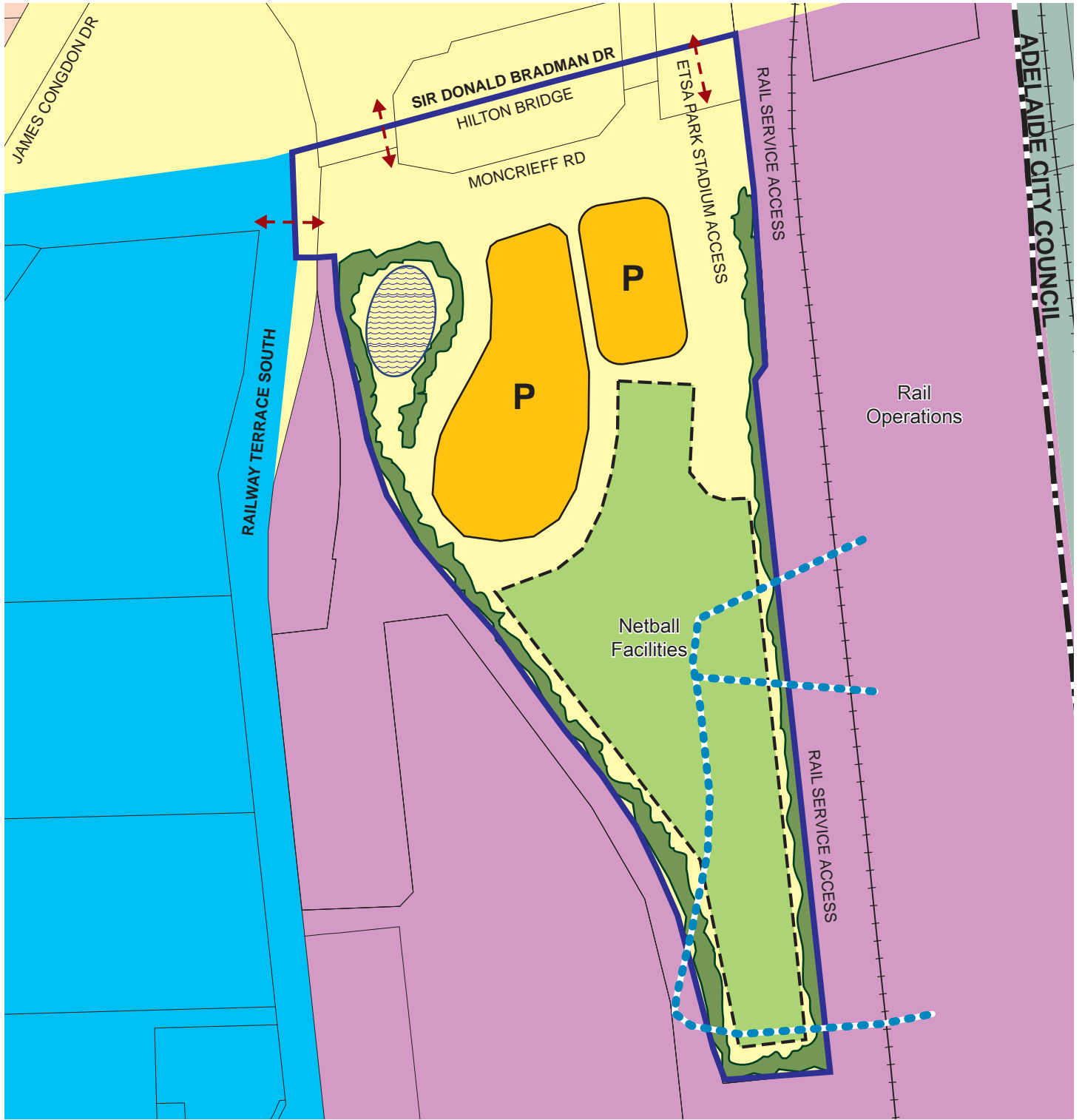










1:5,000








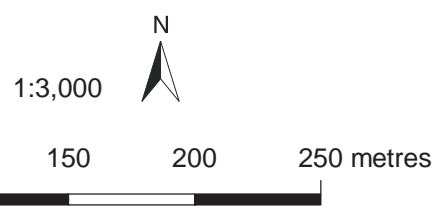
# Concept Plan Map WeTo/10

## MILE END RECREATION



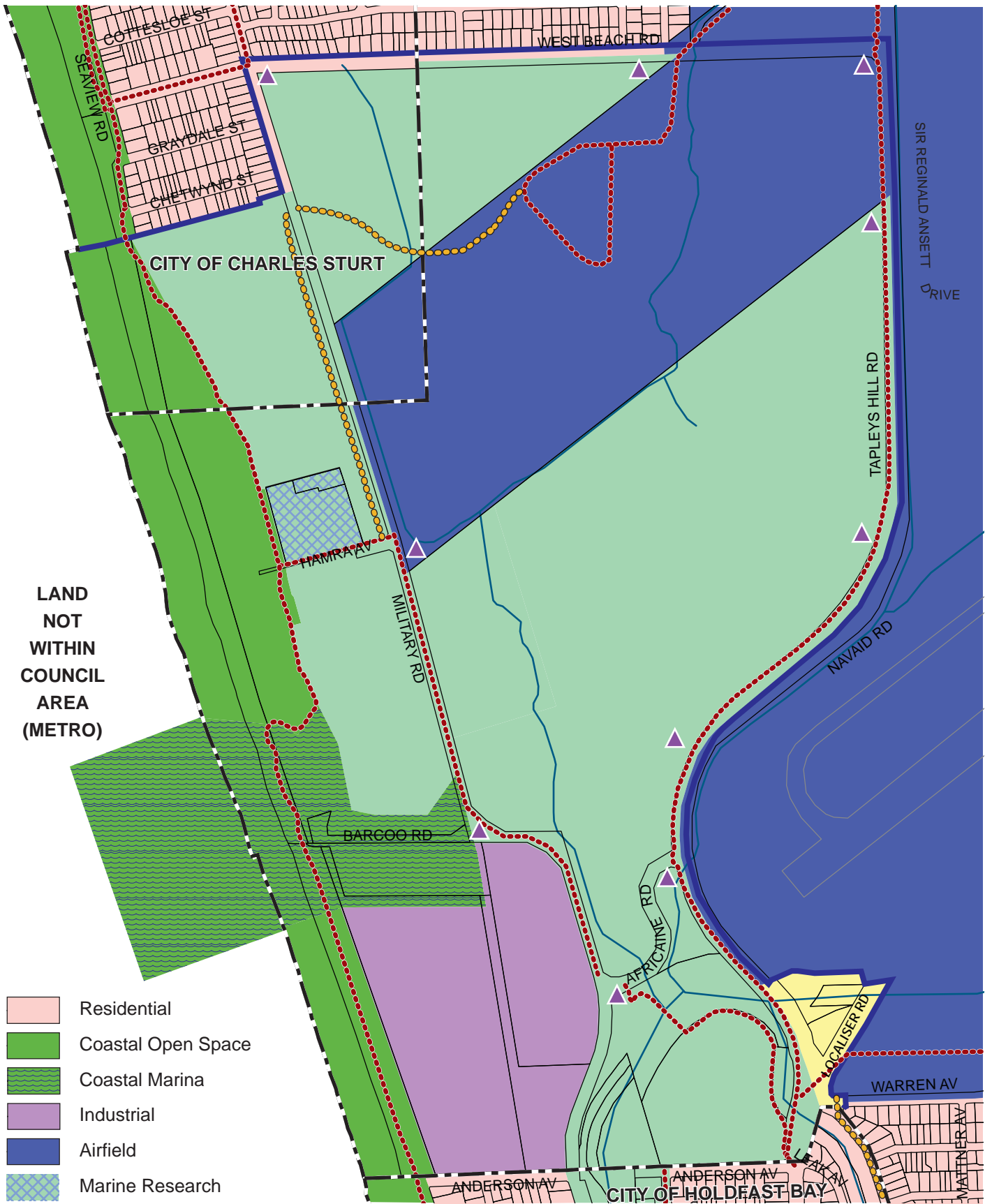
-  Concept Plan Boundary
-  Preferred Building Location
-  Car Parking
-  Detention Basin
-  Existing Stormwater Infrastructure
-  Landscape Buffer
-  Proposed Access Point (two way)
-  Development Plan Boundary

-  Commercial
-  Industrial
-  Recreation
-  Community Facilities
-  Residential



# Concept Plan Map WeTo/11

## MILE END SOUTH RECREATION



LAND NOT WITHIN COUNCIL AREA (METRO)

- Residential
- Coastal Open Space
- Coastal Marina
- Industrial
- Airfield
- Marine Research
- Recreation And Tourism
- Community

- Future Art Feature
- Existing Bike and Pedestrian Path
- Future Bike and Pedestrian Path
- Water Courses
- Concept Plan Boundary
- Development Plan Boundary

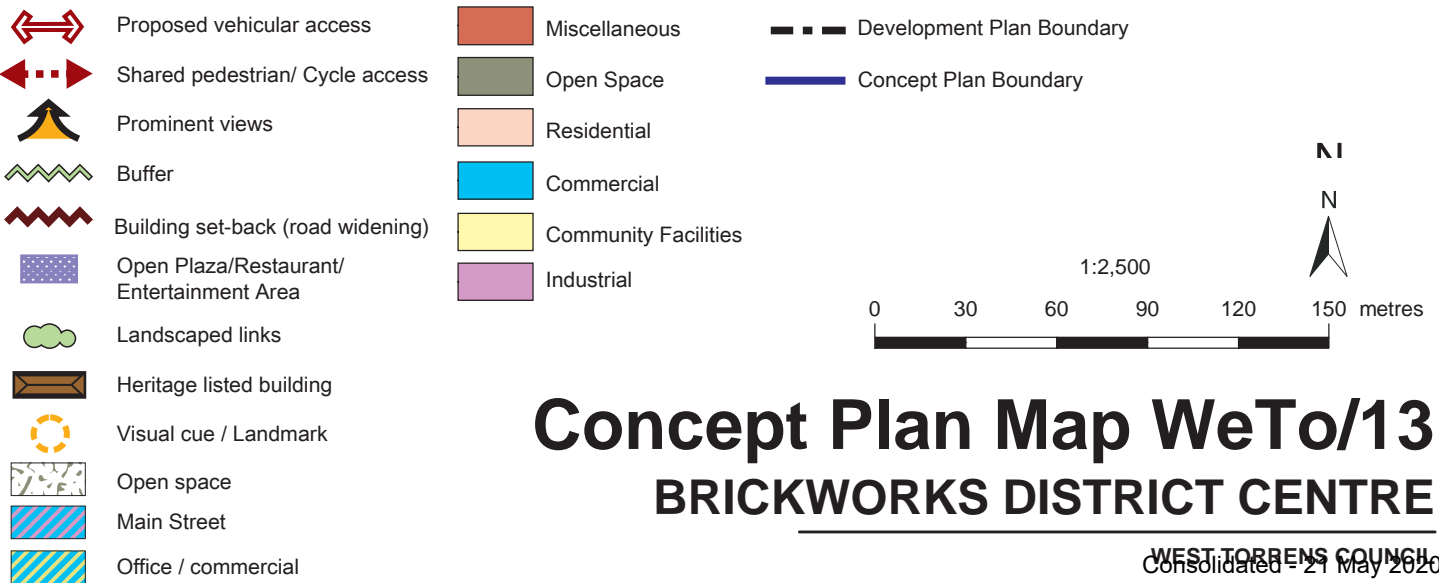
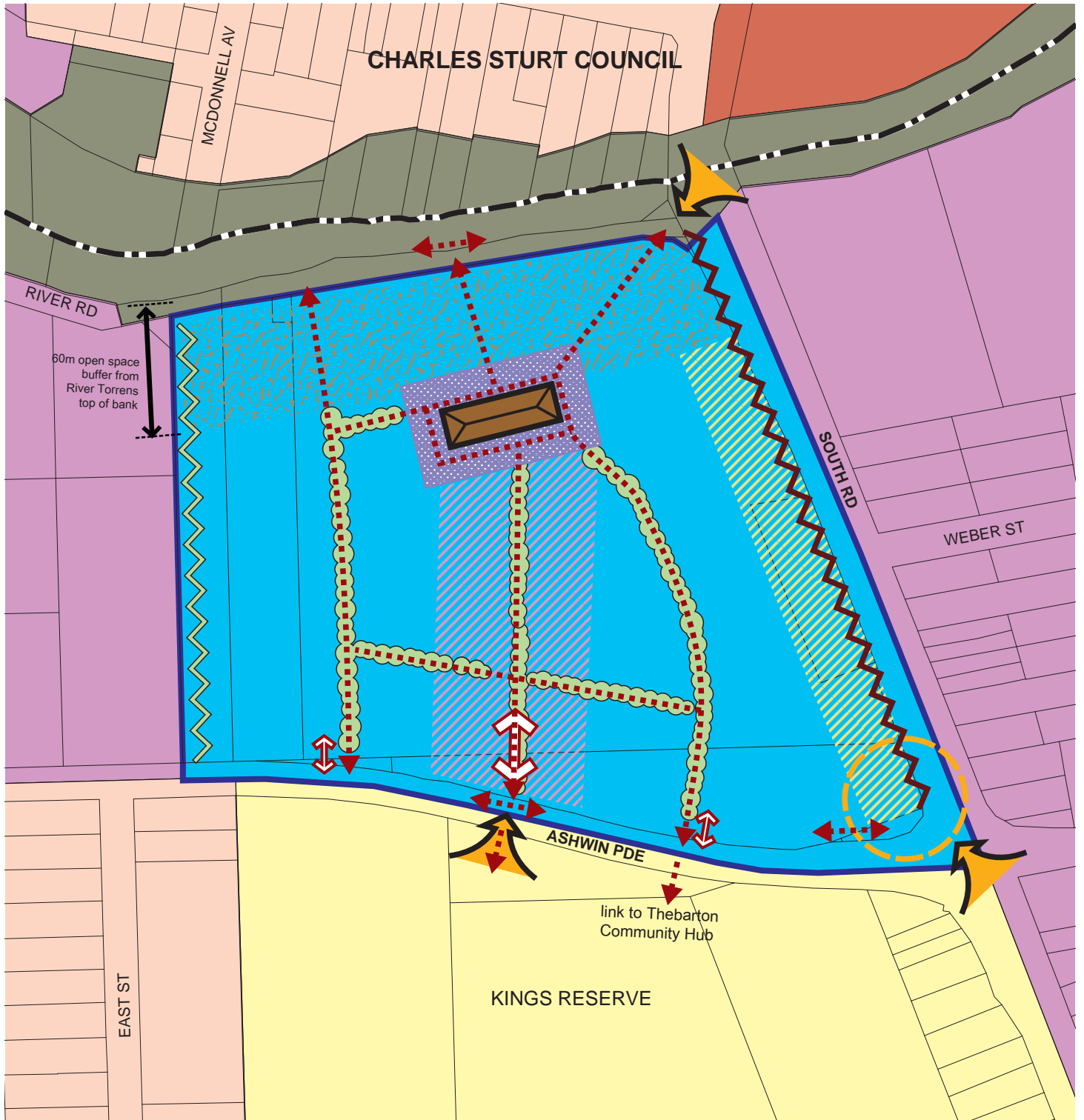


0 100 200 300 400 500 metres

# Concept Plan Map WeTo/12

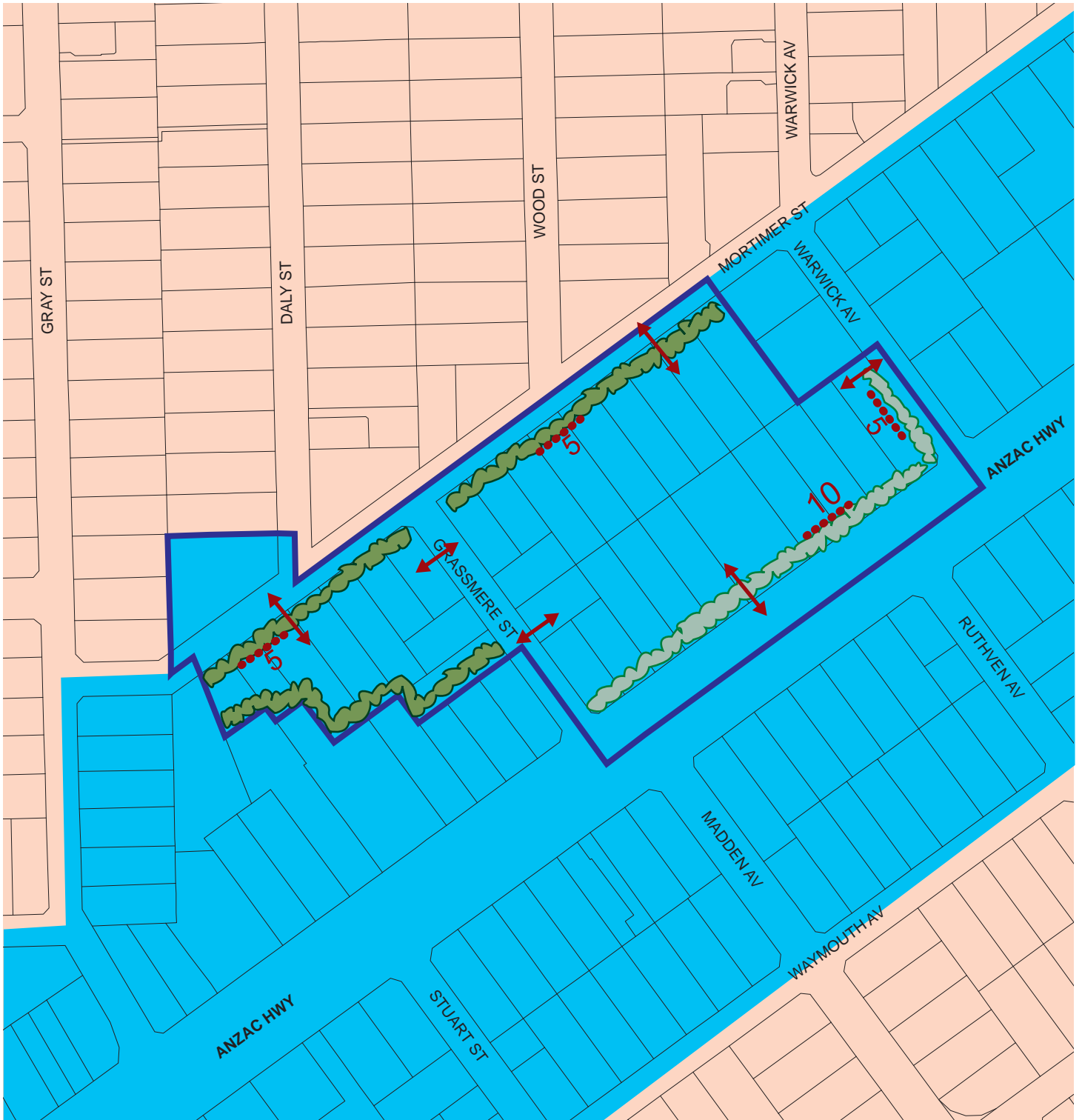
## ADELAIDE SHORES










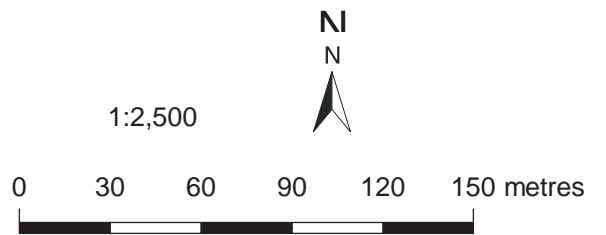


# Concept Plan Map WeTo/13

## BRICKWORKS DISTRICT CENTRE

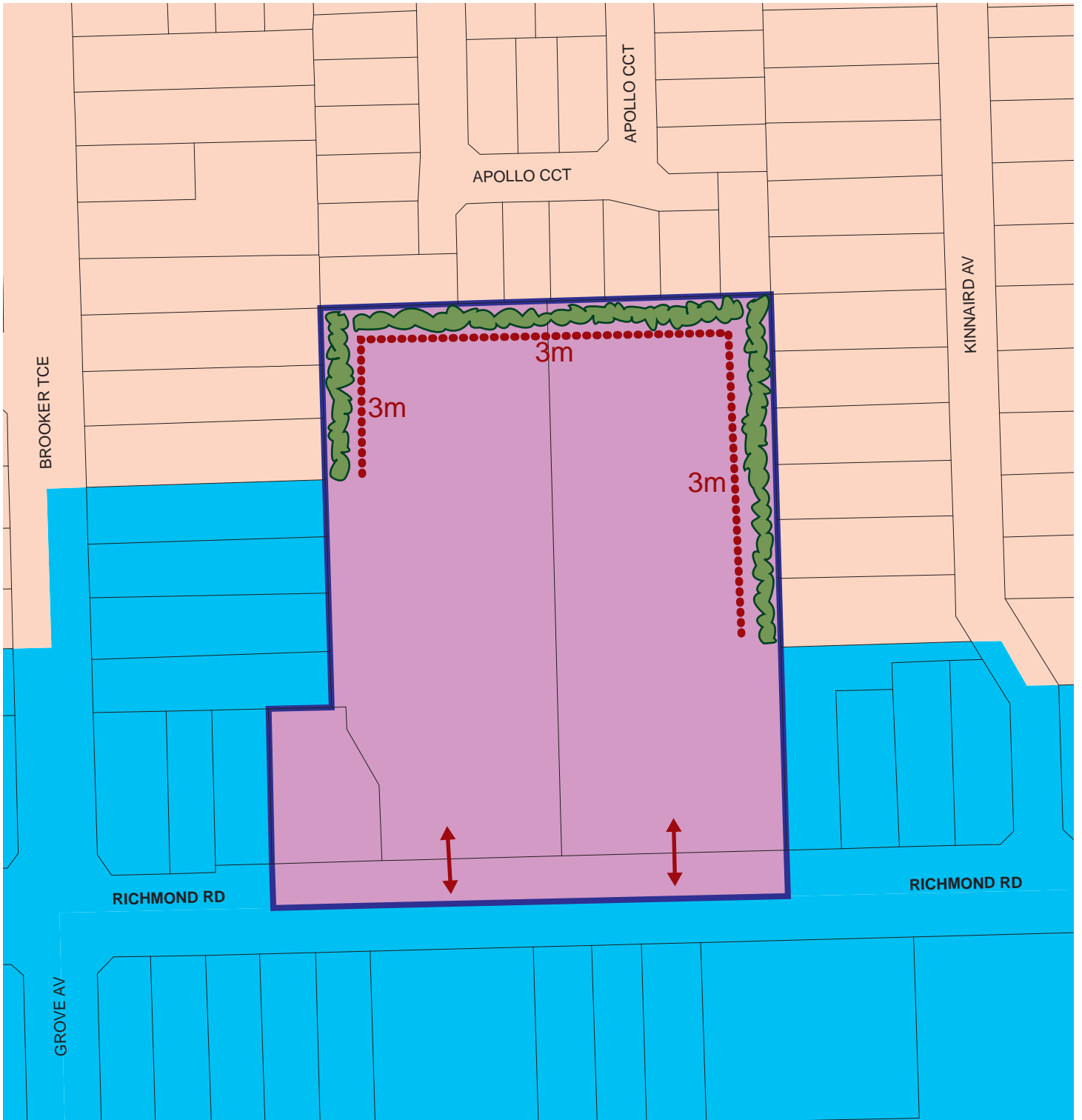









-  Concept Plan Boundary
-  Extensive Landscaping
-  Screen Landscaping
-  Road Setback (metres)
-  Vehicle Access
-  Commercial
-  Residential



# Concept Plan Map WeTo/14

## KURRALTA PARK DISTRICT CENTRE



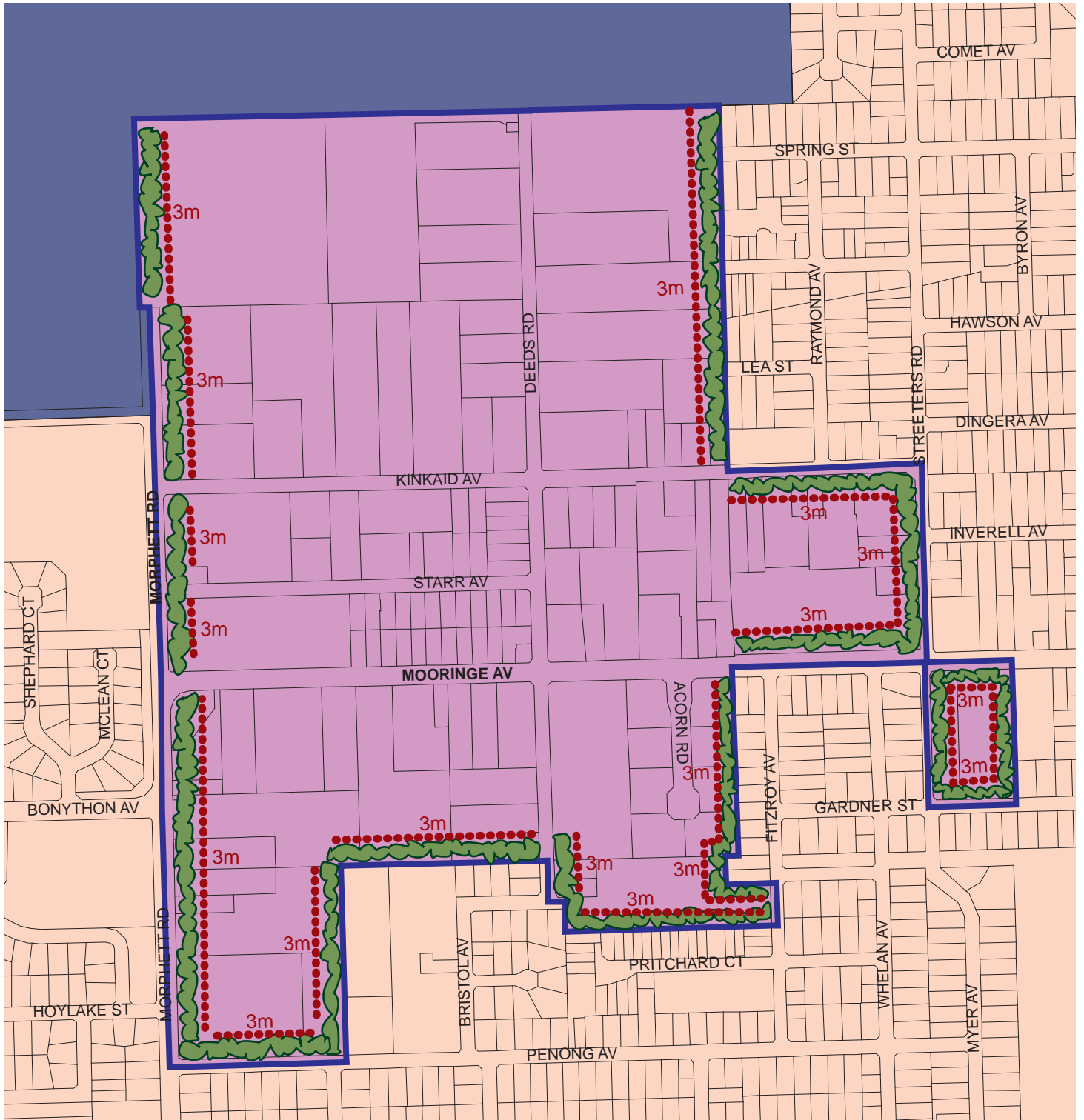
-  Concept Plan Boundary
-  Landscape Buffer
-  Building Setback
-  Vehicle Access
-  Commercial
-  Industrial
-  Residential




1:1,500



# Concept Plan Map WeTo/15

## FORMER APOLLO STADIUM INDUSTRY



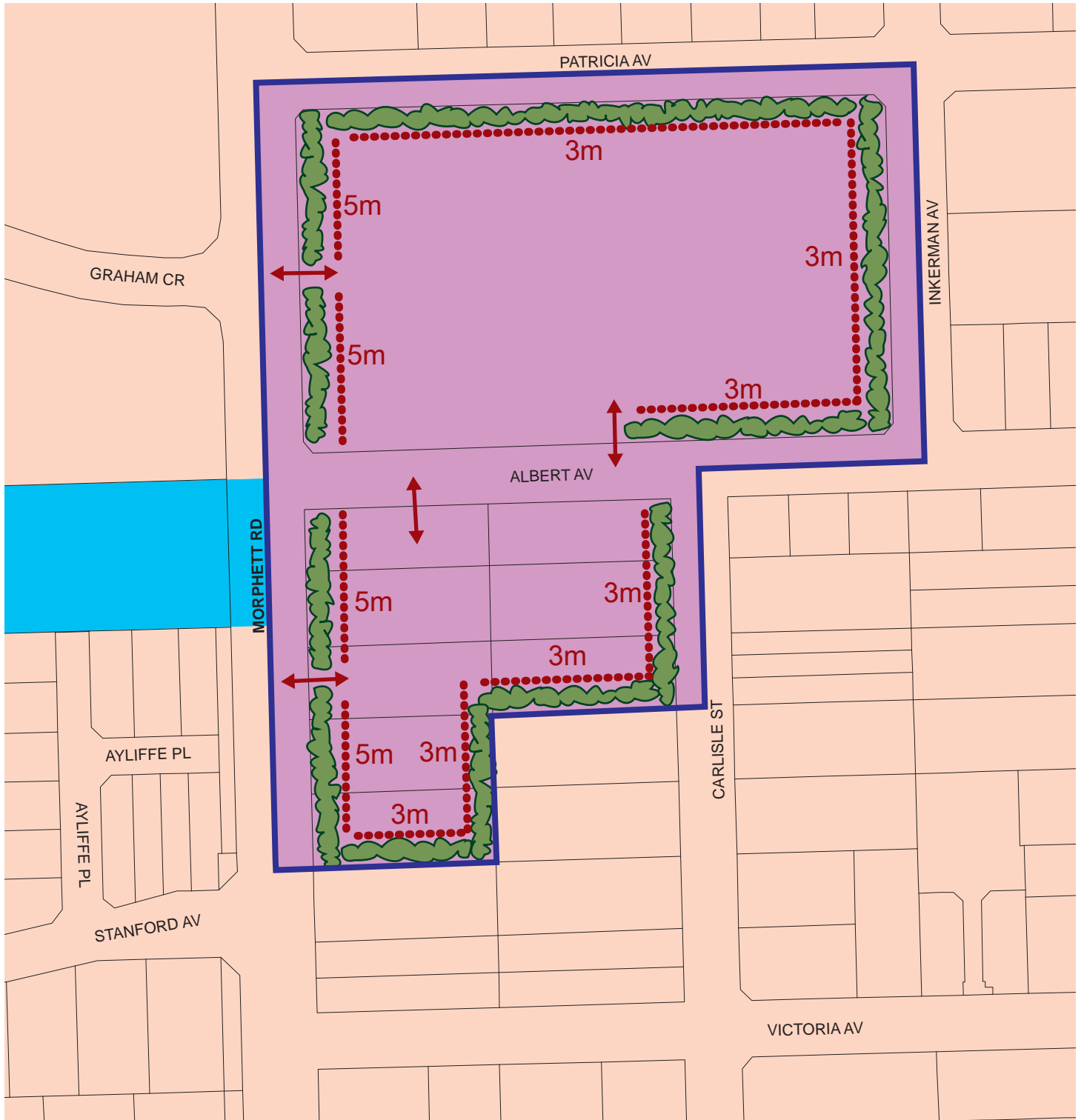
-  Concept Plan Boundary
-  Landscape Buffer
-  3m Building Setback
-  Industrial
-  Airport
-  Residential

1:6,000



# Concept Plan Map WeTo/16

## NORTH PLYMPTON / CAMDEN PARK INDUSTRY

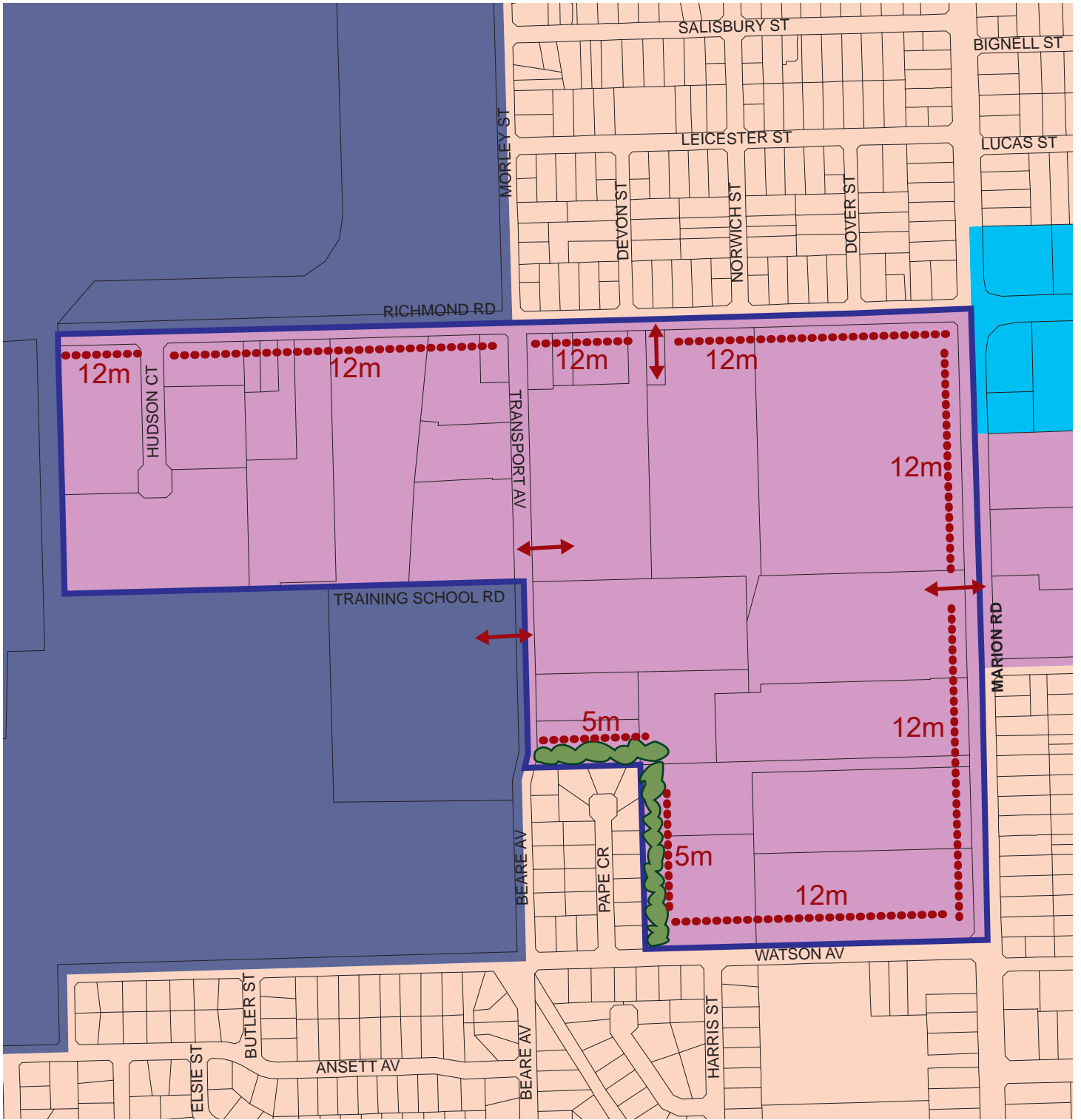


- Concept Plan Boundary
- Landscape Buffer
- 3m Building Setback
- Vehicle Access
- Commercial
- Industrial
- Residential

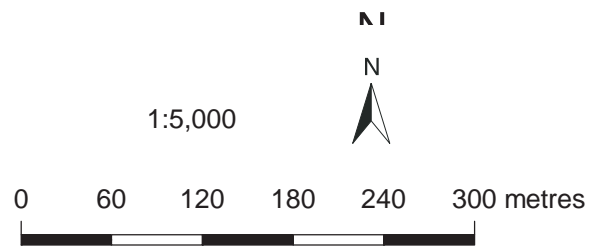


# Concept Plan Map WeTo/17

## NOVAR GARDENS INDUSTRY

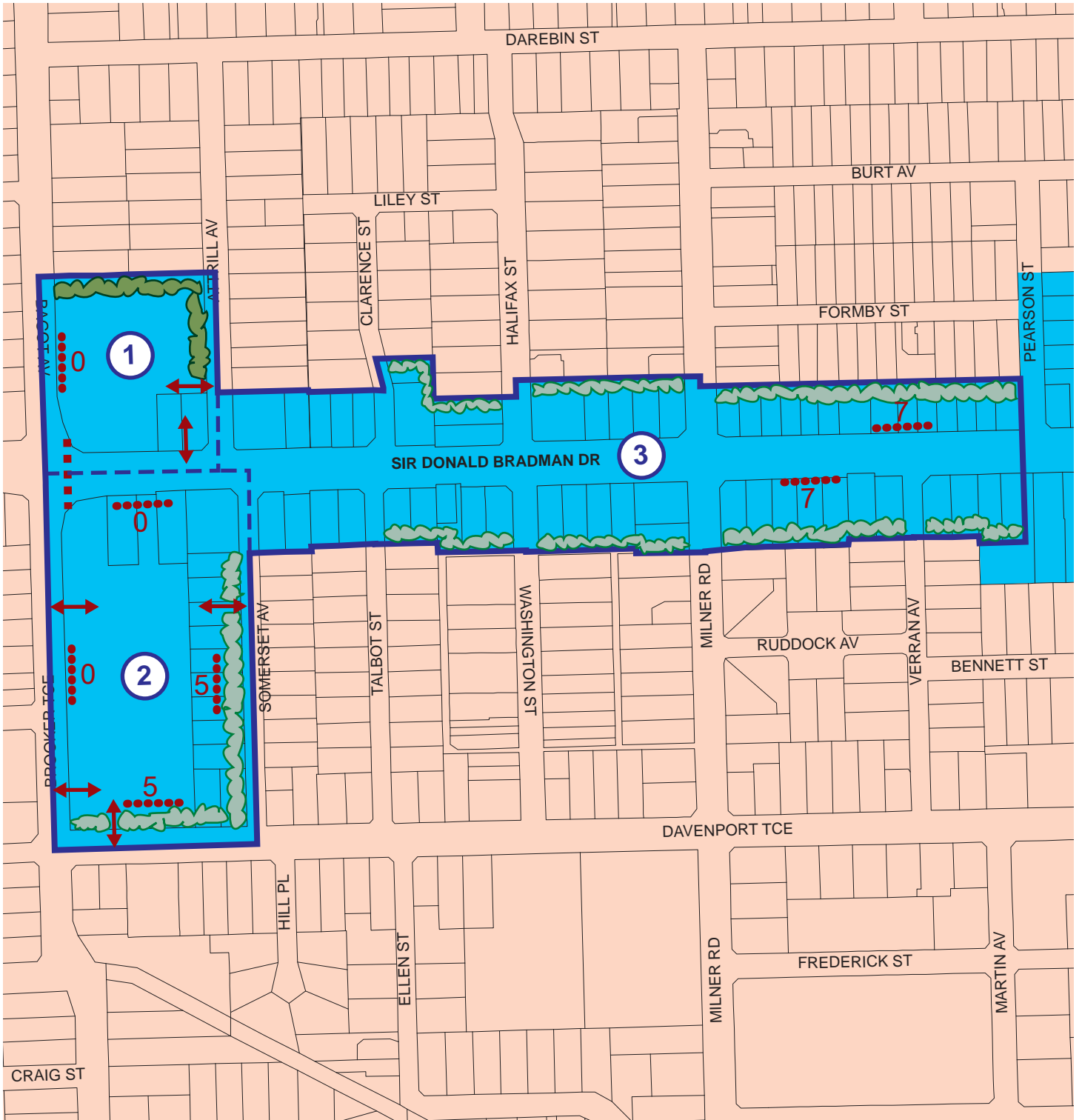


- Concept Plan Boundary
- Landscape Buffer
- 5m Building Setback
- Vehicle Access
- Commercial
- Industrial
- Infrastructure
- Residential



# Concept Plan Map WeTo/18

## NETLEY INDUSTRY



Concept Plan Boundary

Area Boundary

Extensive Landscaping

Screen Landscaping

Building Setback From Property Boundary (metres)

Vehicle Access

Pedestrian Route

Commercial

Residential

**AREAS**

Retail Core

Civic/Community

Office/Commercial

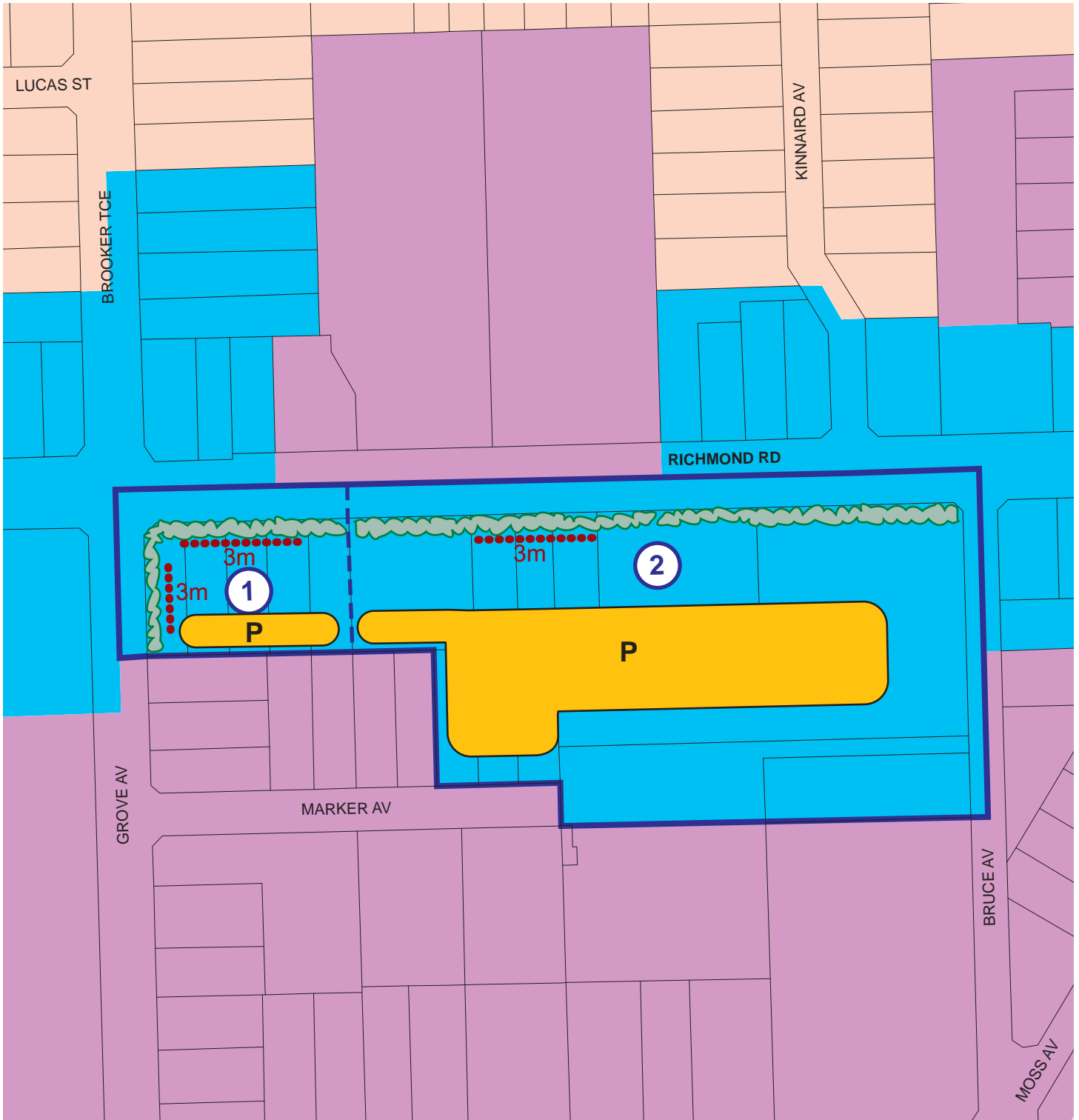
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# Concept Plan Map WeTo/19

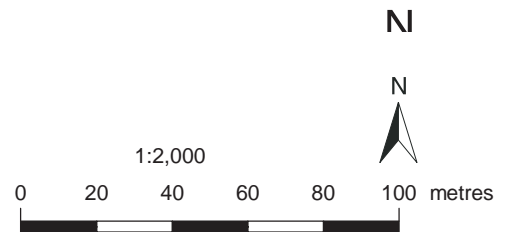
## HILTON

### NEIGHBOURHOOD CENTRE



-  Concept Plan Boundary
-  Area Boundary
-  Car Parking
-  Extensive Landscaping
-  Road Setback (metres)
-  Commercial
-  Industrial
-  Residential

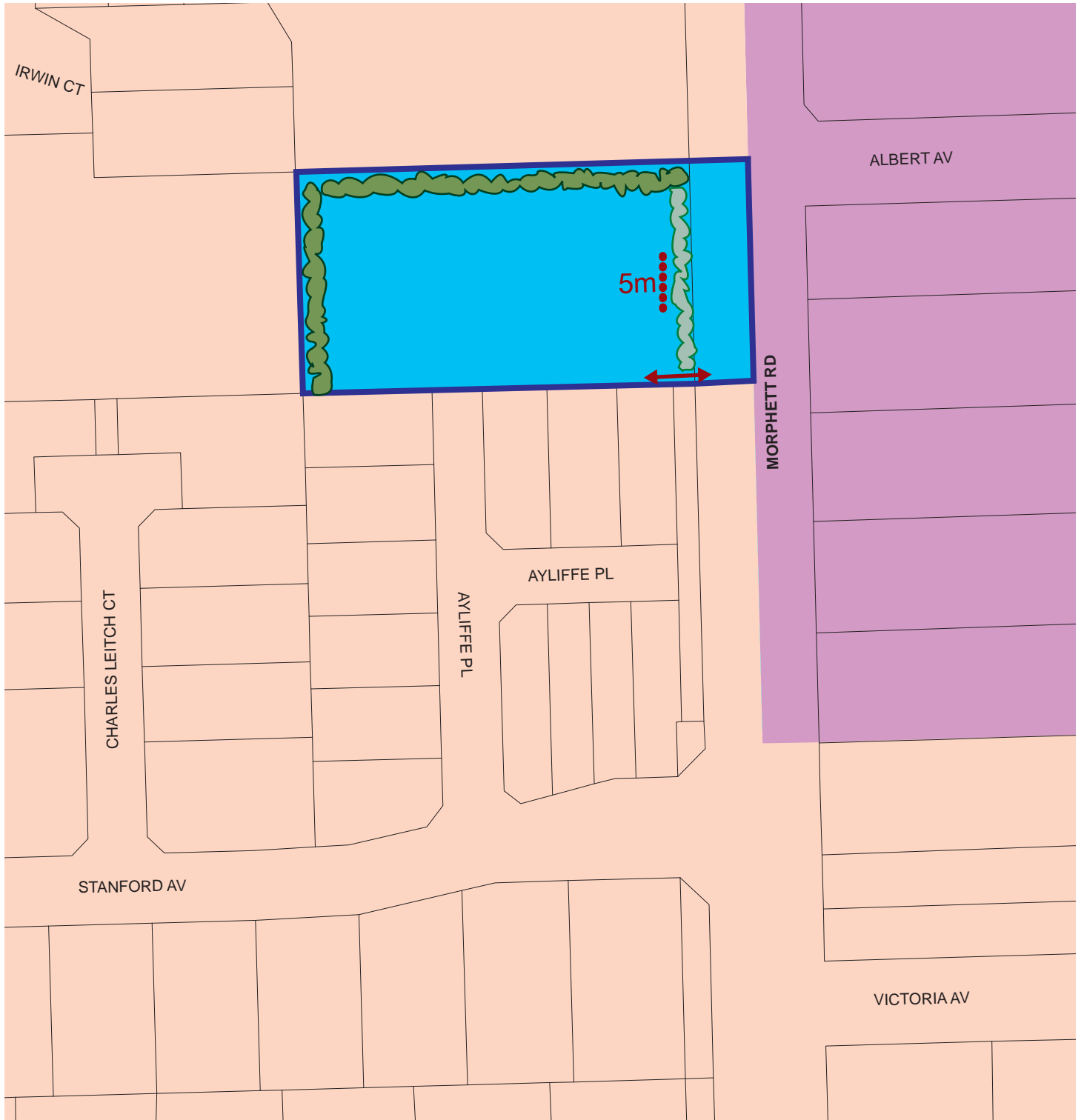
- AREAS**
-  Retail Core
  -  Recreation/Community











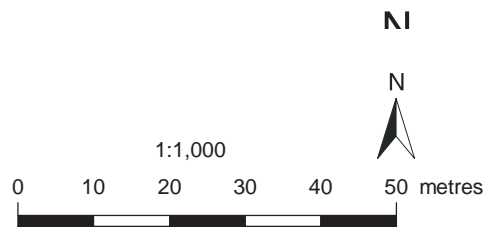
# Concept Plan Map WeTo/20

## MARLESTON NEIGHBOURHOOD CENTRE





-  Concept Plan Boundary
-  Extensive Landscaping
-  Screen Landscaping
-  5m Building Setback
-  Vehicle Access
-  Commercial
-  Industrial
-  Residential



# Concept Plan Map WeTo/21

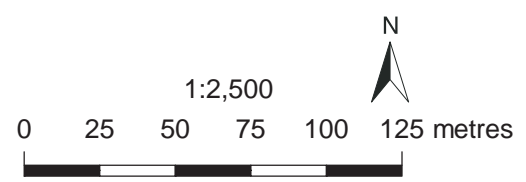
## NOVAR GARDENS

## NEIGHBOURHOOD CENTRE



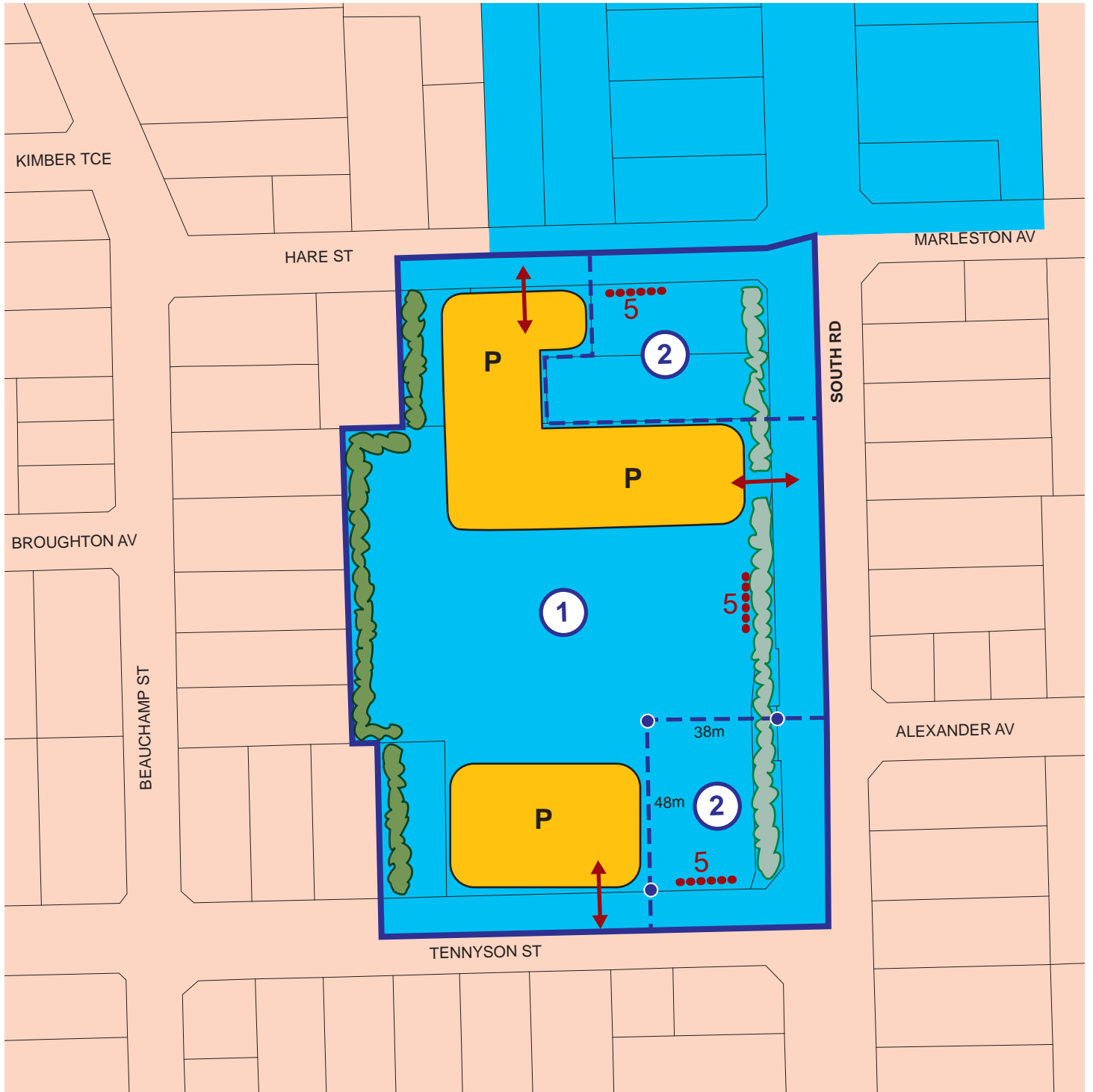
- Concept Plan Boundary
- Area Boundary
- Intensive Landscaping
- Road Setback (metres)
- Vehicle Access
- Pedestrian Route
- P Car Parking Areas
- Commercial
- Residential

- AREAS**
- 1 Retail Core
  - 2 Retail/Office/Commercial



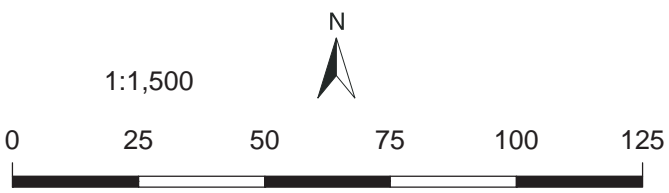
# Concept Plan Map WeTo/22

## RICHMOND NEIGHBOURHOOD CENTRE



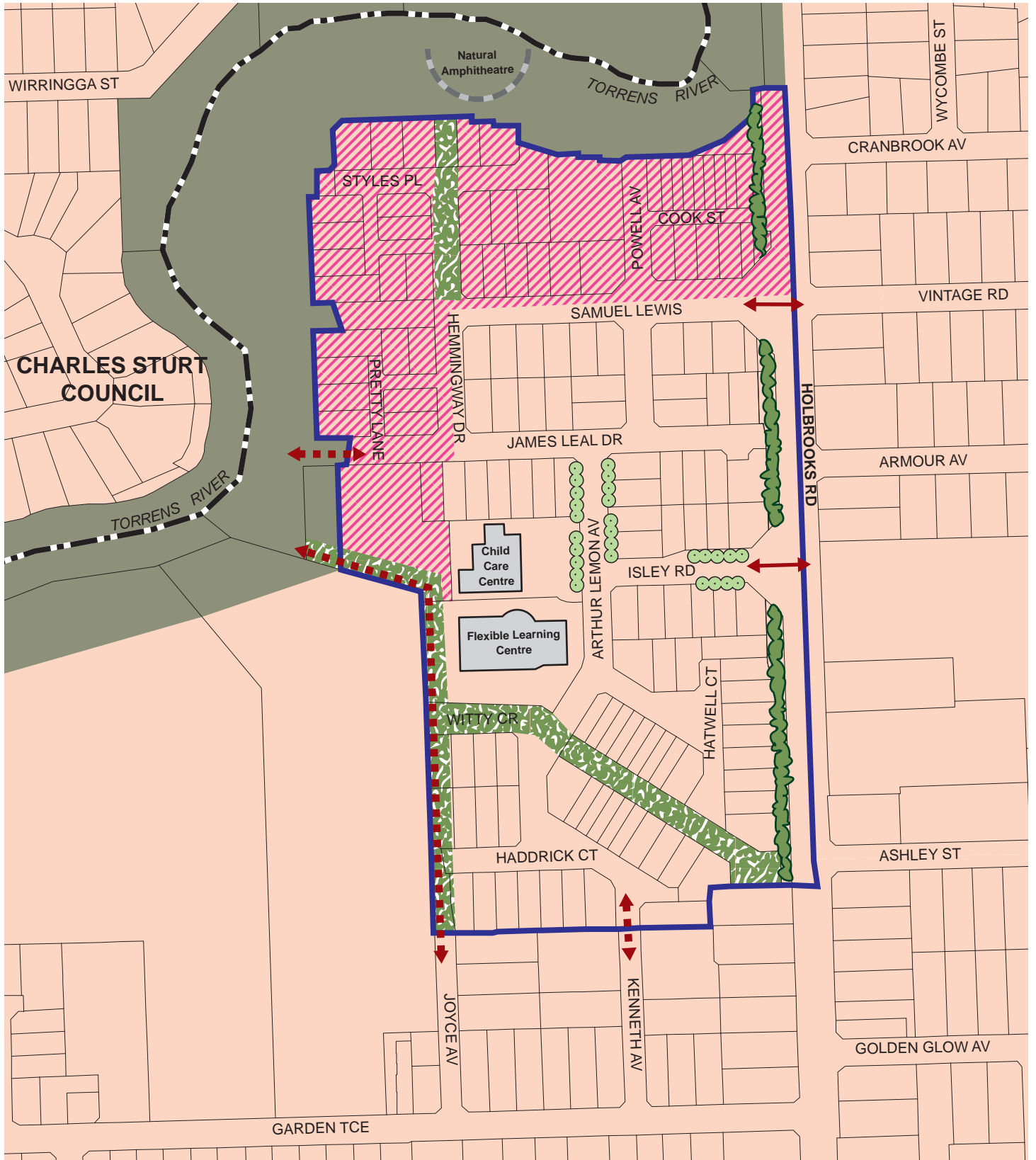
- Concept Plan Boundary
- Area Boundary
- Extensive Landscaping
- Screen Landscaping
- Road Setback (metres)
- Vehicle Access
- Car Parking Areas












- AREAS**
- Retail Core
  - Retail/Office/Commercial
  - Commercial
  - Residential

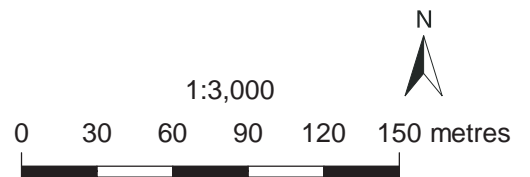


# Concept Plan Map WeTo/23

## TENNYSON STREET NEIGHBOURHOOD CENTRE

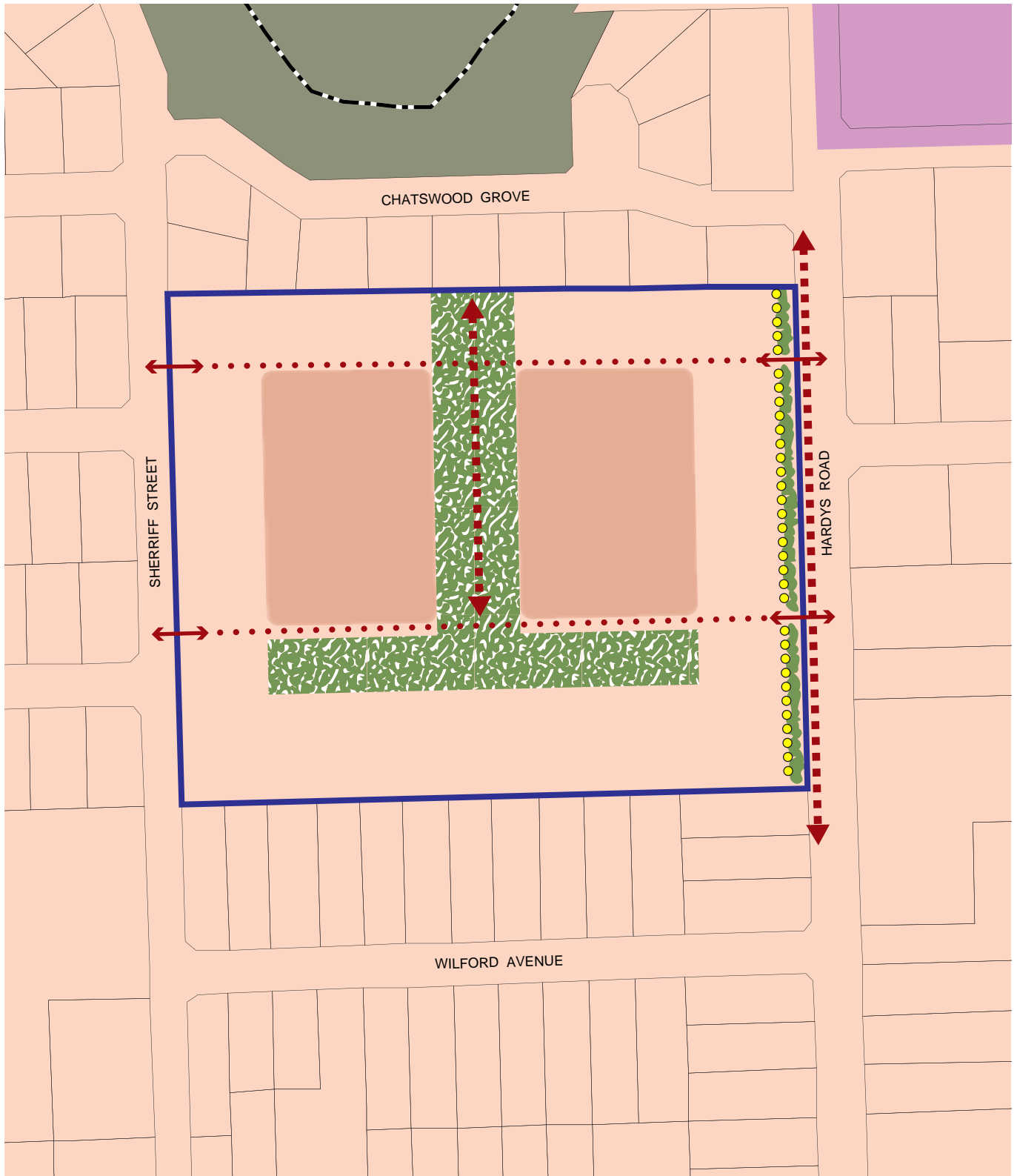


-  Concept Plan Boundary
-  Medium Density Residential
-  Acoustic Protection
-  Residential
-  Tree Lined Avenues
-  Green Connections
-  Open Space
-  Vehicle Access
-  Pedestrian Access
-  Buildings
-  Development Plan Boundary



# Concept Plan Map WeTo/24

## RESIDENTIAL PROMENADE



Lamberts Conformal Conic Projection, GDA94

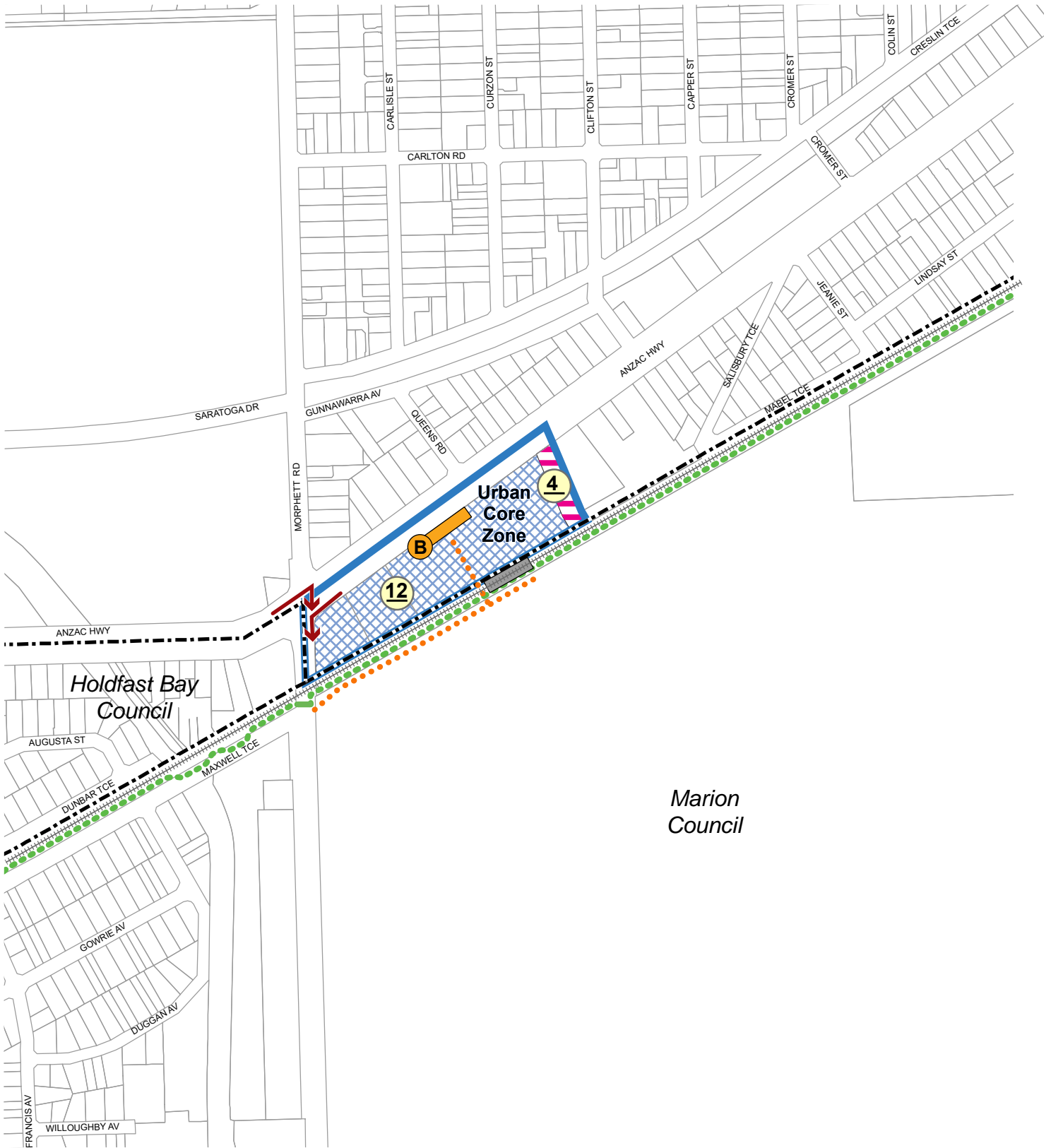
**LEGEND**

- Vehicle and Pedestrian Access
- Indicative indirect east/west local road connection
- Green connection / public open space
- Landscaping
- Cycle / pedestrian shared access
- 5m Building setback from Hardys Road
- Reduced minimum/average site area
- Concept Area Boundary
- Development Plan Boundary



# Concept Plan Map WeTo/25

## UNDERDALE URBAN RENEWAL



- 12 Maximum building height (storeys)
- Transition Area
- Core Area
- Right-in Only
- B Bus Stop (indented off-road)
- Mike Turtur Bikeway
- Potential Pedestrian/Cycle Access
- Tram Line
- Potential Future Tram Stop

- Urban Core Zone Boundary
- Development Plan Boundary



# Concept Plan Map WeTo/26

## MORPHETTVILLE RACECOURSE