

Development Plan

Gawler (CT)

Consolidated - 18 July 2019

Please refer to the Gawler (CT) page at www.sa.gov.au/developmentplans to see any amendments not consolidated.



Government of South Australia

Department of Planning,
Transport and Infrastructure

The following table is a record of authorised amendments and their consolidation dates for the Gawler (CT) Development Plan since the inception of the electronic Development Plan on 12 December 1996 for Metropolitan Adelaide Development Plans. Further information on authorised amendments prior to this date may be researched through the relevant Council, Department of Planning, Transport and Infrastructure or by viewing Gazette records.

CONSOLIDATED	AMENDMENT – [Gazetted date]
12 December 1996	Miscellaneous Amendments – Metropolitan Adelaide Development Plans PAR (Interim) (<i>Ministerial</i>) – [12 December 1996]
26 June 1997	Miscellaneous Amendments – Metropolitan Adelaide Development Plans PAR (<i>Ministerial</i>) – [26 June 1997]
30 April 1998	Gawler Town Centre PAR – [30 April 1998] Section 29(2) Amendment – [30 April 1998]
30 July 1998 (reprinted 27 May 99)	Section 29(2) Amendment – [30 July 1998]
28 October 1999	Waste Disposal (Landfill) PAR (<i>Ministerial</i>) – [19 August 1999] Outdoor Advertising PAR – [28 October 1999]
9 March 2000	Metropolitan Adelaide – Industrial Land and Development PAR (<i>Ministerial</i>) – [25 November 1999] Local Heritage Places PAR (Interim) – [9 March 2000] Historic (Conservation) Policy Areas PAR (Interim) – [9 March 2000] Rural Living in Gawler PAR (Interim) – [9 March 2000] Section 27(5) Amendment - Waste Disposal (Landfill) PAR (<i>Ministerial</i>) – [9 March 2000]
6 April 2000	Section 29(2)(a) Amendment – [23 March 2000]
Not Consolidated	Telecommunications Facilities State-wide Policy Framework PAR Interim (<i>Ministerial</i>) – [31 August 2000]
9 March 2001	Metropolitan Adelaide Significant Tree Control PAR (<i>Ministerial</i>) – [21 December 2000] Local Heritage Places PAR – [8 March 2001] Historic (Conservation) Policy Areas PAR – [8 March 2001] Cessation of the interim operation of the 'Rural Living in Gawler PAR' on 9 March 2001 and its removal from the Gawler (CT) Development Plan – [15 March 2001]
15 March 2001	Rural Living in Gawler PAR – [15 March 2001]
18 May 2001	Development Plan Confirmation PAR – [18 May 2001]
6 September 2001	Telecommunications Facilities State-wide Policy Framework PAR (<i>Ministerial</i>) – [30 August 2001]
24 January 2002	Urban Growth Boundary PAR (Interim) (<i>Ministerial</i>) – [18 January 2002]
	<i>Note – the Urban Growth Boundary PAR (Interim) (Ministerial) was quashed by order of the Supreme Court dated 5 March 2002. This order quashed interim operation of the PAR from its introduction on 18 January 2002. The Development Plan reverts to the previous consolidated version dated 6 September 2001.</i>
28 March 2002	Metropolitan Urban Boundary PAR (Interim) (<i>Ministerial</i>) – [22 March 2002]
9 May 2002	Development Plan Confirmation PAR – [9 May 2002]
27 June 2002	Gawler Rivers Flood Plain PAR (Interim) – [27 June 2002]
19 September 2002	Car Parking PAR – [19 September 2002]
27 March 2003	Gawler Rivers Flood Plain PAR – [20 March 2003] Metropolitan Urban Boundary PAR (<i>Ministerial</i>) – [20 March 2003]
16 October 2003	Wind Farms PAR (<i>Ministerial</i>) – [24 July 2003]
19 February 2004	Residential 1 Zone – Orderly Development PAR (Interim) – [19 February 2004]
23 December 2004	Residential 1 Zone – Orderly Development PAR – [23 December 2004]
17 February 2005	Business Zone PAR – [10 February 2005]
24 November 2005	Gawler Urban Boundary PAR (<i>Ministerial</i>) (Interim) – [17 November 2005]

CONSOLIDATED	AMENDMENT – [Gazetted date]
16 November 2006	Gawler Urban Boundary PAR (<i>Ministerial</i>) – [26 October 2006] Residential, Town Centre and Development Plan Format PAR – [16 November 2006]
8 February 2007	Bushfire Management (Part 2) PAR (<i>Ministerial</i>) – [14 December 2006]
13 September 2007	Section 29(2) Amendment – [6 September 2007]
10 January 2008	Residential Parks and Caravan and Tourist Parks DPA (<i>Ministerial</i>) (Interim) – [13 December 2007]
14 February 2008	Section 29(1)(a) Amendment – [14 February 2008]
11 December 2008	Residential Parks and Caravan and Tourist Parks DPA – (<i>Ministerial</i>) – [11 December 2008]
29 October 2009	Section 27(5)(a) Amendment – Residential Parks and Caravan and Tourist Parks DPA – [29 October 2009]
21 January 2010	Bushfire (Miscellaneous Amendments) DPA (Interim) (<i>Ministerial</i>) – [10 December 2009]
18 February 2010	Gawler Racecourse DPA (<i>Ministerial</i>) – [18 February 2010]
25 February 2010	Section 29(2)(b)(ii) Amendment – [25 February 2010]
3 June 2010	Statewide Bulky Goods DPA (<i>Ministerial</i>) (Interim) – [1 June 2010]
26 August 2010	Gawler East DPA (<i>Ministerial</i>) – [26 August 2010]
14 July 2011	Bushfires (Miscellaneous Amendments) DPA (<i>Ministerial</i>) – [9 December 2010] Statewide Bulky Goods DPA (<i>Ministerial</i>) – [13 January 2011] New Southern Urban Areas DPA – [14 July 2011]
17 May 2012	Statewide Wind Farms DPA (Interim) (<i>Ministerial</i>) – [19 October 2011] Regulated Trees DPA (Interim) (<i>Ministerial</i>) – [17 November 2011] Willaston Main Street DPA – [17 May 2012]
11 July 2013	Termination of the Statewide Wind Farms DPA (<i>Ministerial</i>) and its removal from the Gawler (CT) Development Plan – [18 October 2012] Statewide Wind Farms DPA (<i>Ministerial</i>) – [18 October 2012] Regulated Trees DPA (<i>Ministerial</i>) - [15 November 2012] Section 29(2)(c) Amendment - [29 November 2012] Gawler River Flood Prone Areas DPA – [4 July 2013] Section 29(2)(b)(ii) Amendment – [11 July 2013]
27 November 2014	Hillier DPA – [27 November 2014]
30 April 2015	Section 29(2)(b)(ii) Amendment – [30 April 2015]
28 April 2016	Existing Activity Centres Policy Review DPA (<i>Ministerial</i>) – [21 April 2016]
20 February 2018	Evanston Gardens DPA – [20 February 2018] Commercial and Rural Areas DPA – [20 February 2018]
18 July 2019	Gawler East Structure Plan DPA – [11 July 2019]

Consolidated: The date of which an authorised amendment to a Development Plan was consolidated (incorporated into the published Development Plan) pursuant to section 31 of the *Development Act 1993*.

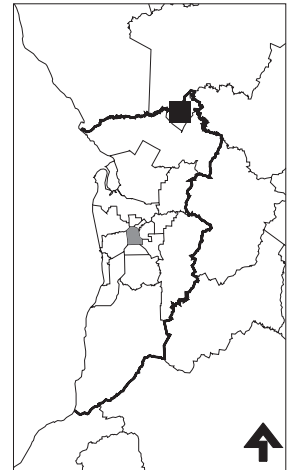
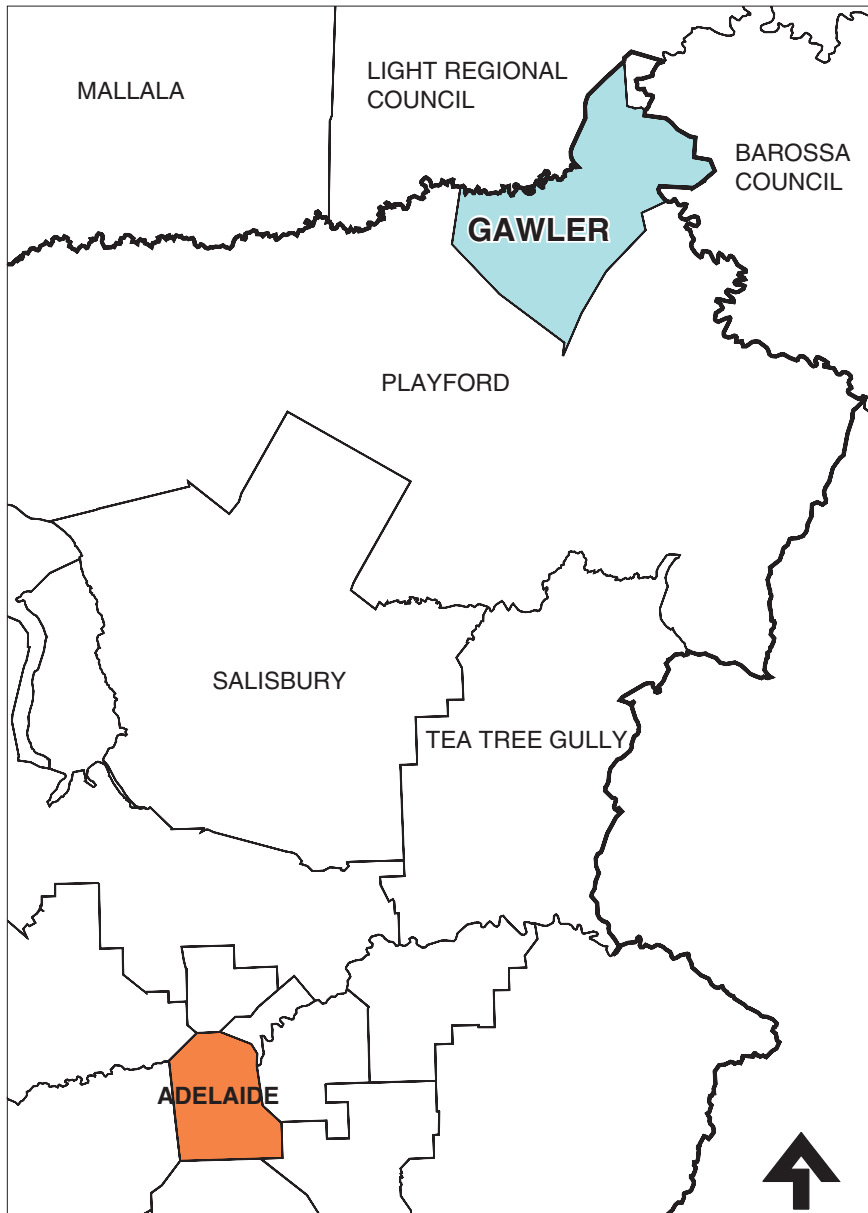
Gazetted: The date of which an authorised amendment was authorised through the publication of a notice in the Government Gazette pursuant to Part 3 of the *Development Act 1993*.

GAWLER (CT)

Preface

The objectives and principles of development control policies that follow apply within the area of the GAWLER (CT) Development Plan as shown on Map Ga/1.

The Development Plan is arranged with the policies for Metropolitan Adelaide, appearing first, followed by the Council Wide policies and in turn more detailed policies relating to particular zones and areas.



Location Map

Enlargement Map


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GAWLER (CT)

Introduction

In addition to the applicable zone policies, the Council Wide polices apply to the area shown on [Map Ga/2](#).

Background

The Town of Gawler located 40km north of Adelaide and 10 km west of the Barossa Valley was South Australia's first country town. It is positioned at the northern fringe of the Adelaide Metropolitan area.

Gawler has a number of distinguishing features; an original town plan prepared by Colonel William Light; a varied topography; and a significant legacy of historic buildings dating from several periods of economic prosperity during the nineteenth & early twentieth century.

In the late 1800's Gawler attracted the soubriquet *the Colonial Athens* – an indicator of not only its burgeoning collection of fine buildings, but also its self-confidence as a place in society, cultural pursuits, commerce and services.

Since the advent of the motor vehicle, and consequent reductions in travel time, Gawler's independent role and significance within South Australia has declined. Influential in this change has been the rise in employment and services at Elizabeth and Munno Para which began to occur from the 1960's onwards.

Whilst its contemporary role has increasingly been seen as a dormitory suburb of Adelaide, the community has sought to restore and strengthen its identity by identifying itself as a metropolitan town, rather than a northern suburb of Adelaide.

Appearance of Land, Buildings and the Public Environment

OBJECTIVES

Objective 1: Amenity of localities not impaired by the appearance of land, buildings and objects.

Urban areas and the surrounding rural areas should be attractive and pleasant places in which to live, as well as being healthy and convenient. Close attention must be given to the aesthetic qualities of both natural and man-made features. The design of individual buildings should be of high standard and related to adjacent buildings where they are of a high quality or acceptable standard.

Development should contribute to the quality and complement the design and character of the public environment of its locality and should provide activity and interest at street level where possible. Inclusion of appropriate elements of public art is desirable.

Many roads through urban areas are lined with commercial, residential and industrial development of varying qualities, with uninhibited advertising and a mixture of street signs, street furniture and overhead wires which do not contribute positively to the urban environment. An improvement in the quality of development is highly desirable.

The face and the skyline of the Mount Lofty Ranges as seen from various points in the metropolitan area should not be spoilt by development.

PRINCIPLES OF DEVELOPMENT CONTROL

1 The appearance of land, buildings, and objects should not impair the amenity of the locality in which they are situated.

- 2 No development should impair:
- (a) the natural character of the face; or
 - (b) the skyline,
- of the Mount Lofty Ranges as seen from that part of metropolitan Adelaide located on the plains.
- 3 Development with frontage to the One Tree Hill Scenic Road shown on [Map Ga/1 \(Overlay 1\) Enlargements Part A and Part B](#) should enhance the function of the scenic road as a gateway to Gawler through the following siting and design techniques:
- (a) buildings are designed to follow the contour of the land and to minimise disturbance to the natural landform;
 - (b) landscaping is provided to form a distinctive landscape character along the road corridor;
 - (c) avoidance of solid fencing or where solid fencing is necessary ensuring a highly articulated finish.
- 4 Development should enhance public spaces in its locality, and reflect the character sought to be achieved in the relevant zone or policy area.
- 5 The apparent bulk, height and scale of any large buildings which may be required for specific functional reasons should be minimised by the careful articulation and composition of building forms and facades.
- 6 Services, mechanical plant and equipment should be located, and where necessary screened, so as not to be visible from adjacent streets and public places nor be detrimental to the character of items or areas of historic significance.
- 7 Development should be design to avoid undesirable microclimatic and solar access effects on other land or buildings, including effects of patterns of wind, temperature, daylight, sunlight, light overspill, glare and overshadow.
- 8 Development should be designed so as to not unduly affect levels of privacy of adjacent residential premises.
- 9 Development should not result in noise emission which would detrimentally affect the amenity of adjacent properties.
- 10 Development should not result in emission of atmospheric, liquid or other pollutants which would prejudice the amenity of adjacent properties.

Aviation and Building Safety

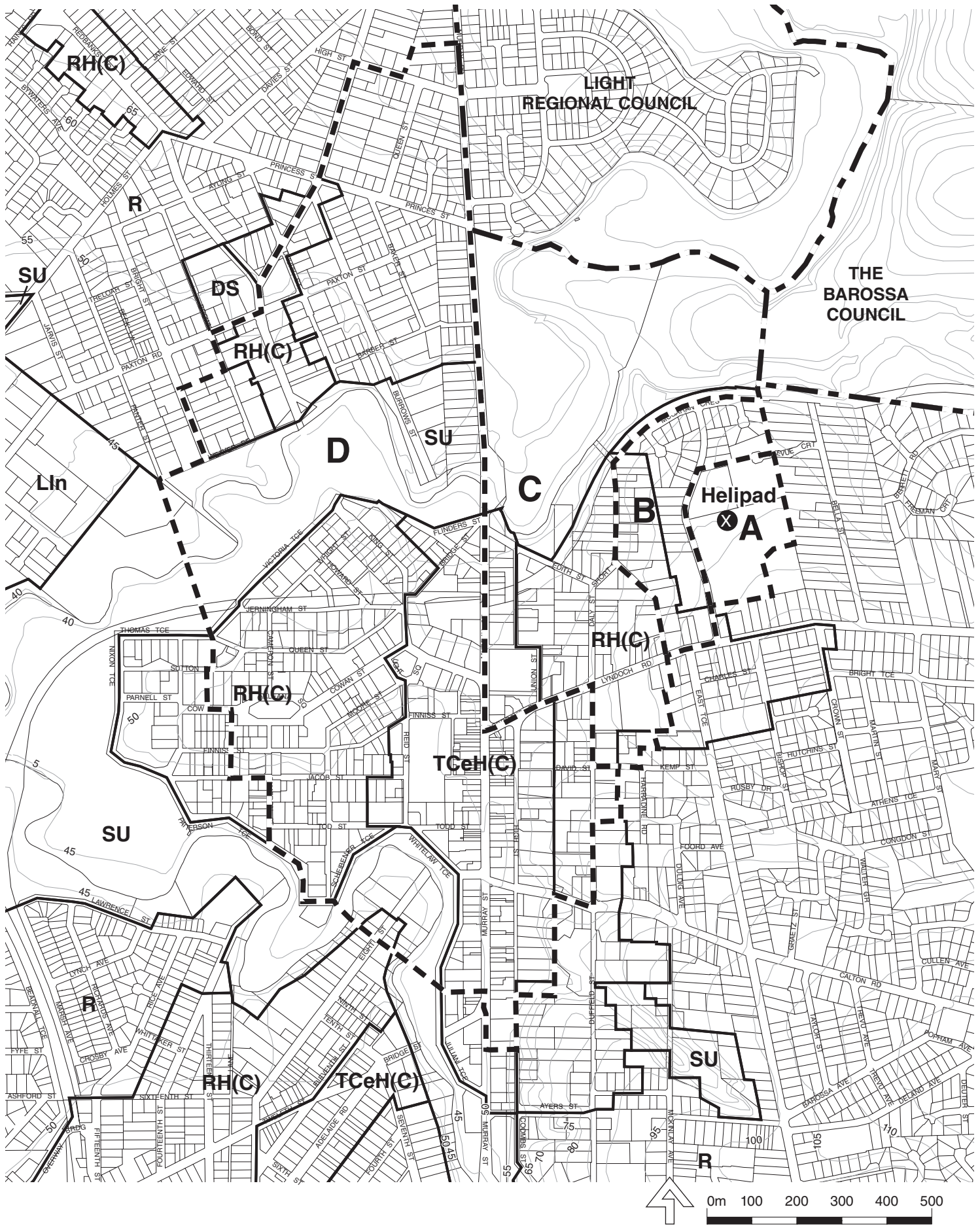
OBJECTIVE


Objective 2: Maintain the operational safety of the Gawler Health Services Helipad and safety of buildings and structures within the take-off and landing path of the helipad.

PRINCIPLES OF DEVELOPMENT CONTROL

- 11 Buildings and attached structures (including flagpoles, power and light poles, antennas), telecommunication structures, free standing structures and the mature height of landscaping should not exceed the acceptable heights indicated in the Table below for each Sector shown in [Figure Hel/1](#).

Note: The maximum possible heights are also indicated for each Sector but this is dependent on demonstrating that the development does not interfere with the Obstacle Limitation Surface for the Helipad.



-  5m Contours
-  Zone Boundary
-  Sector Boundary (A, B, C, D)
-  Development Plan Boundary

GAWLER (CT)
HELIPAD FLIGHTPATH
AND BUILDING HEIGHTS

FIGURE Hel/1
 Consolidated - 18 July 2019

Sector	Acceptable Height (Metres) above ground level	Maximum Possible Height (Metres) above ground level ¹
A	0	12
B	5	40
C	20	100
D	60	190

Building Set-backs from Arterial Roads

- 12** No building should be erected, added to or altered on any land so that any portion of such building is erected, added to or altered nearer to the existing boundary of a road, or to the boundary of any land shown as being required for road widening on the plan deposited under the provisions of the Metropolitan Adelaide Road Widening Plan Act, 1972-1976.

Bushfire Protection

Bushfire Protection Objectives and Principles of Development Control apply to the General, Medium and High Bushfire Risk areas shown on Bushfire Protection Area [Figures Ga\(BPA\)/1 to 5](#), except where exempted.

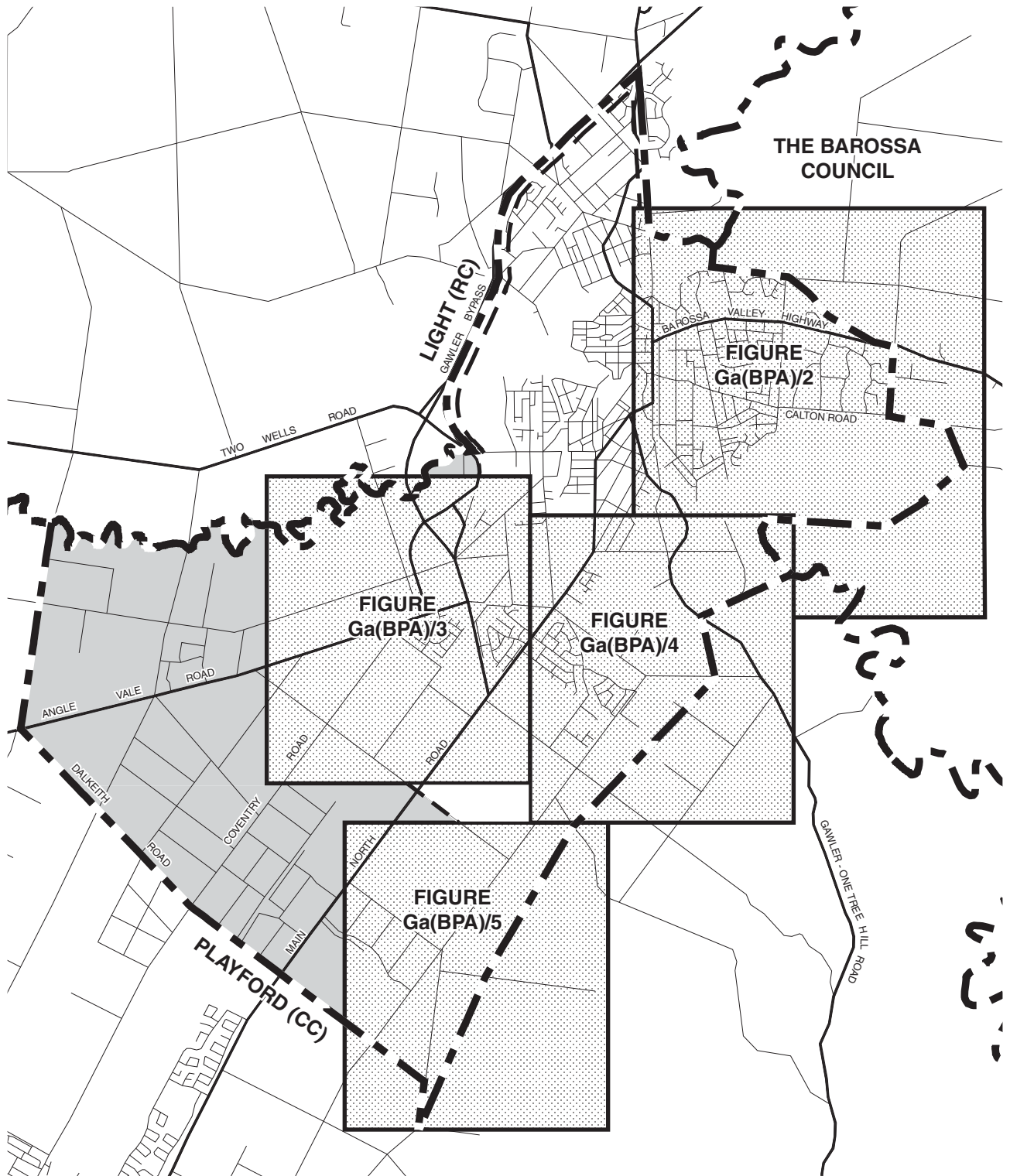
OBJECTIVES

- Objective 3:** Development should minimise the threat and impact of bushfires on life and property while protecting the natural and rural character.
- Objective 4:** Buildings and the intensification of non-rural land uses directed away from areas of high bushfire risk.

PRINCIPLES OF DEVELOPMENT CONTROL

- 13** Buildings and structures should be located away from areas that pose an unacceptable bushfire risk as a result of one or more of the following:
- (a) vegetation cover comprising trees and/or shrubs;
 - (b) poor access;
 - (c) rugged terrain;
 - (d) inability to provide an adequate building protection zone; or
 - (e) inability to provide an adequate supply of water for fire-fighting purposes.
- 14** Residential, tourist accommodation and other habitable buildings should:
- (a) be sited on the flatter portion of allotments and avoid steep slopes, especially upper slopes, narrow ridge crests and the tops of narrow gullies, and slopes with a northerly or westerly aspect;
 - (b) be sited in areas with low bushfire hazard vegetation and set back at least 20 metres from existing hazardous vegetation; and
 - (c) have a dedicated and accessible water supply available at all times for fire fighting.

¹ Subject to an assessment of the impact on Obstacle Height Limitation surface for the Helipad.



THE BAROSSA COUNCIL

FIGURE Ga(BPA)/2

FIGURE Ga(BPA)/3




FIGURE Ga(BPA)/4

FIGURE Ga(BPA)/5

PLAYFORD (CC)

Scale 1:60000



-  General Bushfire Risk
-  Excluded Area from Bushfire Protection Planning Provisions
-  Development Plan Boundary

GAWLER (CT)
INDEX TO
BUSHFIRE PROTECTION AREA
FIGURE Ga(BPA)/1

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FIGURE Ga(BPA)/1 ADJOINS



FIGURE Ga(BPA)/1 ADJOINS

FIGURE Ga(BPA)/1 ADJOINS

FIGURE Ga(BPA)/4 ADJOINS

FIGURE Ga(BPA)/4 ADJOINS

FIGURE Ga(BPA)/1 ADJOINS

NOTE: The boundaries of areas of high bushfire risk identified by this figure align with the boundaries of the Open Space Zone shown on Maps Ga/6, 9 and 22.

- High Bushfire Risk
- Medium Bushfire Risk
- General Bushfire Risk
- Excluded Area from Bushfire Protection Planning Provisions
- Development Plan Boundary

Scale 1:20000

0 ↑ 1,000m

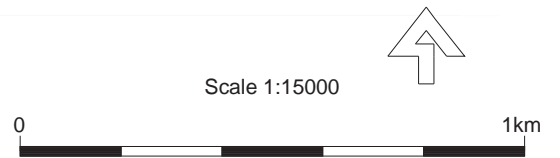
GAWLER (CT) BUSHFIRE PROTECTION AREA FIGURE Ga(BPA)/2

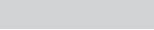


Consolidated - 18 July 2019

FIGURE Ga(BPA)/1 ADJOINS



FIGURE Ga(BPA)/1 ADJOINS



-  General Bushfire Risk
-  Excluded Area from Bushfire Protection Planning Provisions
-  Development Plan Boundary

GAWLER (CT)

BUSHFIRE PROTECTION AREA

FIGURE Ga(BPA)/3

Consolidated - 18 July 2019

FIGURE Ga(BPA)/1 ADJOINS

FIGURE Ga(BPA)/2 ADJOINS

FIGURE Ga(BPA)/3 ADJOINS

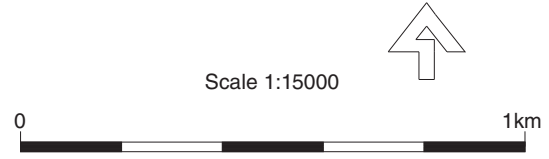
FIGURE Ga(BPA)/1 ADJOINS

FIGURE Ga(BPA)/5 ADJOINS



NOTE: The boundaries of this area of high bushfire risk aligns with the boundaries of the Open Space Zone shown on Maps Ga/6, 9 and 22.

-  High Bushfire Risk
-  Medium Bushfire Risk
-  General Bushfire Risk
-  Excluded Area from Bushfire Protection Planning Provisions
-  Development Plan Boundary



GAWLER (CT)

BUSHFIRE PROTECTION AREA

FIGURE Ga(BPA)/4

Consolidated - 18 July 2019

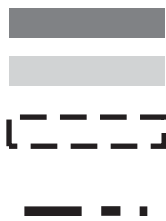
FIGURE Ga(BPA)/1 ADJOINS

FIGURE Ga(BPA)/4 ADJOINS

FIGURE Ga(BPA)/1 ADJOINS



Scale 1:15000



- High Bushfire Risk
- General Bushfire Risk
- Excluded Area from Bushfire Protection Planning Provisions
- Development Plan Boundary

GAWLER (CT)
BUSHFIRE PROTECTION AREA
FIGURE Ga(BPA)/5

Consolidated - 18 July 2019

- 15** Extensions to existing buildings, outbuildings and other ancillary structures should be located and constructed using materials to minimise the threat of fire spread to residential, tourist accommodation and other habitable buildings in the event of bushfire.
- 16** Buildings and structures should be designed and configured to reduce the impact of bushfire through using simple designs that reduce the potential for trapping burning debris against the building or structure, or between the ground and building floor level in the case of transportable buildings.
- 17** Land division for residential or tourist accommodation purposes within areas of high bushfire risk should be limited to those areas specifically set aside for these uses.
- 18** Where land division does occur it should be designed to:
- (a) minimise the danger to residents, other occupants of buildings and fire fighting personnel;
 - (b) minimise the extent of damage to buildings and other property during a bushfire;
 - (c) ensure each allotment contains a suitable building site that is located away from vegetation that would pose an unacceptable risk in the event of bushfire; and
 - (d) ensure provision of a fire hazard separation zone isolating residential allotments from areas that pose an unacceptable bushfire risk by containing the allotments within a perimeter road or through other means that achieve an adequate separation.
- 19** Vehicle access and driveways to properties and public roads created by land division should be designed and constructed to:
- (a) facilitate safe and effective operational use for fire-fighting and other emergency vehicles and residents; and
 - (b) provide for two-way vehicular access between areas of fire risk and the nearest public road.
- 20** Development in a Bushfire Protection Area should be in accordance with those provisions of the *Minister's Code: Undertaking development in Bushfire Protection Areas* that are designated as mandatory for Development Plan Consent purposes.

Centres and Shops

OBJECTIVES

- Objective 5:** Shopping, administrative, cultural, community, entertainment, educational, religious, and recreational facilities should be located in integrated centres.
- Objective 6:** Centres should be established and developed in accordance with a consistent metropolitan hierarchy based on function, so that each type of centre provides a proportion of the total requirement of goods and services commensurate with its role.

There is a metropolitan centres strategy based on the following hierarchy:

- (a) Central Business District of the City of Adelaide;
- (b) Regional Centre;
- (c) District Centre (which includes Gawler Town Centre);
- (d) Neighbourhood Centre;

- (e) Local Centre.

The purpose of the Centre's strategy and hierarchy is to:

- (a) cater for the existing and future population's shopping and community needs;
- (b) provide a degree of choice in the location of centre facilities;
- (c) provide safe and ready access for the population to be served, particularly by public transport, and obviate the need for unscheduled large-scale traffic and transport works;
- (d) minimise adverse impact on residential areas;
- (e) concentrate development on one side of a primary or a primary arterial, road, or one quadrant of a primary or primary arterial road, intersection and have minimal adverse impact on traffic movement on primary and primary arterial roads. Linear extension of centre zones or areas along primary, or primary arterial, roads is to be minimised;
- (f) make effective use of existing investment in public infrastructure, utilities and transport, any costs involved being offset by benefits to the population being served;
- (g) be of a size and shape suitable for its functions, and provide car parking facilities; and
- (h) maintain retail employment levels in the area;

The size of a District Centre and the range of facilities within it, may vary throughout the area of metropolitan Adelaide but should be related to the size and characteristics of the population it serves, the largest district centres should serve a population in the order of 60 000 people.

The following list indicates those facilities which are appropriate in a fully developed district centre:

Ambulance Station	Library
Bank	Offices (general, professional, governmental)
Child Minding/Child Care Centre	Park
Church	Personal Service Establishments
Cinema	Playing Field
Civic Centre	Police Station
Club/Meeting Hall	Pre-school
Commercial Development	Primary School
Community Health Centre	Restaurant
Consulting Room	Secondary School
Day Care Centre	Service Station
Discount Department Store	Special School
Further Education	Specialty Shop
Hospital	Supermarket
Hotel/Tavern	Swimming Pool
Indoor Recreation Centre	

Objective 7: Orderly, economic and consistent provision of goods and services within designated centres that form part of a metropolitan hierarchy.

The grouping of a wide range of facilities in integrated centres provides benefits to the community by encouraging economic, and shared use of facilities, providing a meeting place for communities, and encouraging ready access by both public and private transport.

Objective 8: Development and reinforcement of the Gawler town centre as the main focus of retail, administrative, community, entertainment, educational and cultural activities for the expanding region.

Gawler is undergoing a transition from a self-contained country town that serviced a rural region to an important urban centre which stands on the outer edge of an expanding metropolitan Adelaide. Increasing population in the region is creating a demand for a wider range of goods and services for which the Gawler town centre should remain the primary focus.

Further redevelopment, of underutilized and poorly developed areas of the traditional shopping area, to improve public amenities, the pedestrian environment and shop-front appeal of this area is warranted and must be carefully managed to retain and enhance the significant main street town centre character.

PRINCIPLES OF DEVELOPMENT CONTROL

- 21** Shopping, business, administrative, cultural, community, entertainment, educational, religious and recreational facilities development should be located in a centre, shopping or mixed use zone or associated policy area.
- 22** A shop, or group of shops, with a gross leasable area of 250 square metres or less should not be located on an arterial road (as shown on [Map Ga/1 \(Overlay 1\) Enlargements Part A and Part B](#)) unless located in a centre, shopping, or mixed use zone, or policy area.
- 23** A shop, or group of shops, with a gross leasable area of 250 square metres or less located outside a business, centre, shopping, or mixed use zone, or associated policy area, should:
- (a) not hinder the development or function of any business, centre, shopping, or mixed use zone, or associated policy area; and
 - (b) conform to the design, access, and car parking requirements for business, centre, shopping and mixed use zones or associated policy areas.
- 24** A shop, or group of shops, with a gross leasable area of greater than 250 square metres should be located in a business, centre, shopping or mixed use zone or associated policy area.
- 25** Development or redevelopment within business, centre, shopping or mixed use zone or associated policy areas, should:
- (a) conform with the objectives and desired character of the zone or policy area;
 - (b) preserve buildings of historical or architectural significance;
 - (c) provide for the integration of existing and future facilities so as to promote ease of pedestrian movement and sharing of facilities as well as to retain the opportunity for future expansion within the zone.
 - (d) stage development taking into consideration any future expansion of the zone, or policy area, as a whole.
 - (e) make multiple use of facilities and share utility spaces.
 - (f) unified design of buildings to produce a close relationship between uses in a lively setting.
 - (g) use materials compatible with the natural features of the site and adjacent buildings.
 - (h) be designed and laid out to avoid nuisance or hazard arising from:
 - (i) microclimatic conditions;
 - (ii) excessive noise;
 - (iii) odours;

- (iv) overlooking;
 - (v) overshadowing; or
 - (vi) visual intrusion.
- (i) be designed to be compatible with existing development through:
- (i) a buffer between development in the zone and adjacent areas;
 - (ii) landscaping which complements adjacent development and enhances the visual appearance and character of the zone;
 - (iii) pedestrian paths and spaces which are shaded, defined and protected from the wind; and
 - (iv) service yards, loading areas and outdoor storage areas which are screened;
 - (v) foster human scale, define spaces, reinforce paths and edges, screen utility areas, and generally enhance the visual amenity of the area.
- (j) ensure outdoor signs, both free-standing and attached to buildings, are located and designed in such a way as to:
- (i) be in scale with the development as a whole, the building therein, and the desired character of the zone or policy area, or otherwise be compatible with the character of the locality;
 - (ii) not impair the view of or from nearby developments;
 - (iii) not distract attention from traffic control information; and
 - (iv) illumination from signs or floodlights should not spill over to adjacent areas.
- (k) provide access and car parking for residential areas located within centres separate from the access and car parking areas serving the other centre facilities.
- (l) integrate of public transport requirements and sheltered waiting areas for passengers.
- (m) provide retail showrooms for the trading of bulky goods on the periphery of centres, or in designated precincts within zones or policy areas.
- (n) provide:
- (i) off-street loading, service areas and service vehicle manoeuvring areas;
 - (ii) lighting for buildings and ancillary areas, with no light-spill causing nuisance or hazard; and
 - (iii) unobtrusive facilities for storage and removal of waste materials;
 - (iv) parking, access and facilities for the disabled;
 - (v) public spaces such as malls, plazas and courtyards;
 - (vi) public facilities including toilets, infant changing facilities for parents, seating, telephones and community information boards; and
 - (vii) design of buildings and infrastructure to minimise energy consumption for lighting, heating, cooling and ventilation and reuse of stormwater.

- 26** Provision for the movement of people and goods within business, centre, shopping or mixed use zones or associated policy areas, should:
- (a) not cause inconvenient and unsafe traffic and pedestrian movements;
 - (b) provide safe and convenient access for private cars, cyclists, pedestrians, service vehicles, emergency vehicles and public utility vehicles;
 - (c) concentrate development for pedestrian convenience, and not extend unnecessarily along road frontages; (increasing the depth of development is a more desirable alternative);
 - (d) separate pedestrian and vehicle movements within zones or areas, for pedestrian safety and convenience.
 - (e) provide access points onto the arterial roads as shown on the relevant Structure Plans to: minimize traffic hazards; queuing on the roads; right turn movements; and interference with the function of intersections, junctions and traffic control devices.
 - (f) not cause congestion or detract from the safety of traffic on abutting roads, and minimize: traffic hazards; queuing on public roads; and intrusion into adjacent residential areas.
 - (g) provide for service vehicles and the storage and removal of waste goods and materials.
 - (h) provide sufficient off-street parking to accommodate customer, employee and service vehicles if there is insufficient public car parking in the locality.
 - (i) consolidate and co-ordinate parking areas into convenient groups, rather than located individually, and the access points minimized, and so located and designed that:
 - (i) vehicular movement between them does not require the use of public roads;
 - (ii) the number of access points is minimized; and
 - (iii) it is not necessary for vehicles to back onto public roads.
 - (j) locate, design and orientate car parks in such a way as to facilitate safe, direct and convenient access of pedestrians between them and the facilities they serve, safe and convenient traffic circulation, minimal conflict between customer and service vehicles, and should include adequate provision for manoeuvring into and out of parking bays.
 - (k) provide on-site parking determined by:
 - (i) the amount, type and timing of movement generated by the use;
 - (ii) the design, location and configuration of parking spaces;
 - (iii) the ability of the site to accommodate the parking spaces;
 - (iv) the potential for shared use of parking spaces; and
 - (v) the effect on surrounding activities and uses.
 - (l) share use of car parking between developments to reduce the total extent of car parking areas.
 - (m) provide for landscaping in order to screen, shade and enhance the appearance of car parking areas.
 - (n) ensure that all sources of noise, including refrigeration and air conditioning equipment, garbage collection and car parking, do not cause excessive or disturbing noise at neighbouring properties.

- 27** Development within business, centre, shopping or mixed use zones and associated policy areas that requires traffic control works, public works or facilities as a direct result of a development should make a financial contribution to the cost of such works or facilities.
- 28** Shopping development which is more appropriately located outside business, centre, shopping or mixed use zones, or areas, should:
- (a) be of a size and type which would not hinder the development or function of any business, centre, shopping or mixed use zone or area, in accordance with the objectives and principles of development control for centres and shops, and the objectives and principles of development control for the appropriate zones, or areas;
 - (b) conform to the criteria above, and the design, access, and car parking requirements for business, centre, shopping and mixed use zones, or areas, set out in principles of development control above;
 - (c) result in the expansion of the total range of retail goods and services presently available to the community;
 - (d) result in a maintenance of retail employment in the area; and
 - (e) not demonstrably lead to the physical deterioration of any designated centre.

Commercial Development

OBJECTIVES

Objective 9: Commercial development located in suitable areas.

Commercial areas cater for wholesaling, storage, and associated, activities. Parts of these areas may be suitable for development that does not generate much traffic, such as car and boat sales yards, small offices, tyre sales outlets and premises which are used primarily for the fabrication, storage, and repair, of goods with only a small ancillary retailing area.

The layout of buildings should normally provide for offices and showrooms of high architectural standard to occupy the front of the site, with storage premises at the rear. Buildings should be set well back from the road frontage to allow for landscaping. Space should be provided on the site for car parking, the loading and unloading of vehicles, and the circulation of vehicles.

Attractive and spacious site layouts, with well-designed buildings, are important for storage warehouses. The buildings are usually large, so level, well drained, land with good bearing capacity is essential. Areas set aside for storage warehouses should be close to port, rail and road facilities, with heavy duty roads of sufficient width to service individual sites and railway sidings provided where possible.

The considerable amount of traffic associated with many wholesale warehouses makes it essential that points of access and egress receive careful attention, particularly when the site fronts a main road. The siting of warehouses near residential areas needs care because of the amount of traffic involved.

PRINCIPLES OF DEVELOPMENT CONTROL

- 29** Wholesaling, storage, transport and service industries should be located in designated zones for business or industry.
- 30** Commercial development should be of a high architectural standard.
- 31** Warehouses near residential areas should not impair the amenity of the residential area and points of entrance and exit should be located so that the number of vehicles using nearby residential roads is kept to a minimum.

Community Facilities

OBJECTIVES

Objective 10: Community facilities should be appropriate and conveniently accessible to the population they serve.

Effective education and health services are the basis for the social well-being of a community. It is essential that schools, hospitals, cemeteries and other institutions, are located conveniently for the people they serve.

Demographic change over time affects the community facilities required therefore flexible use of buildings is essential.

Conservation

OBJECTIVES

Objective 11: Conservation, preservation, enhancement or improvement of scenically attractive areas, including land adjoining scenic routes and riverine environments.

Retention of the natural character of the Mount Lofty Ranges is of the utmost importance to present and future generations of city dwellers.

The natural slopes of the foothills and the wooded character of the face of the ranges rising to Mount Lofty, provide a pleasant contrast to the suburbs on the plains, and give Adelaide a special character. It is necessary, therefore, that the face of the ranges and the skyline as seen from various points in the metropolitan area should retain a natural character.

The ranges are still attractively wooded, providing areas of considerable beauty, readily accessible from the suburban plains. However, any action likely to diminish these wooded areas, such as subdivision into unduly small residential allotments, should be resisted in order to conserve biodiversity, avoid soil erosion and protect development from occurring in a bushfire prone area. Acquisition of suitable areas for public use would ensure their retention.

Tree planting should be encouraged, dwellings should be of good design and set well back from the roads. Advertisements should not mar the landscape and overhead services should be carefully sited against tree and hill backgrounds.

Watercourses, with their natural vegetation, are the most significant natural features on the Adelaide Plains. The trees and natural vegetation can add to the attractiveness of suburban areas and, wherever possible, these features should be incorporated in the layout of residential areas whilst also forming biodiversity corridors.

Land bordering watercourses along the Gawler River and North and South Para Rivers should be reserved for public use and rehabilitated and managed through conservation programmes. Buildings should be set well back, and front onto a road and reserve along a watercourse. River reserves should be used for public recreation and provide easy access for maintenance of the watercourse.

The character of the built-up area largely depends on the attractiveness of parks and recreation reserves, and every endeavour should be made to plant and develop reserves as soon as they become available. Reserves should be easily seen from adjoining roads, and housing development should not block out views or back onto reserves. Reserves should also be rehabilitated and managed through conservation programmes.

Objective 12: Retention and enhancement of localities in the Council area of distinctive and valued or historic significance through preservation of State and Local Heritage Places, Contributory Items and other places of historic character, and compatible infill development.

Gawler contains a number of areas of special historic character. In those areas redevelopment and infill should be carried out in preference to demolition which should only be undertaken in association with development which conserves and enhances the special character of those areas.

Outside of recognised areas of special historic character there are many places which are part of the historic character of the Council area. The buildings and structures constructed in the Council area in years prior to about 1930 should be preserved and enhanced to maintain their contribution to the historic character of Gawler.

The character of the heart of the township revolves around the Gawler town centre and adjoining Church Hill, which is of particular significance. That character is largely derived from its setting, framed by the North Para River and South Para River and flanked to the east by the elevated ridge running parallel with the main street, Murray Street. Generous parkland spaces, flanked by wide terraces, encompass the river valleys. The dominating traditional grid road pattern is realigned in response to topographic conditions to create significant entrance points and important vistas. Several landmarks, including the Church Hill town squares are created as significant focal points. Native riverine eucalypts on the North Para River and South Para River parklands are complemented within the town centre area by Moreton Bay Fig trees, pinus species, palms and exotic European trees.

Buildings of historic interest, although containing a diversity of architectural styles from modest, simple colonial cottages to grand villas, and elaborate residences, display a rare cohesiveness, with few disparate new structures. The building form generally consists of:

- (a) shape - orthogonal load-bearing building forms with hip, gable and hip-gable combination roofs. Verandahs are commonly found.
- (b) scale - generally single-storey, but with lofty, high-pitched roofs.
- (c) materials - local building stone (bluestone, limestone) and sandstone, or red brick walls with corrugated iron roofs.
- (d) advertising or advertising displays - integrated with the building's architecture so that details which provide interest (such as arches, columns, decorative panels and lacework) are not obscured or disturbed.

Objective 13: Retention of environmentally-significant areas of native vegetation.

Objective 14: Retention of native vegetation where clearance is likely to lead to problems of soil erosion, soil slip and soil salinization, flooding or a deterioration in the quality of surface waters.

Objective 15: Retention of native vegetation for amenity purposes, for livestock shade and shelter and native wildlife corridors.

Objective 16: Retention and maintenance of wetlands and existing native vegetation for its conservation, biodiversity, and habitat value and environmental management function.

Objective 17: Conservation of Aboriginal sites, items and areas which are of archaeological, cultural, mythological or anthropological significance.

PRINCIPLES OF DEVELOPMENT CONTROL

- 32** The natural character of the North and South Para Rivers and Gawler River valleys should be retained and restored where affected by previous development.
- 33** Development should be undertaken with the minimum effect on natural features, land adjoining water or scenic routes or scenically-attractive areas.
- 34** Trees of historical or local significance and single trees or groups of trees of particular visual significance should be preserved and protected against disfigurement. If it is necessary to fell these trees, replanting should proceed as part of the development.

- 35** Development should not impair the character or nature of buildings or sites of architectural, historical or scientific interest or sites of natural beauty (including those not specifically identified of heritage importance in [Table Ga/2](#) or [Table Ga/5](#)).
- 36** When excavation in historic conservation zones or places and items in [Table Ga/2](#), [Table Ga/5](#) or [Table Ga/6](#) is proposed, consideration should be given to an archaeological assessment prior to excavation. Monitoring should occur during construction to protect and recover artifacts and document important historic features.
- 37** Native vegetation and roadside vegetation should be preserved and replanted with local indigenous species where practical and should not be cleared if it:
- (a) provides important habitat for wildlife;
 - (b) has a high plant species diversity or has rare or endangered plant species and plant associations;
 - (c) has high amenity value;
 - (d) contributes to the landscape quality of an area;
 - (e) has high value as a remnant of vegetation associations characteristic of a district or region prior to extensive clearance for agriculture;
 - (f) is associated with sites of scientific, archaeological, historic, or cultural significance; or
 - (g) is growing in, or is characteristically associated with, a wetland environment.
- 38** Native vegetation should not be cleared if such clearance is likely to:
- (a) create or contribute to soil erosion;
 - (b) decrease soil stability and initiate soil slip;
 - (c) create, or contribute to, a local or regional soil salinity problem;
 - (d) lead to the deterioration in the quality of surface waters; or
 - (e) create or exacerbate the incidence or intensity of local or regional flooding.
- 39** When clearance is proposed, consideration should be given to:
- (a) retention of native vegetation for, or as:
 - (i) corridors or wildlife refuges;
 - (ii) amenity purposes;
 - (iii) livestock shade and shelter; or
 - (iv) protection from erosion along watercourses and the filtering of suspended solids and nutrients from run-off;
 - (b) the effects of retention on farm management; and
 - (c) the implications of retention or clearance on fire control.
- 40** Local indigenous plant species should be considered for landscaping, screening buffer planting and revegetation activities.

Contaminated Land

OBJECTIVES

- Objective 18:** Protection of human health and the environment wherever site contamination has been identified or is suspected to have occurred.
- Objective 19:** Appropriate assessment and remediation of site contamination to ensure land is suitable for the proposed use and provides a safe and healthy living and working environment.

PRINCIPLE OF DEVELOPMENT CONTROL

- 41** Development, including land division, should not occur where site contamination has occurred unless the site has been assessed and remediated as necessary to ensure that it is suitable and safe for the proposed use.

Crime Prevention

OBJECTIVE

- Objective 20:** A safe, secure, crime resistant environment where land uses are integrated and designed to facilitate community surveillance.

PRINCIPLES OF DEVELOPMENT CONTROL

- 42** Development should be designed to maximise surveillance of public spaces through the incorporation of clear lines of sight, appropriate lighting and the use of visible permeable barriers wherever practicable.
- 43** Buildings should be designed to overlook public and communal streets and public open space to allow casual surveillance.
- 44** Development should provide a robust environment that is resistant to vandalism and graffiti.
- 45** Development should provide lighting in frequently used public spaces including those:
- (a) along dedicated cyclist and pedestrian pathways, laneways and access routes
 - (b) around public facilities such as toilets, telephones, bus stops, seating, litter bins, automatic teller machines, taxi ranks and car parks.
- 46** Development, including car park facilities should incorporate signage and lighting that indicate the entrances and pathways to, from and within sites.
- 47** Landscaping should be used to assist in discouraging crime by:
- (a) screen planting areas susceptible to vandalism
 - (b) planting trees or ground covers, rather than shrubs, alongside footpaths
 - (c) planting vegetation other than ground covers a minimum distance of 2 metres from footpaths to reduce concealment opportunities.
- 48** Site planning, buildings, fences, landscaping and other features should clearly differentiate public, communal and private areas.
- 49** Buildings should be designed to minimise and discourage access between roofs, balconies and windows of adjoining dwellings.

- 50** Public toilets should be located, sited and designed:
- (a) to promote the visibility of people entering and exiting the facility (eg by avoiding recessed entrances and dense shrubbery that obstructs passive surveillance)
 - (b) near public and community transport links and pedestrian and cyclist networks to maximise visibility.
- 51** Development should avoid pedestrian entrapment spots and movement predictors (eg routes or paths that are predictable or unchangeable and offer no choice to pedestrians).

Energy Efficiency

OBJECTIVES

Objective 21: Development designed and sited to conserve energy.

Objective 22: Development that provides for on-site power generation including photovoltaic cells and wind power.

PRINCIPLES OF DEVELOPMENT CONTROL

- 52** Development should provide for efficient solar access to buildings and open space all year around.
- 53** Buildings should be sited and designed:
- (a) to ensure adequate natural light and winter sunlight is available to the main activity areas of adjacent buildings;
 - (b) so that open spaces associated with the main activity areas face north for exposure to winter sun.

On-site Energy Generation

- 54** Development should facilitate the efficient use of photovoltaic cells and solar hot water systems by:
- (a) taking into account overshadowing from neighbouring buildings;
 - (b) designing roof orientation and pitches to maximise exposure to direct sunlight.
- 55** Public infrastructure and lighting should be designed to generate and use renewable energy.

Form of Development

OBJECTIVES

Objective 23: Orderly and economic development, consistent also with [Maps Ga/1 \(Overlay 1\) Enlargements A, B, C, D, E, F, G and H](#).

The maps illustrate the distribution of living, business, rural, recreational and conservation areas and the main routes for traffic and transport.

Objective 24: A sustainable urban form that reduces the ecological footprint of the town, whilst also enhancing the quality of life of residents.

Development of new areas within the urban boundary shown on [Map Ga/1](#) should occur in a logical and systematic manner with a clearly defined edge between urban and surrounding rural areas.

Objective 25: Location of suitable areas of land for living, working, recreation and rural production.

Current and anticipated demographic trends within the Adelaide metropolitan area show declining dwelling occupancy rates, particularly in the inner and middle suburbs. This will necessitate increasing dwelling density to maintain population levels and services.

Concerns about population change, increased housing demand, efficient use of urban infrastructure and reducing fringe growth can be addressed by increasing the number of dwellings that can be accommodated within the existing boundary of the metropolitan area, and positively influencing decline in population.

Objective 26: Effective location of public and community facilities by the reservation of suitable land in advance of need.

Development should not hinder the reservation of land needed for transport, public services, schools and the provision of other essential services.

PRINCIPLES OF DEVELOPMENT CONTROL

- 56** Extensions of built-up areas should not be in the form of ribbon development along arterial roads unless indicated in zone policies or structure plans.
- 57** Development in localities having a bad or unsatisfactory layout, or unhealthy or obsolete development, should improve or rectify those conditions.
- 58** Urban development should be confined to areas within the urban boundary of Metropolitan Adelaide and be based on principles of ecologically sustainable development (ESD) that includes water sensitive urban design (WSUD), energy efficiency, biodiversity protection and enhancement, natural resource protection, waste, minimisation, indoor and outdoor environmental quality and sustainable selection and use of materials.
- 59** Development adjacent to the rural/urban interface, as indicated on [Map Ga/1 \(Overlay 1\) Enlargements Part A and Part B](#) should incorporate suitable buffers to minimise the effect of potential impacts.
- 60** Development should minimise the potential for personal and property damage arising from natural hazards including landslip, bushfires and flooding.
- 61** Septic tanks should:
 - (a) not be installed where the effluent is likely to lead to the pollution of surface or underground water; and
 - (b) be installed on allotments large enough to allow the disposal of the effluent within the allotment boundaries.
- 62** Development should not create conditions which are likely to exceed the capacity of existing roads, public utilities, and other community services and facilities.
- 63** Development should be supplied with adequate energy, water, waste disposal and drainage facilities to serve the needs of users.

Hazards

OBJECTIVES

Objective 27: Maintenance of the natural environment and systems by limiting development in areas susceptible to flooding.

Objective 28: Development located away from areas that are vulnerable to, and cannot be adequately and effectively protected from, the risk of flooding.

Objective 29: Critical community facilities such as hospitals, emergency control centres, major service infrastructure facilities, and emergency service facilities located where they are not exposed to flooding.

Objective 30: Development located and designed to minimise the risks to safety and property from flooding.

Objective 31: Development located and designed so as not to impede the flow of flood waters.

PRINCIPLES OF DEVELOPMENT CONTROL

64 Development should be excluded from areas that are vulnerable to, and cannot be adequately and effectively protected from, flooding.

65 There should not be any significant interference with natural processes in order to reduce the exposure of development to the risk of natural hazards.

Flooding

66 Development should not occur on land where the risk of flooding is likely to be harmful to safety or damage property.

67 Development should not be undertaken in areas liable to inundation by tidal, drainage or flood waters unless the development can achieve all of the following:

- (a) it is developed in an area having a public stormwater system capable of catering for a 1 in 100 year average return interval flood event; and
- (b) buildings and structures for human habitation or for the keeping of animals have a finished floor level and gully traps at least 300mm above the Australian Height Datum level of a 1 in 100 year average return interval flood event.

68 Development, including earthworks associated with development, should not:

- (a) impede the flow of floodwaters through the land or other surrounding land; or
- (b) increase the potential hazard risk to public safety of persons during a flood event; or
- (c) aggravate the potential for erosion or siltation or lead to the destruction of vegetation during a flood; or
- (d) cause any adverse effect on the floodway function; or
- (e) increase the risk of flooding of other land; or
- (f) obstruct a watercourse.

69 Development of buildings, structures, farming and horticultural activities should maintain the natural landform in areas subject to flooding by:

- (a) locating and designing driveways, access tracks and parking areas to follow the natural contours of the land; and
- (b) minimising the amount of excavation and limiting the level of fill for driveways, access tracks and parking areas to no more than 300mm above natural or existing surface level; and

- (c) minimising the area of fill required to accommodate buildings above the Australian Height Datum level of a 1 in 100 year average return interval flood event; and
 - (d) avoiding the use of levees or channels for the irrigation or protection of crops.
- 70** Construction and placement of structures, including roads, in a watercourse, a floodplain of a watercourse, a lake, a wetland, or an area subject to inundation should:
- (a) not result in flooding either upstream or downstream; and
 - (b) be constructed in a manner that prevents the structure, or any debris collected by the structure, increasing the risk of damage to property or the risk to safety of persons.
- 71** Development should avoid the discharge or deposit of waste, wastewater and waste treatment systems (including processes such as seepage, infiltration or carriage by wind, rain, stormwater or by the rising of the water table) onto land or into any waters that are subject to inundation by a 1 in 100 year average return interval flood event.
- 72** Development should not occur where essential services cannot be economically provided and maintained having regard to flood risk or where emergency vehicle access would be prevented by a 1 in 100 year average return interval flood event.
- 73** Emergency service facilities such as hospitals, fire stations, police stations and other similar types of facilities should be located above the predicted level for a 1 in 1000 year ARI flood event.

Gawler Rivers Floodplain Area

The following objective and principles of development control apply to land identified in [Figures FI/1 to FI/8](#) as being flood prone land. This land is referred to throughout the Development Plan as the Gawler Rivers Floodplain Area and shown to be at risk of inundation by flood waters.

Where there is inconsistency between the following objectives or principles of development control, and objectives or principles of development control in a specific zone or policy area within a zone, the following objectives or principles of development control will prevail to the extent of the inconsistency.

There are allotments containing both areas of flood prone and non-flood prone land having regard to [Figures FI/1 to FI/8](#). Equally, there are allotments with two or three different hazard flood risk areas. In such situations, it will be necessary to define the site or activity-boundary of the proposed building or land use, and to proceed according to the relevant flood risk status of that area. If more than one hazard flood risk area applies to the site or activity-boundary, the development will be treated as if it is wholly within the higher hazard flood risk area.

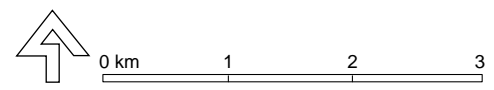
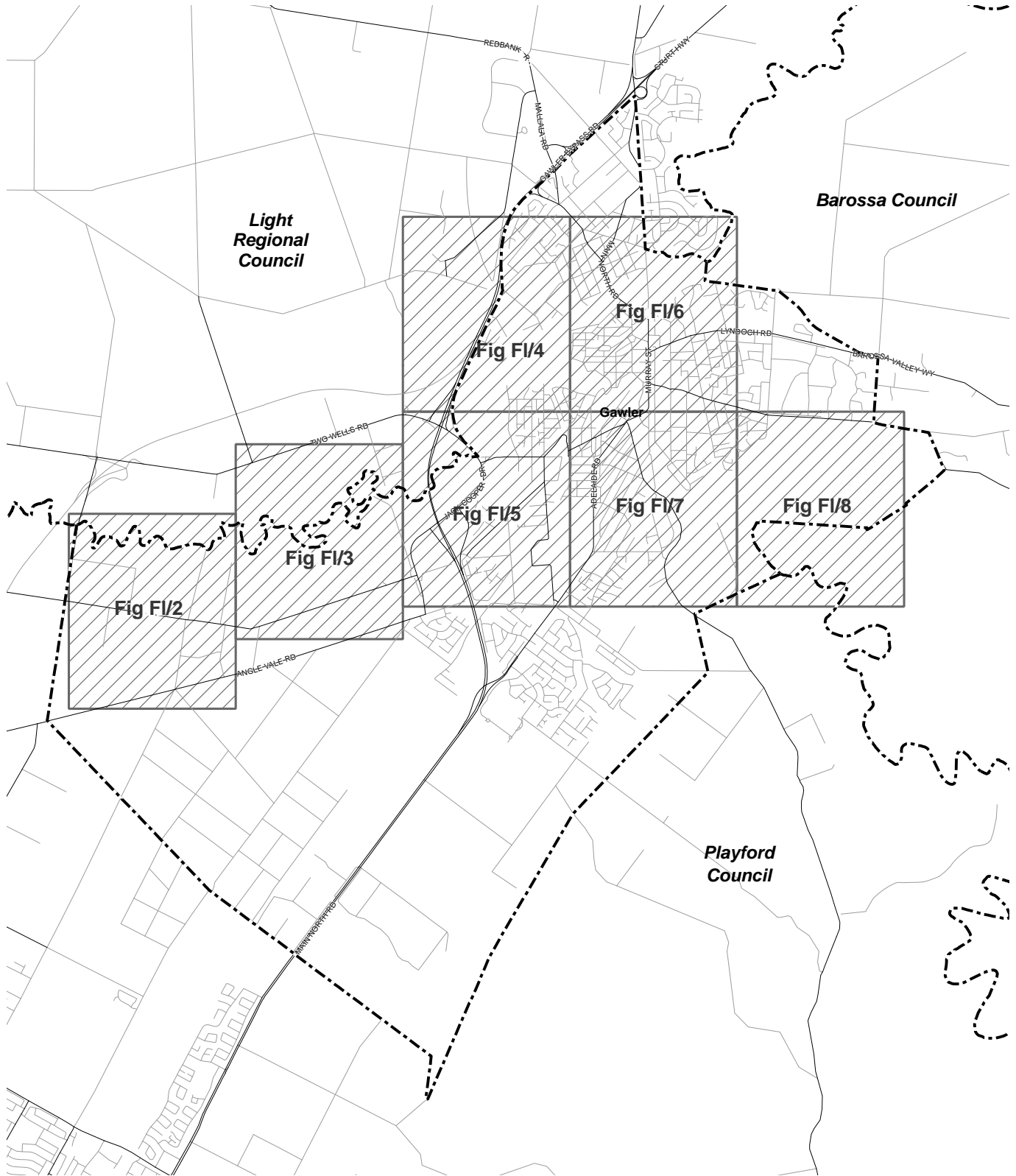
OBJECTIVE

Objective 32: Development within the Gawler Rivers Floodplain Area appropriate to the varying hazard flood risk areas.

The Gawler Rivers Floodplain Area comprises areas of land having three different hazard flood risk areas:

- (a) *Low* relates to low depth and low velocity flooding where evacuation via wading by people is possible and escape by small vehicle is achievable.

Zero to 0.3 metres depth at flood where velocities are generally low (up to 0.3m/s).
- (b) *Medium* relates to areas where the flood depth is deeper and/ or flows are faster. Wading through water by children and elderly is more difficult and evacuation by small vehicle is only possible in the early stages of flooding, with larger 4WD vehicles or trucks required at later stages.



Disclaimer

These figures have been prepared on the basis of survey, hydraulic and hydrologic modelling undertaken in 2007 to predict the risk of flooding along the North Para and Gawler Rivers and other tributaries during a 1 in 100 year ARI flood event.

They are only intended to be used as a policy tool for planning purposes and in broad scale flood risk management, and they should not be relied on for any other purpose.

They do not purport to illustrate actual inundation patterns, which will depend on the size and nature of a particular flood event and other circumstances pertaining to the relevant place and at the relevant time.

--- Development Plan Boundary

GAWLER (CT) GAWLER RIVER FLOOD PRONE AREAS INDEX

Control Date: 13 July 2015
FIGURE F/1

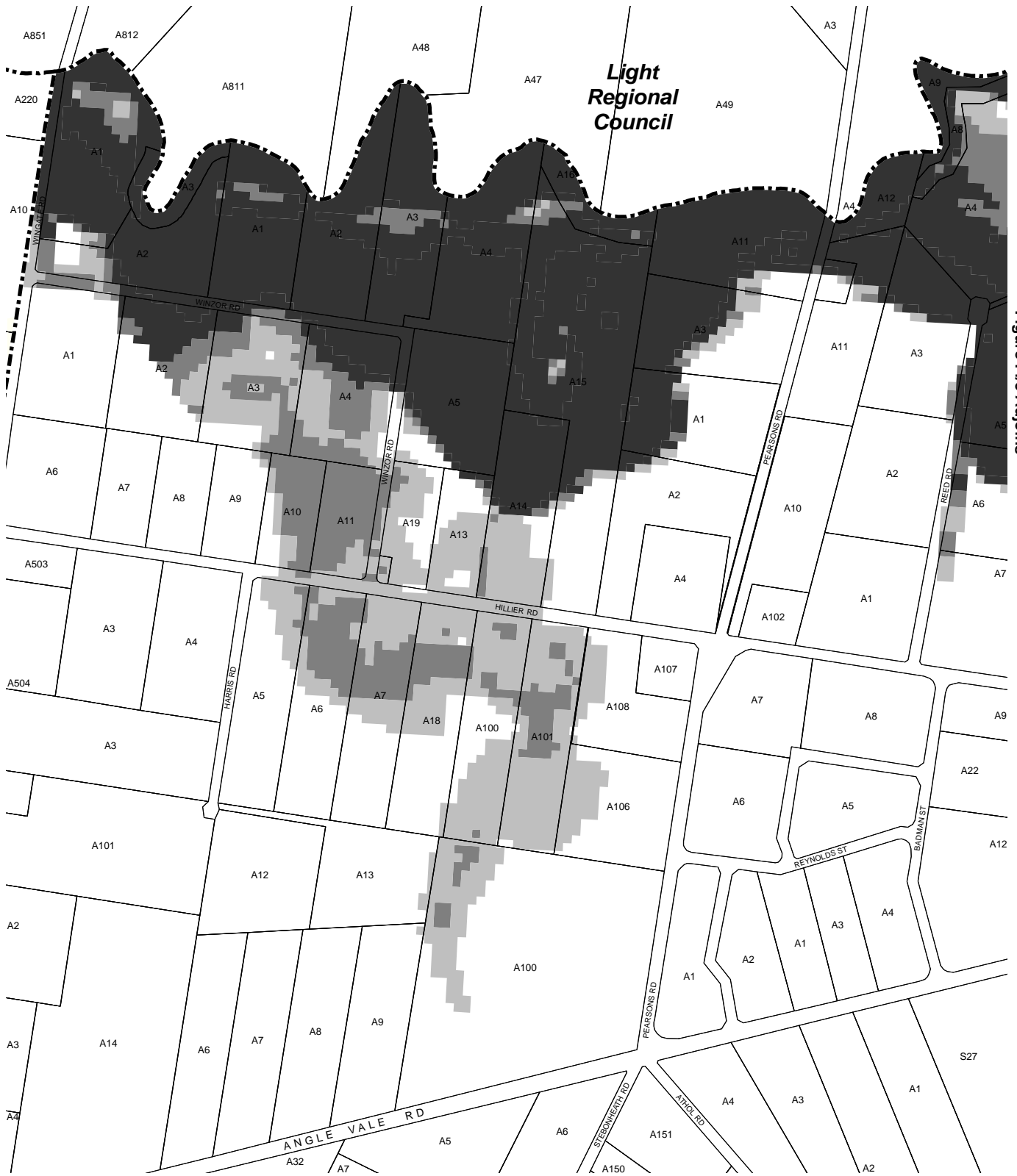
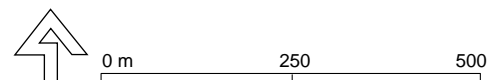


Figure FI/3 Adjoins

Note: See figure FI/1 for flood data definition explanation and disclaimer.



Hazard Flood Risk Areas

- High
- Medium
- Low
- Development Plan Boundary

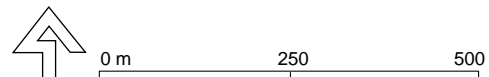
GAWLER (CT)
GAWLER RIVER
FLOOD PRONE AREAS
FIGURE FI/2
 Consolidated - 18 July 2019



Figure FI/2 Adjoins

Figure FI/5 Adjoins

Note: See figure FI/1 for flood data definition explanation and disclaimer.



Hazard Flood Risk Areas

- High
- Medium
- Low
- Development Plan Boundary

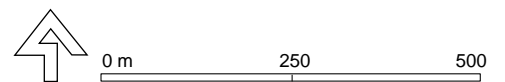
GAWLER (CT)
GAWLER RIVER
FLOOD PRONE AREAS
FIGURE FI/3
 Consolidated - 18 July 2019



Figure FI/5 Adjoins

Figure FI/6 Adjoins

Note: See figure FI/1 for flood data definition explanation and disclaimer.



Hazard Flood Risk Areas

- High
- Medium
- Low
- Development Plan Boundary

GAWLER (CT) GAWLER RIVER FLOOD PRONE AREAS FIGURE FI/4

Consultation July 2014

Figure FI/4 Adjoins

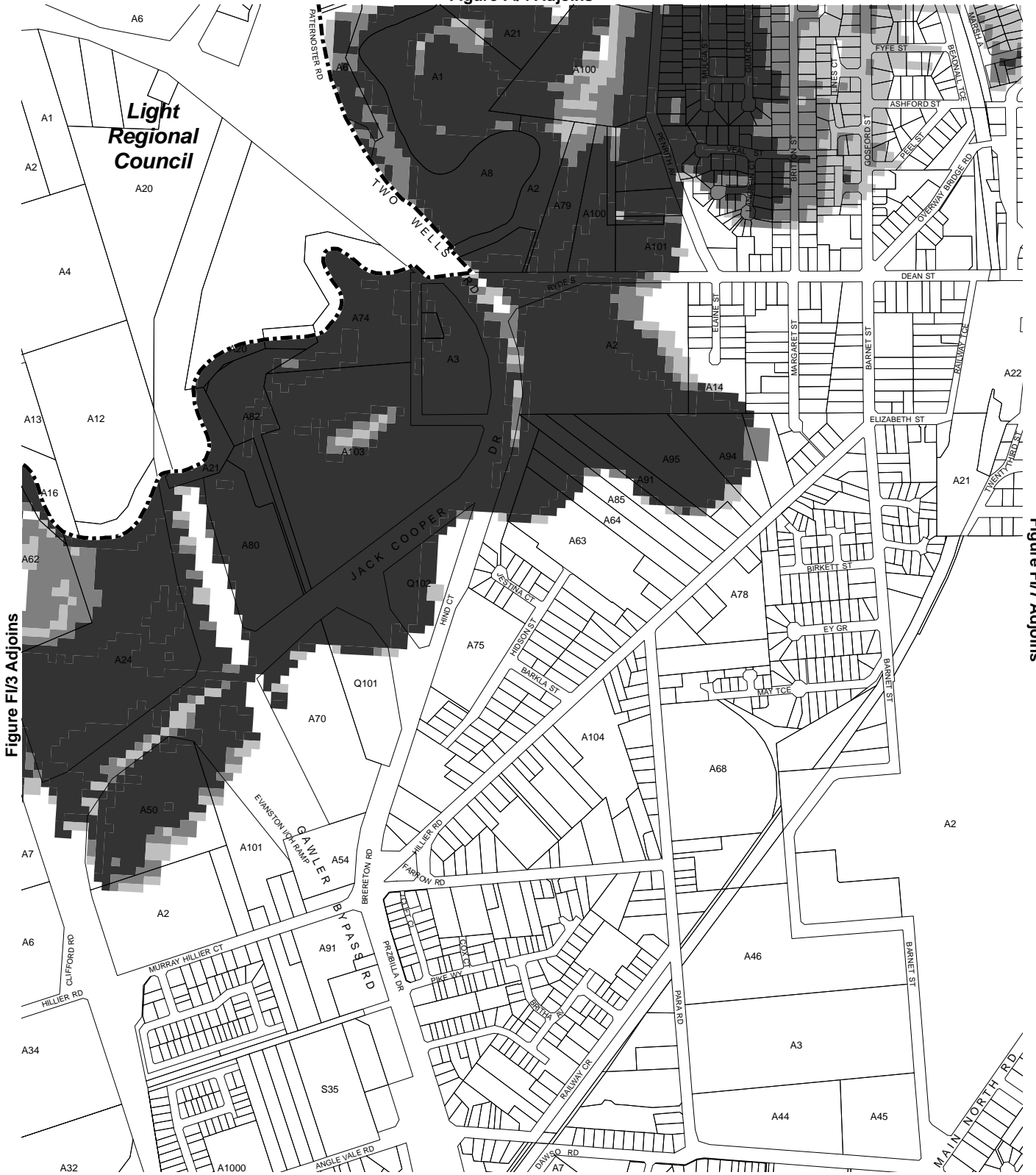
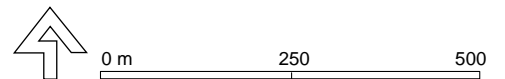


Figure FI/3 Adjoins

Figure FI/7 Adjoins

Note: See figure FI/1 for flood data definition explanation and disclaimer.



Hazard Flood Risk Areas

- High
- Medium
- Low
- Development Plan Boundary

**GAWLER (CT)
GAWLER RIVER
FLOOD PRONE AREAS**

Consolidated July 2010

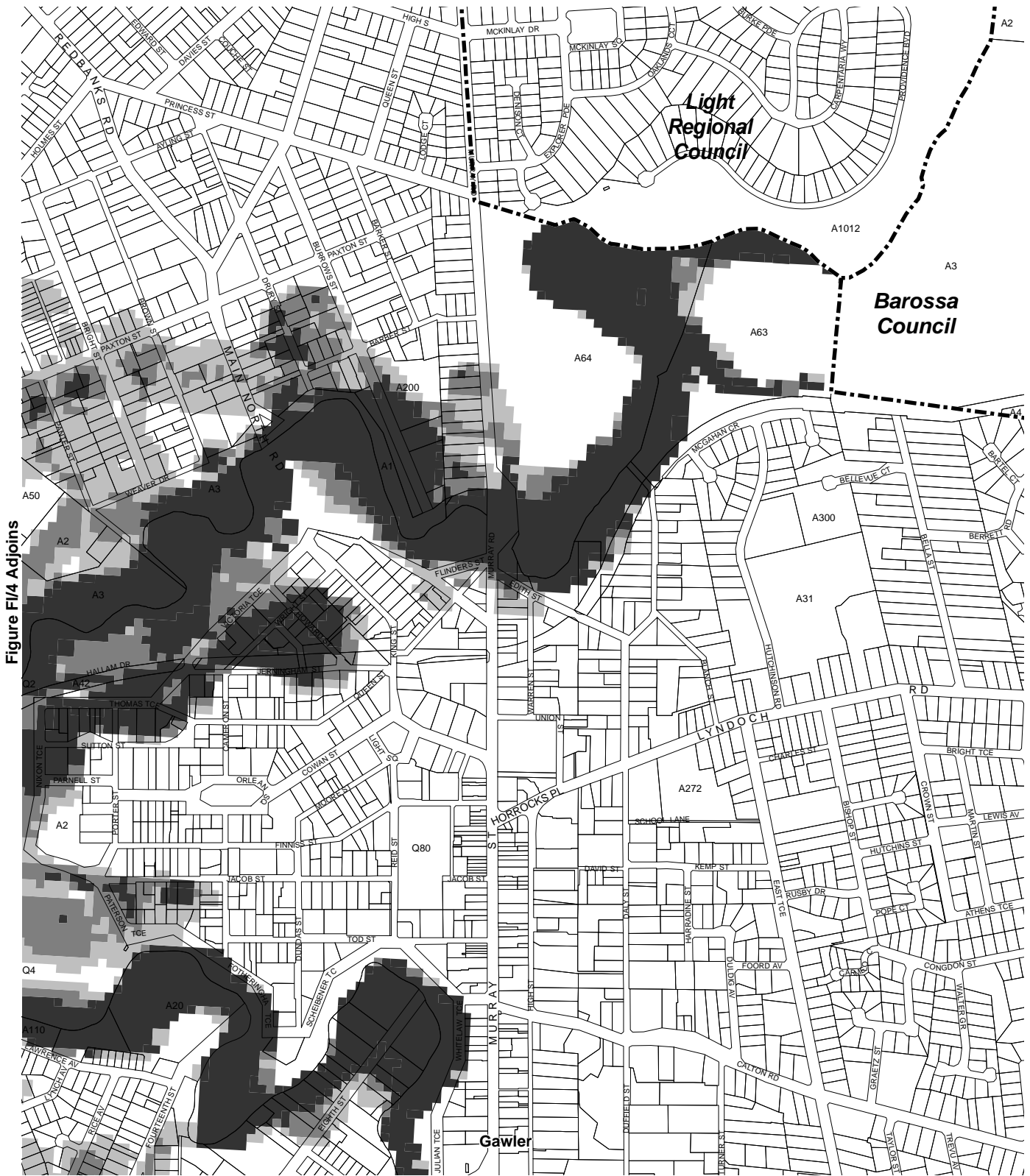


Figure FI/4 Adjoins

Figure FI/7 Adjoins

Note: See figure FI/1 for flood data definition explanation and disclaimer.



0 m 250 500

Hazard Flood Risk Areas

- High
- Medium
- Low
- Development Plan Boundary

GAWLER (CT) GAWLER RIVER FLOOD PRONE AREAS FIGURE FI/6

Consolidated - 16 July 2019

Figure FI/6 Adjoins

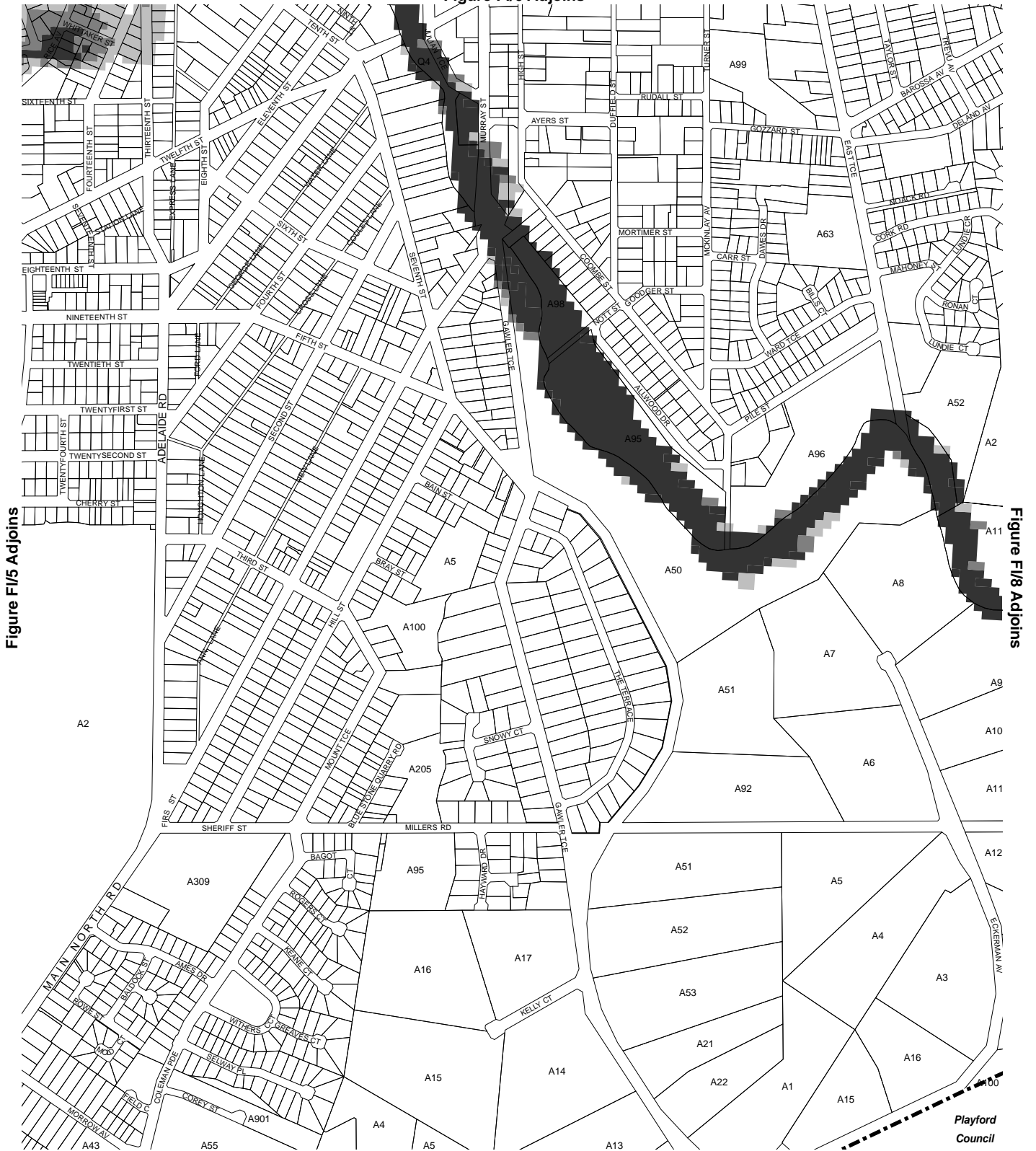


Figure FI/5 Adjoins

Figure FI/8 Adjoins

Note: See figure FI/1 for flood data definition explanation and disclaimer.



0 m 250 500

Hazard Flood Risk Areas

- High
- Medium
- Low
- Development Plan Boundary

GAWLER (CT) GAWLER RIVER FLOOD PRONE AREAS

Consulted 8 July 2019

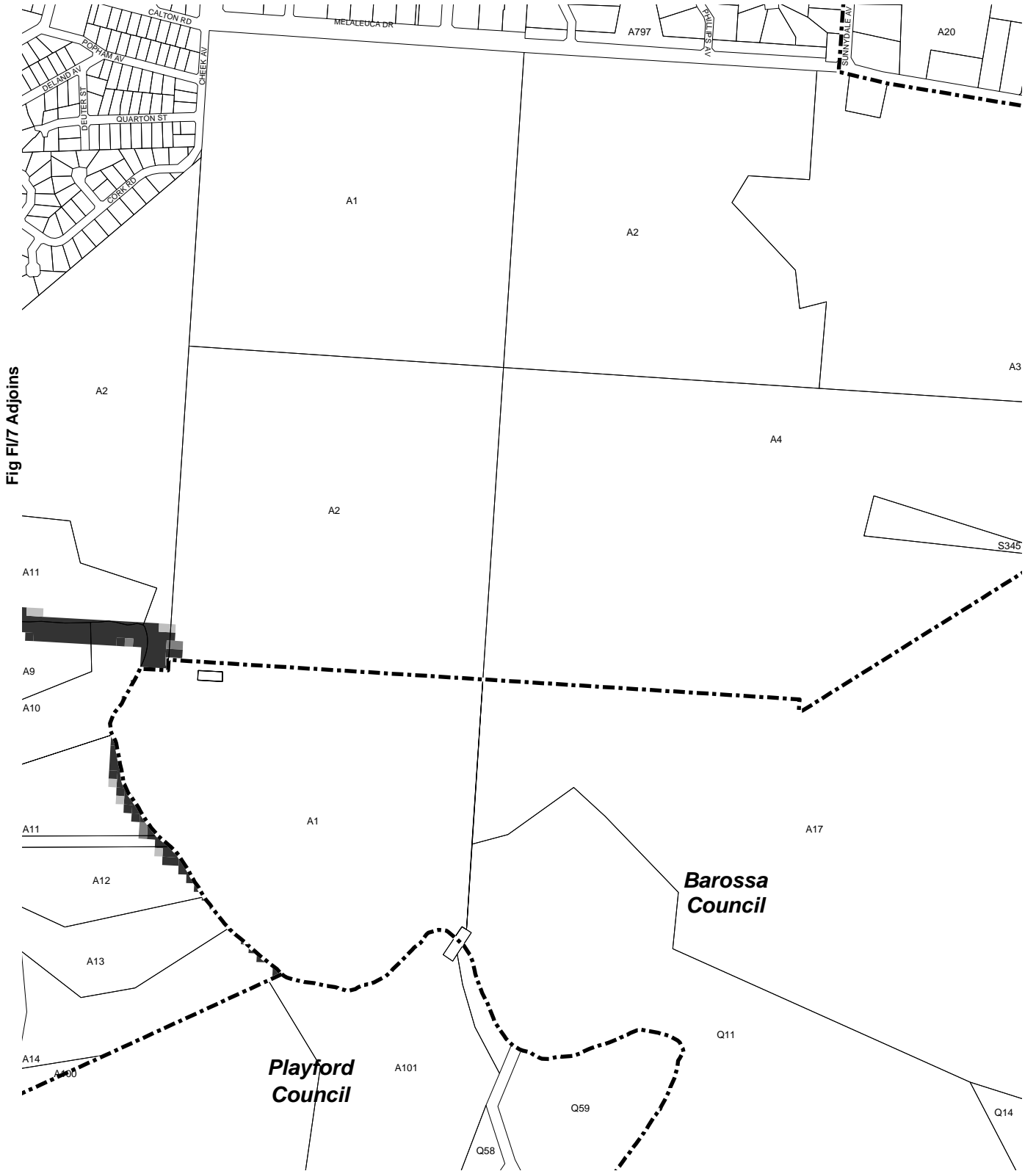


Fig FI/7 Adjoins

Note: See figure FI/1 for flood data definition explanation and disclaimer.



- Hazard Flood Risk Areas
- High
 - Medium
 - Low
 - Development Plan Boundary

GAWLER (CT) GAWLER RIVER FLOOD PRONE AREAS FIGURE FI/8

Consolidated - 18 July 2019

Up to 0.6 metres depth at flood where velocities are low, or from 0.3 metres depth where velocities are high (up to 0.8m/s).

- (c) *High* is deeper and or fast flow of waters. Wading through water is either difficult or impossible for adults and evacuation is required by boat or helicopter.

From 0.6 metres (including areas of two metres plus) depth of flood, even where velocities are very low, or at depth from 0.6 metres where velocities are high (up to 1.5m/s and greater).

PRINCIPLES OF DEVELOPMENT CONTROL

- 74** Development should not occur unless it is sited, designed and undertaken with appropriate precautions being taken against the relevant hazard flood risk.
- 75** Intensive animal keeping should not be established.
- 76** Land should not be divided unless:
- (a) it does not result in an increase in the number of allotments (except where it is for the purpose of creating a separate allotment for an existing dwelling and all resulting allotments will contain at least one existing dwelling); or
 - (b) it provides for public access to the banks of the river in the form of a reserve or easements necessary for the provision of public utility services; or
 - (c) it facilitates the construction of flood mitigation works; or
 - (d) it involves works designed and certified by a suitably qualified engineer to minimise the impact of a 1 in 100 ARI flood event on the subject land and any other land in the area, and where such works would not cause a material adverse impact on other people or property.
- 77** Land division in the form of boundary realignment should only occur if:
- (a) the land does not contain environmental features or significant site constraints; and
 - (b) sufficient space can be provided in appropriate locations outside of the Gawler Rivers Floodplain Area for the siting of buildings, structures and associated services and infrastructure necessary for the intended land use; and
 - (c) it is for the purpose of creating a separate allotment for an existing dwelling and all resulting allotments will contain at least one existing dwelling; and
 - (d) evacuation routes to land and roads outside of flood risk areas are available without the need to pass through medium or high hazard flood risk areas shown on [Figures FI/1 to FI/8](#).
- 78** Development should be provided with all weather vehicular access for the purpose of evacuation in a flood to an area located outside of the medium and high hazard flood risk areas identified in [Figures FI/1 to FI/8](#). No part of an evacuation route should pass through land that is identified within a medium or high hazard flood risk area.
- 79** Additions to dwellings should minimise the impact on the flow of floodwaters, by minimising the extent of total ground floor area of the dwelling.
- 80** Filling of land required to raise the finished floor level of a building should not extend more than 7 metres beyond the external walls of that building and be of good quality composition and compaction providing suitable ground stability in the event of flooding.
- 81** Where fencing is required to provide safety to residents or security to stock, goods or machinery, it should be designed to withstand the flow of floodwater.

- 82** Solid fencing for State and Local heritage listed places is appropriate provided that the level of flood risk in the vicinity is not significantly altered.
- 83** Solid fencing should be limited to the private open space areas required by the zone or policy area and generally be located to the side and rear of dwellings.
- 84** Land uses associated with activities which produce strong organic, chemical or other intractable wastes, or the storage of bio-chemicals or chemical agents should not be established.
- 85** Development should not result in stockpiles or similar obstructions.
- 86** Levees or floodwalls that do not form part of a public flood mitigation scheme should not be established as a flood protection measure unless it can be demonstrated that the levees or floodwalls do not increase the extent or hazard flood risk of land either upstream or downstream.
- 87** Any on-site wastewater treatment system/reuse system or effluent drainage fields should not be located in the Gawler Rivers Floodplain Area.

Industrial Development

OBJECTIVES

Objective 33: An adequate supply of suitable and appropriately located land to accommodate current and projected industrial activities.

Industry requires reasonably level, well drained land, which can be supplied with the appropriate infrastructure and is readily accessible to labour and transport.

Objective 34: Industrial land and activities protected from encroachment by incompatible land uses.

Objective 35: Development at the interface between industrial activities and sensitive uses that is compatible with surrounding activities, particularly those in adjoining zones.

Industrial zones which adjoin residential areas should develop so that lower impact activities and some types of commercial development are located on the periphery of industrial zones. Impacts can be minimised within adjoining industry areas using design treatments.

PRINCIPLES OF DEVELOPMENT CONTROL

- 88** Industrial development should be located in business or industrial areas.
- 89** No dwellings other than caretakers' quarters should be erected in industrial areas.
- 90** Industrial development should be of a high architectural standard.
- 91** The width of roads in an industrial area should be adequate for the type and volume of traffic expected.
- 92** Where industrial areas abut residential areas light industrial development should be located near the residential area to minimise the nuisance to householders.

Infrastructure

OBJECTIVES

Objective 36: Infrastructure provided in an economical and environmentally sensitive manner.

Objective 37: Infrastructure, including social infrastructure, provided in advance of need.

Objective 38: Suitable land for infrastructure identified and set aside in advance of need.

Objective 39: The visual impact of infrastructure facilities minimised.

Objective 40: The efficient and cost-effective use of existing infrastructure.

Objective 41: The protection of existing infrastructure.

PRINCIPLES OF DEVELOPMENT CONTROL

93 Development should not occur without the provision of adequate utilities and services, including:

- (a) electricity supply;
- (b) water supply;
- (c) drainage and stormwater systems;
- (d) waste disposal;
- (e) effluent disposal systems;
- (f) formed all-weather public roads;
- (g) telecommunications services;
- (h) social infrastructure, community services and facilities;
- (i) gas services.

94 Development should only occur only where it provides, or has access to, relevant easements for the supply of infrastructure.

95 Development should incorporate provision for the supply of infrastructure services to be located within common service trenches where practicable.

96 Development should not take place until adequate and coordinated drainage of the land is assured.

97 Development in urban areas should not occur without provision of an adequate reticulated domestic quality mains water supply and an appropriate waste treatment system.

98 In areas where no reticulated water supply is available, buildings whose usage is reliant on a water supply should be equipped with an adequate and reliable on-site water storage system.

99 Urban development should not be dependent on an indirect water supply.

100 Electricity infrastructure should be designed and located to minimise its visual and environmental impacts.

101 In urban areas, electricity supply serving new development should be installed underground (excluding lines having a capacity greater than or equal to 33kv).

102 Utilities and services, including access roads and tracks, should be sited on areas already cleared of native vegetation. If this is not possible, their siting should cause minimal interference or disturbance to existing native vegetation and biodiversity.

103 Utility buildings and structures should be grouped with non-residential development where possible.

- 104** Development in proximity to infrastructure facilities should be sited and be of a scale to ensure adequate separation to protect people and property.
- 105** Existing transmission lines should not be encroached upon by incompatible uses.
- 106** Provision should be made for new distribution substations and overhead major electricity line corridors (having a capacity greater than or equal to 33kv) in optimum locations with adequate access.

Interface Between Land Uses

OBJECTIVES

- Objective 42:** Development located and designed to minimise adverse impact and conflict between land uses.
- Objective 43:** Protect community health and amenity from adverse impacts of development.
- Objective 44:** Protect desired land uses from the encroachment of incompatible development.

PRINCIPLES OF DEVELOPMENT CONTROL

- 107** Development should not detrimentally affect the amenity of the locality or cause unreasonable interference through any of the following:
- (a) the emission of effluent, odour, smoke, fumes, dust or other airborne pollutants;
 - (b) noise;
 - (c) vibration;
 - (d) electrical interference;
 - (e) light spill;
 - (f) glare;
 - (g) hours of operation;
 - (h) traffic impacts.
- 108** Development should be sited and designed to minimise negative impacts on existing and potential future land uses desired in the locality.
- 109** Development adjacent to a **Residential Zone** or residential area within a **Township Zone** should be designed to minimise overlooking and overshadowing of adjacent dwellings and private open space.
- 110** Residential development adjacent to non-residential zones and land uses should be located, designed and/or sited to protect residents from potential adverse impacts from non-residential activities.
- 111** Sensitive uses likely to conflict with the continuation of lawfully existing developments and land uses desired for the zone should be designed to minimise negative impacts.
- 112** Non-residential development on land abutting a residential zone should be designed to minimise noise impacts to achieve adequate levels of compatibility between existing and proposed uses.

Noise Generating Activities

- 113** Development that emits noise (other than music noise) should include noise attenuation measures that achieve the relevant *Environment Protection (Noise) Policy* criteria when assessed at the nearest existing noise sensitive premises.
- 114** Development with the potential to emit significant noise (e.g. industry) should incorporate noise attenuation measures that prevent noise from causing unreasonable interference with the amenity of noise sensitive premises.
- 115** Outdoor areas (such as beer gardens or dining areas) associated with licensed premises should be designed or sited to minimise adverse noise impacts on adjacent existing or future noise sensitive development.
- 116** Development proposing music should include noise attenuation measures that achieve the following desired noise levels:

Noise level assessment location	Desired noise level
Adjacent existing <i>noise sensitive development</i> property boundary	Less than 8 dB above the level of background noise ($L_{90,15min}$) in any octave band of the sound spectrum and Less than 5 dB(A) above the level of background noise ($LA_{90,15min}$) for the overall (sum of all octave bands) A-weighted level
Adjacent <i>land</i> property boundary	Less than 65dB(Lin) at 63Hz and 70dB(Lin) in all other octave bands of the sound spectrum or Less than 8 dB above the level of background noise ($L_{90,15min}$) in any octave band of the sound spectrum and 5 dB(A) overall (sum of all octave bands) A-weighted level

Air Quality

- 117** Development with the potential to emit harmful or nuisance-generating air pollution should incorporate air pollution control measures to prevent harm to human health or unreasonable interference with the amenity of sensitive uses within the locality.
- 118** Chimneys or exhaust flues associated with commercial development (including cafes, restaurants and fast food outlets) should be designed to ensure they do not cause a nuisance or health concerns to nearby sensitive receivers by:
- incorporating appropriate treatment technology before exhaust emissions are released to the atmosphere;
 - ensuring that the location and design of chimneys or exhaust flues maximises dispersion and takes into account the location of nearby sensitive uses.

Rural Interface

- 119** The potential for adverse impacts resulting from rural development should be minimised by:
- not locating horticulture or intensive animal keeping on land adjacent to townships;
 - maintaining an adequate separation between horticulture or intensive animal keeping and townships, other sensitive uses and, where desirable, other forms of primary production.

- 120** Traffic movement, spray drift, dust, noise, odour and the use of frost fans and gas guns associated with primary production should not lead to unreasonable impact on adjacent land uses.
- 121** Existing primary production and mineral extraction should not be prejudiced by the inappropriate encroachment of sensitive uses such as urban development.
- 122** Development that is adjacent to land used for primary production (within either the zone or adjacent zones) should include appropriate setbacks and vegetative plantings designed to minimise the potential impacts of chemical spray drift and other impacts associated with primary production.
- 123** New urban development should provide a buffer of at least 40 metres wide (inclusive of any fuel break, emergency vehicle access or road) separating urban and rural activities.
- 124** Development located within 300 metres of facilities for the handling, transportation and storage of bulk commodities should:
- (a) not prejudice the continued operation of those facilities;
 - (b) be located, designed and developed having regard to the potential environmental impact arising from the operation of such facilities and the potential extended hours of operation.

Land Division

OBJECTIVES

Objective 45: Land division in appropriate localities to create a compact urban area.

Development of the metropolitan area should proceed in an orderly and convenient manner, making proper use of the State's economic resources and avoiding scattered development caused by haphazard and premature division of land.

Development which satisfies urban demands and requirements should be confined to sites within identified urban areas. This objective may be achieved through selective development of infill housing, redevelopment and refurbishment of existing housing, and use of vacant and under-utilised land, with the aim of reducing the social, environmental and economic costs of urban development, and maximising use of the community investment in facilities and services in existing housing areas. While a compact form of development is generally desirable, recognition must be given to areas of particular character of amenity, or to specific constraints such as environmental or historical value, water catchment areas and areas of bushfire hazard.

PRINCIPLES OF DEVELOPMENT CONTROL

- 125** Land should not be divided where community facilities or public utilities are lacking or inadequate or where land in the vicinity has been divided and the allotments have not been substantially developed.
- 126** Land should not be divided:
- (a) in a manner which would prevent the satisfactory future division of the land, or any part thereof;
 - (b) if the proposed use, or the establishment of the proposed use, is likely to lead to undue erosion of the land or land in the vicinity thereof;
 - (c) unless wastes produced by the proposed use of the land, or any use permitted by the principles of development control, can be managed so as to prevent pollution of a public water supply or any surface or underground water resources;

- (d) if the size, shape and location of, and the slope and nature of the land contained in, each allotment resulting from the division is unsuitable for the purpose for which the allotment is to be used;
- (e) if any part of the land is likely to be inundated by tidal or floodwaters and the proposed allotments are to be used for a purpose which would be affected detrimentally when the land is inundated;
- (f) where the proposed use of the land is the same as the proposed use of other existing allotments in the vicinity, and a substantial number of the existing allotments have not been used for that purpose;
- (g) if it would cause an infringement of any provisions of relevant building legislation or any by-law or regulation made thereunder; or
- (h) where existing significant trees or remnant vegetation will be removed or compromised.

127 When land is divided:

- (a) any reserves or easements necessary for the provision of public utility services should be provided;
- (b) stormwater should be capable of being drained safely and efficiently from each proposed allotment and disposed of from the land in a satisfactory manner;
- (c) a water supply sufficient for the purpose for which the allotment is to be used should be made available to each allotment;
- (d) provision should be made for the disposal of waste waters, sewage and other effluents from each allotment without risk to health;
- (e) roads or thoroughfares should be provided where necessary for safe and convenient communication with adjoining land and neighbouring localities;
- (f) each allotment resulting from the division should have safe and convenient access to the carriageway of an existing or proposed road or thoroughfare at all times;
- (g) proposed roads should be graded, or be capable of being graded to connect safely and conveniently with an existing road or thoroughfare;
- (h) for urban purposes, provision should be made for suitable land to be set aside for usable local open space; and
- (i) if it borders a watercourse the land immediately adjoining the watercourse should become public open space, with a public road fronting the open space and be rehabilitated for appropriate public use.

128 Where land which has a frontage onto the Gawler River, North Para River and South Para River is divided, a reserve at least 30 metres wide, when measured from the top of the bank, should be provided along such a frontage.

129 Land division within an area identified as being 'Excluded Area from Bushfire Protection Planning Provisions' on Bushfire Protection Area [Figures Ga\(BPA\)/1 to 5](#) should be designed to make provision for:

- (a) emergency vehicle access through to the Bushfire Protection Area and other areas of open space connected to it;
- (b) a mainly continuous street pattern serving new allotments that eliminates the use of cul-de-sacs or dead end roads; and

- (c) a fire hazard separation zone isolating residential allotments from areas that pose an unacceptable bushfire risk by containing the allotments within a perimeter road or through other means that achieve an adequate separation.

Mining

OBJECTIVES

Objective 46: Continued availability of metallic, industrial and construction, minerals by preventing development likely to inhibit their exploitation.

Building and construction minerals are significant to the metropolitan area due to scarcity of natural timbers for building construction. Adelaide is particularly dependent on resources of clay and shale for brick manufacture, and sand and stone for concrete and mortar aggregate. Equally important are materials such as filling sand and quarry products used in road building and general construction. Transport costs of these bulky low-value products rise rapidly as the distance increases between the workings and the point of consumption, with a consequent increase in price to the consumer.

Although large reserves of most of these materials exist, they can be easily sterilized by other uses of the land. Workable deposits should therefore be kept free of building and other development so that the deposits are available when needed.

Objective 47: Protection of the landscape from undue damage from quarrying and similar extractive and associated manufacturing industries.

Land should not be left derelict following the extraction of minerals, and wherever possible steps should be taken to reclaim the land and put it to a suitable use. After-use plans should form the basis of the working program, indicate the depths and direction of working, access roads, support for abutting roads and adjoining land, disposal of waste and screening of plant and machinery by trees.

The remaining natural environment of land adjoining the Gawler River which has not been affected by loam extraction, should be preserved.

PRINCIPLES OF DEVELOPMENT CONTROL

- 130** Known reserves of economically-workable mineral deposits should be kept free from development until such time as the deposits are able to be exploited.
- 131** Quarrying and similar extractive and associated manufacturing industries should be sited and managed so that their impact on the landscape is minimal.
- 132** Removal of undesirable structures and the beautification of quarry faces by landscaping or restoration of the natural cover of the land, should be undertaken after mining and quarry workings are finished.
- 133** Mining operations in areas of remnant bushland or scenic areas should only proceed following full evaluation of the benefits to the community in retaining bushland or scenery, as opposed to the development of the deposit and the relative abundance of alternative deposits.
- 134** Mining operations should be based on a rehabilitation plan to ensure a close correlation between the operations and the after-use of the site.
- 135** No new loam pits should be opened within the Gawler Rivers Floodplain Area identified on [Figures FI/1 to FI/8](#) and further loam extraction should be:
 - (a) contained within existing approved pits; and
 - (b) worked in accordance with a development and rehabilitation plan that describes the intended stages of rehabilitation and long-term after-use.

136 Development should not prejudice the future mining of significant sources of metallic, industrial or construction materials.

Metropolitan Open Space System (MOSS)

OBJECTIVES

- Objective 48:** A clearly defined and linked Metropolitan Open Space System of public and privately owned land of an open or natural character in and around metropolitan Adelaide which will:
- (a) provide a visual and scenic contrast to the built urban environment;
 - (b) separate different parts of the metropolitan area;
 - (c) assist in the conservation of natural or semi-natural habitats and sites of scientific or heritage interest and re-vegetation;
 - (d) provide corridors for movement of wildlife;
 - (e) accommodate a range of active recreation and sporting facilities of regional or State significance, including facilities which may be used for national and international events;
 - (f) accommodate a range of passive recreation and leisure areas; and
 - (g) provide for the integration of stormwater management in association with recreation, aquifer recharge and water quality management.
- Objective 49:** The use of private land within the Metropolitan Open Space System for low-scale uses such as non-intensive agriculture, rural living or low-impact tourist facilities where the emphasis is on retaining or developing the open, natural or rural character and buildings are located and designed in such a way as to blend into the open character of the area.
- Objective 50:** The development of public land within the Metropolitan Open Space System for active and passive recreation, sporting facilities and conservation with emphasis on retaining the open, natural or rural character with wide landscaped buffers around the perimeter of areas where appropriate, areas of conservation significance retained in their natural state and buildings located and designed in such a way as to minimise their impact.
- Objective 51:** The development of open space recreation reserves through land purchases, contributions of open space, and exchanges of land.

Every opportunity should be taken to increase the extent of public open space within strategic locations of the Metropolitan Open Space System.

Means of encouraging this include:

- (a) purchase of land by councils, other public authorities and community groups;
- (b) encouraging developers who are obliged to provide public open space when land is divided, to fulfil that obligation by purchasing an equivalent area of land within the Metropolitan Open Space System in lieu of providing a public reserve within the land being divided;
- (c) agreements with landholders regarding valuation of land; and
- (d) land management agreements.

PRINCIPLES OF DEVELOPMENT CONTROL

- 137** Development should preserve and enhance the character and amenity of land within the Metropolitan Open Space System as shown on [Map Ga/1 \(Overlay 2\)](#).
- 138** Publicly owned land within the Metropolitan Open Space System should be used to provide natural or landscaped open space, accommodate a range of public recreation, sporting and institutional facilities and uses and to accommodate stormwater management.
- 139** Publicly owned land within the Metropolitan Open Space System should also be used to conserve wildlife habitats and areas of natural vegetation, to allow for movement of wildlife, to conserve sites of scientific, cultural or heritage interest and for re-vegetation.
- 140** Privately owned land within the Metropolitan Open Space System should be used for rural activities and agriculture (but not intensive animal keeping), very low-density residential development, low-impact tourist, or sporting facilities, or conservation purposes.
- 141** Buildings and structures erected on land within the Metropolitan Open Space System should be designed, located and screened so as to be unobtrusive and not detract from the open natural or landscaped character of these areas.
- 142** When land fronting watercourses within the Metropolitan Open Space System is divided land adjoining the watercourse should become a public reserve.
- 143** The width of reserves abutting watercourses within the Metropolitan Open Space System should be sufficient to allow for flood control, stormwater management, retention of the riverine ecosystem and to provide areas of open space which can be used to accommodate a range of recreational and sporting facilities.

Natural Resources

OBJECTIVES

- Objective 52:** Retention, protection and restoration of the natural resources and environment.
- Objective 53:** Protection of the quality and quantity of South Australia's surface waters, including inland and underground waters.
- Objective 54:** The ecologically sustainable use of natural resources including water resources, ground water, surface water and watercourses.
- Objective 55:** Natural hydrological systems and environmental flows reinstated, and maintained and enhanced.
- Objective 56:** Development consistent with the principles of water sensitive design.
- Objective 57:** Development sited and designed to:
- (a) protect natural ecological systems;
 - (b) achieve the sustainable use of water;
 - (c) protect water quality, including receiving waters;
 - (d) reduce runoff and peak flows and prevent the risk of downstream flooding;
 - (e) minimise demand on reticulated water supplies;
 - (f) maximise the harvest and use of stormwater;

(g) protect stormwater from pollution sources.

Objective 58: Storage and use of stormwater which avoids adverse impact on public health and safety.

Objective 59: Native flora, fauna and ecosystems protected, retained, conserved and restored.

Objective 60: Restoration, expansion and linking of existing native vegetation to facilitate habitat corridors for ease of movement of fauna.

Objective 61: Minimal disturbance and modification of the natural landform.

Objective 62: Protection of the physical, chemical and biological quality of soil resources.

Objective 63: Protection of areas prone to erosion or other land degradation processes from inappropriate development.

Objective 64: Protection of the scenic qualities of natural and rural landscapes.

PRINCIPLES OF DEVELOPMENT CONTROL

144 Development should be undertaken with minimum impact on the natural environment, including air and water quality, land, soil, biodiversity, and scenically attractive areas.

145 Development should ensure that South Australia's natural assets, such as biodiversity, water and soil, are protected and enhanced.

146 Development should not significantly obstruct or adversely affect sensitive ecological areas such as creeks and wetlands.

147 Development should be appropriate to land capability and the protection and conservation of water resources and biodiversity.

Water Sensitive Design

148 Development should be designed to maximise conservation, minimise consumption and encourage reuse of water resources.

149 Development should not take place if it results in unsustainable use of surface or underground water resources.

150 Development should be sited and designed to:

- (a) capture and re-use stormwater, where practical;
- (b) minimise surface water runoff;
- (c) prevent soil erosion and water pollution;
- (d) protect and enhance natural water flows;
- (e) protect water quality by providing adequate separation distances from watercourses and other water bodies;
- (f) not contribute to an increase in salinity levels;
- (g) avoid the water logging of soil or the release of toxic elements;
- (h) maintain natural hydrological systems and not adversely affect:
 - (i) the quantity and quality of groundwater;

- (ii) the depth and directional flow of groundwater;
- (iii) the quality and function of natural springs.

151 Water discharged from a development site should:

- (a) be of a physical, chemical and biological condition equivalent to or better than its pre-developed state;
- (b) not exceed the rate of discharge from the site as it existed in pre-development conditions.

152 Development should include stormwater management systems to protect it from damage during a minimum of a 1-in-100 year average return interval flood.

153 Development should have adequate provision to control any stormwater over-flow runoff from the site and should be sited and designed to improve the quality of stormwater and minimise pollutant transfer to receiving waters.

154 Development should include stormwater management systems to mitigate peak flows and manage the rate and duration of stormwater discharges from the site to ensure the carrying capacities of downstream systems are not overloaded.

155 Development should include stormwater management systems to minimise the discharge of sediment, suspended solids, organic matter, nutrients, bacteria, litter and other contaminants to the stormwater system.

156 Stormwater management systems should preserve natural drainage systems, including the associated environmental flows.

157 Stormwater management systems should:

- (a) maximise the potential for stormwater harvesting and re-use, either on-site or as close as practicable to the source;
- (b) utilise, but not be limited to, one or more of the following harvesting methods:
 - (i) the collection of roof water in tanks;
 - (ii) the discharge to open space, landscaping or garden areas, including strips adjacent to car parks;
 - (iii) the incorporation of detention and retention facilities;
 - (iv) aquifer recharge.

158 Where it is not practicable to detain or dispose of stormwater on site, only clean stormwater runoff should enter the public stormwater drainage system.

159 Artificial wetland systems, including detention and retention basins, should be sited and designed to:

- (a) ensure public health and safety is protected;
- (b) minimise potential public health risks arising from the breeding of mosquitoes.

Water Catchment Areas

160 Development should ensure watercourses and their beds, banks, wetlands and floodplains are not damaged or modified and are retained in their natural state, except where modification is required for essential access or maintenance purposes.

- 161** No development should occur where its proximity to a swamp or wetland will damage or interfere with the hydrology or water regime of the swamp or wetland.
- 162** A wetland or low-lying area providing habitat for native flora and fauna should not be drained, except temporarily for essential management purposes to enhance environmental values.
- 163** Along watercourses, areas of remnant native vegetation, or areas prone to erosion, that are capable of natural regeneration should be fenced off to limit stock access.
- 164** Development such as cropping, intensive animal keeping, residential, tourism, industry and horticulture, that increases the amount of surface run-off should include a strip of land at least 20 metres wide (30 metres wide in the case of the Gawler, North Para and South Para Rivers) measured from the top of existing banks on each side of a watercourse that is:
- (a) fenced to exclude livestock;
 - (b) kept free of development, including structures, formal roadways or access ways for machinery or any other activity causing soil compaction or significant modification of the natural surface of the land;
 - (c) revegetated with locally indigenous vegetation comprising trees, shrubs and other groundcover plants to filter run-off so as to reduce the impacts on native aquatic ecosystems and to minimise soil loss eroding into the watercourse.
- 165** Development resulting in the depositing of an object or solid material in a watercourse or floodplain or the removal of bank and bed material should not:
- (a) adversely affect the migration of aquatic biota;
 - (b) adversely affect the natural flow regime;
 - (c) cause or contribute to water pollution;
 - (d) result in watercourse or bank erosion;
 - (e) adversely affect native vegetation upstream or downstream that is growing in or adjacent to a watercourse.
- 166** The location and construction of dams, water tanks and diversion drains should:
- (a) occur off watercourse;
 - (b) not take place in ecologically sensitive areas or on erosion-prone sites;
 - (c) provide for low flow by-pass mechanisms to allow for migration of aquatic biota;
 - (d) not negatively affect downstream users;
 - (e) minimise in-stream or riparian vegetation loss;
 - (f) incorporate features to improve water quality (eg wetlands and floodplain ecological communities);
 - (g) protect ecosystems dependent on water resources.
- 167** Irrigated horticulture and pasture should not increase groundwater-induced salinity.
- 168** Development should comply with the current *Environment Protection (Water Quality) Policy*.

Biodiversity and Native Vegetation

- 169** Development should retain existing areas of native vegetation and where possible contribute to revegetation using locally indigenous plant species.
- 170** Development should be designed and sited to minimise the loss and disturbance of native flora and fauna and their breeding grounds and habitats.
- 171** The provision of services, including power, water, effluent and waste disposal, access roads and tracks should be sited on areas already cleared of native vegetation.
- 172** Native vegetation should be conserved and its conservation value and function not compromised by development if the native vegetation does any of the following:
- (a) provides an important habitat for wildlife or shade and shelter for livestock;
 - (b) has a high plant species diversity or includes rare, vulnerable or endangered plant species or plant associations and communities;
 - (c) provides an important seed bank for locally indigenous vegetation;
 - (d) has high amenity value and/or significantly contributes to the landscape quality of an area, including the screening of buildings and unsightly views;
 - (e) has high value as a remnant of vegetation associations characteristic of a district or region prior to extensive clearance for agriculture;
 - (f) is growing in, or is characteristically associated with a wetland environment.
- 173** Native vegetation should not be cleared if such clearing is likely to lead to, cause or exacerbate any of the following:
- (a) erosion or sediment within water catchments;
 - (b) decreased soil stability;
 - (c) soil or land slip;
 - (d) deterioration in the quality of water in a watercourse or surface water runoff;
 - (e) a local or regional salinity problem;
 - (f) the occurrence or intensity of local or regional flooding.
- 174** Development that proposes the clearance of native vegetation should address or consider the implications that removing the native vegetation will have on the following:
- (a) provision for linkages and wildlife corridors between significant areas of native vegetation;
 - (b) erosion along watercourses and the filtering of suspended solids and nutrients from run-off;
 - (c) the amenity of the locality;
 - (d) bushfire safety;
 - (e) the net loss of native vegetation and other biodiversity.
- 175** Where native vegetation is to be removed, it should be replaced in a suitable location on the site with locally indigenous vegetation to ensure that there is not a net loss of native vegetation and biodiversity.

176 Development should be located and occur in a manner which:

- (a) does not increase the potential for, or result in, the spread of pest plants, or the spread of any non-indigenous plants into areas of native vegetation or a conservation zone;
- (b) avoids the degradation of remnant native vegetation by any other means including as a result of spray drift, compaction of soil, modification of surface water flows, pollution to groundwater or surface water or change to groundwater levels;
- (c) incorporates a separation distance and/or buffer area to protect wildlife habitats and other features of nature conservation significance.

177 Development should promote the long-term conservation of vegetation by:

- (a) avoiding substantial structures, excavations, and filling of land in close proximity to the trunk of trees and beneath their canopies;
- (b) minimising impervious surfaces beneath the canopies of trees;
- (c) taking other effective and reasonable precautions to protect both vegetation and the integrity of structures and essential services.

178 Horticulture involving the growing of olives should be located at least:

- (a) 500 metres from:
 - (i) a national park;
 - (ii) a conservation park;
 - (iii) a wilderness protection area;
 - (iv) the edge of a substantially intact stratum of native vegetation greater than 5 hectares in area.
- (b) 50 metres from the edge of stands of native vegetation 5 hectares or less in area.

179 Horticulture involving the growing of olives should have at least one locally indigenous tree that will grow to a height of at least 7 metres sited at least every 100 metres around the perimeter of the orchard.

Soil Conservation

180 Development should not have an adverse impact on the natural, physical, chemical or biological quality and characteristics of soil resources.

181 Development should be designed and sited to prevent erosion.

182 Development should take place in a manner that will minimise alteration to the existing landform.

183 Development should minimise the loss of soil from a site through soil erosion or siltation during the construction phase of any development and following the commencement of an activity.

Open Space and Recreation

OBJECTIVES

Objective 65: The creation of a network of linked parks, reserves, recreational trails and recreation areas at regional and local levels.

Objective 66: Pleasant, functional and accessible open spaces providing a range of physical environments.

Objective 67: A wide range of settings for active and passive recreational opportunities.

Objective 68: The provision of open space in the following hierarchy:

- (a) State
- (b) Regional
- (c) District
- (d) Neighbourhood
- (e) Local.

PRINCIPLES OF DEVELOPMENT CONTROL

184 Urban development should include public open space and recreation areas.

185 Public open space and recreation areas should be of a size, dimension and location that:

- (a) facilitate a range of formal and informal recreation activities;
- (b) provide for the movement of pedestrians and cyclists;
- (c) incorporate existing vegetation and natural features, watercourses, wildlife habitat and other sites of natural or cultural value;
- (d) link habitats, wildlife corridors, public open spaces and existing recreation facilities;
- (e) enable effective stormwater management;
- (f) provides for the planting and retention of large trees and vegetation.

186 Open space should be designed to incorporate:

- (a) pedestrian, cycle linkages to other open spaces, centres, schools and public transport nodes;
- (b) park furniture, shaded areas and resting places to enhance pedestrian comfort;
- (c) safe crossing points where pedestrian routes intersect the road network;
- (d) easily identified access points;
- (e) frontage to abutting public roads to optimise pedestrian access and visibility;
- (f) re-use of stormwater for irrigation purposes;
- (g) recreational trails where appropriate.

187 Where practical, access points to regional parks should be located close to public transport.

188 District level parks should be at least 3 hectares in size, and provided within 2 kilometres of all households that they serve.

189 Neighbourhood parks should be at least 0.5 hectares and generally closer to 1 hectare in size, and provided within 500 metres of households that they serve.

190 Local parks should be

- (a) a minimum of 0.2 hectares in size;
- (b) centrally located within a residential area, close to schools, shops and generally within 300 metres of households that they serve.

191 No more than 20 percent of land allocated as public open space should:

- (a) have a slope in excess of 1-in-4;
- (b) comprise creeks or other drainage areas;

unless the public open space forms part of a regional integrated open space network.

192 Signage should be provided at entrances to and within public open space to provide clear orientation to major points of interest such as the location of public toilets, telephones, safe routes and park activities.

193 Buildings in open space, including structures and associated car parking areas, should be designed, located and of a scale that is unobtrusive and does not detract from the desired open space character.

194 Development in open space should:

- (a) be clustered where practical to ensure that the majority of the site remains open;
- (b) where practical, be developed for multi-purpose use;
- (c) be constructed to minimise the extent of hard paved areas.

195 Open spaces and recreation areas should be located and designed to maximise safety and security by:

- (a) ensuring that within urban areas, their edges are overlooked by housing, commercial or other development that can provide effective informal surveillance;
- (b) ensuring fenced parks and playgrounds have more than one entrance or exit when fenced;
- (c) locating play equipment where it can be informally observed by nearby residents and users during times of use;
- (d) clearly defining the perimeters of play areas;
- (e) providing lighting around facilities such as toilets, telephones, seating, litter bins, bike storage and car parks;
- (f) focusing pedestrian and bicycle movement after dark along clearly defined, adequately lit routes with observable entries and exits.

196 Landscaping associated with open space and recreation areas should:

- (a) not compromise the drainage function of any drainage channel;
- (b) provide shade and windbreaks along cyclist and pedestrian routes, around picnic and barbecue areas and seating, and in car parking areas;
- (c) maximise opportunities for informal surveillance throughout the park;
- (d) enhance the visual amenity of the area and complement existing buildings;

- (e) be designed and selected to minimise maintenance costs;
- (f) provide habitat for local fauna.

- 197** Development of recreational activities in areas not zoned for that purpose should be compatible with surrounding activities.
- 198** Recreation facilities development should be sited and designed to minimise negative impacts on the amenity of the locality.

Outdoor Advertisements

The context for advertising or advertising displays is critical in assessing the compatibility of the development with the Desired Future Character or character of a locality or zone. Signage types are defined in [Table Ga/4](#).

OBJECTIVES

- Objective 69:** An urban environment and rural landscape not disfigured by advertisements.
- Objective 70:** Advertisements in retail, commercial and industrial urban areas, and centre zones, designed to enhance the appearance of those areas.
- Objective 71:** Advertisements not hazardous to any person.

PRINCIPLES OF DEVELOPMENT CONTROL

- 199** Advertising and advertising displays should, in addition to general provisions, conform with the performance criteria in [Table Ga/4](#).
- 200** The location, siting, size, shape and materials of construction, of advertisements should be:
- (a) consistent with the desired character of areas or zones as described by their objectives;
 - (b) consistent with the predominant character of the urban or rural landscape; or
 - (c) in harmony with any building or site of historic significance or heritage value in the locality.
- 201** Advertisements should not detrimentally affect by way of their siting, size, shape, scale, glare, reflection or colour the amenity of areas, zones, or localities, in which they are situated.
- 202** Advertisements should not impair the amenity of areas, zones, or localities, in which they are situated by creating, or adding to, clutter, visual disorder and the untidiness of buildings and spaces.
- 203** Advertisements should not obscure views of attractive landscapes or particular trees or groups of trees.
- 204** The scale of advertisements should be compatible with the buildings on which they are situated and with nearby buildings and spaces.
- 205** Advertisements should be constructed and designed in a workmanlike manner.
- 206** Advertisements wholly or partly consisting of bunting, streamers, flags, windvanes, and the like should not detrimentally affect the amenity of areas, zones or localities in which they are situated.
- 207** Advertisements on buildings that have a single architectural theme but which contain a number of tenancies, should be attached and displayed so as to be co-coordinated with that theme.

- 208** Advertisements should not be erected in positions close to existing electricity mains so that potentially hazardous situations are created.
- 209** Advertisements should not create a hazard to persons travelling by any means.
- 210** Advertisements should not obscure a driver's view of other road vehicles, of rail vehicles at or approaching level crossings, of pedestrians and of features of the road such as junctions, bends, changes in width, traffic control devices and the like that are potentially hazardous.
- 211** Advertisements should not be so highly illuminated as to cause discomfort to an approaching driver, or create difficulty in his perception of the road, or of persons or objects on it.
- 212** Advertisements should not be liable to interpretation by drivers as an official traffic sign, or convey to drivers information that might be confused with instructions given by traffic signals or other control devices, or impair the conspicuous nature of traffic signs or signals.
- 213** Advertisements should not detract drivers from the primary driving task at a location.
- 214** Advertisements or advertising displays should:
- (a) convey in clear and concise language, symbols, print style, layout, and in a limited number of colours, the owner/occupier and/or generic type of business, merchandise or services;
 - (b) be minimised in number to avoid proliferation of advertisements or advertising displays;
 - (c) be of a form, style, scale and size and in locations appropriate to the desired character and future use of land or character of the locality or zone and to preserve reasonable exposure to the public for all adjacent sites;
 - (d) not have an adverse impact on the amenity of adjacent land uses and areas from which they are visible and without limiting the foregoing, no bunting, whirligigs or other like displays should be displayed;
 - (e) conceal the supporting structure from view wherever possible;
 - (f) be completely contained within the boundaries of the subject property, and if road widening is applicable, be within the proposed property boundary realignment;
 - (g) be unified in colour, style, placement and proportion to present a coordinated theme and design for a site, centre or building; and
 - (h) be designed and located to avoid damage to, pruning or lopping of on-site landscaping or street trees.
- 215** Free-standing advertisements and advertising displays:
- (a) should be of a consistent design theme with other advertising on buildings within the site or complex; and
 - (b) should be of a scale and size compatible and complementary with development on the site.
- 216** Advertisements or advertising displays attached to buildings should:
- (a) be of appropriate colour, scale and proportion, and of an integrated design to be coordinated and complementary with the architectural form and design of the building the advertisement or advertising display is located upon; and
 - (b) not be located upon the roof or above the walls of a building.

217 Advertisements or advertising displays attached to a verandah or which project from a building wall should have a minimum clearance over a footway of 2.3 metres.

218 Advertisements or advertising displays should not be erected upon:

- (a) a vehicle carriageway, dividing strip of traffic island;
- (b) a vehicle adapted and exhibited primarily as an advertisement; or
- (c) residential land, unless erected to fulfill a statutory requirement, or as an advertising display associated with the business use of the land.

219 Portable, easel or A-frame advertisements should only be displayed when:

- (a) no other appropriate opportunity exists for an adequate coordinated and permanent advertisement or advertising display;
- (b) no obstruction or infringement of safety occurs to pedestrians or vehicle movement;
- (c) no unnecessary duplication or proliferation of information or advertisements or advertising displays results; and
- (d) the advertisement is:
 - (i) coordinated and in conformity with the theme and design of all other advertisements on the subject site or building;
 - (ii) restricted to only one per site, or one per major road frontage if located on a corner site;
 - (iii) less than 0.54 square metres in advertisement area per face and 0.9 metres in height; and
 - (iv) only displayed during the hours when the subject premises is open for trading.

220 Advertisements or advertising on buildings of nominated historic character, (State Heritage Items, Local Heritage Items and Contributory Items) or other buildings in Historic (Conservation) Zones, should be erected, displayed and positioned to:

- (a) not conceal architectural detailing;
- (b) be in scale with the building or architectural elements to which it is attached;
- (c) be removable without leaving any permanent damage to the building fabric;
- (d) not obscure the original frontage;
- (e) limit use of colours that relate to the building to which the sign is attached; and
- (f) not use reflective and opalescent paints or materials.

221 Advertisements or advertising displays should only identify premises or the nature of business, goods or services conducted on the land or building.

222 In residential and other non-business zones, advertisements or advertising displays should only be erected on land upon which a lawful land-use exists and in all respects should comply with:

- (a) [Table Ga/4](#); and
- (b) standards applicable to appropriate zones.

Public Utilities

OBJECTIVES

Objective 72: Economy in the provision of public services.

The majority of the urban metropolitan area can be serviced with essential urban services.

Routes of main transmission lines should be defined in advance of development to ensure there is adequate clearance between the two. Infrastructure and buildings should be sited carefully to ensure that the appearance of surrounding areas is not marred by unsightly buildings and equipment. Where conspicuous sites are essential, the site should be large enough to allow for landscape planting.

Provision of adequate stormwater drains are essential to the orderly development of much of the Adelaide Plains which has poorly defined watercourses.

PRINCIPLES OF DEVELOPMENT CONTROL

223 Buildings and structures associated with the supply and maintenance of public utilities should, wherever practicable, be sited unobtrusively and landscaped.

Regulated Trees

OBJECTIVES

Objective 73: The conservation of regulated trees that provide important aesthetic and/or environmental benefit.

Objective 74: Development in balance with preserving regulated trees that demonstrate one or more of the following attributes:

- (a) significantly contributes to the character or visual amenity of the locality;
- (b) indigenous to the locality;
- (c) a rare or endangered species;
- (d) an important habitat for native fauna.

PRINCIPLES OF DEVELOPMENT CONTROL

224 Development should have minimum adverse effects on regulated trees.

225 A regulated tree should not be removed or damaged other than where it can be demonstrated that one or more of the following apply:

- (a) the tree is diseased and its life expectancy is short;
- (b) the tree represents a material risk to public or private safety;
- (c) the tree is causing damage to a building;
- (d) development that is reasonable and expected would not otherwise be possible;
- (e) the work is required for the removal of dead wood, treatment of disease, or is in the general interests of the health of the tree.

226 Tree damaging activity other than removal should seek to maintain the health, aesthetic appearance and structural integrity of the tree.

Renewable Energy Facilities

OBJECTIVES

- Objective 75:** Development of renewable energy facilities that benefit the environment, the community and the state.
- Objective 76:** The development of renewable energy facilities, such as wind farms and ancillary development, in areas that provide opportunity to harvest natural resources for the efficient generation of electricity.
- Objective 77:** Location, siting, design and operation of renewable energy facilities to avoid or minimise adverse impacts on the natural environment and other land uses.

PRINCIPLES OF DEVELOPMENT CONTROL

227 Renewable energy facilities, including wind farms and ancillary development, should be:

- (a) located in areas that maximize efficient generation and supply of electricity; and
- (b) designed and sited so as not to impact on the safety of water or air transport and the operation of ports, airfields and designated landing strips.

Wind Farms and Ancillary Development

228 The visual impacts of wind farms and ancillary development (such as substations, maintenance sheds, access roads and wind monitoring masts) should be managed through:

- (a) wind turbine generators being:
 - (i) setback at least 1000 metres from non-associated (nonstakeholder) dwellings and tourist accommodation;
 - (ii) setback at least 2000 metres from defined and zoned township, settlement or urban areas (including deferred urban areas);
 - (iii) regularly spaced;
 - (iv) uniform in colour, size and shape and blade rotation direction;
 - (v) mounted on tubular towers (as opposed to lattice towers);
- (b) provision of vegetated buffers around substations, maintenance sheds and other ancillary structures.

229 Wind farms and ancillary development should avoid or minimise the following impacts on nearby property owners/occupiers, road users and wildlife:

- (a) shadowing, flickering, reflection or glint;
- (b) excessive noise;
- (c) interference with television and radio signals and geographic positioning systems;
- (d) interference with low altitude aircraft movements associated with agriculture;
- (e) modification of vegetation, soils and habitats striking of birds and bats.

230 Wind turbine generators should be setback from dwellings, tourist accommodation and frequently visited public places (such as viewing platforms) a distance that will ensure that failure does not present an unacceptable risk to safety.

Residential

OBJECTIVES

- Objective 78:** Neighbourhoods and residential development that are safe, pleasant, convenient, efficient and adequately serviced.
- Objective 79:** Housing that meets the needs and preferences of the community.
- Objective 80:** Encouragement of walking, cycling and public transport usage.
- Objective 81:** Public open space that provides diverse recreational opportunities.
- Objective 82:** Development that conserves energy and water.
- Objective 83:** Development that protects and enhances eco-systems and natural site features.
- Objective 84:** Medium density residential development that is in close proximity to centres; public transport; or public open space.
- Objective 85:** Use of contemporary design and development techniques to contain housing costs.

PRINCIPLES OF DEVELOPMENT CONTROL

Residential Land Division

231 Neighbourhoods should be developed and have a layout which:

- (a) integrates movement systems and land-use;
- (b) minimizes local vehicle trips, travel distances and speeds in residential streets;
- (c) facilitates public transport;
- (d) facilitates walking and cycling; and
- (e) promotes water reuse and local detention of stormwater;

232 Neighbourhood identity should be enhanced through:

- (a) a range of community, recreational and local employment opportunities at focal points;
- (b) site features, landmarks and views; and
- (c) building design and appearance which is compatible with the desired character or in its absence the local character with reference to:
 - (i) building mass and proportions;
 - (ii) materials, patterns, textures, colours and decoration elements;
 - (iii) ground floor height above natural ground level;
 - (iv) roof form and pitch;
 - (v) façade articulation and detailing and window proportion;
 - (vi) verandahs eaves and parapets; and
 - (vii) driveway crossovers, fence style and alignment.

233 Land division design should:

- (a) link with the surrounding urban environment;
- (b) facilitate shared use of public facilities with adjoining communities;
- (c) provide access to public open space (through provision of land or linkages to existing open space);
- (d) protect significant vegetation;
- (e) minimise impact on landform and drainage systems;
- (f) retain State, local heritage items and contributory items and the historic layout of pattern of development of historic areas;
- (g) keep flood prone land free from development;
- (h) ensure appropriate evacuation routes are available either in the form of public roads or public land that is located outside of flood prone land known to be subject to inundation by flood waters;
- (i) promote solar access for dwellings and private open space;
- (j) encourage personal safety;
- (k) minimize impact of vehicular traffic; and
- (l) promote water reuse and local detention of stormwater.

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

233.1 *A land division comprising more than 20 allotments should have at least 80 percent of allotments orientated so that their long axis conforms with figure 1:*

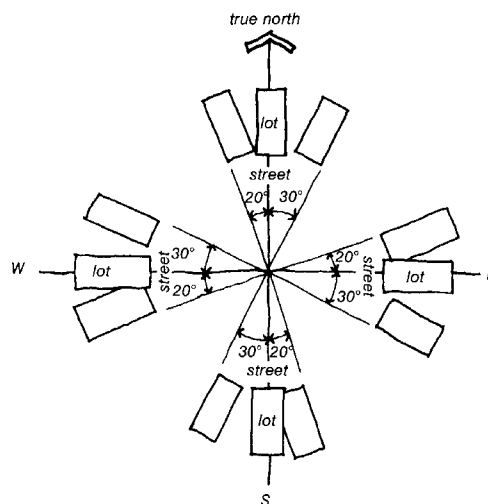


Figure 1 Orientated lots for solar access in temperate and hot-arid climates

234 Residential allotments should have an appropriate area and dimensions for:

- (a) siting and construction of a dwelling and ancillary outbuildings;
- (b) private outdoor space; and
- (c) convenient vehicle access and parking.

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

234.1 Allotments with an area of 450m² or greater are capable of containing a rectangle measuring 10 metres by 15 metres which is:

- (a) set back at least 5.5 metres from the street boundary; and
- (b) set back at least 6 metres from the rear boundary.

234.2 Allotments with an area of 300m² – 450m² inclusive capable of containing a rectangle measuring 9 metres by 15 metres which is:

- (a) set back at least 5.5 metres from the street boundary; and
- (b) set back at least 6 metres from the rear boundary.

234.3 Allotments with an area greater than 240m² but less than 300m² are square or rectilinear in shape with a minimum dimension of 7 metres.

234.4 Allotments with an area less than 240m² are square or rectilinear in shape with a minimum dimension of 5 metres.

234.5 Hammer-head allotments, where vehicular access is shared:

- (a) a minimum width for access driveway of 5 metres to allow for driveway; landscaping and fencing;
- (b) incorporating vehicular passing bays along the vehicular access driveways at 30 metre intervals;
- (c) nominating the shared vehicular driveway as a community lot in a Community Title Scheme.

235 Residential allotments of varying size to encourage housing diversity.

236 Allotments should have an orientation, size and dimensions that facilitates siting of dwellings to:

- (a) protect natural or cultural features;
- (b) minimise the need for earthworks and retaining walls; and
- (c) face streets and open spaces.

237 Residential allotments on roads with projected traffic volumes exceeding 6000vpd should have sufficient size and dimensions so that vehicles enter and exit the allotment in a forward direction.

Residential Movement Networks

238 Traffic's impact on residential environments should be minimised by integrated movement networks. In particular, no ribbon commercial development along arterial roads unless indicated by zone policies or structure plans.

239 Residential streets should be connected to the arterial road network and interconnected for permeability and to minimise trip lengths.

Residential Pedestrian and Cyclist Facilities

240 Residential street and path networks should be designed to encourage walking and cycling with connections to adjoining streets, paths, open spaces, schools, public transport stops and activity centres.

- 241** Cycling facilities and networks should be designed and provided in accordance with the relevant provisions of the Australian Standards and Australian Guide to Traffic Engineering Practice Part 14.
- 242** Land division design with road reserves should be of sufficient width to enable provision of convenient and safe footpaths and cycleways.

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

242.1 *The design of the road reserve includes footpaths where the following traffic volumes exist:*

- (a) *streets with traffic volumes over 300vpd and less than 2000vpd – footpath on one side; and*
- (b) *streets with traffic volumes of 2000vpd and greater – footpaths on both sides*

- 243** Land division design should include safe and convenient footpaths, cycleways and shared paths with a longitude gradient to cater for pedestrians and cyclists, including the aged, the very young, people with prams and those with wheelchairs, and people with a disability.

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

243.1 *The design of the road reserve includes a footpath 1.2 metres wide with a maximum gradient of 15 percent or, where located in a road reserve, no greater than the gradient of the adjacent carriageway*

243.2 *Where a designated or proposed cycle path coincides with a public road, the design of the road reserve includes a cycle path 2.5 metres in width with a maximum longitudinal gradient of no greater than 5 percent, or where located in the road reserve, no greater than that of the adjacent carriageway.*

243.3 *Inclusion of pram ramps.*

- 244** New residential areas should have a road network that allows for use by buses and convenient interconnection with adjoining areas (and bus routes).

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

244.1 *Roads in new residential areas designed to enable buses to safely and conveniently cross major roads when travelling between residential areas by:*

- (a) *utilising an existing or proposed traffic controlled intersection;*
- (b) *enabling a left turn into the road from one area followed by a right turn from the road into the adjoining residential area, ie T-junctions and not four-way intersections; or*
- (c) *use of roundabouts.*

244.2 *Roads intended to accommodate bus routes should be of an alignment and width that provides a reasonably direct route through the middle of residential areas and linking through to adjoining residential areas.*

Residential Street Layout and Design

- 245** Residential roads should have a width, alignment and allotment impact that:

- (a) provides for safe and convenient movement and parking for the projected volumes of vehicles and other users;
- (b) allows vehicles to enter or reverse from an allotment or site in a single movement allowing for a car parked on the opposite side of the street;

- (c) accommodates street tree planting, landscaping and street furniture;
- (d) accommodates the location, construction and maintenance of stormwater drainage and public utilities; and
- (e) provides unobstructed, safe and efficient vehicular access to individual lots and sites.

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

245.1 A road reserve accommodates minimum carriageway widths of:

- (a) 3.0 metres^{1,2} where the projected traffic volume does not exceed 100vpd³;
- (b) 3.5 metres^{1,2} where the projected traffic volume is between 100vpd³ and 300vpd³;
- (c) 5.0 metres^{1,2} where the projected traffic volume is between 300vpd³ and 2000vpd³;
- (d) 6.0 metres where the projected traffic volume is in excess of 2000vpd³.

¹ Unless varied to accommodate turning movements for access to allotments as follows:

Angle of Driveway to Carriageway (degrees)	Minimum Carriageway Width (m)
45	3.5
60	4.9
90	6.0

² Where on-street parking is to be provided indented bays should be provided. Passing bays are also required if length of street exceeds 50 metres.

³ For single dwelling allotments, apply a traffic generation rate of 10 vehicles per dwelling. For multi-unit dwelling allotments apply a traffic generation of 6 vehicles per dwelling.

246 Land division design should make provision for:

- (a) utility services and stormwater drainage within the street reserve;
- (b) junctions and intersections which allow for safe and convenient vehicle movements;
- (c) limited street length and/or distance between bends and slow points to restrict traffic speeds and volumes;
- (d) adequate sight distances for motorists at intersections, junctions, pedestrian and cyclist crossings, crossovers to allotments, and bus zones.

Residential On-Street Parking

247 Land division design should enable sufficient on-street visitor car parking, taking account of:

- (a) the size of proposed lots and sites and opportunities for on-site parking;
- (b) any low-traffic generating residential development;
- (c) the availability and frequency of public transport;
- (d) on street parking demand by non-residential uses such as schools, shops and other community facilities; and
- (e) proposed driveway positioning.

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

247.1 *One on-street car parking space provided for every two lots.*

248 Land division design should have on-street visitor car parking convenient to dwellings.

Residential Public Open Space

249 Public open space should be of a size, dimension and location that:

- (a) facilitates a range of active and passive recreational activities;
- (b) allows movement of pedestrians and cyclists;
- (c) incorporates existing significant vegetation, rocks, streams, wildlife habitat and other sites of natural or cultural value;
- (d) links habitats, wildlife corridors, public open spaces and existing recreation facilities; and
- (e) enables effective stormwater management.

Residential Stormwater Management

250 Land division design should:

- (a) facilitate major storm drainage system to safely convey major stormwater flows;
- (b) incorporate where practicable, provision for on-site stormwater detention, retention and use (including, where practicable, the collection and storing of water from roofs and communal car parks in appropriate devices);
- (c) provide for on-site infiltration, where practicable, having regard to:
 - (i) availability of unsealed areas or areas which are not built-up;
 - (ii) the capacity of soils to absorb water;
 - (iii) the capacity of building footings on and adjacent to the site to withstand the likely effects of retained water; and
 - (iv) potential adverse impacts on the level of groundwater;
- (d) allow convenient access to all components of the drainage system for maintenance purposes and not cause damage or nuisance flows on the site or onto adjoining properties.

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

250.1 *Land division design that facilitates the major storm drainage system having the capacity to safely convey stormwater flows for ARI = 100 years, assuming 50 percent minor system blockage.*

250.2 *The major storm drainage system design outflow is matched to the capacity of any existing downstream system.*

251 Land division design should integrate major storm drainage system with:

- (a) creeks and vegetation;
- (b) sports grounds and other types of low flood sensitive land uses;

- (c) detention and retention basins, where required;
- (d) improvements to residential amenity; and
- (e) the open space system and provides recreational opportunities.

252 Land division design should facilitate a minor storm drainage system which:

- (a) does not overload adjoining downstream systems; and
- (b) where practicable, provides for stormwater flows to be detained and retained close to its source.

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

252.1 Land division design that facilitates:

- (a) the minor storm drainage system having the capacity to convey stormwater flows for ARI = 2 years for suburban residential lots with neighbourhood densities² not greater than 20 dwellings per hectare, and ARI = 10 years for neighbourhood densities greater than 20 dwellings per hectare; and
- (b) the minor system design outflow being matched to the capacity of any existing downstream system.

Residential Water Quality Management

253 Land division design should facilitate a storm drainage system that:

- (a) maximises the interception, retention and removal of water-borne pollutants (including sediment, litter, nutrients, microbial contaminants and other potential toxic materials) prior to their discharge to receiving water, whether surface or underground;
- (b) ensures the continuation of or assisting in the establishment of healthy and diverse wetland environments; and
- (c) minimises the potential for sewage overflows to enter the system.

Residential Stormwater Use

254 Land division design should, where practicable, facilitates stormwater systems, providing the community with non-potable water to reduce mains water consumption, by collecting, applying physical treatment to, and storing and retrieving storm run-off for subsequent use.

Residential Building Appearance and Neighbourhood Character

255 Development should:

- (a) protect existing locality features, including significant trees, other substantial vegetation, natural creek lines and items or features of conservation or heritage value;
- (b) minimise the need for cut and fill;
- (c) provide sufficient space for the planting of trees to:
 - (i) complement an existing treed landscape character; or

² Neighbourhood density means the ratio of the number of dwellings to the area of the land (including associated neighbourhood or local facilities) they occupy. The area includes internal public streets, all areas of public open space, local or neighbourhood shops, primary and secondary schools, local community services, local employment areas, and half the width of adjoining arterial roads.

- (ii) enhance the landscape character of a neighbourhood deficient in trees; or
 - (iii) screen areas of storage, service and parking; and
- (d) where practicable and cost effective, protect trees³ from damage to their root systems.

256 Building appearance should be compatible with, and does not detract from, the existing or desired character of the locality in terms of built form elements such as:

- (a) building height;
- (b) building mass and proportion;
- (c) external materials, patterns, textures, colours and decorative elements;
- (d) ground floor height above natural ground level;
- (e) roof form and pitch;
- (f) facade articulation and detailing and window and door proportions;
- (g) verandahs, eaves and parapets;
- (h) driveway crossovers, fence style and alignment; and
- (i) historic character of a locality, area or individual heritage places listed in [Table Ga/2](#) or [Table Ga/5](#).

257 Dwellings adjacent to streets with doorways or window treatments should face the primary frontage⁴.

258 The visual bulk of buildings adjacent to street frontages and private open space should be reduced through use of colour, building materials, detailing and articulation

259 Buildings should be designed and sited to minimise the impact of building bulk on the private open space of adjacent sites by:

- (a) separating upper storey parts of buildings from neighbouring private open space;
- (b) articulating building elements;
- (c) use of colour; and
- (d) materials and detailing.

Residential Garages/Carports and Fences

260 Garages and carports should:

- (a) be visually unobtrusive in comparison with the main dwelling as seen from the street;
- (b) have a roof form and pitch, scale, building materials and detailing that complement those of the associated dwelling.

³ Trees requiring protection include "significant trees", as defined in the *Development Act 1993*, and other large, healthy trees that contribute to the character and amenity of the immediate locality, located on the subject or adjacent land and street trees.

⁴ Primary frontage means that frontage to a street that represents the main address of a dwelling which has a frontage to more than one street.

Design Technique (Design Techniques illustrate ONE WAY of satisfying the above principle)

260.1 Carports and garages facing the street are designed:

- (a) with a maximum width of garage or carport opening of 6 metres or 30 percent of the frontage width, whichever is less; and
- (b) where an enclosed double carport or garage is set back less than 8 metres from the street:
 - (i) two separate doors are provided with a distance of not less than 300 millimetres between them; or
 - (ii) double tilt-up doors are provided with moulded door panels having a maximum width of 5 metres.

261 Fences and walls abutting streets (excluding service lanes) should:

- (a) be complementary with the associated development and existing attractive fences and walls in the locality;
- (b) enable good visibility of buildings from and to the street to enhance safety and allow surveillance; and
- (c) be incorporated in site developments in localities of historic development.

Primary and Secondary Street Frontages in New Areas

262 Dwellings should be set back from allotment or site frontage to:

- (a) contribute to the existing or desired (by zone provisions) streetscape character;
- (b) provide adequate visual and acoustic privacy by separating habitable rooms from pedestrian and vehicle movement;
- (c) provide for the efficient use of the site.

Design Technique (Design Techniques illustrate ONE WAY of satisfying the above principle)

262.1 Set-backs from the street boundary are in accordance with the following table:

Estimated traffic volume	Minimum primary frontage set-back ² (m)	Minimum secondary frontage set-back ³ (m)
<100vpd ¹	3.0	0.0
100 – 300vpd ¹	3.0	1.0
300 – 2000vpd ¹	5.5	1.0
2000 – 6000vpd ¹	5.5	2.0

¹ For single dwellings, apply a traffic generation rate of 10vpd. For multi-unit dwellings, apply a generation rate of 6vpd per dwelling.

² The set-back is measured to the main face of the dwelling, excluding any verandah, porch, etc.

³ The set-back is measured to any part of the dwelling.

Primary Street Frontages in Established Areas

262.2 *Development (excluding any verandah, porch, etc) is set back:*

- (a) *the same distance as one or the other of the adjoining buildings, provided the difference between the set-backs of the two adjoining buildings is less than or equal to 2 metres; or*
- (b) *not less than the average of the set-backs of the adjoining buildings, if the difference between the set-backs of the adjoining buildings is greater than 2 metres.*

Secondary Street Frontages in Established Areas

262.3 *Set-backs (including any verandah, porch, etc) to secondary street frontages are in accordance with the following table*

<i>Height of wall (m)</i>	<i>Width of site to the primary street frontage (m)</i>	<i>Minimum set-back in established areas (m)</i>	<i>Minimum set-back in developing areas (m)</i>
≤ 3	≤ 10	2	2
	> 10	3	2
> 3	≤ 10	3	3
	> 10	4	3

263 Carports and garages should be set back to:

- (a) not diminish the attractiveness of the streetscape;
- (b) not dominate views of the dwelling from the street; and
- (c) provide for adequate on-site car parking.

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

263.1 *Set-backs of garages and carports from primary street frontages are:*

- (a) *not less than 0.5 metres behind the main face⁵ of the associated dwelling; or*
- (b) *in line with the main face of the associated dwelling if the dwelling incorporates minor elements such as projecting windows, verandahs, porticos, etc which provide articulation in the building as it presents to the street.*

263.2 *Where more than one on-site parking space is required and only one covered space is provided, garages and carports are set back not less than 5.5 metres from the street from which they are accessed.*

Dwellings not sited on side boundaries

264 Other than in the Residential (Gawler East) Zone, dwelling set-backs from side and rear boundaries should be progressively increased as height increases to:

- (a) minimise visual impact of buildings from adjoining properties;
- (b) minimise overshadowing of adjoining properties; and

⁵ "Main face" of a dwelling means the closest external wall of a habitable room to the street frontage or, in the case of a dwelling which has a frontage to more than one street, the primary frontage, but excludes elements projecting from the wall such as windows, verandahs, porticos etc

- (c) maintain adequate daylight to existing and future adjoining dwellings and private open space.

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

- 264.1 *Side walls with a maximum height of 3 metres are set back a minimum of 1 metre.*
- 264.2 *Side walls with a maximum height of 6 metres are set back a minimum of 2 metres.*
- 264.3 *Side walls greater in height than 6 metres are set back 2 metres plus the increase in wall height above 6 metres.*
- 264.4 *Single storey components of buildings are set back a minimum of 3 metres from the rear boundary (except where the rear boundary adjoins a service lane, in which case the distance may be reduced to 0 metres).*
- 264.5 *Two storey components of buildings are set-back a minimum of 8 metres from the rear boundary.*
- 264.6 *The following may encroach beyond the side and rear boundaries set-back provided that a minimum width of 1 metre is preserved down one side of the building:*
 - (a) *fascias, gutters, down-pipes and eaves up to 0.45 metres;*
 - (b) *masonry chimneys, flues and pipes;*
 - (c) *unroofed balconies, landings, steps or ramps not more than 1 metre in height.*

Dwellings Sited on Side Boundaries

265 Side boundary walls should be located and limited in length and height to:

- (a) minimise visual impact from adjoining properties;
- (b) minimise overshadowing of adjoining properties;
- (c) maintain adequate daylight to adjoining dwellings; and
- (d) reduce risk damage to mature vegetation on adjoining properties taking into account potential major damage to root systems.

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

- 265.1 *Side boundary walls are located immediately abutting the wall of an existing or simultaneously constructed building on the adjoining site and are constructed to the same or to a lesser length and height.*
- 265.2 *Side boundary walls do not exceed the length of an existing or simultaneously constructed adjacent boundary wall (refer to figure 2).*
- 265.3 *Side boundary walls:*
 - (a) *have a maximum height of 3 metres; and*
 - (b) *have a maximum length of 8 metres; and*
 - (c) *are constructed along one side of the allotment only and are within 14 metres of the front boundary;*

unless they abut a side service lane in which case the maximum height is 6 metres.

265.4 Side boundary walls have a maximum height of 3 metres and do not exceed 40 percent of the length of the boundary (or 60 percent where the boundary wall is less than 2 metres high when measured from ground level of the abutting site – refer to figure 3), unless they abut a side service lane in which case the maximum height is 6 metres.

265.5 Where there is an existing adjacent boundary wall set back greater than 1 metre from the front set-back standard established for the rest of the street, side boundary walls are located not more than 1 metre closer to the primary street frontage (otherwise the standard set-back requirements apply).

265.6 Single storey components of buildings are set back a minimum of 3 metres from the rear boundary (except where the rear boundary adjoins a service lane, in which case the distance may be reduced to 0 metres).

265.7 Two storey components of buildings are set back a minimum of 8 metres from the rear boundary.

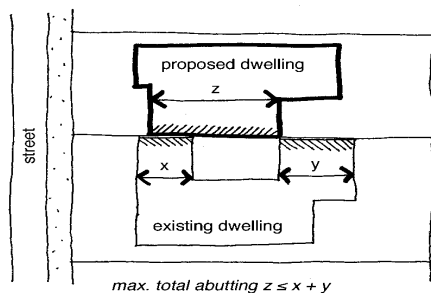


Figure 2 Length of building on boundary

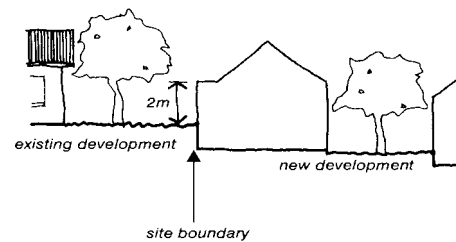


Figure 3 Boundary walls on sloping sites

Dwelling Set-backs to Shared Driveways/Car parks

266 External noise intrusion and vehicle headlight glare to bedrooms should be minimised by separating or shielding bedroom windows from shared driveways and car parking areas.

Design Technique (Design Techniques illustrate ONE WAY of satisfying the above principle)

266.1 Bedroom windows are set-back from shared driveways and parking areas a minimum of 2 metres and:

- (a) there is an intervening solid fence at least 1.8 metres above the level of the driveway and/or parking area; or
- (b) the window sill height is at least 1.5 metres above the level of the parking area and/or driveway.

Side Set-backs for Garages/Carports Under the Main Roof of the Dwelling

267 Garages and carports located on side boundaries should be limited in length and height to:

- (a) minimise visual impact of buildings from adjacent properties;
- (b) minimise overshadowing of adjoining properties; and
- (c) maintain adequate daylight to existing and future adjoining dwellings and private open space.

Residential Site Coverage

268 The site coverage of a site should provide sufficient space for:

- (a) pedestrian and vehicle access and vehicle parking;

- (b) storage and clothes drying;
- (c) private open space and landscaping; and
- (d) front, side and rear boundary set-backs appropriate to the locality.

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

268.1 *Other than in the Residential (Gawler East) Zone, the site coverage⁶ does not exceed the following:*

Site area (m ²)	Maximum site coverage (%)
≤ 300	55
301 – 450	50
451 – 800	45
> 800	40

Residential Building Height

269 Building height should have a compatible scale with adjacent development.

Residential Daylight and Sunlight

270 Adequate daylight should be available within habitable rooms.

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

270.1 *Habitable rooms have windows with a horizontal distance between any facing building, measured perpendicular to the face of the window, of 0.9 metres minimum which is clear to sky (ie 0.9 metres between fascias/gutters/eaves).*

271 Access of winter sunlight should be available to:

- (a) ground level private open space of existing adjacent dwellings;
- (b) upper level balconies which provide the primary open space area for any existing dwelling;
- (c) habitable room windows of adjacent dwellings.

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

271.1 *In relation to Principle 271(b), sunlight to at least 50 percent (or 35m² with minimum dimension 2.5 metres, whichever is the lesser area) of the ground level private open space of existing adjacent properties is not reduced to less than two hours between 9.00am and 3.00pm on 21 June. Where existing overshadowing by buildings and fences is greater than this, sunlight is not reduced to less than 80 percent of that formerly available.*

⁶ Site coverage represents the proportion of a site covered by the ground floor level of a building including the dwelling, garage and carport and free standing residential outbuildings, but excluding unroofed balconies, verandahs and pergolas.

Residential Energy Conservation and Comfort

272 Roof orientation and pitch should facilitate efficient use of solar collectors and photovoltaic cells.

Design Technique (Design Techniques illustrate ONE WAY of satisfying the above principle)

272.1 A roof incorporates an area of at least 10m² which:

- (a) faces between 30° and 20° east and west of north respectively;
- (b) has a pitch of greater than 18°

Residential Private Open Space

273 Private open space should be provided for each dwelling.

Design Technique (Design Techniques illustrate ONE WAY of satisfying the above principle)

273.1 Areas of private open space directly accessible from a living room and are in the form of:

- (a) ground level courtyard or other private open space screened from adjoining properties and public areas (eg, public roads, public open space) by a solid fence of at least 1.8 metres in height;
- (b) balconies, roof patios or other elevated outdoor areas.

274 Private open space areas should be of sufficient area, shape and gradient, and appropriately located to be functional for occupant needs.

Design Technique (Design Techniques illustrate ONE WAY of satisfying the above principle)

274.1 Areas of private open space about the dwelling and:

- (a) have a minimum dimension of 2.5 metres for ground level or roof top space; or
- (b) have a minimum dimension of 2.0 metres for upper level balconies or terraces.

274.2 Dwellings with ground level habitable rooms include private open space which conforms with the requirements of the following table:

Site Area of Dwelling	Minimum Area of Private Open Space
>250m ²	<ul style="list-style-type: none"> (a) 20 percent of site area; (b) balconies, roof patios, etc can comprise part of this area provided the area of each balcony, roof patio, etc is 10m² or greater; (c) one part of the space is directly accessible from a habitable room and has an area equal to or greater than 10 percent of the site area with a minimum dimension of 5 metres and a maximum gradient of 1 in 10.
<250m ²	<ul style="list-style-type: none"> (a) 35m²; (b) balconies, roof patios, etc. can comprise part of this area provided the area of each balcony, roof patio, etc is 8m² or greater; (c) one part of the space is directly accessible from a habitable room and has an area of 16m² with a minimum dimension of 4 metres and a maximum gradient of 1 in 10.

274.3 Dwellings with no ground level habitable rooms include private open space directly accessible from a habitable room which conforms with the requirement of the following table:

<i>Site Area of Dwelling (m²)</i>	<i>Private Open Space for Above Ground Dwellings - Minimum Area (m²)</i>	<i>Private Open Space for Above Ground Dwellings - Minimum Dimension (metres)</i>
>250	10	2
<250	8	2

275 Private open space should be located to:

- (a) take advantage of natural features of the site;
- (b) minimise overlooking from adjacent buildings; and
- (c) achieve, where possible, separation from bedroom windows on adjoining sites to minimise acoustic impacts.

276 An adequate proportion of the total private open space provided at ground level should be open to the sky for amenity, landscaping and reduction in stormwater runoff.

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

276.1 At least 70 percent of the total private open space provided is not covered by roof overhangs, verandahs or any other impermeable structures.

276.2 At least 40 percent of the total private open space provided is not paved, or otherwise sealed, and is designed to accommodate soft landscaping.

Residential Visual Privacy

277 Direct overlooking from upper level habitable room windows⁷ and external balconies, terraces and decks to habitable room windows and the useable private open spaces of other dwellings should be minimised by:

- (a) building layout;
- (b) location and design of windows and balconies;
- (c) screening devices;
- (d) landscaping; or
- (e) adequate separation.

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

277.1 Direct views¹ from upper level habitable room windows to the habitable room windows or useable private open space² of on adjacent dwelling are restricted (assuming a viewing height of up to 1.5 metres above floor level) by:

- (a) *permanently fixed translucent glazing in a part of the window below 1.5 metres above floor level; or*
- (b) *window sill heights of 1.5 metres above floor level; or*
- (c) *permanently fixed external screens, including wing walls, solid or translucent panels and planter boxes; or*

⁷ Upper level windows are those having a sill height greater than 2.5 metres above natural ground level. Upper level external balconies, terraces and decks have a floor level greater than 1.5 metres above natural ground level.

- (d) *permanently fixed external perforated panels or trellises which have a maximum of 25 percent openings*

¹ *For the purposes of 190.1, a direct view occurs if windows or outdoor areas overlooked are located within a horizontal distance of 15 metres from the vertical centre line of the overlooking window and beyond a 45° angle from the plane of the wall containing the overlooking window (refer to figure 4).*

² *Useable private open space comprises:*

- (a) *any private open space with a minimum dimension of 4 metres; or*
 (b) *where a dwelling has no area of private open space with a dimension of 4 metres, the whole of the private open space; and*
 (c) *a balcony of minimum dimension of 2 metres connected to a living area.*

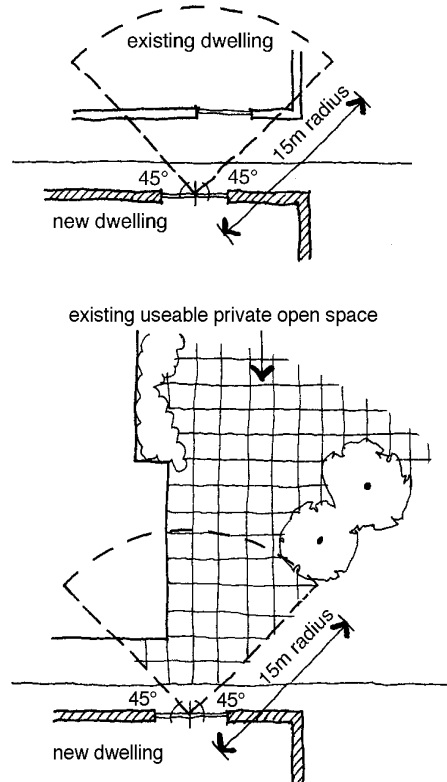


Figure 4

277.2 *Direct views³ from external upper level balconies, terraces and decks to the habitable room windows or a useable area of private open space of an adjacent dwelling are restricted by permanently fixed external screens, including:*

- (a) *wing walls;*
 (b) *solid or translucent panels; or*
 (c) *perforated panels or trellises which have a maximum of 25 percent openings.*

³ *For the purposes of 260.2, a direct view occurs if windows or outdoor areas overlooked are located within a horizontal distance of 15 metres from any point of the balcony or deck (refer to figure 5).*

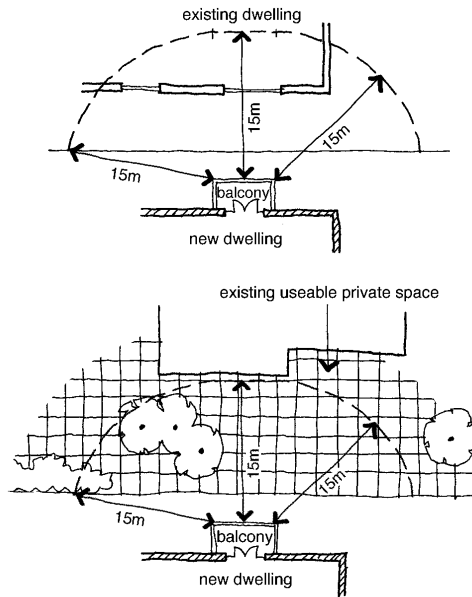


Figure 5

277.3 Windows and balconies within a residential flat building are designed to prevent (assuming a viewing height of up to 1.5 metres above floor level) overlooking of more than 50 percent of the useable private open space of a lower-level dwelling within the building (refer to figure 6).

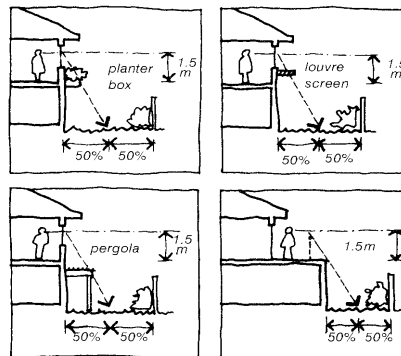


Figure 6

278 Permanently fixed external screening devices designed and coloured to blend with the predominant associated building materials

Residential Acoustic Privacy

279 Dwellings close to high-noise sources (eg, major roads, railway lines, tram lines, industry) should be designed with noise-sensitive rooms and private open spaces located away from noise sources, or protected by appropriate noise shielding techniques.

280 The number of dwellings within a residential flat building sharing a common entry should be minimised to limit noise generation in internal accessways.

Design Technique (Design Techniques illustrate ONE WAY of satisfying the above principle)

263.1 Common entries service a maximum of eight dwellings

Residential On-site Car parking and Access

281 On-site car parking should be provided, taking account of:

- (a) the number and size of the dwellings;
- (b) availability of employment and centre facilities within walking distance of the dwellings;
- (c) anticipated mobility characteristics of the occupants, such as aged persons;
- (d) availability of public transport; and
- (e) availability of on-street car parking;

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

281.1 *Detached, semi-detached and row dwellings are provided with two on-site parking spaces, one of which is covered (the second space can be tandem); or*

281.2 *Group dwellings and residential flat buildings provided in accordance with the following table:*

<i>Dwelling size or no. of bedrooms</i>	<i>Average spaces per dwelling</i>
<i>Small (1 bedroom or floor area of <75m²)</i>	<i>0.75</i>
<i>Medium (2 bedrooms or floor area of 75m² to 130m²)</i>	<i>1.00</i>
<i>Large (3+ bedrooms or floor area of > 130m²)</i>	<i>1.25</i>
<i>Add for visitors per dwelling</i>	<i>0.25</i>

282 Vehicular access should be located and designed to minimise the width of the crossover to:

- (a) accommodate street trees and other streetscape elements and infrastructure;
- (b) reduce loss of on-street parking spaces.

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

282.1 *Crossovers located at least 1 metre from side entry pits, the trunks of existing street trees and identified street tree planting locations.*

282.2 *Crossover width no greater than 3 metres (where double garages are provided, driveways should be flared within the site).*

Residential Design

283 Car parking areas for more than one dwelling should be located and dimensioned to:

- (a) efficiently, conveniently and safely serve users, including pedestrians, cyclists and motorists;
- (b) provide adequate space for vehicles to manoeuvre between the street and the parking area;
- (c) provide ingress and egress from streets having the environmental capacity to accommodate projected vehicle movements; and
- (d) reinforce or contribute to attractive streetscapes.

Design Technique (Design Techniques illustrate ONE WAY of satisfying the above principle)

- 283.1 (a) the dimensions of car parking spaces and access comply with figure 7; and
- (b) car parking spaces, accessways and driveways are located and dimensioned in accordance with Australian/New Zealand Standard 2890.1:2004 (1986).

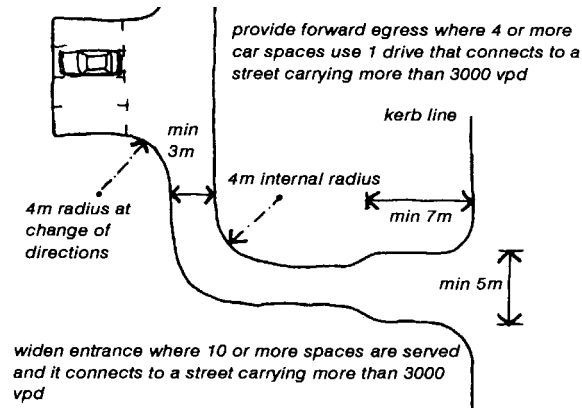


Figure 7

Residential On-Site Stormwater Drainage and Harvesting

284 Site drainage on larger sites (ie, over 600 square metres) should provide on-site infiltration where practicable, having regard to:

- (a) the availability of unbuilt upon or unsealed areas;
- (b) the ability of soils to absorb and drain water;
- (c) the potential impact on building foundations and footings on or adjacent to the site;
- (d) the potential adverse impacts on the level and quality of groundwater; and
- (e) the ability to safely direct surplus flows to a public street without causing nuisance to adjoining properties.

Residential Safety and Security

285 Dwellings, wherever reasonably practical, should be designed to overlook public and communal streets and public open space (particularly facilities commonly used in those areas) to allow casual surveillance.

Design Technique (Design Techniques illustrate ONE WAY of satisfying the above principle)

285.1 Dwellings adjacent to public or communal streets or public open space have at least one habitable room window facing such areas and a sill height of 1500 millimetres or less.

286 Buildings should be designed to minimise access between roofs, balconies and windows of adjoining dwellings.

287 Pedestrian access and car parking within communal areas should be clearly defined and appropriately lit.

Residential Site Facilities and Storage

288 Site facilities for group dwellings and residential flat buildings should include:

- (a) a common mail box structure located close to the major pedestrian entrance to the site;

- (b) garbage and recyclable material collection areas located for efficient collection; and
- (c) for dwellings which do not incorporate ground level private open space, external clothes drying areas;

which are readily accessible to each dwelling and complement the development and streetscape character.

289 Dwellings without ground level private open space or having less than 50 square metres of private open space should incorporate adequate areas for the storage of goods and chattels other than food and clothing either:

- (a) in the dwelling (but not including a habitable room);
- (b) in a garage, carport, or outbuilding; or

within an on-site communal facility.

Design Technique (Design Techniques illustrate ONE WAY of satisfying the above principle)

289.1 A storage area of not less than 8m³ provided for each dwelling

Housing on Major Traffic Routes

290 Residential development on sites abutting roads with existing or projected traffic volumes exceeding 3000vpd sited, should be designed and constructed so that:

- (a) intrusion of traffic noise into dwellings will not significantly reduce the amenity of occupants;
- (b) front fences and walls supplement the noise control of the building façade; and
- (c) the room layout within buildings reduces the impact of noise on the rooms which are most sensitive to noise (eg, bathrooms, hallways/stairways, storage rooms and garages should be located between the noise source and noise sensitive rooms).

Design Technique (Design Techniques illustrate ONE WAY of satisfying the above principle)

290.1 Buildings are constructed in accordance with Australian Standard 3671: Acoustics – Road Traffic Noise Intrusion, Building Siting and Construction and Australian Standard 2107: Acoustics – Recommended Design Sound Levels and Reverberation Times for Building Interiors

291 Buildings located on roads with existing or projected traffic volumes exceeding 6000vpd should be sited to avoid the need for vehicles to reverse on to the road (unless the dimensions of the site make this impractical).

292 Landscaping between the road and dwellings should be provided to screen and protect the dwellings from dust and visual impacts of the road

Dependent Accommodation

293 Dependent accommodation⁸ should only be developed on the site of an existing dwelling where:

- (a) the site is of adequate size and configuration;
- (b) the accommodation forms part of the same allotment as the associated dwelling;

⁸ Dependent accommodation is defined as accommodation for dependent relatives located on the same allotment as the main dwelling and connected to the same services as the main dwelling.

- (c) the accommodation has a small floor area relative to the main dwelling;
- (d) adequate outdoor space is provided;
- (e) adequate on-site car parking can be provided;
- (f) the building is designed to, and consists of colours and materials which will, complement the original dwelling; and
- (g) incorporates wheelchair access and bathing facilities in accordance with the current disability code.

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

293.1 *In relation to (a), dependent accommodation is located where the site of the existing dwelling is greater than 600m².*

293.2 *In relation to (b), the residence comprising dependent accommodation does not exceed 60m² in floor area.*

293.3 *In relation to (d), private open space of at least 100m² is available to be shared by both residences.*

293.4 *In relation to (e), an additional car parking space is provided on the site which can be used exclusively by the occupant(s) of the dependent accommodation.*

Housing for Older People and People with Disabilities

294 Residential accommodation for older people and people with disabilities should be:

- (a) located within easy walking distance to essential facilities such as convenience shops, health and community services and public transport;
- (b) located where on-site movement of residents is not unduly restricted by the slope of the land;
- (c) located and designed to promote interaction with other sections of the community, without compromising privacy;
- (d) of a scale and appearance that reflects the residential style and character of the locality; and
- (e) provided with public and private open space and landscaping to meet the needs of residents.

295 Access roads within residential development for older people and people with disabilities should:

- (a) not have steep gradients;
- (b) provide convenient access for emergency vehicles, visitors and residents;
- (c) provide space for car and community bus manoeuvring;
- (d) include kerb ramps at pedestrian crossing points; and
- (e) have level surface passenger loading areas.

296 Car parking associated with accommodation for older people and people with disabilities should:

- (a) be conveniently located on site within easy walking distance to resident units;

- (b) be adequate for residents, staff, service providers and visitors;
- (c) include private parking spaces for independent living units;
- (d) include separate and appropriately marked places for people with disabilities and spaces for small electrically powered vehicles;
- (e) include covered and secure parking for residents' vehicles;
- (f) have slip-resistant surfaces with low gradients;
- (g) allow ease of vehicle manoeuvrability;
- (h) be designed to allow the full opening of all vehicle doors; and
- (i) minimise the impact of car parking on adjacent residences due to visual intrusion, noise and emission of fumes.

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

296.1 *In relation to (b), car parking is provided at the following rates:*

- (a) *1 space for each dwelling;*
- (b) *1 space per 2 dwellings for staff, service providers and visitors; and*
- (c) *1 space for every 3 beds in any nursing home, rest home or hostel component.*

296.2 *In relation to (f), the gradient of the car parking space is not steeper than 1:20.*

Residential Outbuildings

297 Outbuildings should not:

- (a) detract from the visual appearance of the site as viewed from neighbouring properties due to their size and location relative to property boundaries;
- (b) result in unreasonable overshadowing or visual impact on habitable room windows of adjacent dwellings; and
- (c) result in a significant loss of private open space.

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

297.1 *In relation to (a) and (b), an outbuilding constructed:*

- (a) *within 3 metres a side or rear boundary has:*
 - (i) *a total floor area not exceeding 60m² for sites in excess of 600m², 40m² for sites 400m² – 600m² and 30m² for sites less than 400m²;*
 - (ii) *walls not exceeding 3 metres in height above natural ground level;*
 - (iii) *walls not exceeding 9 metres in length; and*
 - (iv) *a maximum height of 4.5 metres;*
- (b) *on a side or rear boundary has:*
 - (i) *a floor level not exceeding 300mm above natural ground level;*

- (ii) *a total floor area not exceeding 60m² for sites in excess of 600m², 40m² for sites 400m² – 600m² and 30m² for sites less than 400m²;*
- (iii) *boundary walls not exceeding a total of 8m in length on any common boundary, provided the total length of existing and proposed boundary walls does not exceed 30 percent of the total common boundary length;*
- (iv) *wall heights (measured above natural ground level at the common boundary) not exceeding 2.4 metres on the boundary elevation and 3 metres on any other elevation;*
- (v) *a maximum height of 4.5 metres; and*
- (vi) *a set-back of 6 metres from any existing structure on the site located on the same boundary.*

Residential On-site Landscaping

298 Landscaping should be provided to all areas of communal space, driveways and shared car parking areas.

299 Landscaping should be designed to:

- (a) enhance attractive site attributes;
- (b) incorporate existing vegetation where practicable;
- (c) complement built form;
- (d) complement existing attractive landscape character and take account of existing street tree planting and landscaping;
- (e) promote safety and opportunities for casual surveillance;
- (f) improve privacy and minimise overlooking;
- (g) contribute to and not compromise energy efficiency and amenity;
- (h) provide a buffer between vehicle access ways and habitable room windows of dwellings;
- (i) provide a buffer between buildings (both on-site and on neighbouring properties);
- (j) minimise maintenance and irrigation requirements.

Residential Finished Floor Level

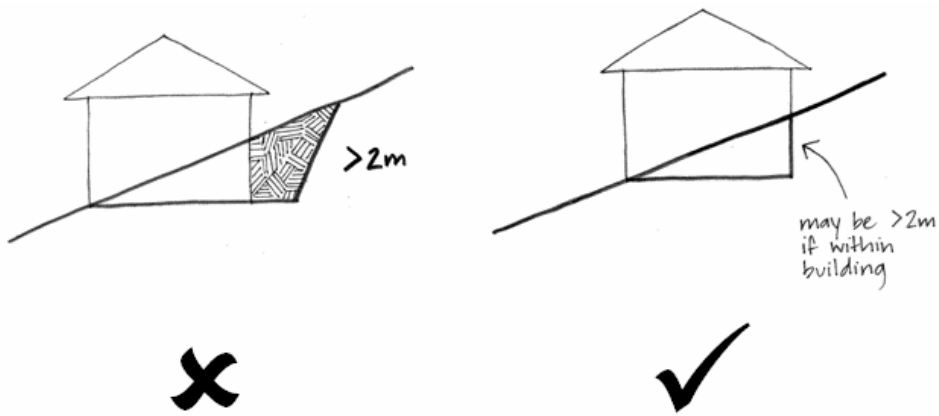
300 Buildings without rear of allotment drainage to a common drainage system, should be positioned 300 millimetres above the street water table.

301 Other than in the Residential (Gawler East) Zone, filling of land should be kept to a minimum and should not exceed 1.0 metre above existing ground level.

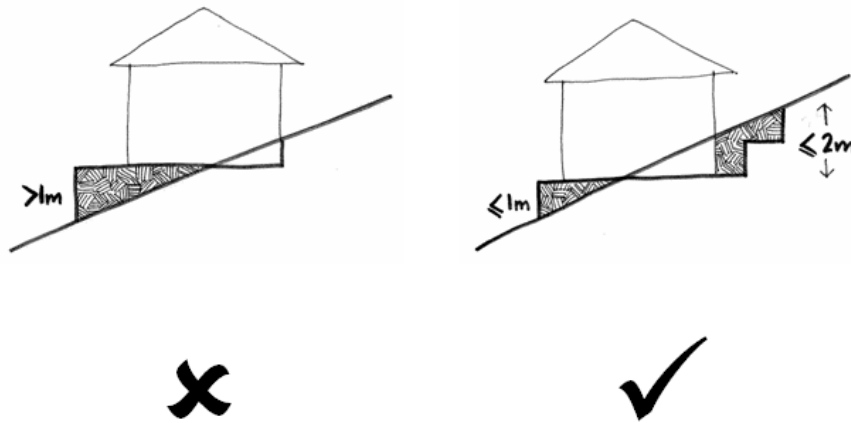
302 The depth of excavation should not exceed 2.0 metres below existing ground level, except for the portion of excavation which is within a building so that it is fully underground, or underground homes, pools, underground tanks, cellars, pipelines, and waste disposal and treatment systems.

Design Technique (Design Techniques illustrate ONE WAY of satisfying the above principle)

302.1



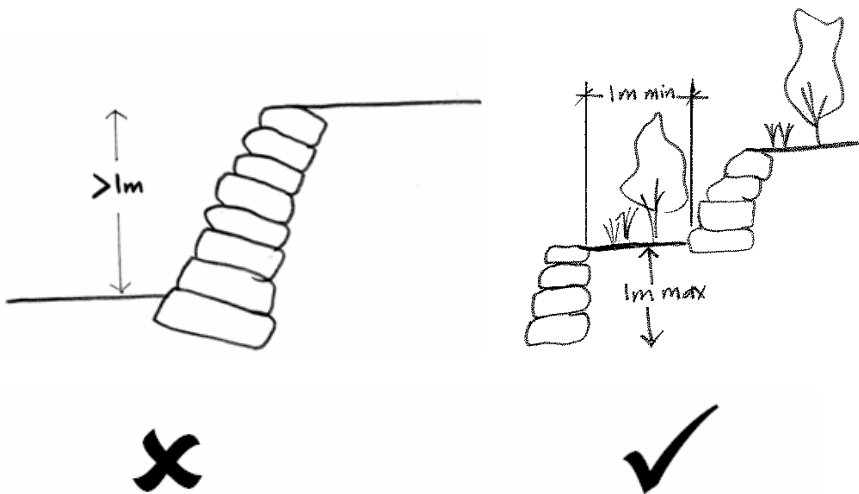
302.2



303 Retaining walls external to buildings should be designed and landscaped in such a way that they are a series of low walls of a maximum height of 1.0 metre and a minimum berm of 1.0 metre that is constructed of materials that complement the local existing or desired streetscape or landscape character.

Design Technique (an illustration of ONE WAY of satisfying the requirements of the Principle of Development Control)

303.1



Non-Residential Use

- 304** Residential development adjoining or near non-residential uses, either within a residential zone or area or adjoining or near a non-residential zone or use, eg railway line, should be located and designed for protection from air and noise pollution, traffic disturbance and other harmful effects on health or amenity to provide an acceptable level of residential amenity.
- 305** Non-residential development in residential zones should provide adequate protection for residents from air and noise pollution, traffic disturbances and other harmful effects on health or amenity.

Residential Parks and Caravan and Tourist Parks

PRINCIPLES OF DEVELOPMENT CONTROL

- 306** Residential parks which are principally designed for residents should be located in areas with access to employment, shops, schools, public transport and community and recreation facilities.
- 307** Residential parks and Caravan and Tourist parks should be designed to:
- (a) minimise potential conflicts between long-term residents and short-term tourists
 - (b) protect the privacy and amenity of occupants through landscaping and fencing
 - (c) minimise traffic speeds and provide a safe environment for pedestrians
 - (d) include centrally located recreation areas
 - (e) include extensive landscaping to enhance the appearance of the locality, with a landscape buffer around the perimeter of the site
 - (f) ensure that adequate amenity blocks (showers, toilets and laundry facilities) and service facilities (eg public telephones, kiosks and restaurants) are provided to serve the population to be accommodated by the facility.
- 308** Visitor car parking should be provided at the rate of:
- (a) one space per 10 sites to be used for accommodation for parks with less than 100 sites
 - (b) one space per 15 sites to be used for accommodation for parks with greater than 100 sites.
- 309** On-site visitor parking in Caravan and Tourist parks should:
- (a) be designed and located to be accessible to visitors at all times
 - (b) not dominate the internal site layout
 - (c) be clearly defined as visitor spaces and not specifically associated with any particular accommodation site.
- 310** Long-term occupation of Caravan and Tourist parks should not lead to the displacement of existing tourist accommodation, particularly in important tourist destinations, such as in coastal or riverside locations.
- 311** A minimum of 12.5 percent of a park should comprise communal open space, landscaped areas and recreation areas.
- 312** Landscaping should comprise locally indigenous species that are appropriate to the development and the subject land, and facilitate amenity and environmental sustainability.

Rural Development

OBJECTIVES

Objective 86: Retention of rural areas for agricultural, horticultural and grazing purposes and the maintenance of the natural character and rural beauty of such areas.

Land in rural areas should continue to be used primarily for agricultural purposes. Defining land for rural purposes will assist in a more intensive use of the land for food production, prevent land speculation and the uneconomic spread of the metropolitan area.

The kinds of development which could take place in a rural area, in addition to agriculture, would be large institutions, mineral workings and large individual projects where special arrangements are made for the provision of public services.

In some localities the trend has been for agricultural land to be divided into small holdings of two to four hectares and subsequently into residential allotments. Unless care is taken such a trend may conflict with the orderly development of the urban area. Difficulties can occur in trying to secure the economic provision of schools, public services and public transport services. It is also difficult to secure a satisfactory overall layout when individual areas of about two hectares are subsequently re-subdivided into housing allotments. The haphazard and premature division of land causes good agricultural land to go out of production for purely speculative purposes.

It is desirable that division of land into small holdings should be on land where water is available, and where there is little likelihood of conflict with the future expansion of the built-up area. Some areas, currently designated rural, may ultimately be developed for urban purposes. Accordingly, in such areas the aim is to have development compatible with housing, and to prevent development likely to prejudice orderly conversion of the land to urban use.

PRINCIPLES OF DEVELOPMENT CONTROL

- 313** Rural areas should be retained primarily for agricultural, horticultural and grazing purposes, but other kinds of development, such as large institutions, mining operations and large, specially-serviced projects may be appropriate.
- 314** Primary production should be carried out with regard to water conservation, the preservation of bushland remnants and landscape beauty.
- 315** Rural land should not be divided unless the proposed allotment would comprise, and be used as, a property for the business of primary production.
- 316** Intensive animal keeping, such as piggeries, meat chicken (broiler) farms, cattle feedlots and other buildings for the shelter of animals, which may generate noise or odour, should only be established where they can be sufficiently separated from flood prone land and existing or likely sensitive receptors.
- 317** Buildings in rural areas should be designed to be unobtrusive and in harmony with the natural environment.
- 318** The design, scale and siting of buildings in rural areas, and the materials used in construction, should accord with existing natural features and the character of surrounding development, and should not detract from the views from any public roadway.
- 319** Development in rural areas should not affect detrimentally the environmental quality of its locality in terms of:
- (a) degradation of significant land forms and ecosystems;
 - (b) excessive excavation and/or land filling;

- (c) soil erosion, silting of watercourses and the creation of unstable embankments or cuttings;
- (d) detrimental impact on hydrology, including drainage patterns;
- (e) generation of noise, smoke, dust, odours, light spill, traffic or any other nuisance, particularly in relation to nearby residential areas;
- (f) reduction of scenic views;
- (g) safety hazards; and
- (h) any other factor likely to affect detrimentally the environment, including the health and welfare of the community.

320 Development in rural areas should not take place unless there is available:

- (a) an SA Water Corporation reticulated water supply; or
- (b) an adequate alternative water supply source.

Note: Within the Northern Adelaide Plains Proclaimed Region (as declared under the Water Resources Act), an alternative water supply source does not include underground water unless the issue of adequate licences for withdrawal of underground waters is approved by the Minister of Water Resources in respect of the proposed development.

321 Development in rural areas should not cause pollution of surface or underground water.

Rural Living

OBJECTIVES

Objective 87: Low-density living areas in defined locations with a rural character.

Significant Trees

OBJECTIVES

Objective 88: Conservation of significant trees in Metropolitan Adelaide which provide important aesthetic and environmental benefit.

Trees are a highly valued part of the Metropolitan Adelaide environment and are important for a number of reasons including high aesthetic value, conservation of bio-diversity, provision of habitat for fauna, and conservation of original and remnant vegetation.

While indiscriminate and inappropriate significant tree removal should be generally prevented, the conservation of significant trees should occur in balance with achieving appropriate development.

PRINCIPLES OF DEVELOPMENT CONTROL

322 Where a significant tree:

- (a) makes an important contribution to the character or amenity of the local area; or
- (b) is indigenous to the local area and/or a species is listed under the National Parks and Wildlife Act 1972 as a rare or endangered native species; or
- (c) represents an important habitat for native fauna; or
- (d) is part of a wildlife corridor of a remnant area of native vegetation; or

- (e) is important to the maintenance of biodiversity in the local environment; or
- (f) forms a notable visual element to the landscape of the local area;

development should preserve these attributes.

323 Development should be undertaken with the minimum adverse affect on the health of a significant tree.

324 Significant trees should be preserved and tree-damaging activity should not be undertaken unless:

- (a) in the case of tree removal;
 - (1) (i) the tree is diseased and its life expectancy is short; or
 - (ii) the tree represents an unacceptable risk to public or private safety; or
 - (iii) the tree is within 20 metres of a residential, tourist accommodation or habitable building and is a bushfire hazard within a Bushfire Protection Area; or
 - (iv) the tree is shown to be causing or threatening to cause substantial damage to a substantial building or structure of value; and

all other reasonable remedial treatments and measures have been determined to be ineffective.

- (2) it is demonstrated that all reasonable alternative development options and design solutions have been considered to prevent substantial tree-damaging activity occurring.

- (b) in any other case;
 - (i) the work is required for the removal of dead wood, treatment of disease, or is in the general interests of the health of the tree; or
 - (ii) the work is required due to unacceptable risk to public or private safety; or
 - (iii) the tree is within 20 metres of a residential, tourist accommodation or habitable building and is a bushfire hazard within a Bushfire Protection Area; or
 - (iv) the tree is shown to be causing, or threatening to cause damage to a substantial building or structure of value; or
 - (v) the aesthetic appearance and structural integrity of the tree is maintained; or
 - (vi) it is demonstrated that all reasonable alternative development options and design solutions have been considered to prevent substantial tree-damaging activities occurring.

325 Development involving ground work activities such as excavation, filling, and sealing of surrounding surfaces (whether such work takes place on the site of a significant tree or otherwise) should only be undertaken where the aesthetic appearance, health and integrity of a significant tree, including its root system, will not be adversely affected.

326 Land should not be divided or developed where the division or development would be likely to result in a substantial tree-damaging activity occurring to a significant tree.

Sloping Land

OBJECTIVES

Objective 89: Development on sloping land designed to minimise environmental and visual impacts and protect soil stability and water quality.

PRINCIPLES OF DEVELOPMENT CONTROL

- 327** Development and associated driveways and access tracks should be sited and designed to integrate with the natural topography of the land and minimise the need for earthworks.
- 328** Development and associated driveways and access tracks, including related earthworks, should be sited, designed and undertaken in a manner that:
- (a) minimises their visual impact;
 - (b) reduces the bulk of the buildings and structures;
 - (c) minimises the extent of cut and/or fill;
 - (d) minimises the need for, and the height of, retaining walls;
 - (e) does not cause or contribute to instability of any embankment or cutting;
 - (f) avoids the silting of watercourses;
 - (g) protects development and its surrounds from erosion caused by water run-off.
- 329** Driveways and access tracks across sloping land should be accessible and have a safe, all-weather trafficable surface.
- 330** Development sites should not be at risk of landslip.
- 331** Development on steep land should include site drainage systems to minimise erosion and avoid adverse impacts on slope stability.
- 332** Steep sloping sites in unsewered areas should not be developed unless the physical characteristics of the allotments enable the proper siting and operation of an effluent drainage field suitable for the development intended.

Transportation and Access

OBJECTIVES

Objective 90: A comprehensive, integrated, affordable and efficient air, rail, sea, road, cycle and pedestrian transport system that will:

- (a) provide equitable access to a range of public, community and private transport services for all people;
- (b) ensure a high level of safety;
- (c) effectively support the economic development of the State;
- (d) have minimal negative environmental and social impacts;
- (e) maintain options for the introduction of suitable new transport technologies.

Objective 91: Development that:

- (a) provides safe and efficient movement for all motorised and non-motorised transport modes;
- (b) ensures access for vehicles including emergency services, public infrastructure maintenance and commercial vehicles;
- (c) provides off street parking;
- (d) is appropriately located so that it supports and makes best use of existing transport facilities and networks.

Objective 92: A road hierarchy that promotes safe and efficient transportation in an integrated manner throughout the State.

Objective 93: Provision of safe, pleasant, accessible, integrated and permeable pedestrian and cycling networks.

Objective 94: Safe and convenient freight movement throughout the State.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

333 Land uses arranged to support the efficient provision of sustainable transport networks and encourage their use.

Movement Systems

334 Development should be integrated with existing transport networks, particularly major rail and road corridors and designed to minimise its potential impact on the functional performance of the transport networks.

335 Transport corridors should be sited and designed so as to not unreasonably interfere with the health and amenity of adjacent sensitive land uses.

336 Roads should be sited and designed to blend with the landscape and be in sympathy with the terrain.

337 Land uses that generate large numbers of visitors such as shopping centres and areas, places of employment, schools, hospitals and medium to high density residential uses should be located so that they can be serviced by existing transport networks and encourage walking and cycling.

338 Development generating high levels of traffic, such as schools, shopping centres and other retail areas, entertainment and sporting facilities, should incorporate passenger pick-up and set down areas. The design of such areas should ensure interference to existing traffic is minimised and give priority to pedestrians, cyclists and public and community transport users.

339 The location and design of public and community transport set-down and pick-up points should maximise safety and minimise the isolation and vulnerability of users.

340 Development should provide safe and convenient access for all anticipated modes of transport including cycling, walking, public and community transport, and motor vehicles.

341 Development at intersections, pedestrian and cycle crossings, and crossovers to allotments should maintain or enhance sightlines for motorists, cyclists and pedestrians to ensure safety for all road users and pedestrians.

342 Driveway cross-overs affecting pedestrian footpaths should maintain the level of the footpath.

- 343** Development should discourage commercial and industrial vehicle movements through residential streets and adjacent other sensitive land uses such as schools.
- 344** Industrial/commercial vehicle movements should be separated from passenger vehicle car-parking areas.
- 345** Development should make sufficient provision on site for the loading, unloading and turning of all traffic likely to be generated.

Cycling and Walking

- 346** Development should ensure that a permeable street and path network is established that encourages walking and cycling through the provision of safe, convenient and attractive routes with connections to adjoining streets, paths, open spaces, schools, public and community transport stops and activity centres.
- 347** Development should provide access, and accommodate multiple route options, for cyclists by enhancing and integrating with:
- (a) open space networks, recreational trails, parks, reserves and recreation areas;
 - (b) Adelaide's Metropolitan Open Space System.
- 348** Cycling and pedestrian networks should be designed to be permeable and facilitate direct and efficient passage to neighbouring networks and facilities.
- 349** New developments should give priority to and not compromise existing designated bicycle routes.
- 350** Where development coincides with, intersects or divides a proposed bicycle route or corridor, development should incorporate through-access for cyclists.
- 351** Developments should encourage and facilitate cycling as a mode of transport by incorporating end-of journey facilities including:
- (a) showers, changing facilities, and secure lockers;
 - (b) signage indicating the location of bicycle facilities;
 - (c) secure bicycle parking facilities.
- 352** Pedestrian facilities and networks should be designed and provided in Accordance with relevant provisions of the *Australian Standards and Austroads Guide to Traffic Engineering Practice Part 13*.
- 353** Cycling facilities and networks should be designed and provided in accordance with the relevant provisions of the *Australian Standards and Austroads Guide to Traffic Engineering Practice Part 14*.

Access

- 354** Development should have direct access from an all weather public road.
- 355** Development should be provided with safe and convenient access which:
- (a) avoids unreasonable interference with the flow of traffic on adjoining roads;
 - (b) accommodates the type and volume of traffic likely to be generated by the development or land use and minimises induced traffic through over-provision;
 - (c) is sited and designed to minimise any adverse impacts on the occupants of and visitors to neighbouring properties.

- 356** Development should not restrict access to publicly owned land.
- 357** The number of vehicle access points onto arterial roads should be minimised, and where possible access points should be:
- (a) limited to local roads;
 - (b) shared between developments.
- 358** The number of access points for cyclists and pedestrians onto all adjoining roads should be maximised.
- 359** Development with access from roads with existing or projected traffic volumes exceeding 6000 vehicles per day should be sited to avoid the need for vehicles to reverse on to the road.
- 360** Driveways, access tracks and parking areas should be designed and constructed to:
- (a) follow the natural contours of the land;
 - (b) minimise excavation and/or fill;
 - (c) minimise the potential for erosion from run-off;
 - (d) avoid the removal of existing vegetation;
 - (e) be consistent with *Australian Standard AS 2890 Parking facilities*.

Access for People with Disabilities

- 361** Development should be sited and designed to provide convenient access for people with a disability.

Vehicle Parking

- 362** Development should be consistent with *Australian Standard AS 2890 Parking facilities*.
- 363** Vehicle parking areas should be sited and designed in a manner that will:
- (a) facilitate safe and convenient pedestrian linkages to the development and areas of significant activity or interest in the vicinity of the development;
 - (b) include safe pedestrian and bicycle linkages that complement the overall pedestrian and cycling network;
 - (c) not inhibit safe and convenient traffic circulation;
 - (d) result in minimal conflict between customer and service vehicles;
 - (e) avoid the necessity to use public roads when moving from one part of a parking area to another;
 - (f) minimise the number of vehicle access points to public roads;
 - (g) avoid the necessity for backing onto public roads;
 - (h) provide the opportunity for shared use of car parking and integration of car parking areas with adjoining development to reduce the total extent of vehicle parking areas and the requirement for access points;
 - (i) not dominate the character and appearance of a centre when viewed from public roads and spaces;

- (j) provide landscaping that will shade and enhance the appearance of the vehicle parking areas.

364 Vehicle parking areas should be designed to reduce opportunities for crime by:

- (a) maximising the potential for passive surveillance by ensuring they can be overlooked from nearby buildings and roads;
- (b) incorporating walls and landscaping that do not obscure vehicles or provide potential hiding places;
- (c) being appropriately lit;
- (d) having clearly visible walkways.

365 Where parking areas are not obviously visible or navigated, signs indicating the location and availability of vehicle parking spaces associated with businesses should be displayed at locations readily visible to customers.

366 Parking areas that are likely to be used during non daylight hours should provide floodlit entrance and exit points and site lighting directed and shaded in a manner that will not cause nuisance to adjacent properties or users of the car park.

367 Parking areas should be sealed or paved in order to minimise dust and mud nuisance.

368 To assist with stormwater detention and reduce heat loads in summer, vehicle parking areas should include soft (living) landscaping.

369 Parking areas should be line-marked to indicate parking bays, movement aisles and direction of traffic flow.

Infrastructure

370 A Traffic Impact Study should be undertaken to determine the potential impact of developments on the surrounding arterial road network. Works that are required as a direct result of providing safe and efficient access to any proposed development should be provided.

Telecommunications Facilities

OBJECTIVES

Objective 95: Telecommunications facilities provided to meet the needs of the community.

Objective 96: Telecommunications facilities located and designed to minimise visual impact on the amenity of the local environment.

Telecommunications facilities are an essential infrastructure required to meet the rapidly increasing community demand for communications technologies. To meet this demand there will be a need for new telecommunications facilities to be constructed.

The Commonwealth Telecommunications Act 1997 is pre-eminent in relation to telecommunications facilities. The Telecommunications (Low-impact Facilities) Determination 1997 identifies a range of facilities that are exempt from State planning legislation. The development of low impact facilities to achieve necessary coverage is encouraged in all circumstances where possible to minimise visual impacts on local environments.

Where required, the construction of new facilities is encouraged in preferred industrial and commercial and appropriate non-residential zones. Recognising that new facility development will be unavoidable in more sensitive areas in order to achieve coverage for users of communications technologies, facility design and location in such circumstances must ensure visual impacts on the amenity of local environments are minimised.

PRINCIPLES OF DEVELOPMENT CONTROL

371 Telecommunications facilities should:

- (a) be located and designed to meet the communication needs of the community;
- (b) utilise materials and finishes that minimise visual impact;
- (c) have antennae located as close as practical to the support structure;
- (d) primarily be located in industrial, commercial, business, office, centre, and rural zones;
- (e) incorporate landscaping to screen the development, in particular equipment shelters and huts; and
- (f) be designed and sited to minimise the visual impact on the character and amenity of the local environment, in particular visually prominent areas, main focal points or significant vistas.

372 Where technically feasible, co-location of telecommunications facilities should primarily occur in industrial, commercial, business, office, centre and rural zones.

373 Telecommunications facilities in areas of high visitation and community use should utilise, where possible, innovative design techniques, such as sculpture and art, where the facilities would contribute to the character of the area.

374 Telecommunications facilities should only be located in residential zones if sited and designed so as to minimise visual impact by:

- (a) utilising screening by existing buildings and vegetation;
- (b) where possible being incorporated into, and designed to suit the characteristics of an existing structure that may serve another purpose; and
- (c) taking into account existing size, scale, context and characteristics of existing structures, land forms and vegetation so as to complement the local environment.

375 Telecommunications facilities should not detrimentally affect the character or amenity of Historic Conservation Zones or Policy Areas, Local Heritage Items, State Heritage Items, or State Heritage Areas.

Transportation (Movement of People and Goods)

OBJECTIVES

Objective 97: A comprehensive, integrated, and efficient, public and private transport system which will:

- (a) provide access to adequate transport services for all people, at an acceptable cost;
- (b) effectively support the economic development of metropolitan Adelaide and the State;
- (c) ensure a high level of safety; and
- (d) maintain the options for the introduction of suitable new transport technologies.

Objective 98: A road hierarchy which forms the basis of development controls and serves as a guide to the investment of road funds in order to ensure a safe and efficient traffic flow and to promote the saving of fuel and time.

Objective 99: A network of roads, paths and tracks, which accommodates satisfactorily a variety of vehicular, cycle and pedestrian traffic.

Objective 100: A compatible arrangement between land uses and the transport system which will:

- (a) ensure minimal noise and air pollution;
- (b) protect amenity of existing and future land uses;
- (c) provide adequate access; and
- (d) ensure maximum safety.

Objective 101: Development adjoining arterial roads should:

- (a) not detract from the safe and efficient movement of traffic;
- (b) not have the effect of creating commercial ribbon development;
- (c) not generate large amounts of traffic outside designated shopping/centre zones;
- (d) provide for adequate off-street parking;
- (e) provide limited and safe points of access and egress; and
- (f) provide sufficient on-site manoeuvring area to enable all vehicles to enter and exit the site in a forward direction.

The main elements of the transport system are shown on [Map Ga/1 \(Overlay 1\) Enlargements A to G](#).

PRINCIPLES OF DEVELOPMENT CONTROL

376 Development and associated points of access and egress should not create conditions that cause interference with the free flow of traffic on adjoining roads.

377 Access to arterial roads should be designed to cater for the simultaneous two-way movements of the largest vehicles expected to enter and exit the site.

378 Access points to arterial roads should be limited in number, be in safe locations where there is adequate sight distance, and be restricted to side roads/service roads wherever possible.

379 A Traffic Impact Study should be undertaken to determine the potential impact of developments on the surrounding arterial road network. The 'User Pays' principle should apply for any works that are required as a direct result of providing safe and efficient access to any proposed development.

380 Development should include appropriate provision on the site to enable the parking, loading, unloading, turning and fuelling of vehicles.

381 Car parking areas should be located and designed to:

- (a) meet the foreseeable demands of employees and customers, in accordance with [Table Ga/1](#);
- (b) promote safe and convenient traffic circulation, and which minimises the use of public roads for movement between individual parking areas;
- (c) minimise interference with traffic on adjoining roads from vehicles by providing adequate space for vehicles to queue or manoeuvre into parking spaces clear of the road;

- (d) provide for safe and convenient pedestrian access to and from vehicles;
- (e) make adequate provision for convenient manoeuvring into and out of parking spaces; and
- (f) incorporate shade trees and other appropriate features and facilities to provide shelter for parked cars and assist to break up the open paved expanse of parking areas.

382 Car parks should, where possible, be integrated with adjoining car parks to share usage and reduce access points.

383 Roads should be designed in accordance with the following hierarchy of roads:

- (a) arterial and major collector roads bounding residential neighbourhoods to which no property access is generally provided;
- (b) local crossing roads serving to collect local residential traffic and as a convenient bus route, and on which centre, school and key neighbourhood facilities are located; and
- (c) local streets and access places serving principally a property access function.

384 Road design should:

- (a) economically provide for the anticipated traffic levels and assigned access function;
- (b) provide adequately for service and emergency vehicle access and turning;
- (c) provide an appropriate level of on-street parking.

385 The movement of pedestrians and cyclists should be safely and conveniently provided for within linear open space corridors connecting with residential streets, and within separate rights-of-way along major traffic thoroughfares.

386 Development should cater for the safety and convenience of the users of land by providing, where appropriate, pedestrian pathways, ramps and car parking spaces located and designed for ease of movement by aged or disabled persons, and persons accompanied by children.

387 Development should provide off-street vehicle parking and specifically marked disabled car parking places to meet anticipated demand in accordance with [Table Ga/1](#) Car Parking Requirements.

388 Development should be consistent with Australian Standard AS2890 Parking Facilities.

Waste

OBJECTIVES

Objective 102: Development that, in order of priority, avoids the production of waste, minimises the production of waste, reuses waste, recycles waste for reuse, treats waste and disposes of waste in an environmentally sound manner.

Objective 103: Development that includes the treatment and management of solid and liquid waste to prevent undesired impacts on the environment including, soil, plant and animal biodiversity, human health and the amenity of the locality.

PRINCIPLES OF DEVELOPMENT CONTROL

389 Development should be sited and designed to prevent or minimise the generation of waste (including wastewater) by applying the following waste management hierarchy in the order of priority as shown below:

- (a) avoiding the production of waste;
- (b) minimising waste production;
- (c) reusing waste;
- (d) recycling waste;
- (e) recovering part of the waste for re-use;
- (f) treating waste to reduce the potentially degrading impacts;
- (g) disposing of waste in an environmentally sound manner.

390 The storage, treatment and disposal of waste materials from any development should be achieved without risk to health or impairment of the environment.

391 Development should avoid the discharge or deposit of waste (including wastewater) onto land or into any waters (including processes such as seepage, infiltration or carriage by wind, rain, sea spray, stormwater or by the rising of the water table).

392 Untreated waste should not be discharged to the environment, and in particular to any water body.

393 Development should include appropriately sized area to facilitate the storage of receptacles that will enable the efficient recycling of waste.

394 Development that involves the production and/or collection of waste and/or recyclable material should include designated collection and storage area(s) that are:

- (a) screened and separated from adjoining areas;
- (b) located to avoid impacting on adjoining sensitive environments or land uses;
- (c) designed to ensure that wastes do not contaminate stormwater or enter the stormwater collection system;
- (d) located on an impervious sealed area graded to a collection point in order to minimise the movement of any solids or contamination of water;
- (e) protected from wind and stormwater and sealed to prevent leakage and minimise the emission of odours;
- (f) stored in such a manner that ensures that all waste is contained within the boundaries of the site until disposed of in an appropriate manner.

Wastewater

395 The disposal of wastewater to land should only occur where methods of wastewater reduction and reuse are unable to remove the need for its disposal, and where its application to the land is environmentally sustainable.

396 Wastewater lagoons should not be sited in any of the following areas:

- (a) within land subject to a 1-in-100 year average return interval flood event;
- (b) within 50 metres of the top of the bank of a watercourse;
- (c) where the base of the lagoon would be below any seasonal water table.

- 397** Artificial wetland system for the storage of treated wastewater, such as wastewater lagoons, should be:
- (a) sufficiently separated from adjoining sensitive uses to minimise potential adverse odour impacts;
 - (b) sited and designed to minimise potential public health risks arising from the breeding of mosquitoes.

Waste Treatment Systems

- 398** Development that produces any sewage or effluent should be connected to a waste treatment system that complies with (or can comply with) the relevant public and environmental health legislation applying to that type of system.
- 399** The methods for, and siting of, effluent and waste storage, treatment and disposal systems should minimise the potential for environmental harm and adverse impacts on:
- (a) the quality of surface and groundwater resources;
 - (b) public health;
 - (c) the amenity of a locality;
 - (d) sensitive land uses.
- 400** Waste treatment should only occur where the capacity of the treatment facility is sufficient to accommodate likely maximum daily demands including a contingency for unexpected high flows and breakdowns.
- 401** Any on-site wastewater treatment system/ re-use system or effluent drainage field should be located within the allotment of the development that it will service.
- 402** A dedicated on-site effluent disposal area should not include any areas to be used for, or could be reasonably foreseen to be used for, private outdoor open space, driveways, car parking or outbuildings.
- 403** The spreading or discharging of treated liquid or solid waste onto the ground should only occur where the disposal area consists of soil and vegetation that has the capacity to store and use the waste without contaminating soil or surface or ground water resources or damaging crops.
- 404** Stock slaughter works, poultry processors, saleyards, piggeries, cattle feedlots, milking sheds, milk processing works, fish processing works, wineries, distilleries, tanneries and fellmongeries, composting works and concrete batching works should have a wastewater management system that is designed so as not to discharge wastes generated by the premises:
- (a) into any waters;
 - (b) onto land in a place where it is reasonably likely to enter any waters by processes such as:
 - (i) seepage;
 - (ii) infiltration;
 - (iii) carriage by wind, rain, sea spray, or stormwater;
 - (iv) the rising of the watertable.

405 Winery waste management systems should be designed to ensure:

- (a) surface runoff does not occur from the wastewater irrigation area at any time;
- (b) wastewater is not irrigated onto waterlogged areas, land within 50 metres of a creek, or swamp or domestic or stock water bore, or land subject to flooding, steeply sloping land, or rocky or highly permeable soil overlaying an unconfined aquifer;
- (c) wastewater is not irrigated over an area which is within 50 metres of any residence on neighbouring land or 10 metres of any type of publicly owned land;
- (d) wastewater is released using low trajectory low pressure sprinklers, drip irrigators or agricultural pipe, and is not sprayed more than 1.5 metres into the air or in fine droplets if there is a potential for the spread of diseases from the wastewater;
- (e) stormwater run-off from areas which are contaminated with grape or grape products is drained to winery waste management systems during vintage periods;
- (f) stormwater from roofs and clean hard paved surfaces is diverted away from winery waste management systems and disposed of in an environmentally sound manner or used for productive purposes.

Waste Disposal (Landfill)

OBJECTIVES

Objective 104: Orderly and economic development of landfill facilities in appropriate locations.

Objective 105: Minimization of environmental impacts from the location, operation, closure and post management of landfill facilities.

Objective 106: Landfill facilities to be protected from incompatible development.

PRINCIPLES OF DEVELOPMENT CONTROL

406 Landfill facilities should be located, sited, designed and managed to minimise adverse impacts on surrounding areas due to surface water and ground water pollution, traffic, noise, fumes, odour, dust, vermin, weeds, litter, landfill gas and visual impact.

407 Landfill facilities should not be located in existing or future urban, township, living, residential, commercial, centre, office, business, industry or institutional zones or environment protection, conservation, landscape, open space or similar zones, and the Gawler Rivers Floodplain Area identified on [Figures FI/1 to FI/8](#).

408 Landfill facilities should be appropriately buffered to minimise adverse impacts on the surrounding area and land uses.

409 Land uses and activities which are compatible with a landfill facility may be located within any legislative separation distances established.

410 Land uses and activities which are not compatible with a landfill facility should not be located within any legislative separation distances.

411 Where appropriate, landfill facilities may include resource recovery facilities, provided there is a sufficient separation distance between potentially incompatible land uses and activities.

412 Landfill and associated facilities for the handling of waste should be located at least a distance of 500 metres from the boundaries of the landfill site. A lesser distance may be provided within the landfill site where the landfill facility is considered compatible with the surrounding area, land uses and activities so that an effective separation distance of 500 metres can be provided and maintained between the landfill facility and potentially incompatible land uses and activities.

- 413** The area of landfill operations on a site should:
- (a) be located a minimum distance of 100 metres from any creek, river, inlet, wetland or marine estuarine area and not within the area of 1-in-100-year flood event; and
 - (b) not be located on areas with ground slopes of greater than 10 percent except where the site incorporates a disused quarry; and
 - (c) not be located on land subject to land slipping; and
 - (d) not be located within three kilometres of an airport used by commercial aircraft. If located closer than three kilometres the landfill facility should incorporate bird control measures to minimise the risk of bird strikes to aircraft.
- 414** The landfill site should be landscaped to screen views of the landfill facilities and operational areas.
- 415** Sufficient area should be provided on a landfill site to ensure on-site containment of potential ground water contaminants and for the diversion of stormwater.
- 416** Where necessary an acoustic buffer should be provided between any excessive noise generating part of the landfill facility and any development on an adjacent allotment to mitigate potential noise pollution.
- 417** Litter control measures which minimise the incidence of windblown litter should be provided on the site of a landfill facility.
- 418** Leachate from landfill should be contained within the property boundary of the landfill facility site and should not contaminate surface water or ground water.
- 419** The interface between any engineered landfill liner and the natural soil should be:
- (a) greater than 15 metres from unconfined aquifers bearing ground water with a water quality of less than 3000 milligrams per litre of total dissolved salts; or
 - (b) greater than five metres from ground water with a water quality between 3000 milligrams per litre of total dissolved salts and 12 000 milligrams per litre of total dissolved salts; or
 - (c) greater than two metres from ground water with a water quality exceeding 12 000 milligrams per litre of total dissolved salts.
- 420** Surface water run-off from landfill should not cause unacceptable sediment loads in receiving waters.
- 421** Landfill activities that have a total storage capacity exceeding 230 000 cubic metres should sustainably utilize landfill gas emissions. For smaller landfill activities, if the sustainable utilization of the landfill gas emissions is not practically feasible then flaring is appropriate to avoid gases being vented directly to the air.
- 422** Chain wire mesh or pre-coated painted metal fencing to height of two metres should be erected on the perimeter of a landfill site to prevent access other than at appropriate site entries.
- 423** Plant, equipment or activities that could cause a potential hazard to the public within a landfill site should be enclosed by a security fence.
- 424** Landfill sites should not be located where access to the site using non-arterial roads in adjoining residential areas is required or likely.
- 425** Landfill facilities should be accessed by an appropriately constructed and maintained road.

- 426** Traffic circulation movements within the landfill facility should be adequate in dimension and construction to support all vehicles hauling waste and to enable forward direction entry to and exit from the site.
- 427** Suitable access for emergency vehicles to the landfill site should be provided.
- 428** A proposal to establish, extend or amend a landfill facility should include an appropriate Landfill Environment Management Plan that addresses the following:
- (a) the prevention of ground water and surface water contamination;
 - (b) litter control, dust control, noise control, the control of fumes and odours, and sanitary conditions generally;
 - (c) the monitoring or management of landfill gas;
 - (d) fire safety;
 - (e) security;
 - (f) maintenance of landscaping and the general condition of the site; and
 - (g) the post closure monitoring and maintenance of the facility to ensure compatibility with the surrounding landscape and to enable a suitable after use of the site. This should include a final contour plan, surface water diversion and drainage controls, the design of the final cover, the monitoring of groundwater, surface water, leachate and landfill gas.

Overlay - Noise and Air Emissions

Refer to [Maps Ga/1 \(Overlay 3 and 4\)](#) that relate to this overlay. The following policies apply to the 'designated area' marked on the relevant Overlay Map.

INTERPRETATION

Where the Objectives and/or Principles of Development Control that apply in relation to this overlay are in conflict with the relevant General Objectives and/or Principles of Development Control in the Development Plan, the overlay will prevail.

OBJECTIVES

Objective 107: Protect community health and amenity from adverse impacts of noise and air emissions.

PRINCIPLES OF DEVELOPMENT CONTROL

- 429** Noise and air quality sensitive development located adjacent to high noise and/or air pollution sources should:
- (a) shield sensitive uses and areas through one or more of the following measures:
 - (i) placing buildings containing less sensitive uses between the emission source and sensitive land uses and areas
 - (ii) within individual buildings, place rooms more sensitive to air quality and noise impacts (e.g. bedrooms) further away from the emission source
 - (iii) erecting noise attenuation barriers provided the requirements for safety, urban design and access can be met

- (b) use building design elements such as varying building heights, widths, articulation, setbacks and shapes to increase wind turbulence and the dispersion of air pollutants provided wind impacts on pedestrian amenity are acceptable
- (c) locate ground level private open space, communal open space and outdoor play areas within educational establishments (including childcare centres) away from the emission source.

BUSINESS ZONE

Introduction

In addition to the Council Wide policies, the Business Zone policies apply to the areas shown in [Maps Ga/8 and 9](#).

OBJECTIVES

- Objective 1:** A zone accommodating a range of specified business activities.
- Objective 2:** An attractively developed and landscaped zone.
- Objective 3:** Uses that have low external impacts and do not detract from the amenity of nearby residential areas and educational uses.
- Objective 4:** Orderly and economic division of land appropriate for business use.
- Objective 5:** Minimising the production of wastes (solid and water).

DESIRED CHARACTER

The purpose of the Zone is to accommodate a wide range of local services and goods retailing, bulky goods outlets, service trade premises, warehousing and service industry.

The Zone should develop as a well-contained precinct separated from adjoining uses by attractively landscaped areas. Landscaping will also perform the function of retaining and disposing of stormwater run-off and enhancing the arterial road frontage of the zone.

The site at the corner of Main North Road and Gordon Road at Kudla has high levels of exposure to passing traffic, and good access. This land provides the opportunity for a major motor showroom and associated ancillary facilities.

Building design and site development should be well-integrated within a uniform and specified framework of structural forms, building materials and colours, advertising styles and landscaping to ensure an attractive appearance along the main approach road to Gawler.

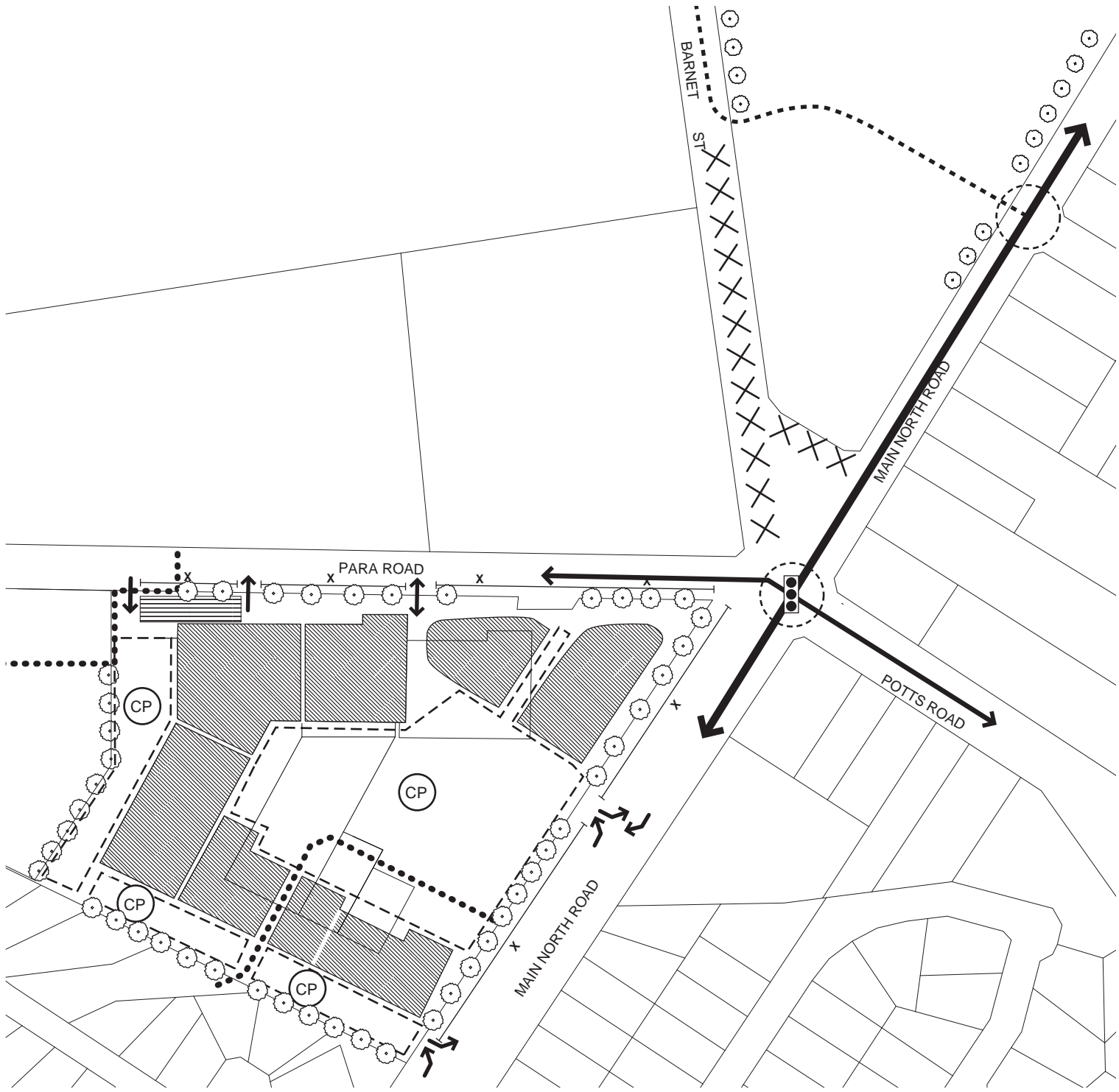
There should be a strong emphasis on minimizing impacts on residential uses where they adjoin the Zone. Whilst this will be achieved through a range of design techniques, a critical factor is the limited scale of the uses intended in the zone.










Where possible existing landscaping should be retained and incorporated into proposals.





The design and layout of community access roads; car parking; entry points onto public roads; landscaping; and direction of traffic movements within the Evanston area should be in accordance with the structure in [Fig B/1](#).


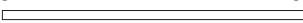
Visual interest and amenity appeal will be achieved through a consistent layout and design of buildings and structures, with a particular emphasis on the following elements:

- (a) an integrated vehicle movement system and shared car parking;
- (b) building set-back from car parks and internal roads;
- (c) consistency in the use of building materials, colours and decorative elements;
- (d) use of roof forms;
- (e) coordinated signage;
- (f) uniform landscape treatments;



-  Carparking Areas (inclusive of Landscaping)
-  Vehicular Access
-  No Direct Vehicular Access
-  Anticipated Development Areas
-  Drop Off Area / Carparking
-  Main Public Pedestrian Linkages
-  Landscaping
-  Signalised Intersection/
Pedestrian & Vehicular Movement
-  Road Closure

-  Road Realignment
-  Intersection treatment
-  Secondary Arterial Road
-  Local Road


Scale 1:2500
0  100m

GAWLER (CT) BUSINESS ZONE STRUCTURE/SITE LAYOUT PLAN Fig B/1

- (g) consistency in fencing design, set-back and location of outdoor storage areas; and
- (h) building or floor areas which are not excessive (and within the area shown on [Fig B/1](#) less than 1500 square metres, apart from one which should not exceed 3000 square metres)

The site on the corner of Angle Vale Road and Jack Cooper Drive at Evanston Gardens has previously been used as a CFS depot and may contain contaminants. Investigations and any appropriate consequent remediation will need to be undertaken should the site be proposed to be used for a more sensitive land use.

Land uses or activities that are suitable in the zone, subject to design considerations, including:

- Bulky Goods Outlet
- Motor Showroom
- Motor Repair Station
- Petrol Filling Station except on the site on the south-western corner of Main North Road and Gordon/Tiver Road intersection. On the site on the south-western corner of Main North Road and Gordon/Tiver Road intersection, vehicle refuelling is contemplated provided that it is ancillary to the main use of a Motor Showroom and vehicle delivery centre and limited to the refuelling of vehicles directly associated with the operation of the site. Refuelling/recharging facilities for hydrogen and electric vehicles are anticipated when ancillary to a Motor Showroom and delivery centre.
- Plant Nursery
- Restaurant (less than 300 square metres)
- Service Industry
- Service Trade Premises
- Veterinary Practice
- Warehousing

PRINCIPLES OF DEVELOPMENT CONTROL

Design Techniques are one way in which a relevant principle of development control may be satisfied.

There may be other appropriate *design techniques* that could be applied, depending upon local circumstances. It is not necessary for every proposal to satisfy every *design technique*.

- 1 Development should be consistent with the desired character and use of land for the Zone, and within the area shown on [Fig B/1](#).
- 2 A Petrol Filling Station is considered to be an envisaged land use within the Business Zone, except on the site on the south-western corner of Main North Road and Gordon/Tiver Road intersection. On the site on the south-western corner of Main North Road and Gordon/Tiver Road intersection, vehicle refuelling and recharging is contemplated provided that it is ancillary to the main use of a Motor Showroom and vehicle delivery centre and limited to the refuelling of vehicles directly associated with the operation of the site.
- 3 Development located within the zone to minimize impact on the locality. Reference should be given to the scale of the buildings; the intensity of use; the hours of operation; and generation of emissions.

Environmental Management

- 4 Activities which have the potential for off-site environmental impacts should be appropriately located in relation to more sensitive land uses.
- 5 Intensity and/or nature of a use should minimize land, water or air pollution.
- 6 Wastes and emissions should be managed effectively to minimize environmental impact.

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

- 6.1 *All waste liquids associated with any activity undertaken on the premises are discharged into a sewer (with the approval of the relevant State Government authority), or to a holding tank, which is protected from stormwater intrusion, prior to regular removal offsite to a licensed waste depot by a licensed waste carrier; and*
- 6.2 *Any vehicle, plant or equipment washing/cleaning activity undertaken within an area that is:*
- (a) *Roofed;*
 - (b) *Designed to contain all the wastewater likely to pollute stormwater;*
 - (c) *Bonded with an impervious material, such as concrete, to facilitate wastewater collection;*
 - (d) *Of sufficient size to prevent 'splash-out' or 'over-spray' of wash/wastewater from the washing/cleaning area; and*
 - (e) *Designed to drain to either a treatment device (such as sediment traps and a coalescing plate oil separator) with subsequent disposal to sewer or a Septic Tank Effluent Disposal Scheme (with the approval of the relevant State or Local Government authority) or to a holding tank prior to regular removal off-site to a licensed waste depot by a licensed waste carrier; and*
- 6.3 *Dedicated rubbish skips are provided for the collection of all solid waste material, including general office rubbish, which are:*
- (a) *Covered at all times to prevent the entry of stormwater or dispersal by wind;*
 - (b) *Sealed to prevent leakage;*
 - (c) *Located on 'hard standing' areas graded to a collection point in order to minimize the movement of any solids or contaminated water, and to prevent the entry of external stormwater; and*
 - (d) *Not used for oils, solvents, partly used paints, flammable materials, other liquids or sludges (oil-stained rags are acceptable) or toxic materials, including paint residues, particularly if they contain lead. These wastes are to be stored in covered bunded areas or, if not contained under cover, in airtight containers within bunded areas such that there is no airborne or waterborne migration from the designated storage areas.*
- 6.4 *Measures are incorporated into development such as recycling or reuse of materials onsite including wastewater to minimize the generation of waste.*
- 6.5 *Where wastewater cannot be recycled or reused, its appropriate treatment or disposal is ensured through an approved trade waste connection.*

Energy

- 7 Energy consumption for lighting, heating, cooling, processing and ventilation should be minimized by:
- (a) orientation and siting of buildings to:
 - (i) maximise opportunities for passive heating and cooling;
 - (ii) reduce energy use; and

- (iii) maximise access to natural light; and
- (b) incorporating landscaping into development which minimizes access to light in winter, and shade in summer.

Emissions

8 The nature, scale and intensity of the activity should:

- (a) minimize the levels of noise, vibration, dust, odours or other airborne emissions;
- (b) not detrimentally affect nearby residential, recreational or educational uses; and
- (c) incorporate management practices, technology or design techniques to minimize the impact on the environment from all emissions likely to be generated by development.

Advisory Note:

An emissions assessment may be required to demonstrate that the activity has minimal impact on the environment.

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

- 8.1 *Development does not exceed the maximum pollution levels identified by the Environmental Protection (Air Quality) Policy 1994.*
- 8.2 *Dust emissions are controlled through the incorporation of dust suppressors.*
- 8.3 *All roadways, entrances and main traffic areas are compacted and sealed.*

Parking, Access and Servicing

9 No development within the area shown on [Fig B/1](#) should occur within the zone until alterations to the roads and traffic signals depicted in [Fig B/1](#) have been completed and other traffic management infrastructure constructed including but not limited:

- (a) acceleration/turning lanes adjoining the land on Main North Road;
- (b) modification of the open swale drain across Main North Road;
- (c) roadside landscaping and shoulder works;

10 Access to individual sites should:

- (a) be coordinated and if the land is divided into individual allotments, shared facilities should be managed communally;
- (b) minimize traffic hazards;
- (c) ensure the safety of the public and the free flow of traffic in the locality;
- (d) minimize traffic hazards and queuing on roads; and
- (e) ensure vehicles exit in a forward direction.

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

- 10.1 *Access onto roads designed and located to enable all vehicle entry/exit from the site to occur in a forward direction.*

10.2 *The dimensions and arrangements of all parking, loading and manoeuvring areas are established in accordance with Australian Standards 2890.2-1989: Commercial vehicle facilities; and*

10.3 *A designated loading/unloading area is provided and separated from customer and employee car parking.*

- 11 Vehicle and bicycle parking should be provided commensurate with the intensity of the activity proposed and allowing safe and convenient access.
- 12 Car parking areas and associated manoeuvring areas should be efficient and coordinated and be sufficient to enable safe, convenient and efficient parking and traffic circulation.

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

12.1 *Car parking spaces and areas are designed, constructed and line marked in accordance with Australian/New Zealand Standard 2890.1:2004-1986: Off-street car parking.*

- 13 Surface treatment of all car parking and vehicular manoeuvring areas should be designed to withstand vehicular traffic in all weather conditions and to prevent soil erosion, dust and drainage problems.

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

13.1 *Car parking spaces and manoeuvring areas are sealed with an all-weather hard paved surface (such as bitumen, concrete or brick paving).*

- 14 Car parking areas should be suitably planted with canopy trees and screened with landscaping to reduce visual impact.
- 15 Outdoor display areas associated with motor showrooms should be attractively landscaped in a manner that provides amenity whilst enabling an appropriate level of exposure from the street.
- 16 Servicing, including garbage and recycling collection services, should not impact on adjoining developments and streets.

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

16.1 *All servicing, including garbage collection, is capable of being carried out wholly within the site, with collection points being positioned at convenient locations.*

16.2 *Provision is made for shared recycling in the zone.*

Stormwater Drainage

- 17 Site drainage should:
- (a) incorporate where practicable, provision for on-site stormwater detention, retention and use (including, where practicable, the collection and storing of water from roofs and communal car parks in appropriate devices);
 - (b) provide on-site infiltration, where practicable, having regard to:
 - (i) the availability of unsealed areas or areas which are not built up;
 - (ii) the capacity of soils to absorb water;
 - (iii) the capacity of building footings on and adjacent to the site to withstand the likely effects of retained water; and
 - (iv) potential adverse impacts on the level of groundwater;

- (c) allow convenient access to all components of the drainage system for maintenance purposes; and
- (d) not cause damage or nuisance flows on the site or onto adjoining properties.

- 18** The storm drainage system should maximize the interception, retention and removal of waterborne physical, chemical and biological pollutants prior to their discharge to surface or underground receiving waters. Disposal should be via on-site treatment or authorised disposal to a sewer or licensed waste depot by a licensed waste carrier.

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

- 18.1 *Rainfall run-off from the roof of any building is discharged directly to the street water table or an approved stormwater system and not mixed with rainfall run-off originating from surfaces such as car parks, outdoor storage areas and display areas; and*
- 18.2 *Rainfall from ground surfaces is directed to open swales (long depressions that guide surface runoff) via purpose designed and built traps for litter, sediment grease, oil and other substances capable of contaminating stormwater, and a high flow bypass is provided in conjunction with the traps, to enable water from extreme rainfall events to discharge direct to stormwater swales or to Council stormwater systems; and*
- 18.3 *Swales are designed in combination with roads and other paved surface areas to detain the whole run-off from a 1 in 10 year rainfall event flow, and a 1 in 100 year event storm, for a period of one hour.*

- 19** Stormwater discharge should be minimized through the adoption of reuse and recycling techniques.

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

- 19.1 *Stormwater discharge does not exceed the capacity of existing or planned external stormwater systems by:*
- (a) *the collection of roof water in above and/or below ground tanks or detention basins designed in accordance with the average rainfall for the area and roof size and located on site and where necessary overflows that connect into the stormwater system;*
 - (b) *the incorporation of on-site detention facilities and the utilisation and re-use of stormwater for:*
 - (i) *irrigation of landscaped areas; or*
 - (ii) *aquifer recharge; or*
 - (iii) *process purposes.*
- 19.2 *Roof water that is not contaminated with other water (such as air conditioning or cooling tower wastewater) and cannot be reused or recycled on site should be discharged (via underground pipes) directly into the Council stormwater system.*
- 19.3 *Where stormwater runoff is generated from car parks, driveways, hard paved areas, and rubbish bin/skip storage areas, and other contaminated stormwater, the runoff is treated by diversion into an approved stormwater treatment system/device that is capable of removing litter, sediment and oil products.*
- 19.4 *Treated stormwater is discharged:*
- (a) *into grassed swales, vegetation or garden strips adjacent to car parks and the property boundaries; or*

- (b) *into stone filled trenches either open to surface or underground, similar to a septic tank absorption field; or*
 - (c) *by a method approved by a hydrological specialist.*
- 19.5 *Where stormwater does not contain solid waste or runoff from potentially polluted surfaces such as driveways, car parks and paved areas, the runoff is directed onto a suitable vegetation filter strip before being redirected off the site.*
- 19.6 *Wastewater from air conditioning units, cooling towers and compressors is disposed of to sewer or collected by an authorised carrier and disposed of at an approved waste depot.*

Chemicals and Materials Storage

- 20** Chemicals and materials should be used, stored and managed on-site and disposed of in a manner which guards against the risk of explosion, spills, fire and exposure to air.

Design Technique (Design Techniques illustrate ONE WAY of satisfying the above principle)

- 20.1 *Chemicals and materials are stored separately in covered, bunded areas or, if not under cover, in airtight containers within bunded areas, so that there is no airborne or waterborne migration from the designated storage areas; and*
- 20.2 *Bunded areas are protected from external stormwater intrusion with drainage to sewer with the approval of the relevant State authority, or to a holding tank prior to regular removal off-site to a licensed waste depot by a licensed waste carrier; and*
- 20.3 *Loading/unloading areas are designed and constructed to prevent the entry of external stormwater, to contain any spilt materials and ‘washdown’ likely to pollute stormwater, with drainage to sewer (with the approval of the relevant State or Local Government authority) or to a holding tank prior to regular removal off-site to a licensed waste depot by a licensed waste carrier. Where loading/unloading activities involve quantities of hazardous chemicals, the operator has an emergency spill contingency plan.*
- 20.4 *The capacity of the bunded compound/area:*
- (a) *takes into account the volume displaced by containers stored within the bunded compound;*
 - (b) *is at least 120 percent of the volume of the largest vessel it contains; or*
 - (c) *where many small containers are stored, the bund must be capable of holding 25 percent of the total volume stored within the bund.*
- 20.5 *Bund walls and floors are of impervious construction to retain the materials being stored.*
- 20.6 *Bunded areas include a waste retaining sump, holding tank or pumping sump either within or draining the bunded compound/ area and of sufficient size to contain any spills and washdown material.*
- 20.7 *Bunded areas, compounds/ storage, waste retaining sumps, holding tanks or pumping sumps do not have external drains or valves. Spilled or leaked materials including clean up materials are:*
- (a) *contained within the bund, waste retaining sump, holding tank or pumping sump; and*
 - (b) *removed by a portable or manual pump, to be collected for reuse, or treated and disposed of by an authorised waste disposal contractor.*

20.8 Bunded compounds/areas/storages are:

- (a) designed and constructed to enable sufficient cleaning;
- (b) provided with appropriate signage to indicate the chemical types held within the storage area; and
- (c) protected and secured from public access.

Hours of Operation and Noise

21 Development should not adversely affect the level of amenity in adjoining non-business areas by virtue of hours of operation and noise emitted.

Design Technique (Design Techniques illustrate ONE WAY of satisfying the above principle)

21.1 Where the business operation is to be located within 60 metres of a residential area:

- (a) delivery trucks only arrive at the premises after 7.00 am and depart before 10.00 pm on any weekday or Saturday, and after 8.00 am and before 10.00 pm on a Sunday or Public Holiday; and
- (b) work shift breaks are after 7am and before 10.00 pm on any weekday or Saturday, and after 8.00 am and before 10.00 pm on a Sunday or Public Holiday; and

21.2 The noise level (leq) emanating from premises measured and adjusted at the residential boundary is less than 52Db(a) leq between the hours of 7.00 am and 10.00 pm and less than 42Db(a) between 10.00 pm and 7.00 am⁹.

21.3 An appropriately designed acoustic barrier in the form of a solid masonry wall or a similar structure (that does not unreasonably overshadow adjoining properties), is provided between the noise source and adjoining residential areas.

Outdoor Lighting

22 Outdoor lighting designed and installed so that it does not intrude on other properties or roads in the locality.

Design Technique (Design Techniques illustrate ONE WAY of satisfying the above principle)

- 22.1 Outside lighting is directed down and towards the site to prevent spillage onto surrounding properties or thoroughfares; and
- 22.2 Outdoor lighting is provided in accordance with 'Interim Australian Standard 4282 – 1995: Control of the obtrusive effects of outdoor lighting'.

Amenity and Urban Design

23 Building layout and appearance should be compatible with the desired character of the zone.

Building Size

24 In the area depicted on [Fig B/1](#) a maximum of one building of 3000 square metres is appropriate, however all other individual buildings or structures in the area shown on [Fig B/1](#) should not exceed 1500 square metres in area.

⁹ Leq measurement includes the addition of 5Db(a) penalties where tonal or modulation (amplitude or frequency) or impulse noise is being assessed. All measurements are made at any residential boundary outside the business zone. The onus of proof that engineered noise containment preventing noise impacts escaping from the development site will achieve this level of performance to rest with the proponent or applicant.

- 25** In all other areas of the zone except the site on the south-western corner of the intersection of Main North Road/Gordon/Tiver Road, buildings should not exceed 1500 square metres.

Set-backs

- 26** For development in the area depicted on [Fig B/1](#), building set-backs should generally conform with the Development Areas (buildings, outdoor display, loading areas) in [Fig B/1](#) to achieve a defined and where appropriate, articulated building line and good streetscape quality.
- 27** Other buildings should have setbacks appropriate to the site and locality.

Internal Facades Facing Internal/Core Car Park

- 28** All buildings are to incorporate design features to provide a human scale, weather protection and entry definition.
- 29** In the area depicted on [Fig B/1](#) specifically, buildings should be designed to provide:

- (a) building facades broken into discreet frontages of varying length. The maximum 'unbroken' façade length is 30 metres;
- (b) entries and markers approximately 3.0 metres wide by 6.0 metres long by 5.0 metres high to punctuate the verandahs randomly at approximately 12.0 metre centres.

Rear (Delivery) Façade (South and East Elevations)

- 30** Buildings in the area depicted on [Fig B/1](#) are to incorporate:
- (a) projecting canopies to protect delivery points of 4.0 metres above the roadway, 1.0 metre high, 4.0 metre wide and projecting by 3.0 metres incorporating a roof element at a 18 degree pitch, of a contrasting non-reflective steel coloured finish;
 - (b) wall panels should be treated with a surface treatment pattern which creates a play of shadow and texture and a lower scale, in panels sized approximately 4.0 metres wide by 1.5 metres high in an alternating, subtle checkerboard pattern;
 - (c) parapets should alternate in height from between 5.5 metres and 6.5 metres high. This variety should be somewhat random to avoid a 'castellated' look and should match the wall panel width of around 4.0 metres.

Para Road Frontage

- 31** Buildings fronting Para Road should incorporate:
- (a) façades with verandahs approximately 3.0 metres which are punctuated by markers to identify building functions, ie doorways, windows etc;
 - (b) a window pattern that relieves a long continuous façade.

Building Materials, Colours and Decorative Elements

- 32** Coloured masonry or galvanized iron should be used using soft or muted colours for all buildings in the area depicted on [Fig B/1](#). Decorative elements should be confined to louvred roof vents and wall vents.

Design Technique (Design Techniques illustrate ONE WAY of satisfying the above principle)

32.1 *Appropriate wall colours: Birch; Armour Grey; Off White; Gull Grey;*

32.2 *Appropriate trim colours: Mountain Blue; Torrens Blue; Slate Grey; Atoll Green; Heritage Red.*

Wall Heights and Roof Pitch

- 33** For development in the area depicted on [Fig B/1](#), a maximum wall height of 6.0 metres, with an expressed mansard roof element commencing at 5.0 metres and rising to 6.5 metres pitched at 45 degrees.
- 34** In all other areas, the wall height and roof pitch should be appropriate to the locality and in most cases the total building height should be no greater than 11.0 metres.

Fenestration (Window) Pattern

- 35** The primary building facades should be approximately 50 percent glazed, in a modulated or informal pattern, based on 2.0 metre wide entrance door or display window, alternating with a solid 2.0 metre wall panel.

Landscaping

- 36** Landscaping should be provided to facilitate amenity, environmental sustainability and buffer the impact of business uses on adjacent areas. Where possible existing mature landscaping should be retained and incorporated into new developments.

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

- 36.1 *Landscaping for sustainability to include minimal lawn areas; moisture monitors; water timers; garden mulch; collection and re-use of rainwater.*

Outdoor Storage and Service Areas

- 37** Outdoor storage and service areas should be located, designed and managed to be screened from public areas (including car parking areas) and avoid impacts on the surrounding locality.

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

- 37.1 *Outdoor storage areas and services and service structures including fire services, pipes, flues, cooling or heating plant or appliances are screened from public view by landscaping, or a fence or enclosure in pre-coloured sheet metal or of materials matching those of the main buildings or by an appropriate combination of solid fencing and landscaping. Services on roofs are designed and integrated into the structure and design of the building.*
- 37.2 *Outdoor storage and services areas are designed and managed to ensure that all litter is contained within those areas.*
- 37.3 *Storage areas for outdoor waste and refuse bins are paved and drained to a collection system to prevent polluted wastewater from bin washdown entering the stormwater system.*
- 37.4 *Outdoor and service areas located behind buildings and without or limited exposure to public areas.*

Safety and Security

- 38** Provision should be made to ensure personal safety and security through:
- (a) the avoidance of areas which have the potential to become traps for pedestrians; and
 - (b) ensuring that safe pedestrian access is provided adjacent to intensive activity nodes.

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)38.1 *Facilitation of visibility and legibility of areas through the:*

- (a) *provision of lighting in accordance with AS 1158.1-1997: SAA Public Lighting Code and AS 4242 – 1997: Control of the Obtrusive Effects of Outdoor Lighting;*
- (b) *maintenance of clear lines of sight.*

38.2 *Incorporation of passive surveillance by:*

- (c) *designing buildings to overlook public and open spaces, pathways, car parking and bicycle parking areas; and*
- (d) *incorporating visually permeable landscaping and fencing to minimize concealment opportunities.*

38.3 *Site design facilitates opportunities for escape, communication or help when in danger through:*

- (a) *legible design; and*
- (b) *comprehensive and legible signage.*

- 39** Development should ensure the security of property, and in areas where there is potential for graffiti vandalism, the use of materials from which graffiti can be easily removed.

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)39.1 *Development:*

- (a) *provides clear definition of ownership and legitimate use of private, public and community space; and*
- (b) *minimize access between roofs, balconies and windows of adjoining buildings.*

Lighting**40** Lighting should be provided to:

- (a) *facilitate the security, safety and amenity of the area and avoid detrimental effects on adjacent areas;*
- (b) *not impair the amenity of the locality of any residential zone as a result of light spill or reflection;*
- (c) *be vandal resistant to minimize maintenance*

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)40.1 *The establishment of lighting:*

- (a) *is in accordance with AS 1158.1 – 1997: SAA Public Lighting Code; and*
- (b) *avoids distraction to vehicle drivers on internal or external roads in accordance with AS 4282 – 1997: Control of the Obtrusive effects of Outdoor Lighting;*

Fencing

- 41** Fencing should be of a consistent type in the area depicted on [Fig B/1](#), and in all cases be designed to not detrimentally affect the amenity or streetscape of the area or dominate the streetscape of roads, or set-back areas from street alignments.

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

41.1 Security fencing adjacent to public roads is set back:

in line with the building façade; or

behind the building line;

41.2 Wire mesh fencing is avoided.

Outdoor Advertising

- 42** The scale, type, design, location, materials, style and illumination of any sign should conform with the performance standards in [Table Ga/4](#) and:

- (a) contribute positively to the appearance of development and be coordinated in design, colour and graphics complementing adjacent buildings, structures and other well designed signs;
- (b) be compatible with the design and character of the buildings and land to which it is related and the streetscape and amenity of its locality; and
- (c) not endanger public safety or detrimentally affect the amenity of adjacent premises due to their location, position, construction or design.

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

42.1 Signs:

- (a) *are designed so that structural supports are concealed from public view or are of minimal visual impact;*
- (b) *are coordinated and complementary on buildings or sites occupied by a number of tenants or occupiers and be limited to a single structure designed in a coordinated manner with provision for each tenant or occupier;*
- (c) *do not dominate or obscure other signs or result in visual clutter;*
- (d) *are constructed of durable materials and maintained in good condition;*
- (e) *do not emit excessive glare or reflection from internal or external illumination;*
- (f) *do not obscure a driver's view of vehicles, pedestrians or potentially hazardous road features;*
- (g) *are not confused with or reduce the effectiveness of traffic control devices;*
- (h) *are limited in message to the firm's name and insignia;*
- (i) *restricted to: Pylon or Freestanding; Flat Wall, desirably painted directly onto the fabric of the building; Verandah fascia; Under-verandah; Shop-front;*
- (j) *do not include: Bunting; Mobile; Projecting above verandah; Roof; and Sandwich Board;*

- (k) *in respect of Flat Wall signs have maximum dimensions of: length 8.0 metres; height 0.65 metres and text size of 0.4 metres.*
- (l) *limited to a zone of a maximum of 1.5 metres high on the building façade from 3.5 metres to 5.0 metres above the footpath.*

Land Division

- 43** Any land division should result in allotments and roadways where the dimensions are of suitable size and shape to accommodate the intended use of the land and an integrated form of development.
- 44** The design of roadways should ensure safe and convenient road access in accordance with the intended use of the land and the layout shown in [Fig B/1](#).

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

- 44.1 *Roadways have a minimum carriageway of 13.5 metres and 3.75 metres of verge on either side of the carriage way.*
- 44.2 *Verges are structured to enable a tree planting area clearance of 1.6 metres from hard fixtures and common service trenches.*
- 44.3 *Road paving is designed to accommodate the type and volume of anticipated traffic and in accordance with best engineering practice, and in accordance with Austroads – Pavement Design: A Guide to the Structural Design of Road Pavements, 1992.*
- 45** The layout of allotments and roadways should provide for adequate provision for drainage through:
 - (a) the development of overland flow paths, which take into consideration and where possible make use of existing flow paths; and
 - (b) the provision of detention or retention basins that accommodate excess flows.
- 46** Useable and appropriately located land should be provided for:
 - (a) vegetated buffers in road reserves;
 - (b) easements required for the provision of or existing public utilities and services; and
 - (c) access to easements for maintenance.

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

- 46.1 *Easements of a minimum width of 5 metres are provided to accommodate existing or required infrastructure and its future maintenance.*
- 47** Services and infrastructure should be:
 - (a) provided to facilitate current and future development; and
 - (b) designed, sited and constructed to minimize potential detrimental visual or other impact

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

- 47.1 *All services to new allotments are located in underground service trenches.*
- 47.2 *Approved sewerage and wastewater disposal systems are provided to meet the likely needs of future land uses in accordance with AS 3500.3.*

- 47.3 *Where overland stormwater drainage is required, it is incorporated into roadways to cater for major flows (100 year ARI).*
- 47.4 *Stormwater generated by a site is managed by a minor system (underground pipe network) and the major system (road network) for the gap flows between the minor system (10 year ARI) and the 100 year ARI storm event.*
- 48** Where additional off-site infrastructure is required to accommodate stormwater generated by a development, the cost associated of such infrastructure should be borne by the developer.
- 49** Development adjacent to, or immediately opposite a zone boundary, should be designed and sited so as not to detract from the amenity of the adjacent zone through:
- (a) the provision of a visual buffer in the form of landscape plantings, attractive and consistent style fencing, siting of buildings or other similar means is provided between any car parking, service area, outdoor storage area or any other activity likely to have an impact on the adjacent zone; and
 - (b) the reception, storage and processing of bulky materials occurring within a building or where adequately screened by fencing, landscaping, buildings or other structures on the site.

Design Technique (*Design Techniques illustrate ONE WAY of satisfying the above principle*)

- 49.1 *An acoustic barrier is provided between any noise generating source of development and adjacent non-industrial zones comprising either:*
- (a) *walls of solid masonry or similar sound attenuating properties; or*
 - (b) *an intensively landscaped buffer with a minimum width of 2 metres; whichever is appropriate to the situation.*

Access, Parking and Traffic Management

- 50** No direct vehicular access or egress for business purposes should be provided to any allotment to or from Main North Road or Para Road.

PROCEDURAL MATTERS

Non-complying Development

- 51** The following kinds of development including expansion of an existing use, are **non-complying** in the Business Zone:

Agistment and Holding of Stock
 Builders Yard
 Buildings and structures with a floor area in excess of 3000 square metres except on the site on the south-western corner of the Main North Road and Gordon/Tiver Road intersection
 Caravan Park
 Community Centre
 Consulting Room (other than veterinary consulting room)
 Drive-In Theatre
 Dwelling
 Educational Establishment
 Fast Food Restaurant on the site on the south-western corner of the Main North Road and Gordon/Tiver Road intersection
 General Industry
 Hotel
 Intensive Animal Keeping
 Landscape Supplies
 Motel

Motor Racing Track
 Non-residential Club
 Office, other than an office ancillary to a desired use including any expansion, addition, alteration or the like, where the resultant floor area does not exceed 250 square metres
 Petrol Filling Station on the site on the south-western corner of the Main North Road and Gordon/Tiver Road intersection, except refuelling/recharging facilities for hydrogen and electric vehicles, when ancillary to a Motor Showroom and delivery centre
 Place of Worship
 Prescribed Mining Operations
 Public Works Depot
 Racecourse
 Recreation or Entertainment Centre
 Residential Club
 Shop (excluding bulky goods outlet, restaurant under 300 square metres gross leasable floor space, showroom, or shop under 500 square metres gross leasable floor area on CT 5448/384)
 Special Industry Stadium
 Stock Sales Yard
 Store (except in association with an existing or contemplated land use)
 Waste Disposal Depot

The following advertising displays:

- (i) flashing or animated signs;
- (ii) roof mounted advertisements projecting above the roof line;
- (iii) parapet mounted advertisements projecting above the top of the parapet;
- (iv) advertising hoardings where third party advertisements or advertisements that display messages or advertise products that are not directly related to the activity on land on which the advertisement is being displayed (except on CT 5448/384).

Public Notification

52 Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*. Further, the following forms of development are designated:

Category 1

The following forms of development, or combination thereof, on the site on the south-western corner of the Main North Road and Gordon/Tiver Road intersection:

Car Dealership
 Motor Repair Station
 Motor Showroom
 Service Trade Premises
 Vehicle Distribution Centre

Category 2

Car Dealership
 Motor Repair Station
 Motor Showroom
 Petrol Filling Station (except on the south-western corner of the Main North Road and Gordon/Tiver Road intersection)
 Service Trade Premises
 Vehicle Distribution Centre

COMMUNITY ZONE

The Objectives and Principles of Development Control that follow apply to the Community Zone as shown on [Map Ga/8](#). They are additional to those expressed for the whole Council area.

OBJECTIVES

- Objective 1:** A zone accommodating community, educational, recreational and health care facilities for the general public's benefit.
- Objective 2:** Development that is integrated in function and provides a coordinated base to promote efficient service delivery.
- Objective 3:** Development that contributes to the desired character of the zone.

DESIRED CHARACTER

The Community Zone recognizes the regional role of Trinity College as a multi-purpose education provider with associated private and public sporting and recreation facilities.

New buildings that include multiple stories will be acceptable and should be designed to have a height and form that complements Gawler's urban gateway.

Sufficient on-site vehicle parking and vehicle movement systems are critical considerations given the education use is a high generator of movement demand. Integration of vehicle, cyclists and pedestrian infrastructure with adjoining public systems and public transport in accordance with the relevant structure plan is essential.

The zone and use provides excellent opportunities for best practice environmental design, and reducing water, waste and energy demand.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the zone:
 - Community centre
 - Consulting room
 - Educational establishment
 - Emergency services facility
 - Hall
 - Health facility
 - Library
 - Office associated with community service
 - Place of worship
 - Public administration office
 - Recreation centre
 - Theatre
 - Welfare institution.
- 2 Development listed as non-complying is generally inappropriate.
- 3 Development should not be undertaken if it would inhibit or prejudice the integrated development of land within the zone for further community and institutional uses.

Form and Character

- 4 Development should not be undertaken unless it is consistent with the desired character for the zone.

PROCEDURAL MATTERS

Complying Development

- 5 Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

Non-complying Development

- 6 Development (including building work, a change in the use of land, or division of an allotment) for the following is **non-complying**:

- Fuel depot
- Horticulture
- Industry
- Intensive animal keeping
- Major public service depot
- Motor repair station
- Petrol filling station
- Road transport terminal
- Service trade premises
- Shop or group of shops except where the gross leasable area is less than 250 square metres.
- Stock sales yard
- Stock slaughter works
- Store
- Warehouse
- Waste reception, storage, treatment or disposal
- Wrecking yard

Public Notification

- 7 Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

DEFERRED URBAN ZONE

Introduction

The objectives and principles of development control that follow apply to the Deferred Urban Zone shown on [Maps Ga/4, 8 and 9](#). They are additional to those Council Wide provisions expressed for the whole of the council area.

Portions of the Zone are also within the Gawler Rivers Floodplain Area identified on [Figures FI/1 to FI/8](#).

DESIRED CHARACTER

The Deferred Urban Zone identifies those areas within the Urban Boundary that have been set aside for urban expansion. The development of land is dependent on resolution of outstanding issues; the timing of detailed structure planning and demand for future housing in the region.

The Deferred Urban Zone has been established to prevent development that is incompatible with future housing or that is likely to prejudice the orderly conversion of the land to urban residential use.

Urban development should only occur after detailed structure planning and rezoning has been undertaken to ensure the orderly extension of existing urban areas and to prevent the creation of isolated communities that are remote from infrastructure and services. In addition, future rezoning should have regard to flood prone land and the hazard categories identified on [Figures FI/1 to FI/8](#), and the potential presence of contaminated land that will require remediation to a level suitable for residential development.

The future character of this land should be defined by development that incorporates:

- (a) Water sensitive urban design principles.
- (b) Movement linkages to the balance of the Gawler Town ship and existing public transport services, with a focus on walking and cycling.
- (c) Community services and facilities, such as education facilities, accessible to the community they serve.
- (d) Active and vibrant public spaces that promote community interaction.
- (e) Provision of effective noise attenuation buffers to major transport corridors.

The progressive rezoning of land within the suburbs of Evanston Gardens and Evanston South is planned to occur in order to maintain an adequate land supply within the region while ensuring undue pressure is not placed on infrastructure and social services.

OBJECTIVES

- Objective 1:** A zone accommodating a range of low-intensity rural uses that do not compromise future opportunities for co-ordinated urban development.
- Objective 2:** A zone comprising land to be used primarily for non-intensive farming purposes until required for future urban expansion.
- Objective 3:** Prevention of development likely to be incompatible with long-term urban development, or likely to be detrimental to the orderly and efficient servicing and conversion of the land for urban use.
- Objective 4:** Use of buffers to accommodate stormwater and waste water management practices and provision of linear buffers along transport corridors and adjacent rural zones to manage potential sources of impact.

Objective 5: Development that contributes to the desired character of the zone.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 Development undertaken in the Deferred Urban Zone should be primarily low intensity farming unlikely to inhibit the range of possible future urban uses.
- 2 Development that may be prejudicial to the orderly and economic development of future urban land uses within the zone should not be undertaken.

Form and Character

- 3 Development should not be undertaken unless it is consistent with the desired character for the zone.
- 4 Development within 60 metres of a rural zone should seek to minimise potential conflict between urban uses and rural uses by:
 - (a) establishing a landscaped buffer to provide sufficient separation for sensitive uses;
 - (b) creating allotments of a suitable size and shape that can accommodate the intended use as well as the extent of any landscaped buffer deemed necessary; and
 - (c) appropriate design and orientation of buildings and structures.
- 5 An allotment should not contain more than one detached dwelling.

Land Division

- 6 Land division should not be undertaken, other than for the purposes of consolidating allotments into larger allotments, or undertaking minor boundary re-alignments that do not result in the creating of allotments less than 4 hectares in area.

Hazards

The following principles apply to land within the Gawler Rivers Floodplain Area identified on [Figure FI/1 to FI/8](#).

Flooding

- 7 Land division should not result in an additional allotment or allotments.
- 8 Dwellings, other than detached dwellings, should not be developed.
- 9 Detached dwellings should only be developed where:
 - (a) the finished floor level is equal to or greater than 300 millimetres above the predicted level of a 1 in 100 ARI flood event; and
 - (b) in **Low** hazard flood risk areas:
 - (i) site coverage for all buildings on the allotment does not exceed 50 percent of the area of the allotment; and
 - (ii) set-backs of detached dwellings to side and rear boundaries, but excluding boundaries to a public road, are at least:
 - (A) one metre to side boundaries; and

- (B) four metres to rear boundaries.
 - (c) in **Medium** hazard flood risk areas:
 - (i) site coverage for all buildings on the allotment does not exceed 40 percent of the area of the allotment; and
 - (ii) set-backs of detached dwellings to side and rear boundaries, but excluding boundaries to a public road, are at least:
 - (A) two metres to side boundaries; and
 - (B) five metres to rear boundaries.
 - (d) in High hazard flood risk areas:
 - (i) site coverage for all buildings on the allotment does not exceed 30 percent of the area of the allotment; and
 - (ii) set-backs of detached dwellings to side and rear boundaries, but excluding boundaries to a public road, are at least:
 - (A) three metres to side boundaries; and
 - (B) six metres to rear boundaries.
- 10** Ground floor additions to dwellings should:
- (a) not exceed a 40 square metre increase in floor area; and
 - (b) be the same or higher finished floor level as the existing dwelling; and
 - (c) comply with the applicable site coverage and boundary set-back requirements in Principle of Development Control 9.
- 11** Dwelling additions in excess of a 40 square metre increase in the existing floor area should:
- (a) be in the form of upper level additions; or
 - (b) have a finished floor level equal to or greater than 300 millimetres above the predicted level of a 1 in 100 ARI flood event; and
 - (c) comply with the applicable site coverage and boundary setback requirements in Principle of Development Control 9.
- 12** Outbuildings may be constructed at natural ground level providing:
- (a) there is no permanent storage of goods or equipment below 300 millimetres above the predicted level of a 1 in 100 ARI flood event level for the applicable hazard flood risk area; and
 - (b) the maximum site coverage requirements in Principle of Development Control 9 are met.

PROCEDURAL MATTERS

Non-complying Development

13 The following kinds of development are **non-complying** in the Deferred Urban Zone:

- Advertisement
- Advertising Hoarding
- Amusement Machine Centre
- Commercial Forestry
- Community Centre
- Consulting Room
- Crematorium
- Dairy
- Dwelling, other than a detached dwelling
- Fuel Depot
- Horticulture
- Hospital
- Hotel
- Indoor Recreation Centre
- Industry
- Intensive Animal Keeping

Land Division other than the re-alignment of allotment boundaries, provided:

- (a) no allotment is less than 4 hectares in area; and
- (b) each allotment has frontage to a public road of greater than 25 percent of the average depth of the allotment.

- Mining
- Motel
- Motor Repair Station
- Nursing Home
- Office
- Petrol Filling Station
- Place Of Worship
- Pre-school
- Restaurant
- Road Transport Terminal
- Service Trade Premises
- Shop
- Stock Sales Yard
- Stock Slaughter Works
- Store
- Tourist Accommodation
- Warehouse
- Waste reception, storage, treatment or disposal
- Wrecking Yard

DISTRICT COMMERCIAL ZONE

Introduction

In addition to the Council Wide policies, the District Commercial Zone policies apply to the areas shown on [Map Ga/5](#).

OBJECTIVE

Objective 1: A zone accommodating major servicing, wholesaling, storage and associated activities, including road transport terminals.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development undertaken in the District Commercial Zone should accommodate major servicing, wholesaling, storage and associated activities, including road transport terminals.

PROCEDURAL MATTERS

Non-complying Development

- 2 The following kinds of development including expansion of an existing use are **non-complying** in the District Commercial Zone:

Abattoir
 Agistment and Holding of Stock
 Agriculture
 Caravan Park
 Cemetery
 Community Centre
 Crematorium

Demolition:

- (i) in total; or
- (ii) of the frontage or side wall returns visible from a street or other elements visible from a public place of a place listed in [Table Ga/2](#) or [Table Ga/5](#)

Detached Dwelling
 Educational Establishment
 General Industry
 Golf Course
 Health Centre
 Hospital
 Library
 Meeting Hall
 Multiple Dwelling
 Place of Worship
 Plant Nursery
 Police Station
 Prescribed Mining Operations
 Primary School
 Refuse Destructor
 Residential Flat Building
 Row Dwelling
 Semi-detached Dwelling
 Shop or group of shops with a gross leasable area greater than 450 square metres
 Special Industry
 Welfare Institution

GENERAL INDUSTRY ZONE

Introduction

In addition to the Council Wide policies, the General Industry Zone and Gawler Rivers Floodplain Area policies apply to the areas shown on:

General Industry Zone [Maps Ga/5](#)
 Gawler Rivers Floodplain Area [Figures FI/1 to FI/8](#)

DESIRED CHARACTER

The zone is intended to accommodate a wide range of uses including service and manufacturing industry, wholesaling, warehousing and storage.

The zone's image will be improved with the infill of vacant and underutilised land with attractive and suitably scaled development, except on land within the high hazard flood risk area identified on [Figures FI/1 to FI/8](#), where infill development should be avoided. Paxton Street and Kellys Road should be transformed by the development of attractive streetscapes with large street trees to form a more impressive entrance to this industrial precinct.

Signage should provide a single and clear identification of each site with Pylon or Freestanding signs being the most appropriate for this purpose. Flat wall signs involving direct application to large scale walls is encouraged providing the overall size is in proportion to the building. Priority should be given to legibility and avoiding clutter, both in terms the number of signs and unnecessary detailing. Third party or product advertising is inappropriate in the zone.

Desired Uses:

- Emergency Services
- General Industry
- Industry
- Light Industry
- Motor Repair Station
- Offices associated with the desired uses for the zone
- Public Service Depot
- Service Industry
- Showroom
- Store
- Timberyard
- Warehouse
- Wholesaling

OBJECTIVE

Objective 1: A zone primarily accommodating general industrial development.

PRINCIPLES OF DEVELOPMENT CONTROL

Form of Development

- 1 Development should not be undertaken within the zone unless it is consistent with the Desired Character and Desired Use of Land for the zone.
- 2 Development undertaken in the General Industry Zone should be, primarily, industrial development other than special or extractive industrial development.

Signs

- 3 All signs should be developed in accordance with the relevant provisions applying throughout the Council area and with the Performance Standard in [Table Ga/4](#).

- 4 The following sign types, advertising and advertising displays of nominated specification and design requirements are appropriate in the zone:
- (a) Pylon or Freestanding;
 - (b) Flat Wall:
 - (i) directly painted onto the fabric of the building is desirable;
 - (ii) lettering size of 1000 millimetres is desirable;
 - (c) Verandah Fascia;
 - (d) Under Verandah;
 - (e) Shop Front; and
 - (f) First Party Advertising.
- 5 The following sign types, advertising and advertising displays are inappropriate in the zone:
- (a) Bunting;
 - (b) Mobile;
 - (c) Projecting;
 - (d) Above Verandah;
 - (e) Roof;
 - (f) Sandwich Board; and
 - (g) Third Party Advertising.

Hazards

The following provisions apply to land within the Gawler Rivers Floodplain Area identified on [Figures FI/1 to FI/8](#).

Flooding

- 6 New buildings (in excess of 40 square metres floor area) should :
- (a) have a finished floor level equal to or greater than 300 millimetres above the predicted level of a 1 in 100 ARI flood event level applicable to the hazard flood risk area; and
 - (b) ensure there is no permanent storage of chemicals, goods or equipment below 300 millimetres above the predicted 1 in 100 ARI flood event level for the applicable hazard flood risk area.
- 7 Ground floor additions to buildings should :
- (a) not exceed a 40 square metre increase on the existing floor area; and
 - (b) be at the same or higher finished floor level as the existing building.
- 8 Ground floor additions to buildings in excess of a 40 square metre increase in the existing floor area should:

- (a) have a finished floor level equal to or greater than 300 millimetres above the predicted level of a 1 in 100 ARI flood event level applicable to the hazard flood risk area; and
- (b) ensure there is no permanent storage of goods or equipment below 300 millimetres above the predicted level of a 1 in 100 ARI flood event level for the applicable hazard flood risk area.

- 9** Fencing should be avoided. Where fencing is required to provide safety or security to stock, goods or machinery, it should be in the form of open wire fencing and designed to withstand the flow of floodwaters.

PROCEDURAL MATTERS

Non-complying Development

- 10** The following kinds of development including expansion of an existing use are **non-complying** in the General Industry Zone:

Abattoir
 Agistment and Holding of Stock
 Agriculture
 Amusement Hall
 Amusement Park
 Billiard Saloon
 Bowling Alley
 Bunting
 Caravan
 Cemetery
 Community Centre
 Concert Hall
 Crematorium
 Dance Hall

Demolition:

- (a) in total; or
- (b) of the frontage or side wall returns visible from a street or other elements visible from a public place of a place listed in [Table Ga/2](#) or [Table Ga/5](#)

Detached Dwelling
 Dog Track
 Drive-in Theatre
 Educational Establishment
 Exhibition Hall
 Extractive Industry
 Fun Fair
 Golf Course
 Golf Driving Range
 Gymnasium
 Health Centre
 Hospital

Intensive animal keeping in the Gawler Rivers Floodplain Area identified on [Figures FI/1 to FI/8](#)

Land division in the Gawler Rivers Floodplain Area on land identified in [Figures FI/1 to FI/8](#), except where the division:

- (a) does not result in an increase in the number of allotments; and
- (b) provides for public access to the banks of the river in the form of a reserve or easements necessary for the provision of public utility services; and
- (c) facilitates the construction of flood mitigation works.

Library
 Meeting Hall

Mobile Sign
Motor Race Track
Multiple Dwelling
Place of Worship
Plant Nursery
Park Police Station
Prescribed Mining Operations
Primary School
Racecourse
Refuse Destructor
Residential Flat Building
Roof Signs
Row Dwelling
Semi-detached Dwelling
Shop or group of shops with a gross leasable area greater than 450 square metres
Show Ground
Skating Rink
Special Industry
Squash Court
Stadium
Theatre
Welfare Institution

LIGHT INDUSTRY ZONE

Introduction

In addition to the Council Wide policies, the Light General Zone and Gawler Rivers Floodplain Area policies apply to the areas shown on:

Light Industry Zone [Maps Ga/5](#)
Gawler Rivers Floodplain Area [Figures FI/1 to FI/8](#)

Desired Character

The zone is intended to accommodate a wide range of uses including service and manufacturing industry, wholesaling, warehousing and storage.

The zone's image will be improved with the infill of vacant and underutilised land with attractive and suitably scaled development except on land within the high hazard category identified on [Figures FI/1 to FI/8](#) where infill development should be avoided. Paxton Street and Kellys Road should be transformed by the development of attractive streetscapes with large street trees to form a more impressive entrance to this industrial precinct.

Signage should provide a single and clear identification of each site with pylon and freestanding signs being the most appropriate. Flat wall signs involving direct application to large scale walls is encouraged providing the overall size is in proportion to the building. Priority should be given to legibility and avoiding clutter, in terms of both the number of signs and unnecessary detailing. Third party or product advertising is inappropriate in the zone.

Desired Uses:

Emergency Services outside any hazard flood risk identified on [Figures FI/1 to FI/8](#).
Industry
Light Industry
Motor Repair Station
Offices associated with the desired uses for the zone
Public Service Depot
Service Industry
Showroom
Store
Timberyard
Warehouse
Wholesaling

OBJECTIVE

Objective 1: A zone primarily accommodating industries which manufacture on a small-scale and which do not create any appreciable nuisance or generate heavy traffic.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should not be undertaken within the zone unless it is consistent with the Desired Use of Land for the zone.
- 2 Development undertaken in the Light Industry Zone should be, primarily, industries which manufacture on a small-scale and which do not create any appreciable noise, smoke, smell, dust or other nuisance, or generate heavy traffic.

Signs

- 3 All signs should be developed in accordance with the relevant provisions applying throughout the Council area and with the Performance Standards in [Table Ga/4](#).

- 4 The following sign types, advertising and advertising displays of nominated specification and design requirements are appropriate in the zone:
- (a) Pylon or Freestanding;
 - (b) Flat Wall:
 - (i) directly painted onto the fabric of the building is desirable;
 - (ii) lettering size of 100 millimetres is desirable;
 - (c) Verandah Fascias;
 - (d) Under Verandah; and
 - (e) Shop Front.
- 5 The following sign types, advertising and advertising displays are inappropriate in the zone:
- (a) Bunting;
 - (b) Mobile;
 - (c) Projecting;
 - (d) Above Verandah;
 - (e) Roof; and
 - (f) Sandwich Board.

Hazards

The following provisions apply to land within the Gawler Rivers Floodplain Area identified on [Figures FI/1 to FI/8](#).

Flooding

- 6 New buildings (in excess of 40 square metres floor area) should:
- (a) have a finished floor level equal to or greater than 300 millimetres above the predicted level of a 1 in 100 ARI flood event level applicable to the hazard flood risk area; and
 - (b) ensure there is no permanent storage of chemicals, goods or equipment below 300 millimetres above the predicted level of a 1 in 100 ARI flood event level for the applicable hazard flood risk area.
- 7 Ground floor additions to buildings should:
- (a) not exceed a maximum of 40 square metre increase on the existing floor area; and
 - (b) be at the same or higher finished floor level as the existing building.
- 8 Ground floor additions to buildings in excess of a 40 square metre increase in the existing floor area should:
- (a) have a finished floor level equal to or greater than 300 millimetres above the predicted level of a 1 in 100 ARI flood event level applicable to the hazard flood risk area; and

- (b) ensure there is no permanent storage of goods or equipment below 300 millimetres above the predicted level of a 1 in 100 ARI flood event level for the applicable hazard flood risk area.

- 9 Fencing should be avoided. Where fencing is required to provide safety or security to stock, goods or machinery, should be in the form of open wire fencing and designed to withstand the flow of floodwaters.

PROCEDURAL MATTERS

Non-complying Development

- 10 The following kinds of development including expansion of an existing use are **non-complying** in the Light Industry Zone:

Abattoir
 Agistment and Holding of Stock
 Agriculture
 Amusement Hall
 Amusement Park
 Billiard Saloon
 Bowling Alley
 Bunting
 Caravan
 Cemetery
 Community Centre
 Concert Hall
 Crematorium
 Dance Hall

Demolition:

- (a) in total; or
- (b) of the frontage or side wall returns visible from a street or other elements visible from a public place of a place listed in [Table Ga/2](#) or [Table Ga/5](#)

Detached Dwelling
 Dog Track
 Drive-in Theatre
 Educational Establishment
 Exhibition Hall
 Extractive Industry
 Fun Fair
 Golf Course
 Golf Driving Range
 Gymnasium
 Health Centre
 Hospital

Intensive animal keeping in the Gawler Rivers Floodplain Area identified on [Figures FI/1 to FI/8](#)

Land division in the Gawler Rivers Floodplain Area identified on [Figures FI/1 to FI/8](#), except where the division:

- (a) does not result in an increase in the number of allotments; and
- (b) provides for public access to the banks of the river in the form of a reserve or easements necessary for the provision of public utility services; and
- (c) facilitates the construction of flood mitigation works.

Library
 Meeting Hall
 Mobile Signs
 Motor Race Track
 Multiple Dwelling

Place of Worship
Plant Nursery
Park Police Station
Prescribed Mining Operations
Primary School
Racecourse
Refuse Destructor
Residential Flat Building
Roof Signs
Row Dwelling
Semi-detached Dwelling
Shop or group of shops with a gross leasable area greater than 450 square metres
Show Ground
Skating Rink
Special Industry
Squash Court
Stadium
Theatre
Welfare Institution

MIXED USE HISTORIC (CONSERVATION) ZONE

In addition to the Council Wide policies (including the Gawler Rivers Flood Plain Policy Area), the Mixed Use Historic (Conservation) Zone policies apply to the areas shown on:

Gawler Rivers Flood Plain Policy Area: [Map Ga/15](#).

Gawler Health Services Helipad: [Figure Hel/1](#).

Mixed Use Historic (Conservation) Zone: [Map Ga/5](#).

OBJECTIVES

Objective 1: A zone with diverse uses, subordinate to the Town Centre, accommodating a mix of smaller scale commercial, community, residential, office and retail uses to serve the surrounding neighbourhood.

Objective 2: Development that contributes to the desired character of the zone.

Objective 3: Development that minimises any adverse impacts upon the amenity of the zone and adjacent zones.

Objective 4: Retention/restoration, adaptation, enhancement and re-use of all places and items (including landscape elements) which contribute to the historic character of the zone.

Objective 5: Protection and reinforcement of the historic value, character and significance of the locality.

Objective 6: A built character which:

- (a) is strongly identified with the zone;
- (b) comprises separate, discrete visual components with common unifying architectural elements;
- (c) presents an articulated façade to the public street frontage and enhances these public spaces;

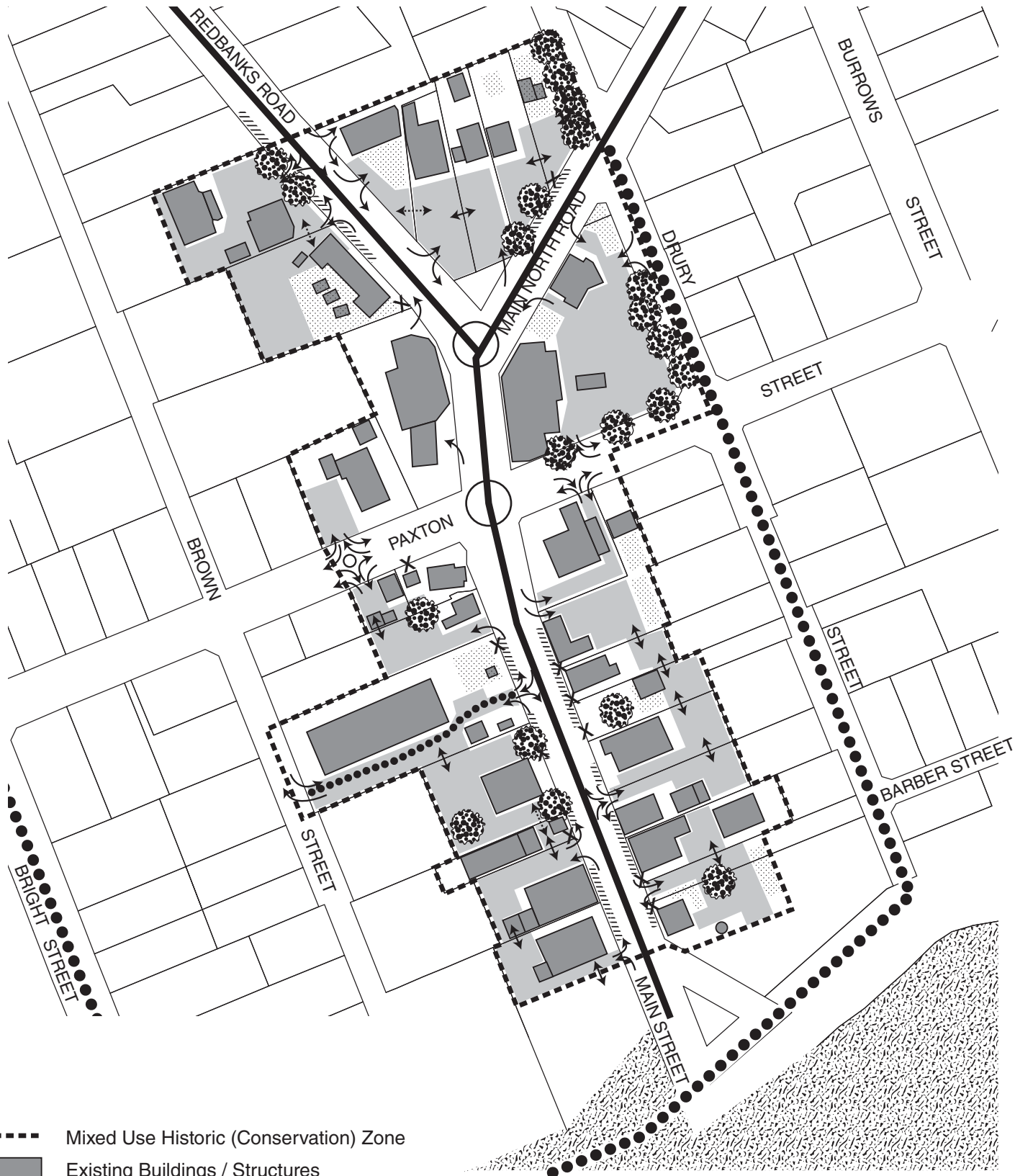
Objective 7: Co-ordinated development and shared car parking areas, typically located at the rear of buildings, to:

- (a) optimize available car parking spaces;
- (b) limit the number of access points to the Secondary Arterial Roads and residential streets, while allowing vehicle access to Drury Street, Willaston in accordance with Concept Plan [Figure MUH\(C\)/1](#); and,
- (c) enhance the appeal and public amenity of the locality.

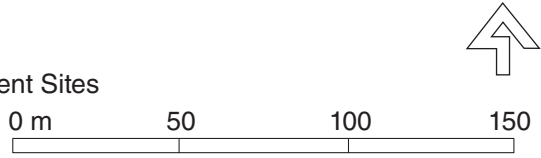
DESIRED CHARACTER

The zone is strongly defined by the main street of the Willaston township, which was officially laid out on 21 October 1848. The historic significance of the Willaston township is evident to this day, with many of the original buildings of the settlement, dating from the period 1848-1900, remaining in use and defining the historic and desired character of built form.

As the suburban area of Willaston has developed around it, Willaston's Main Street has retained its traditional function, with a diverse mixture of retail, community, service trade, bulky goods and residential uses serving the Willaston locality as well as offering convenience and specialist services to a wider catchment. This mixture of uses is a defining feature of small South Australian township main streets and the retention and reinforcement of this pattern is important to securing the fabric and relevance of the precinct into the future.



- Mixed Use Historic (Conservation) Zone
- Existing Buildings / Structures
- Possible Development Footprint
- ↔ Access / Egress Location and Direction and Connection to Adjacent Sites
- ⋯↔ Possible Connections with Adjacent Sites
- X Access Closure
- Car Parking and Driveway Areas
- Tree Icon Trees to Remain
- Pedestrian Link
- Traffic Control Measure
- Bicycle Route
- == Secondary Arterial Road
- === On Street Car Parking
- ▨ Park Lands



**GAWLER (CT)
MIXED USE HISTORIC
(CONSERVATION)
ZONE
CONCEPT PLAN
FIGURE MUH(C)/1**

Consolidated - 18 July 2019

With its original function as a country town centre, together with its historic built form, the Willaston township remains distinct in size and function to the larger – regionally focussed – town centre of Gawler.

Development within the zone should complement and reinforce the historic character of built form, which is defined by a single storey building scale and building footprint, with variable depths to front gardens, verandahs and porches linked to their original use, together with variable side building setbacks.

Infill development along the southern side of the junction of the Secondary Arterial Roads (i.e. the point where the two roads converge) should produce a more commercial and enclosed character than elsewhere in the zone. This means minimal or zero setbacks to the road, overhanging verandahs and taller wall heights of up to eight metres.

New residential infill development generally is likely to occur as smaller buildings, typically in the form of traditional outbuildings at the rear of sites and adjoining the residential zone. These uses will be integrated with and share vehicular access and car parking.

Development in the zone should be designed and managed to mitigate environmental impacts on premises and residents both within the zone and in the surrounding residential areas. Uses that would produce excessive levels of noise and/or environmental pollution are not appropriate and commercial traffic should not rely on the utilisation of residential streets. Vehicle access points along the Main North Road frontage should be limited to those shown on Concept Plan [Figure MUH\(C\)/1](#). Vehicle access points to Drury Street should be limited to the area north of Paxton Street.

Off-street car parking shall be provided with all new development at the rate prescribed in [Table Ga/1](#) (a financial contribution to the Council's car parking fund to offset any shortfall does not apply in this zone).

Signs are appropriate in the zone but should be constrained in number and size. Signs should have a minor impact on the streetscape and should be developed in scale with the low rise nature of both the built form and the streetscape character. Where buildings are set-back from the road a Free Standing Sign, together with a single Flat Wall Sign, are appropriate. For buildings set-back between zero and three metres, Flat Wall, Projecting and Under Verandah Signs are appropriate.

Signage should achieve a high level of integration and respect for the architecture of individual buildings where signs are attached. Visual clutter should be avoided and sign coverage should be restrained and colours chosen carefully to contrast but not clash with building colours and detailing.

The Gawler Rivers Floodplain Policy Area traverses part of the zone. Any development within the Gawler Rivers Floodplain Policy Area should be designed to reduce risk of damage by floodwaters.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

1 The following forms of development are envisaged in the zone:

- Bank
- Childcare facility
- Community facility
- Consulting room with a maximum gross leasable floor space in the order of 450 square metres
- Detached dwelling
- Group dwelling
- Hotel (existing)
- Institutional facility
- Multiple dwelling
- Office with a maximum gross leasable floor space in the order of 450 square metres
- Residential flat building
- Restaurant
- Row dwelling
- Service trade premises with a gross leasable area up to 450 square metres

Shop or group of shops with a maximum gross leasable floor space in the order of 450 square metres.

- 2 Development listed as non-complying is generally inappropriate.

Form and Character

- 3 Development should not be undertaken unless it is consistent with the desired character for the zone.
- 4 Development within the zone should be carried out in accordance with the concept in [Figure MUH\(C\)/1](#).
- 5 Development should be sited and designed so as not to obstruct the implementation of the proposals in [Figure MUH\(C\)/1](#), particularly with regard to coordinated and integrated car parking provisions.
- 6 Development should not detrimentally affect the amenity of the locality (including adjacent residential areas) or cause unreasonable interference through any of the following:
- the emission of effluent, odour, smoke, fumes, dust or other airborne pollutants;
 - noise;
 - vibration;
 - electrical interference;
 - light spill;
 - glare;
 - hours of operation; and,
 - traffic impacts.

- 7 Development within the zone should be carried out, where applicable, in accordance with the Heritage Design Guidelines of Fig 7.1 and Fig 7.2.

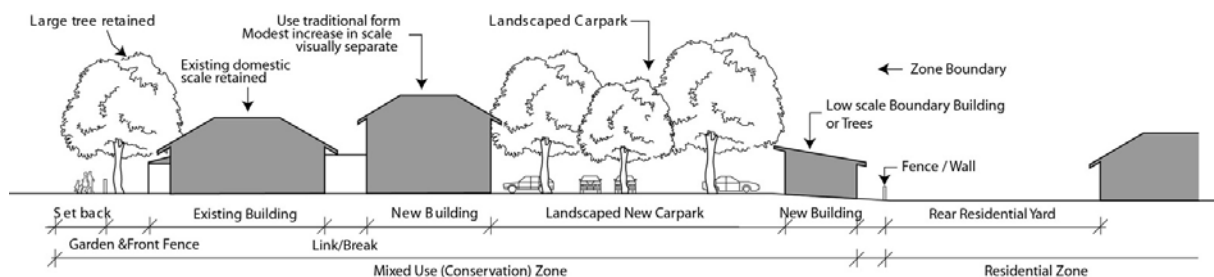


Figure 7.1 – Gawler (CT) Heritage Design Guideline

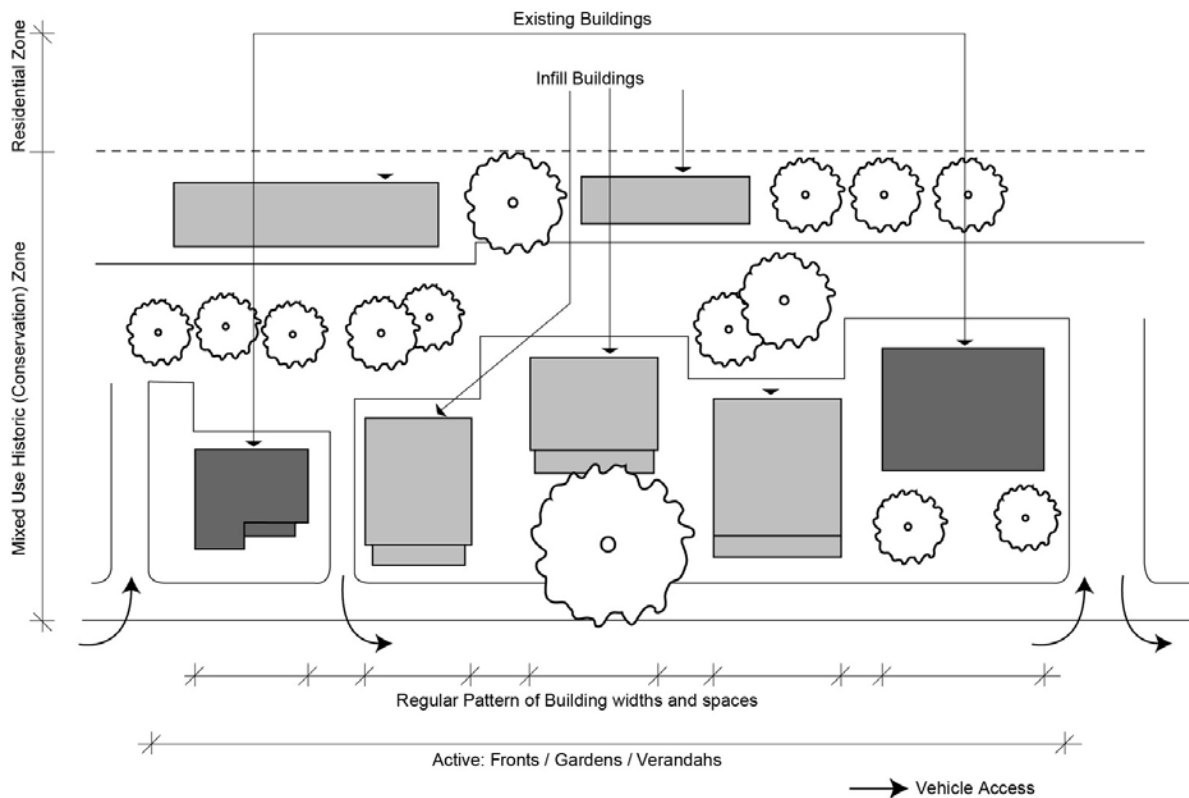


Figure 7.2 – Gawler (CT) Heritage Design Guideline

- 8 Development should reinforce and complement the historic character and significance of the area and the integrity of any places and items identified in [Tables Ga/3, 6 or 7](#), or in [Figure Ga\(HPCI\)/3](#).
- 9 Development should conserve, enhance and contribute to the visual cohesiveness and historic character of the zone by reference to the external appearance of the building (architectural detail, materials and colours) and any new buildings should complement the existing historic character in terms of height, scale, setback, form and external appearance, and display creative and diverse examples of high standard contemporary architecture.
- 10 Colours of external painted surfaces and materials should be chosen to preserve and enhance the desired future character and historic character of the zone. The use of primary colours should be limited to signage, posts, window frames, door frames, door panels and limited areas on new structures, only where they make an important architectural contribution.
- 11 Primary colours should not be used in decorative mouldings or render and corporate colour schemes should be restrained in terms of building coverage.
- 12 Painting on stone work and fluorescent finishes are not appropriate.
- 13 Places and items identified in [Tables Ga/3, 6 or 7](#) or in [Figure Ga\(HPCI\)/3](#), should not be demolished, but instead be retained, upgraded and adapted such that:
 - (a) the quality or condition which gives the place its significance is protected or enhanced. This should include elements of the place which have significant cultural associations;
 - (b) the place is put to a use which supports the maintenance and restoration of that building, item, structure or place;
 - (c) the place is only altered so as to retain or restore the original external form, materials, and colours in a manner compatible with its structural visual, material and historic integrity; and

- (d) where internal arrangements and finishes are of significance to places and items identified in [Tables Ga/3, 6 or 7](#) or in [Fig Ga\(HPCI\)/3](#), the place should only be altered in a manner compatible with its structural visual, material and historic integrity.
- 14** Development abutting or in close proximity to a place or item identified in [Tables Ga/3, 6 or 7](#) or in [Fig Ga\(HPCI\)/3](#), should:
- (a) respect the historic character of the area and the integrity of the particular place or item of significance and be designed to a high architectural standard; and
 - (b) be compatible in respect of its design, siting, scale, building and roof shape, bulk, height, materials and colours, fences and landscaping and any advertising signs and external illumination.
- 15** Any alteration or addition to a place identified in [Tables Ga/3, 6 or 7](#) or in [Fig Ga\(HPCI\)/3](#), should reinforce and complement the historic character of the place in terms of its design, siting, boundary set-backs, scale, built form and roof shape, bulk, height, materials and external finishes and where applicable:
- (a) should minimise their impact on the streetscape by siting to the rear or the side of existing buildings and not forward of existing buildings;
 - (b) the height of eaves and gutters on additions should be complementary with the existing building;
 - (c) the proportions (vertical and horizontal) of additions visible from the street, should be complementary with those of the existing building;
 - (d) materials may include masonry (stone and brick) render, timber boarding and corrugated steel sheeting (walls and roof);
 - (e) materials such as mirror glass, flat profile steel sheeting, unpainted zincalume sheeting, wide fibre cement sheet planking and polished face concrete panels should not be used where they are visible from the street, public places or car parking areas;
 - (f) roof conversions or additional upper floors are not desirable where they compromise the historic integrity of the building or the view of the building from the street;
 - (g) roof conversions are acceptable where the alterations are fully incorporated under the existing roof and dormer style windows are acceptable where they are not visible from the street; and
 - (h) external colour schemes should be contemporary, but should not dramatically contrast with the original colour scheme of the existing place.
- 16** Front fences and gates should be retained, restored and reinstated when missing; new fences and gates should:
- (a) reflect and conserve the traditional period, style and form of the associated building;
 - (b) generally be of low timber pickets, low pier and plinth masonry, wrought iron, brush or masonry;
 - (c) be in the range of 1.2 to 1.5 metres in height.
- 17** Rear and side boundary fences located behind the front dwelling alignment should be no more than 1.8 metres in height.

Car parking and Access

- 18** Car parking areas should be located on site and coordinated (and shared) with the car parking areas of adjacent sites in accordance with the concept in [Figure MUH\(C\)/1](#).

- 19 Off-street parking and vehicle access points should be located such that breaks in the continuity of street front buildings are minimised.
- 20 Vehicular access to sites that have a non-residential use should not occur from Brown Street.
- 21 Development should provide car parking spaces in accordance with the requirements of [Table Ga/1](#) either on the site of the development or another convenient and accessible site in the locality (as shown on the concept in [Figure MUH\(C\)/1](#), provided permanent ongoing arrangements for the parking are provided.
- 22 Onsite car parking shall incorporate:
- a level of provision that meets the requirements of [Table Ga/1](#)¹⁰.
 - a design that meets the current Australian Design Standards for car park layout, commercial vehicle loading, disabled parking, and bicycle parking;
 - car parking spaces for people with disabilities in location which are convenient and accessible to the building/or use on the site;
 - secure and convenient spaces for bicycles;
 - landscaped design to shade vehicles and enhance the appearance of the car park (refer Fig 22(e) below);
 - opportunities for interconnection of access driveways with adjoining sites or car parks to facilitate shared use;
 - limited number of crossover points onto public roads in accordance with the concept in [Figure MUH\(C\)/1](#);
 - design elements that promote personal and property safety (eg lighting, surveillance and minimisation of entrapment areas); and
 - lighting designed to meet performance requirements while minimising energy requirements; and,
 - incorporate storm water infiltration techniques (refer Fig 22(j) below).

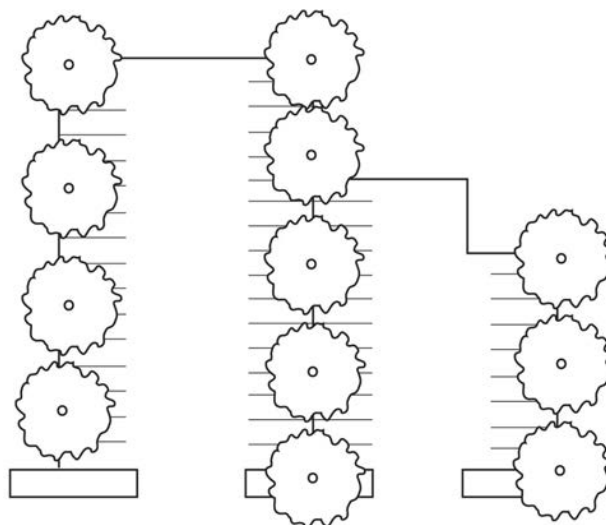


Figure 22(e) – Pattern and Preferred Location for Shade Trees in Carparks

¹⁰The amount of any car parking shortfall (calculated with reference to [Table Ga/2](#)) cannot be made-up by a financial contribution to the Council Car Park Fund in this zone.

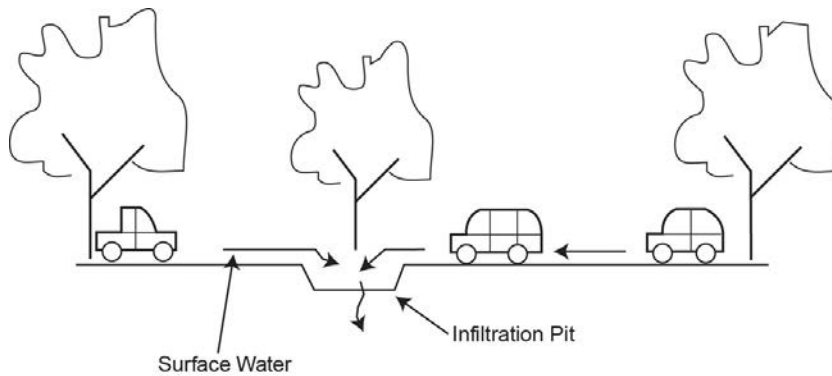


Figure 22(j) – On-site Stormwater Detention and Infiltration

Signs

23 All signs should be developed in accordance with the relevant provisions applying throughout the Council area and with the Performance Standards in [Table Ga/4](#).

24 Advertisements and/or advertising hoardings should:

- (a) be of a size, colour, shape and materials that enhances the character of the locality; and
- (b) not dominate or detract from the prominence of any place and/or area of historic significance.

25 The following sign types are appropriate in the zone:

- (a) freestanding (internal or external illumination);
- (b) flat wall;
- (c) projecting;
- (d) verandah fascia;
- (e) under verandah;
- (f) shop-front;
- (g) sandwich board; and
- (h) first party advertising.

26 The following sign types are inappropriate in the zone:

- (a) above-verandah;
- (b) bunting;
- (c) mobile;
- (d) roof;
- (e) internally illuminated signs on buildings of heritage significance listed in [Tables Ga/3, 6 or 7](#) or in [Fig Ga\(HPCI\)/3](#).
- (f) third party advertising;

- (g) banner; and
- (h) pylon.

Vending Machines and Automatic Teller Machines

- 27** Siting of vending machines and automatic teller machines should have regard to:
- (a) consistency with the relevant zone provisions;
 - (b) maintenance of character and continuity of activity along street frontages; and
 - (c) maintenance of unimpeded pedestrian movement. Where necessary sufficient setback should be provided from front boundaries to accommodate queues.
- 28** Advertising displays relating to vending machines and automatic teller machines should be restrained in size.

Land Division

- 29** When land is divided, shared arrangements for vehicle access to and between off-street carparking should be secured by permanent arrangement such as easements.

Hazards

The following principles apply to land within the Gawler Rivers Floodplain Area identified on [Figure FI/1 to FI/8](#).

Flooding

- 30** All new buildings should:
- (a) have a finished floor level equal to or greater than 300 millimetres above the predicted 1 in 100 ARI flood event level applicable to the hazard zone; and
 - (b) ensure there is no permanent storage of goods or equipment below 300 millimetres above the predicted level of a 1 in 100 ARI flood event level for the applicable hazard flood risk area.
- 31** Additions to existing buildings should:
- (a) be a maximum of 40 square metre increase on the existing floor area; and
 - (b) be at the same or higher finished floor level as the existing building.
- 32** Car parking areas should be provided at natural ground level or with a maximum level increase of 300 millimetres.

PROCEDURAL MATTERS

Complying Development

- 33** Complying developments are prescribed in schedule 4 of the *Development Regulations 2008*. In addition, the following forms of development (except where the development is non-complying) are complying:

A change of use to a shop, office, consulting room or any combination of these uses where all of the following are achieved:

- (a) the area to be occupied by the proposed development is located in an existing building and is currently used as a shop, office, consulting room or any combination of these uses;

- (b) the building is not a State heritage place;
- (c) it will not involve any alterations or additions to the external appearance of a local heritage place as viewed from a public road or public space;
- (d) if the proposed change of use is for a shop that primarily involves the handling and sale of foodstuffs, it achieves either (i) or (ii):
 - (i) all of the following:
 - (A) areas used for the storage and collection of refuse are sited at least 10 metres from any Residential Zone boundary or a dwelling (other than a dwelling directly associated with the proposed shop);
 - (B) if the shop involves the heating and cooking of foodstuffs in a commercial kitchen and is within 30 metres of any Residential Zone boundary or a dwelling (other than a dwelling directly associated with the proposed shop), an exhaust duct and stack (chimney) exists or is capable of being installed for discharging exhaust emissions;
 - (ii) the development is the same or substantially the same as a development, which has previously been granted development approval under the *Development Act 1993* or any subsequent Act and Regulations, and the development is to be undertaken and operated in accordance with the conditions attached to the previously approved development;
- (e) if the change in use is for a shop with a gross leasable floor area greater than 250 square metres and has direct frontage to an arterial road, it achieves either (i) or (ii):
 - (i) the primary vehicle access (being the access where the majority of vehicles access/ egress the site of the proposed development) is from a road that is not an arterial road;
 - (ii) the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared;
- (f) off-street vehicular parking is provided in accordance with the rate(s) specified in [Table Ga/1 - Car Parking Requirements](#) to the nearest whole number, except in any one or more of the following circumstances:
 - (i) the building is a local heritage place;
 - (ii) the development is the same or substantially the same as a development, which has previously been granted development approval under the *Development Act 1993* or any subsequent Act and Regulations, and the number and location of parking spaces is the same or substantially the same as that which was previously approved;
 - (iii) the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared.

Non-complying Development

- 34** Development (including building work, a change in the use of land, the expansion of an existing use of land, or division of an allotment) for the following is **non-complying**:

Above-Canopy Signs
Bunting

Demolition:

- (i) in total; or
- (ii) of the frontage or side wall returns visible from a street or other elements visible from a public place of a place listed in [Tables Ga/3 or 6](#).

Fuel depot

Horse keeping

Hotel, other than in relation to the Willaston Hotel

Mobile sign

Motor vehicle showroom, except where the gross leaseable area is less than 450 square metres

Roof Sign

Service trade premises or group of Service trade premises, except where the gross leaseable area is less than 450 square metres

Wrecking yard

Public Notification

- 35** Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development (except where the development is non-complying) are designated:

Category 1

All forms of development (except where the development is classified as non-complying), or where the site of the development is adjacent land to land in a zone under the relevant Development Plan which is different to the zone that applies to the site of the development.

Category 2

All forms of development not listed as Category 1 (except where the development is classified as non-complying).

OPEN SPACE ZONE

Introduction

The Objectives and Principles of Development Control that follow apply in the Open Space Zone shown on Gawler [Map Ga/5, 6, 8, 9, 12 and 20](#). They are additional to those expressed for the whole of the Council area.

OBJECTIVES

Objective 1: A zone:

- (a) in which the open space character is preserved to provide a visual contrast to the surrounding urban area
- (b) comprising open space that accommodates a range of public and private activities in an open space and natural setting, including:
 - (i) passive and active recreation land uses
 - (ii) habitat conservation and restoration.

Objective 2: Private land within the Metropolitan Open Space System (MOSS) contributing to regional open space networks and providing an open, natural and rural character accommodating low-scale uses such as non-intensive agriculture.

Objective 3: Public land within the MOSS that provides for recreation areas and facilities, sporting facilities and conservation of the open, natural character.

Objective 4: A linear park that:

- (a) provides an open space corridor across Regional South Australia
- (b) promotes the use, awareness and preservation of the Riverine environment and enhancement of natural or semi natural habitats for the movement of wildlife and conservation of biodiversity.

Objective 5: A river system which provides for the maintenance of stormwater capacity and flood mitigation measures for adjoining areas.

Objective 6: Development that contributes to the desired character of the zone.

DESIRED CHARACTER

South Para River

The Open Space Zone recognises the special qualities of the South Para River and environs, and it is intended that the zone will manage the river and its interface.

The zone already has natural character and function based on the existing South Para River, topographically steep contours, floodplains and remnant vegetation. This character and function will be maintained and enhanced through appropriate vegetation.

Areas within the zone contain significant flora and fauna, wetlands and permanent waterholes and river corridors and adjoin grassy woodland and grassland habitats.

Steeper environments and watercourses that are proposed to be vested to Council, should be restored to an appropriate natural state in consultation with the Town of Gawler.

Steeper slopes result in higher bushfire risk. Providing vehicle access between the Open Space Zone and residential development together with appropriate buffers assists in managing bushfire risk. Reference to Australian Standard AS3959 – 2009 (incorporating amendments to 2009 and 2011) is recommended to inform and reduce bushfire risk. Any revegetation of areas will take into account the level of bushfire risk, local topography and the need for ongoing management of bushfire risk, ensuring significant hazards are neither created nor exacerbated.

The aesthetic and natural landscape, together with biodiversity value of the zone will be maintained and enhanced in a manner which encourages appropriate human enjoyment and interaction. Whilst the primary intent of the zone is to enhance the natural character of the area, carefully managed development which has a demonstrated benefit to the community is envisaged.

Facilities will include but are not limited to walking and cycling trails, interpretive and recreational activities, formalised open space and car parking. It is envisaged that roads will be developed in the zone to enable movement between areas as shown on Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement G](#).

Reserves for public purposes such as active and passive recreation and stormwater management, including the provision of associated infrastructure, will be designed to an appropriate standard and level of service suitable for the locality to ensure long term economic, environmental and social sustainability.

An Aboriginal heritage survey has been undertaken for the Gawler East area. It indicates that prior to colonial contact the South Para River and its tributaries were associated with long-term seasonal Aboriginal occupation, with adjoining hill crests providing views of culturally significant landscape features and surveillance for the Kaurna Nation peoples. While there has since been a high level of ground disturbance in the area, there is the potential for future activities to disturb sub-surface archaeological deposits particularly along the banks of the South Para River. Activities within this area which may damage, disturb or interfere with any Aboriginal site or object need to be undertaken in accordance with the provisions of the *Aboriginal Heritage Act 1988*.

Main North Road

A country experience south of Gawler will be provided by creating expansive views over the landscape to cropped and open paddocks and the Adelaide hills face through the removal of existing boundary tree plantations and restrictions on buildings and structures close to Main North Road. Occasional groups of new feature tree plantings will frame views to the hills and farmland towards Gawler River and the Adelaide Plains. All other plantings, earthworks, buildings and structures will use minimal vertical elements so to promote both an alternative distinctive urban character and reinforce the surrounding landscape and distant views. Fencing will be low and open in style.

The zone will provide for recreation activities within a specific and structured landscape framework.

On the western side of Main North Road will be broad areas of natural grasslands, path networks and new native tree and shrub planting, allowing this passive recreational landscape to be sensitively integrated with the new urban environment adjoining while applying a design that draws on the area's original open landscape structure and agricultural land use. Native grasslands and bands of trees will be planted in an arrangement that loosely interprets the grid pattern of 'field cropping'. A seasonal creek will provide a watercourse linear trail that links the open space with the existing path network running west into Evanston Gardens and east towards Trinity College.

The eastern side of Main North Road provides a more structured recreational landscape. Active open space facilities are proposed to strengthen the existing recreational hub around Trinity College. A detention basin and wetland will provide contrasting landscape elements. Opportunities exist for smaller pocket parks for playspace, picnicking opportunities and shared paths for walking and cycling. A mixture of both native and exotic tree plantings will create a more formal, ordered landscape.

Artwork will complement the landscape concept either as a stand-alone signature piece or to influence and modify either the entry statements or proposed structural planting of feature trees.

Celebration of the 'journey' and 'arrival/departure' at the township will be announced at the southern bypass underpass where images can be mounted to existing underpass walls.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the zone:
 - Conservation work
 - Farming
 - Outbuilding associated with open space maintenance
 - Lighting
 - Playground
 - Recreation area
 - Sporting club facility (only in the Main North Road area)
 - Structure associated with a public facility such as car parking, picnic / barbeque area, shelter and toilet
 - Toilet block and barbeque facility.
- 2 Development listed as non-complying is generally inappropriate.
- 3 Publicly owned land within the MOSS should be used for any of the following:
 - (a) to provide natural or landscaped open space using locally indigenous plant species;
 - (b) to accommodate a range of public recreation, sporting and institutional facilities and uses;
 - (c) to accommodate stormwater recreation and management;
 - (d) to conserve and restore areas of remnant native vegetation and wildlife habitats and corridors;
 - (e) to conserve sites of scientific, cultural or heritage interest;
 - (f) for revegetation purposes using locally indigenous plant species;
 - (g) to provide a buffer to adjoining areas of conservation significance.
- 4 Privately owned land within the MOSS should be used for any of the following:
 - (a) rural activities and agriculture (but not intensive animal keeping);
 - (b) low-impact sporting facilities;
 - (c) conservation purposes.
- 5 Development should allow for unstructured passive and active recreation where no harm will occur to the natural features and native flora and fauna of the area.

Form and Character

- 6 Development should not be undertaken unless it is consistent with the desired character for the zone.
- 7 Development should only occur where it is integral to the aesthetic, drainage or recreation function of the zone.
- 8 Buildings should be:
 - (a) restricted in size and number;
 - (b) sited so as to not detract from the open natural character of the zone;

- (c) constructed of materials which blend with the Riverine landscape.
- 9 Development should ensure coordinated design with an emphasis on the creation of pedestrian areas.
- 10 Landscaped buffers should be provided around the perimeter of recreation or sporting facilities.
- 11 Landscaping should comprise locally indigenous species and incorporate existing remnant vegetation.
- 12 Landscaping of the Main North Road Open Space Zone area should be consistent with the desired character for the area.
- 13 Development should ensure that public access to the Linear Park is retained and enhanced for cyclists and pedestrians.
- 14 Development, landscaping, and paths for pedestrians and cyclists should:
- take into account the changing flow regime and width of waterways; and
 - be constructed of permeable material where practical to reduce stormwater runoff.
- 15 Buildings and structures adjoining Main North Road (apart from extensions to existing buildings or ancillary buildings located behind existing buildings) should be designed with the following parameters:

Parameter	Value
Minimum setback from Main North Road (eastern side)	100 metres
Minimum setback from Main North Road (western side and south of Clarke Road)	100 metres
Minimum setback from Main North Road (Western side and north of Clarke Road)	20 metres
Maximum building height	7 metres
Fencing	Open style

Land Division

- 16 Land division should not be undertaken except where:
- it will facilitate the development of an envisaged use in the zone or policy area;
 - it will facilitate the development of an envisaged road in the zone or policy area; or
 - no additional allotments are created.
- 17 Land division creating reserves containing steeper environments and watercourses, that are proposed to be vested to Council, should be restored to an appropriate natural state through the removal of declared plant species, control of other environmental weeds and the planting and management of native vegetation in order to achieve desired amenity, recreation, conservation, biodiversity, management of bushfire risk and ongoing management outcomes.
- 18 Land division creating reserves for public purposes such as active and passive recreation and stormwater management, including the provision of associated infrastructure, should be designed to an appropriate standard and level of service suitable to the locality to ensure long term economic, environmental and social sustainability.

High Pressure Gas Pipeline

- 19 Development, including land division, in the vicinity of a high pressure pipeline, as depicted on Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement G](#), should:
- (a) not affect the continued operation of the pipeline infrastructure
 - (b) be located, designed, and developed having regard to the location of the infrastructure and the potential public safety impacts.

PROCEDURAL MATTERS

Complying Development

- 20 Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

Non-complying Development

- 21 Development (including building work, a change in the use of land, or division of an allotment) for the following is **non-complying**:

Advertisement and /or advertising hoarding
 Amusement machine centre
 Consulting room
 Crematorium
 Dairy
 Dwelling
 Education facility
 Fuel depot
 Hospital
 Hotel
 Industry
 Intensive animal keeping
 Motel
 Motor repair station
 Nursing home
 Office except in association with recreation facilities
 Petrol filling station
 Place of worship
 Pre-school
 Prescribed mining operations
 Restaurant
 Road transport terminal
 Service trade premises
 Shop of group of shops except where the gross leasable area is less than 80 square metres
 Stock sales yard
 Stock slaughter works
 Store
 Tourist accommodation
 Warehouse
 Waste reception, storage, treatment or disposal
 Wrecking yard

Public Notification

- 22 Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

LOCAL CENTRE ZONE

Introduction

The Objectives and Principles of Development Control that follow apply to the Local Centre Zone as shown on [Maps Ga/8 and 9](#). They are additional to those expressed for the whole Council area.

OBJECTIVES

- Objective 1:** A centre accommodating small-scale convenience shopping, office, medical and community facilities to serve the local community.
- Objective 2:** A centre characterised by traditional corner stores or small groups of shops located within easy walking distance of the population they serve.
- Objective 3:** A centre accommodating residential development and affordable housing in conjunction with non-residential development.
- Objective 4:** Development that contributes to the desired character of the zone.

DESIRED CHARACTER

The Local Centre Zone at Evanston South will be developed as a mixed centre providing daily purchase opportunities for the population living at Evanston Park and Evanston South. A supermarket of the order of 1000 to 1500 square metres is anticipated with the intention that this use does not dominate the function of the zone. Other supporting commercial, retail and residential activities are envisaged.

Buildings will be designed to encourage active street front and minimize the visual impact of vehicle parking from the public streets and the public open space corridor along the watercourse that runs parallel with Alexander Avenue.

Apartment and dwellings located above shops are encouraged in the zone.

The layout of buildings will be consistent with Local Centre Zone Concept Plan [Figure LCe/1](#).

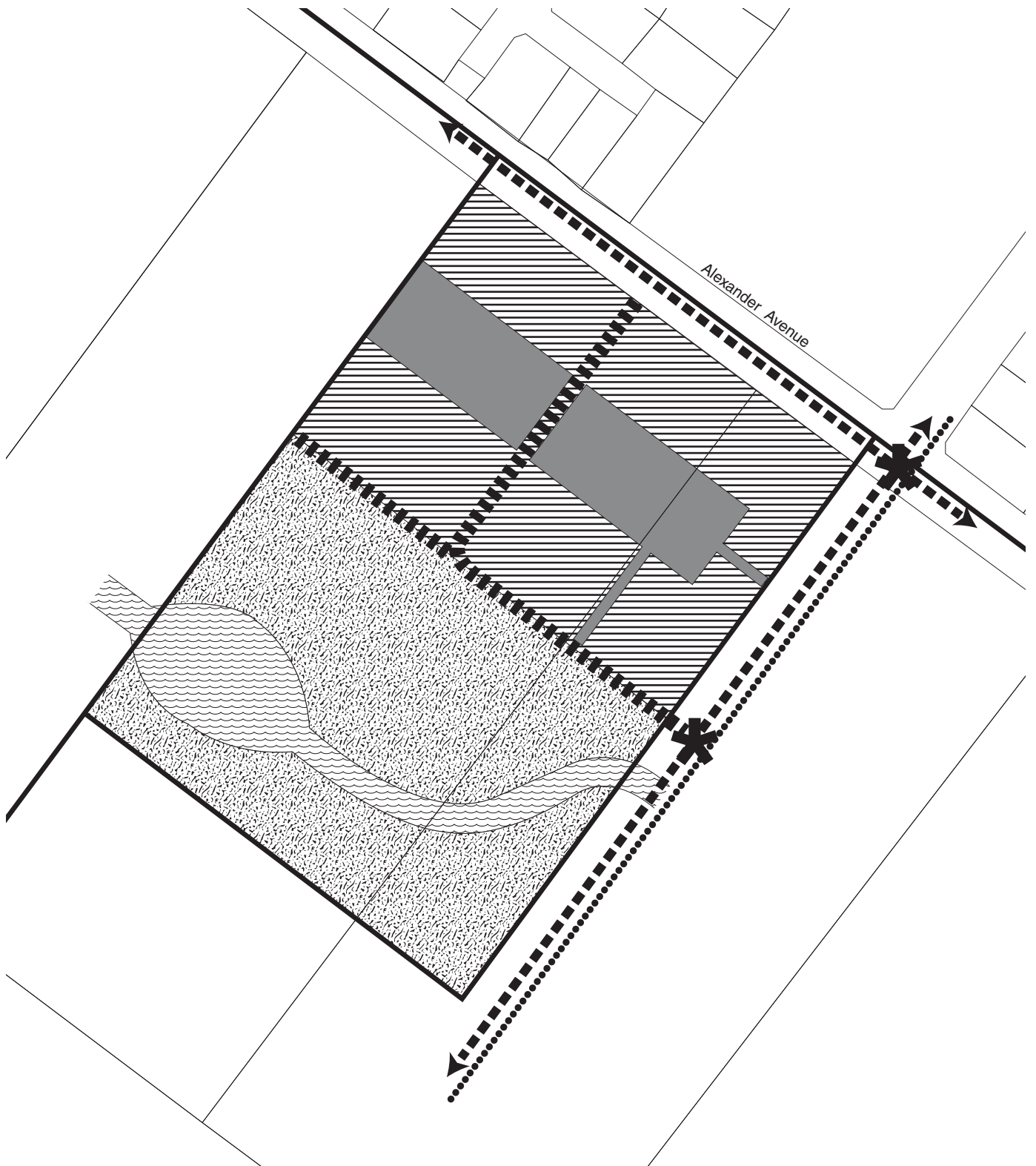
PRINCIPLES OF DEVELOPMENT CONTROL










Land Use

- 1 The following forms of development are envisaged in the zone:
 - Advertisement
 - Community facility
 - Consulting room
 - Multi storey dwellings
 - Office
 - Office and dwelling
 - Shop
 - Shop and dwelling
 - Car parking
- 2 Development listed as non-complying is generally inappropriate.
- 3 Residential development and development comprising a variety of residential and non-residential uses should only be undertaken if such development does not prejudice the operation of existing or future retail activity within the zone.

Form and Character

- 4 Development should not be undertaken unless it is consistent with the desired character for the zone and in accordance with Local Centre Zone Concept Plan [Figure LCe/1](#).



-  Car Park / Access Road
-  Retail / Commercial
-  Open Space
-  Watercourse
-  New Road
-  Collector Road
-  Pedestrian / Cycle Pathway
-  Traffic Control
-  Zone Boundary



GAWLER (CT)
LOCAL CENTRE ZONE
CONCEPT PLAN
FIGURE LCe/1
 Consolidated - 18 July 2019

Land Division

- 5 Land division in the zone is appropriate provided new allotments are of a size and configuration to ensure the objectives of the zone can be achieved.

Signs

- 6 All signs should be developed in accordance with the relevant provisions applying throughout the Council area and with the Performance Standards in [Table Ga/4](#).

- 7 The following sign types are appropriate in the zone:

Pylon or freestanding (internal or external illumination);
Flat wall
Projecting
Verandah fascia
Under verandah
Shop-front
Sandwich board
First party advertising.

- 8 The following sign types are inappropriate in the zone:

Above verandah
Bunting
Mobile
Roof
Third party advertising
Banner.

Vehicle Parking

- 9 Development should provide vehicle parking spaces in accordance with the requirements of [Table Ga/1](#), either on the site of the development, or another convenient and accessible site in the locality, providing permanent ongoing arrangements for the parking shortfall are provided on another site.

- 10 Roof top, undercroft or multi-level car parking may be developed in a manner that:

- (a) provides for convenient and safe pedestrian access and movement;
- (b) is compatible with the desired built character and is integrated with buildings and land form and screened with vegetation;
- (c) promotes personal and property safety (eg lighting and minimization of entrapment areas); and
- (d) provides lighting that is designed to meet performance requirements while minimizing energy requirements.

- 11 On site car parking shall incorporate:

- (a) a level of provision that meets the requirements of [Table Ga/1](#);
- (b) a design that meets the current Australian Design Standards for car park layout, commercial vehicle loading, disabled parking and bicycle loading;
- (c) car parking spaces for people with disabilities in locations which are convenient and accessible to the building or use on the site;
- (d) secure and convenient spaces for bicycles;

- (e) landscaped design to shade vehicles and enhance the appearance of the car park (refer Figure 1 below);
- (f) opportunities for interconnection of access driveways with adjoining sites or car parks to facilitate shared use;
- (g) limited number of crossover points onto public roads;
- (h) design elements that promote personal and property safety (eg lighting, surveillance and minimisation of entrapment areas); and
- (i) lighting designed to meet performance requirements while minimizing energy requirements; and
- (j) incorporate stormwater infiltration techniques (refer Figure 2 below).

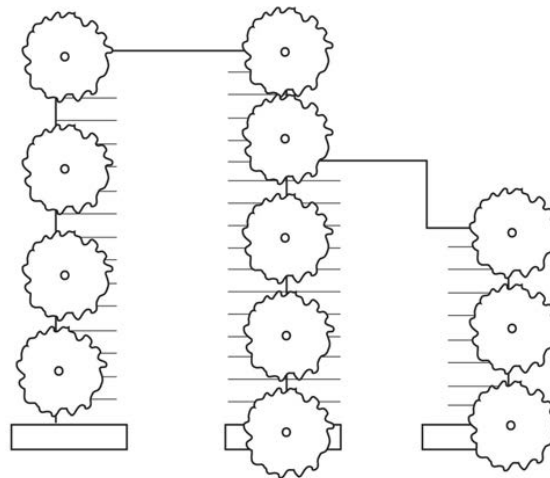


Figure 1: Pattern and Preferred Location for Shade Trees in Car Parks

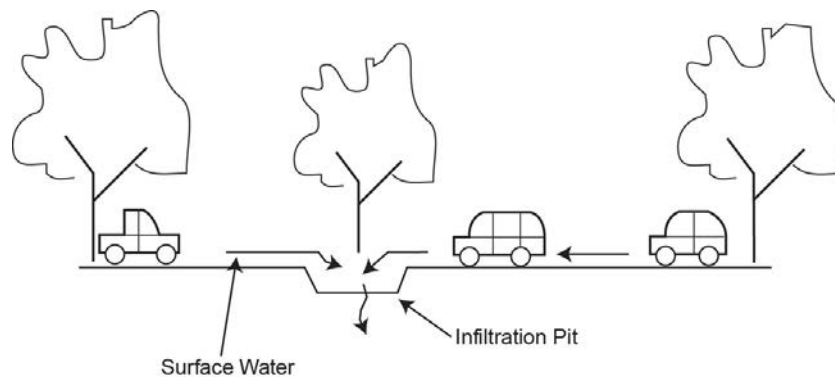


Figure 2: On-Site Stormwater Detention and Infiltration

- 12 Car parking should be coordinated with other parking sites and preferably located at the rear of buildings which incorporate clearly defined pedestrian links.

Vending Machines and Automatic Teller Machines

- 13 Siting of vending machines and automatic teller machines should have regard to:
- (a) consistency with relevant zone provisions;
 - (b) maintenance of character and continuity of activity along street frontages;

- (c) maintenance of unimpeded pedestrian movement. Where necessary sufficient set-back should be provided from front boundaries to accommodate queues.

14 Advertising displays relating to vending machines and automatic teller machines should be restrained in size.

PROCEDURAL MATTERS

Complying Development

15 Complying developments are prescribed in schedule 4 of the *Development Regulations 2008*. In addition, the following forms of development (except where the development is non-complying) are complying:

A change of use to a shop, office, consulting room or any combination of these uses where all of the following are achieved:

- (a) the area to be occupied by the proposed development is located in an existing building and is currently used as a shop, office, consulting room or any combination of these uses;
- (b) the building is not a State heritage place;
- (c) it will not involve any alterations or additions to the external appearance of a local heritage place as viewed from a public road or public space;
- (d) if the proposed change of use is for a shop that primarily involves the handling and sale of foodstuffs, it achieves either (i) or (ii):
 - (i) all of the following:
 - (A) areas used for the storage and collection of refuse are sited at least 10 metres from any Residential Zone boundary or a dwelling (other than a dwelling directly associated with the proposed shop);
 - (B) if the shop involves the heating and cooking of foodstuffs in a commercial kitchen and is within 30 metres of any Residential Zone boundary or a dwelling (other than a dwelling directly associated with the proposed shop), an exhaust duct and stack (chimney) exists or is capable of being installed for discharging exhaust emissions;
 - (ii) the development is the same or substantially the same as a development, which has previously been granted development approval under the *Development Act 1993* or any subsequent Act and Regulations, and the development is to be undertaken and operated in accordance with the conditions attached to the previously approved development;
- (e) if the change in use is for a shop with a gross leasable floor area greater than 250 square metres and has direct frontage to an arterial road, it achieves either (i) or (ii):
 - (i) the primary vehicle access (being the access where the majority of vehicles access/ egress the site of the proposed development) is from a road that is not an arterial road;
 - (ii) the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared;
- (f) off-street vehicular parking is provided in accordance with the rate(s) specified in [Table Ga/1 - Car Parking Requirements](#) to the nearest whole number, except in any one or more of the following circumstances:
 - (i) the building is a local heritage place;

- (ii) the development is the same or substantially the same as a development, which has previously been granted development approval under the *Development Act 1993* or any subsequent Act and Regulations, and the number and location of parking spaces is the same or substantially the same as that which was previously approved;
- (iii) the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared.

Non-complying Development

- 16** Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

Above canopy signs
Bunting
Fuel depot
Horticulture
Industry
Motor repair station
Petrol filling station
Public service depot
Road transport terminal
Roof sign
Service trade premises
Warehouse
Waste reception, storage, treatment or disposal
Wrecking yard.

Public Notification

- 17** Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

NEIGHBOURHOOD CENTRE ZONE

Introduction

In addition to the Council Wide policies, the Neighbourhood Centre Zone policies apply to the areas shown in [Map Ga/9](#).

OBJECTIVES

- Objective 1:** A centre providing a range of shopping, community, business and recreational facilities for the surrounding neighbourhood.
- Objective 2:** A centre that provides the main focus of business and community life outside a district centre, and provides for the more frequent and regularly recurring needs of a community.
- Objective 3:** A centre accommodating residential development in conjunction with non-residential development.
- Objective 4:** Development that contributes to the desired character of the zone.

DESIRED CHARACTER

The purpose of the Zone is to serve as a centre for the daily to weekly shopping and business needs for the surrounding areas. It is envisaged that the Centre will include a supermarket, specialty shops, community uses and facilities, commercial, leisure and consulting rooms to service its catchment population to the south of the existing Town Centre.

Shops (excluding retail showrooms or bulky goods outlets) within the Zone should have a maximum gross leasable floor area in the order of 5900 square metres.

The centre will also provide for residential development where in conjunction with non-residential land uses.

Building design and site development will be well-integrated with the locality with a focus on crime prevention and not detract from the amenity of nearby residential areas or the adjacent education precinct.

A network of clearly recognisable pedestrian and bicycle linkages will be provided within the Centre, to the schools, the racecourse, the railway stations, Barnet Street and across Main North Road. Development will support public transport through the provision of bus shelters, appropriate access and pavement structure.

A uniform approach to building materials and colours, advertising styles and landscaping will achieve an attractive appearance along the main approach road to Gawler. Buildings will be sited close to Main North Road in order to activate the road frontage and to establish a visual gateway formed by articulated buildings sited in order to avoid continuous facades along the Main North Road frontage. Space will be provided to the rear of buildings for the loading and unloading of goods, storage and waste collection areas, the circulation of vehicles and parking.

Landscaping will be provided throughout the zone to provide an attractive precinct with dense landscape plantings required between buildings and Barnet Street.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the zone:

- Bank
- Bulky goods outlet
- Child care facility

Consulting room
 Dwelling in conjunction with non-residential land use
 Educational establishment
 Health centre
 Library
 Office
 Petrol filling station
 Place of worship
 Playing field
 Pre-school
 Primary school
 Recreation area
 Restaurant
 Retail showroom
 Shop
 Supermarket
 Tourist accommodation.

- 2 Development listed as non-complying is generally inappropriate.
- 3 Residential development and development comprising a variety of residential and non-residential uses should only be undertaken if such development does not prejudice the operation of existing or future retail activity within the zone.
- 4 Residential development and other sensitive uses should be designed and sited to minimise adverse impacts arising from nearby non-residential activities and major traffic routes.
- 5 Development south of the realigned Barnet Street should consist of low intensity activities such as offices, consulting rooms or tourist accommodation.

Form and Character

- 6 Development should not be undertaken unless it is consistent with the desired character for the zone.
- 7 Development should be undertaken in accordance with the Structure Plan [Fig NCe/1](#).
- 8 Development should facilitate the realignment of Barnet Street with Main North Road.
- 9 The realigned Barnet Street should intersect with Main North Road at a location no closer to the Potts Road/Para Road intersection than Morrow Avenue.
- 10 Dwellings should be located only behind or above non-residential uses on the same allotment.









Land Division

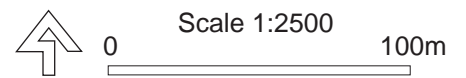
- 11 Land division in the zone is appropriate provided new allotments are of a size and configuration to ensure the objectives of the zone can be achieved.

Design and Appearance

- 12 The design of a building may be of a contemporary nature and exhibit an innovative style provided the overall form is sympathetic to the scale of development in the locality and with the context of its setting with regard to shape, size, materials and colour.
- 13 Development within the zone should provide:
 - (a) public spaces such as malls, plazas and courtyards
 - (b) street furniture, including lighting, signs, litter bins, seats and bollards, that is sited and designed to complement the desired character



- | | | | |
|---|---|---|-------------------------|
|  | Neighbourhood Centre |  | Secondary Arterial Road |
|  | Landscaping |  | Local Road |
|  | Road Closure |  | Road Realignment |
|  | Intersection treatment | | |
|  | Signalised Intersection/
Pedestrian & Vehicular Movement | | |



**GAWLER (CT)
STRUCTURE/SITE
LAYOUT PLAN
FIGURE NCe/1**

Consolidated - 18 July 2019

- (c) unobtrusive facilities for the storage and removal of waste materials
 - (d) public facilities including toilets, infant changing facilities for parents, seating, litter bins, telephones and community information boards
 - (e) access for public and community transport and sheltered waiting areas for passengers
 - (f) lighting for pedestrian paths, buildings and associated areas
 - (g) a single landscaping theme
 - (h) safe and secure bicycle parking.
- 14** Buildings should be designed and sited to avoid creating extensive areas of uninterrupted walling facing areas exposed to public view.
- 15** Buildings should be oriented towards the primary road frontage and not towards internal parking areas.
- 16** Buildings should be designed to reduce their visual bulk and visual dominance through design elements such as:
- (a) articulation, including but not limited to the use of verandahs
 - (b) colour and detailing
 - (c) small vertical and horizontal components
 - (d) design and placing of windows
 - (e) variations to facades.
- 17** Structures located on the roofs of buildings to house plant and equipment should form an integral part of the building design in relation to external finishes, shaping and colours.
- 18** Building design should emphasise pedestrian entry points to provide perceptible and direct access from public street frontages and vehicle parking areas.
- 19** The setback of buildings from public roads should:
- (a) be similar to, or compatible with, setbacks of buildings on adjoining land and other buildings in the locality
 - (b) contribute positively to the streetscape character of the locality
 - (c) not result in or contribute to a detrimental impact upon the function, appearance or character of the locality.
- 20** Outdoor lighting should not result in light spillage on adjacent land.

Vehicle Parking

- 21** Vehicle parking areas should be sited and designed in a manner that will:
- (a) facilitate safe and convenient pedestrian linkages to the development and areas of significant activity or interest in the vicinity of the development
 - (b) include safe pedestrian and bicycle linkages that complement the overall pedestrian and cycling network
 - (c) not inhibit safe and convenient traffic circulation

- (d) result in minimal conflict between customer and service vehicles
- (e) avoid the necessity to use public roads when moving from one part of a parking area to another
- (f) minimise the number of vehicle access points to public roads
- (g) avoid the necessity for backing to and from public roads
- (h) where reasonably possible, provide the opportunity for shared use of car parking and integration of car parking areas with adjoining development to reduce the total extent of vehicle parking areas and the requirement for access points
- (i) not dominate the character and appearance of a centre when viewed from public roads and spaces.

Landscaping

22 Development should incorporate open space and landscaping and minimise hard paved surfaces in order to:

- (a) complement built form and reduce the visual impact of larger buildings (eg taller and broader plantings against taller and bulkier building components)
- (b) enhance the appearance of road frontages
- (c) screen service yards, loading areas and outdoor storage areas
- (d) minimise maintenance and watering requirements
- (e) enhance and define outdoor spaces, including car parking areas
- (f) maximise shade and shelter
- (g) assist in climate control within and around buildings
- (h) minimise heat absorption and reflection
- (i) maintain privacy
- (j) maximise stormwater re-use
- (k) complement existing vegetation, including native vegetation.
- (l) contribute to the viability of ecosystems and species
- (m) promote water and biodiversity conservation
- (n) provide screening of loading areas as viewed from Barnet Street.

23 Dense landscape screening should be provided along the Barnet Street boundary of the zone.

24 Landscaping within the zone should retain existing vegetation where it contributes to the amenity of the area.

Stormwater

25 Development should be sited and designed to:

- (a) minimise surface water runoff

- (b) not obstruct a watercourse
 - (c) prevent soil erosion and water pollution
 - (d) protect stormwater from pollution sources
 - (e) protect and enhance natural water flows required to meet the needs of the natural environment
 - (f) protect water quality by providing adequate separation distances from watercourses and other water bodies
 - (g) not contribute to an increase in salinity levels
 - (h) avoid the water logging of soil or the release of toxic elements
 - (i) maintain natural hydrological systems and not adversely affect:
 - (i) the quantity and quality of groundwater
 - (ii) the depth and directional flow of groundwater
 - (iii) the quality and function of natural springs.
- 26** Development should include stormwater management systems to mitigate peak flows and manage the rate and duration of stormwater discharges from the site to ensure downstream systems are not overloaded.
- 27** Development should include stormwater management systems to minimise the discharge of sediment, suspended solids, organic matter, nutrients, bacteria, litter and other contaminants to the stormwater system.
- 28** Stormwater management systems should maximise the potential for stormwater harvesting and reuse, including aquifer recharge, either on-site or as close as practicable to the source.
- 29** Where not detained or disposed on site, stormwater should be drained to a public stormwater disposal system.

Outdoor Advertising

- 30** Freestanding advertisements and/or advertising hoardings should be limited to only one primary advertisement per site or complex.
- 31** Freestanding advertisements and/or advertising hoardings for multiple-business tenancy complexes should:
- (a) incorporate the name or nature of each business or activity within the site or complex in a single advertisement
 - (b) be integrally designed and mounted below the more predominant main complex or site identity advertisement.

PROCEDURAL MATTERS

Complying Development

- 32** Complying developments are prescribed in schedule 4 of the *Development Regulations 2008*. In addition, the following forms of development (except where the development is non-complying) are complying:

A change of use to a shop, office, consulting room or any combination of these uses where all of the following are achieved:

- (a) the area to be occupied by the proposed development is located in an existing building and is currently used as a shop, office, consulting room or any combination of these uses;
- (b) the building is not a State heritage place;
- (c) it will not involve any alterations or additions to the external appearance of a local heritage place as viewed from a public road or public space;
- (d) if the proposed change of use is for a shop that primarily involves the handling and sale of foodstuffs, it achieves either (i) or (ii):
 - (i) all of the following:
 - (A) areas used for the storage and collection of refuse are sited at least 10 metres from any Residential Zone boundary or a dwelling (other than a dwelling directly associated with the proposed shop);
 - (B) if the shop involves the heating and cooking of foodstuffs in a commercial kitchen and is within 30 metres of any Residential Zone boundary or a dwelling (other than a dwelling directly associated with the proposed shop), an exhaust duct and stack (chimney) exists or is capable of being installed for discharging exhaust emissions;
 - (ii) the development is the same or substantially the same as a development, which has previously been granted development approval under the *Development Act 1993* or any subsequent Act and Regulations, and the development is to be undertaken and operated in accordance with the conditions attached to the previously approved development;
- (e) if the change in use is for a shop with a gross leasable floor area greater than 250 square metres and has direct frontage to an arterial road, it achieves either (i) or (ii):
 - (i) the primary vehicle access (being the access where the majority of vehicles access/ egress the site of the proposed development) is from a road that is not an arterial road;
 - (ii) the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared;
- (f) off-street vehicular parking is provided in accordance with the rate(s) specified in [Table Ga/1 - Car Parking Requirements](#) to the nearest whole number, except in any one or more of the following circumstances:
 - (i) the building is a local heritage place;
 - (ii) the development is the same or substantially the same as a development, which has previously been granted development approval under the *Development Act 1993* or any subsequent Act and Regulations, and the number and location of parking spaces is the same or substantially the same as that which was previously approved;
 - (iii) the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared.

Non-complying Development

- 33** Development (including building work, a change in the use of land, or division of an allotment) for the following is **non-complying**:

Dwelling except where in conjunction with a non-residential development
Fuel depot
Horticulture
Industry
Major public service depot
Motor repair station
Residential flat building
Road transport terminal
Store
Warehouse
Waste reception, storage, treatment or disposal except in the form of stormwater management
Wrecking yard

Public Notification

- 34** Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

NEIGHBOURHOOD CENTRE (EVANSTON GARDENS) ZONE

Introduction

In addition to the Council Wide policies, the Neighbourhood Centre (Evanston Gardens) Zone policies apply to the area as shown on [Map Ga/8](#).

OBJECTIVES

- Objective 1:** A zone providing a range of shopping, community, business and recreational facilities for the surrounding neighbourhood.
- Objective 2:** A zone that provides the main focus of business and community life outside a district centre, and provides for the more frequent and regularly recurring needs of the community.
- Objective 3:** A zone accommodating residential development and affordable housing in conjunction with non-residential development.
- Objective 4:** Development that contributes to the desired character of the policy area.

DESIRED CHARACTER

The Zone will be developed as a mixed use centre including community facilities and services for the Evanston Gardens community and nearby rural areas. A supermarket of the order of 2500 to 3500 square metres is anticipated, with the intention that this use does not dominate the function of the zone. Other supporting commercial, retail and residential activities are envisaged.

The form of the Centre will be modeled on a traditional town centre with buildings for retail and business use addressing Angle Vale Road with no or minimal setback. Generally all vehicle parking will be to the rear of buildings and individual uses provide car parking so that it integrates with an adjoining car parking to maximize efficiency and safety, and reduce crossing points to roads.

A central public space either side of Angle Vale Road will be provided to give the Centre individual identity. This space will be designed for shared use and community events.

Apartment and *Shop Top* housing is encouraged in the zone.

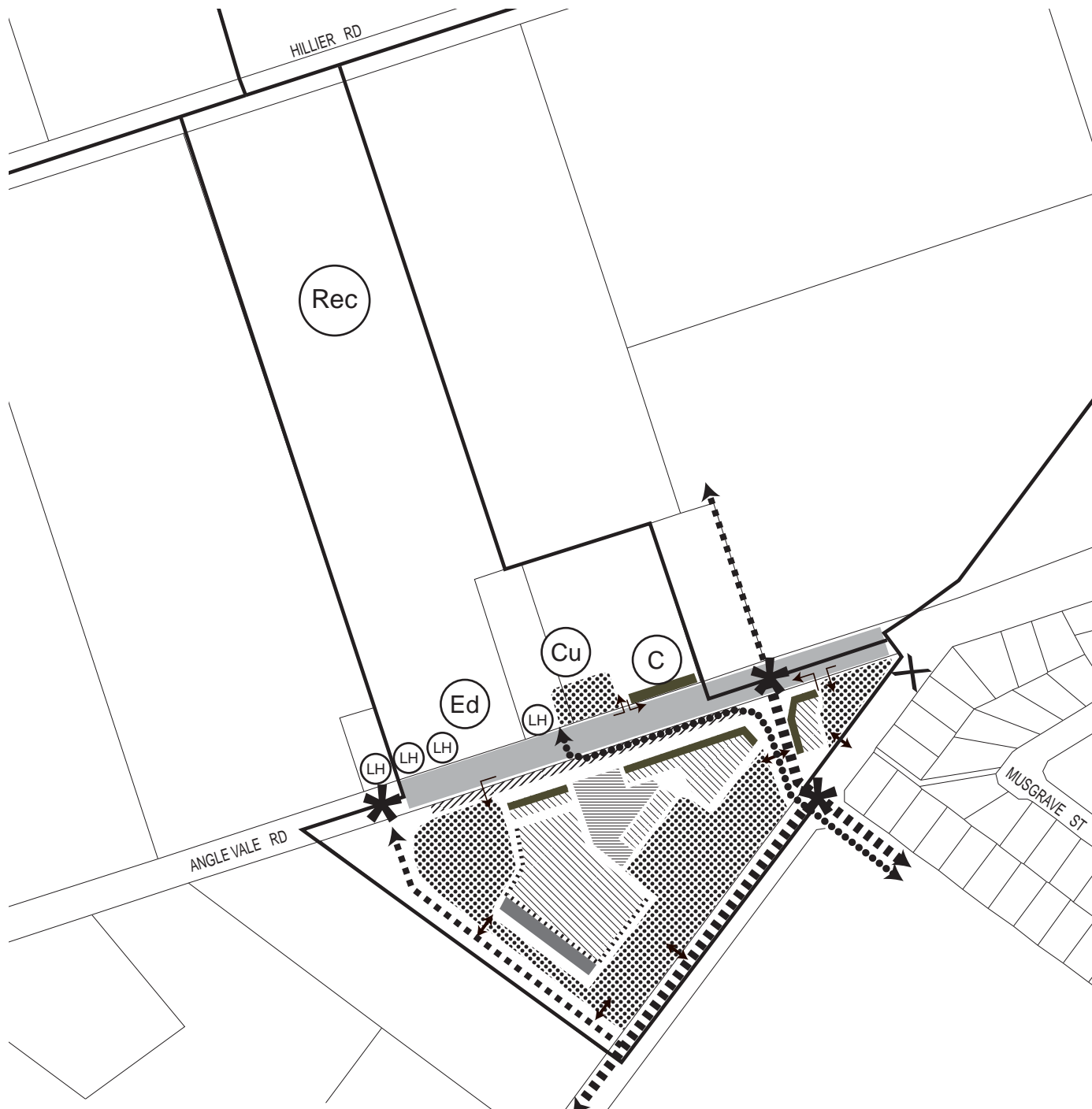
The layout of buildings will be consistent with Neighbourhood Centre (Evanston Gardens) Zone Concept Plan [Figure NCe\(EG\)/1](#).

















PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the zone:

- Bank
- Bulky goods outlet
- Child care facility
- Consulting room
- Dwelling in conjunction with non-residential land use
- Educational establishment
- Health care
- Library
- Office
- Place of worship
- Playing field
- Pre-school
- Primary school
- Recreation area



-  Car Parking and Landscaping
-  Retail
-  Town Square
-  Traffic Main Street Environment
-  Buildings to Address Public Road
-  Road Widening Requirement (4 metres)
-  Service Deliveries
-  Collector Road
-  Distributor Road
-  Major Pedestrian Route and Crossing
-  Traffic Control
-  Road Closure
-  Vehicle Access (all movements)
-  Vehicle Access (left in/left out only)
-  Vehicle Access (left in only)
-  Articulate Building Facade

-  Commercial
-  Community
-  Education
-  Recreation
-  Local Heritage
-  Zone Boundary

0 m 100 200



**GAWLER (CT)
NEIGHBOURHOOD CENTRE
(EVANSTON GARDENS) ZONE
CONCEPT PLAN
FIGURE NCE(EG)/1**

Restaurant
 Bulky goods outlet and retail showroom
 Shop
 Supermarket
 Tourist accommodation.

- 2 Development listed as non-complying is generally inappropriate.
- 3 Residential development and development comprising a variety of residential and non-residential uses should only be undertaken if such development does not prejudice the operation of existing and future retail activity within the zone.
- 4 Residential development and other sensitive uses should be designed and sited to minimise adverse impacts arising from nearby non-residential activities and major traffic routes.

Form and Character

- 5 Development should not be undertaken unless it is consistent with the desired character and in accordance with Neighbourhood Centre (Evanston Gardens) Zone Concept Plan [Figure NCe\(EG\)/1](#).

Land Division

- 6 Land division in the zone is appropriate provided new allotments are of a size and configuration to ensure the objectives of the zone can be achieved.

Vehicle Parking

- 7 Development should provide vehicle parking spaces in accordance with the requirements of [Table Ga/1](#), either on the site of the development, or another convenient and accessible site in the locality, providing permanent ongoing arrangements for the parking shortfall are provided on another site.
- 8 Roof top, undercroft or multi-level car parking may be developed in a manner that:
 - (a) provides for convenient and safe pedestrian access and movement;
 - (b) is compatible with the desired built character and is integrated with buildings and land form and screened with vegetation;
 - (c) promotes personal and property safety (eg lighting and minimization of entrapment areas); and
 - (d) provides lighting that is designed to meet performance requirements while minimizing energy requirements.
- 9 On site car parking shall incorporate:
 - (a) a level of provision that meets the requirements of [Table Ga/1](#);
 - (b) a design that meets the current Australian Design Standards for car park layout, commercial vehicle loading, disabled parking and bicycle loading;
 - (c) car parking spaces for people with disabilities in locations which are convenient and accessible to the building or use on the site;
 - (d) secure and convenient spaces for bicycles;
 - (e) landscaped design to shade vehicles and enhance the appearance of the car park (refer Figure 1 below);

- (f) opportunities for interconnection of access driveways with adjoining sites or car parks to facilitate shared use;
- (g) limited number of crossover points onto public roads;
- (h) design elements that promote personal and property safety (eg lighting, surveillance and minimisation of entrapment areas); and
- (i) lighting designed to meet performance requirements while minimizing energy requirements; and
- (j) incorporate stormwater infiltration techniques (refer Figure 2 below).

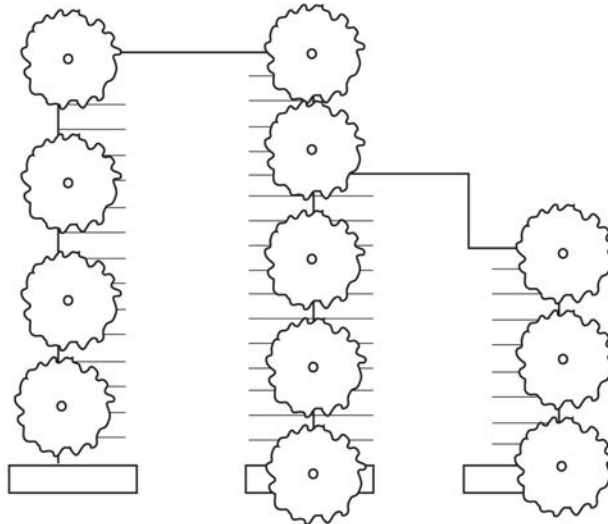


Figure 1: Pattern and Preferred Location for Shade Trees in Car Parks

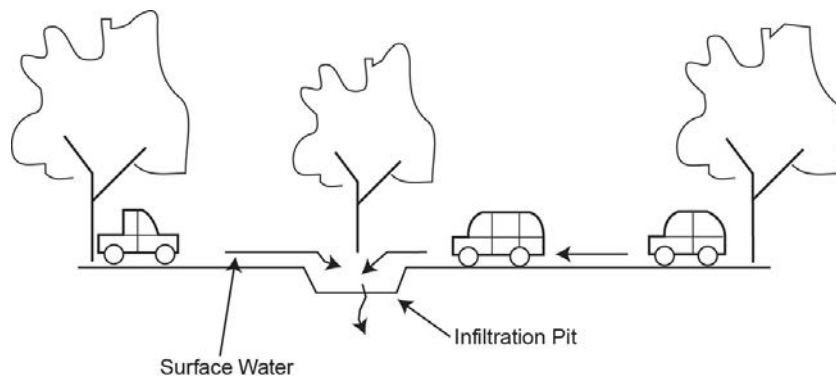


Figure 2: On-Site Stormwater Detention and Infiltration

- 10** Car parking should be coordinated with other parking sites and preferably located at the rear of buildings which incorporate clearly defined pedestrian links.

Vending Machines and Automatic Teller Machines

- 11** Siting of vending machines and automatic teller machines should have regard to:
- (a) consistency with relevant zone provisions;
 - (b) maintenance of character and continuity of activity along street frontages;
 - (c) maintenance of unimpeded pedestrian movement. Where necessary sufficient set-back should be provided from front boundaries to accommodate queues.

- 12 Advertising displays relating to vending machines and automatic teller machines should be restrained in size.

PROCEDURAL MATTERS

Complying Development

- 13 Complying developments are prescribed in schedule 4 of the *Development Regulations 2008*. In addition, the following forms of development (except where the development is non-complying) are complying:

A change of use to a shop, office, consulting room or any combination of these uses where all of the following are achieved:

- (a) the area to be occupied by the proposed development is located in an existing building and is currently used as a shop, office, consulting room or any combination of these uses;
- (b) the building is not a State heritage place;
- (c) it will not involve any alterations or additions to the external appearance of a local heritage place as viewed from a public road or public space;
- (d) if the proposed change of use is for a shop that primarily involves the handling and sale of foodstuffs, it achieves either (i) or (ii):
 - (i) all of the following:
 - (A) areas used for the storage and collection of refuse are sited at least 10 metres from any Residential Zone boundary or a dwelling (other than a dwelling directly associated with the proposed shop);
 - (B) if the shop involves the heating and cooking of foodstuffs in a commercial kitchen and is within 30 metres of any Residential Zone boundary or a dwelling (other than a dwelling directly associated with the proposed shop), an exhaust duct and stack (chimney) exists or is capable of being installed for discharging exhaust emissions;
 - (ii) the development is the same or substantially the same as a development, which has previously been granted development approval under the *Development Act 1993* or any subsequent Act and Regulations, and the development is to be undertaken and operated in accordance with the conditions attached to the previously approved development;
- (e) if the change in use is for a shop with a gross leasable floor area greater than 250 square metres and has direct frontage to an arterial road, it achieves either (i) or (ii):
 - (i) the primary vehicle access (being the access where the majority of vehicles access/ egress the site of the proposed development) is from a road that is not an arterial road;
 - (ii) the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared;
- (f) off-street vehicular parking is provided in accordance with the rate(s) specified in [Table Ga/1 - Car Parking Requirements](#) to the nearest whole number, except in any one or more of the following circumstances:
 - (i) the building is a local heritage place;

- (ii) the development is the same or substantially the same as a development, which has previously been granted development approval under the *Development Act 1993* or any subsequent Act and Regulations, and the number and location of parking spaces is the same or substantially the same as that which was previously approved;
- (iii) the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared.

Non-complying Development

- 14** Development (including building work, a change in the use of the land, or division of an allotment) for the following is **non-complying**:

Above canopy signs
Bunting
Dwelling except where in conjunction with a non-residential development
Fuel depot
Horticulture
Industry
Major public service depot
Motor repair station
Residential flat building
Road transport terminal
Roof sign
Store
Warehouse
Waste reception, storage, treatment or disposal except in the form of stormwater management
Wrecking yard.

Public Notification

- 15** Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

RECREATION ZONE

Introduction

In addition to the Council Wide policies, the Recreation Zone policies apply to the areas shown in [Maps Ga/5, 8 and 9](#).

OBJECTIVES

- Objective 1:** A zone accommodating sporting, entertainment, cultural and recreational activities, tourist accommodation and associated spectator and administrative facilities.
- Objective 2:** Development of integrated recreational areas and facilities that accommodate a range of activities accessible to the community.
- Objective 3:** Buildings, facilities and car parks located and designed to blend in with existing or additional trees, vegetation and landscaping.
- Objective 4:** A zone accommodating district level capture, storage and re-use of stormwater.
- Objective 5:** Development that contributes to the desired character of the zone.

DESIRED CHARACTER

The Recreation Zone comprises the Gawler Racecourse and environs.

The zone primarily accommodates the Gawler Racecourse together with activities which are ancillary to or compatible with the racecourse, including race meetings, horse keeping, club activities, catering, exhibitions, veterinary services and facilities for the training and exercise of horses. The racecourse will continue to form an important area for the staging of equestrian events. Opportunities exist to support the important role that horse trails and equestrian events have played in Gawler.

In addition to this core function the zone will accommodate allied business activities and events such as function centre, licensed premises and tourist accommodation, as well as providing the setting for special events and showgrounds. The racecourse will also provide opportunity for education and vocation links with the adjacent Gawler High School.

Building design and site development will be well-integrated with the locality with a focus on crime prevention and not detract from the amenity of nearby residential areas or the adjacent education precinct. Buildings and structures will be concentrated on the western side or central portion of the race track as distinct from the Adelaide Road frontage. Clearly recognisable pedestrian and bicycle linkages will be provided to the adjacent schools, neighbourhood centre, Barnet Street and the railway stations.

In addition to accommodating these uses, the zone will provide and retain a significant amenity function to the township by facilitating vistas over open and landscaped space from Adelaide Road and areas for general recreational purposes.

Opportunity exists for the capture, storage and re-use stormwater from across the District in the centre of the racecourse. Any storage and treatment areas will be located central to the race track and will enhance the amenity of the open space.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the zone:

- Active outdoor recreation
- Car parking
- Clubrooms associated with sports facility

Community centre
 Community hall
 Entertainment, cultural, function and exhibition facility
 Golf course
 Hard surface court
 Horse stabling and agistment
 Indoor and outdoor recreation facility
 Licensed premise
 Lighting for night use of facilities
 Meeting hall
 Office associated with community or recreation facility
 Oval
 Playground
 Racecourse

Shops or groups of shops where:

- (a) ancillary to recreation development and
- (b) with a gross leasable area of less than 80 square metres

Showground
 Sports grounds and associated facility
 Special event
 Spectator and administrative facilities ancillary to recreation development
 Stadium
 Swimming pool
 Tourist accommodation.

- 2 Development listed as non-complying is generally inappropriate.
- 3 A shop or group of shops should only be developed in this zone where:
 - (a) it is ancillary to recreation and sport development
 - (b) the total gross leasable area is less than 80 square metres.

Form and Character

- 4 Development should not be undertaken unless it is consistent with the desired character for the zone.
- 5 Strong thematic landscaping should be instituted on individual sites to improve the landscape, provide shade and shelter, create interest, provide habitat, retain existing native vegetation, use locally indigenous plant species in plantings where possible and define different activity areas.
- 6 All car parking areas should be shaded and screened with vegetation to improve the amenity of the zone.

Land Division

- 7 Land division or the rearrangement of existing allotment boundaries should take place as part of a coordinated development scheme, or as a rationalisation of land holdings that is designed to allow more efficient and economic use of land consistent with the objectives for the zone.
- 8 Additional allotments should only be created where it would support an envisaged use in the zone.

Design and Appearance

- 9 Buildings should be designed and sited to avoid creating extensive areas of uninterrupted walling facing areas exposed to public view.

- 10 Buildings should be designed to reduce their visual bulk and provide visual interest through design elements such as:
 - (a) articulation
 - (b) colour and detailing
 - (c) small vertical and horizontal components
 - (d) design and placing of windows
 - (e) variations to facades.
- 11 Building form should not unreasonably restrict existing views available from neighbouring properties and public spaces.
- 12 Structures located on the roofs of buildings to house plant and equipment should form an integral part of the building design in relation to external finishes, shaping and colours.
- 13 Building design should emphasise pedestrian entry points to provide perceptible and direct access from public street frontages and vehicle parking areas.
- 14 Development should be designed and sited so that outdoor storage, loading and service areas are screened from public view by an appropriate combination of built form, solid fencing and/or landscaping.
- 15 Outdoor lighting should not result in light spillage on adjacent land.

Vehicle Parking

- 16 Vehicle parking areas should be sited and designed in a manner that will:
 - (a) facilitate safe and convenient pedestrian linkages to the development and areas of significant activity or interest in the vicinity of the development
 - (b) include safe pedestrian and bicycle linkages that complement the overall pedestrian and cycling network
 - (c) not inhibit safe and convenient traffic circulation
 - (d) result in minimal conflict between customer and service vehicles
 - (e) avoid the necessity to use public roads when moving from one part of a parking area to another
 - (f) minimise the number of vehicle access points to public roads
 - (g) avoid the necessity for backing to and from public roads
 - (h) where reasonably possible, provide the opportunity for shared use of car parking and integration of car parking areas with adjoining development to reduce the total extent of vehicle parking areas and the requirement for access points
 - (i) not dominate the character and appearance of a centre when viewed from public roads and spaces
 - (j) provide landscaping that will shade and enhance the appearance of the vehicle parking areas.

Landscaping

- 17 Development should incorporate open space and landscaping in order to:
- (a) complement built form and reduce the visual impact of larger buildings (eg taller and broader plantings against taller and bulkier building components)
 - (b) enhance the appearance of road frontages
 - (c) screen service yards, loading areas and outdoor storage areas
 - (d) minimise maintenance and watering requirements
 - (e) enhance and define outdoor spaces, including car parking areas
 - (f) provide shade and shelter
 - (g) assist in climate control within buildings
 - (h) maintain privacy
 - (i) maximise stormwater re-use
 - (j) complement existing native vegetation
 - (k) contribute to the viability of ecosystems and species
 - (l) promote water and biodiversity conservation.

Stormwater

- 18 Development should be sited and designed to:
- (a) minimise surface water runoff
 - (b) not obstruct a watercourse
 - (c) prevent soil erosion and water pollution
 - (d) protect stormwater from pollution sources
 - (e) protect and enhance natural water flows required to meet the needs of the natural environment
 - (f) protect water quality by providing adequate separation distances from watercourses and other water bodies
 - (g) not contribute to an increase in salinity levels
 - (h) avoid the water logging of soil or the release of toxic elements
 - (i) maintain natural hydrological systems and not adversely affect:
 - (i) the quantity and quality of groundwater
 - (ii) the depth and directional flow of groundwater
 - (iii) the quality and function of natural springs.
- 19 Development should include stormwater management systems to mitigate peak flows and manage the rate and duration of stormwater discharges from the site to ensure downstream systems are not overloaded.

- 20** Development should include stormwater management systems to minimise the discharge of sediment, suspended solids, organic matter, nutrients, bacteria, litter and other contaminants to the stormwater system.
- 21** Stormwater management systems should preserve natural drainage systems, including the associated environmental flows.
- 22** Stormwater management systems should:
- (a) maximise the potential for stormwater harvesting and reuse, including aquifer recharge, either on-site or as close as practicable to the source
 - (b) utilise, but not be limited to, one or more of the following harvesting methods:
 - (i) the collection of roof water in tanks
 - (ii) the discharge to open space, landscaping or garden areas, including strips adjacent to car parks
 - (iii) the incorporation of detention and retention facilities
 - (iv) aquifer recharge.
- 23** Where it is not practicable to detain or dispose of stormwater on site, only clean stormwater runoff should enter the public stormwater drainage system.
- 24** Artificial wetland systems, including detention and retention basins, should be sited and designed to:
- (a) ensure public health and safety is protected
 - (b) minimise potential public health risks arising from the breeding of mosquitoes.

Outdoor Advertising

- 25** Freestanding advertisements and/or advertising hoardings should be limited to only one primary advertisement per site or complex.
- 26** Freestanding advertisements and/or advertising hoardings for multiple-business tenancy complexes should:
- (a) incorporate the name or nature of each business or activity within the site or complex in a single advertisement
 - (b) be integrally designed and mounted below the more predominant main complex or site identity advertisement.

PROCEDURAL MATTERS

Complying Development

- 27** Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

Non-complying Development

- 28** Development (including building work, a change in the use of land, or division of an allotment) for the following is **non-complying**:

Consulting room
Crematorium
Dwelling

Fuel depot
 Hospital
 Industry
 Intensive animal keeping except where stabling of horses
 Motor repair station
 Nursing home
 Office except where associated with community or recreation facilities
 Petrol filling station
 Place of worship
 Public service depot
 Residential flat building
 Road transport terminal
 Service trade premises
 Shop or group of shops except where the gross leasable area is less than 80 square metres
 Stock sales yard
 Stock slaughter works
 Store
 Warehouse
 Waste reception, storage, treatment or disposal except detention and treatment of stormwater
 Wrecking yard

Public Notification

- 29 Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development (except where the development is non-complying) are designated:

Category 1

Active outdoor recreation
 Car parking
 Clubrooms associated with sports facility
 Community centre
 Community hall
 Entertainment, cultural, function and exhibition facility
 Golf course
 Hard surface court
 Indoor and outdoor recreation facility
 Licensed premise
 Meeting hall
 Office associated with community or recreation facility
 Oval
 Playground
 Racecourse

Shops or groups of shops where:

- (a) ancillary to recreation development and
- (b) with a gross leasable area of less than 80 square metres

Showground
 Sports grounds and associated facility
 Special event
 Spectator and administrative facilities ancillary to recreation development
 Stadium
 Swimming pool
 Tourist accommodation

Category 2

Lighting for night use of facilities

RESIDENTIAL ZONE

Introduction

In addition to the Council Wide policies, the Residential Zone and Policy Area policies apply to the areas shown in:

Gawler Health Services Helipad: [Figure Hel/1](#)
 Residential Zone: [Maps Ga/3, 5, 6, 8, 9, 11, 12](#)
 Gawler Rivers Floodplain Area: [Figures FI/1 to FI/8](#)

Development in the Gawler Rivers Floodplain Area will not result in an increase in densities or site coverage. Buildings will be designed to prevent the entry by floodwaters. This will primarily be achieved through the raising of floor levels above the predicted flood level, together with increases in setbacks and decreases in site coverage aligned to hazard flood risk areas. In circumstances where this is likely to impact negatively on the historic character, alternative approaches may be appropriate provided flood proofing measures are included within any design and construction.

OBJECTIVES

Objective 1: The Residential Zone is primarily for residential development of a range of housing types, together with local facilities that support a healthy and convenient living environment.

Eight Policy Areas are identified in [Fig Res/1](#) according to:

- (a) future character for each;
- (b) the type and nature of development considered appropriate; and
- (c) other features that differentiate one area from the other.

In some localities, which are specifically nominated in Policy Areas, limited types of business use or mixed residential/business use will be appropriate.

Objective 2: Conservation and enhancement of State and Local Heritage Places located in the Residential Zone through consideration of:

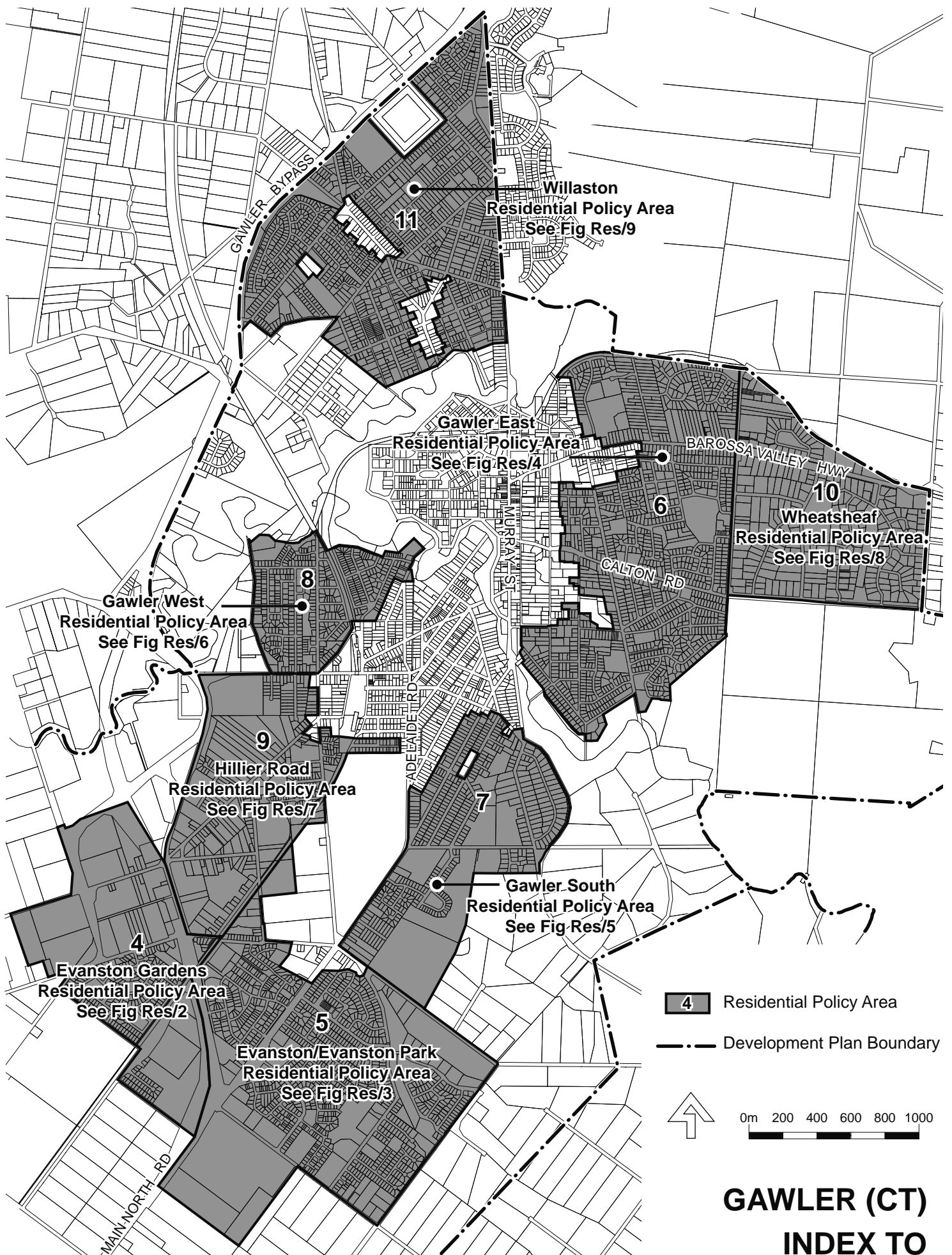
- (a) building alignment and set-backs;
- (b) building form, materials, and colours;
- (c) site layout, landscaping and fencing;
- (d) the form, scale; detailing and setting of adjoining development.

Objective 3: Buildings and structures located and designed to:

- (a) avoid damage from floodwaters;
- (b) not impede the flow of floodwaters.

Portions of the zone are within the Gawler Rivers Floodplain Area identified on [Figures FI/1 to FI/8](#). In those areas, increased densities are discouraged and development is limited to detached dwellings at low densities.

Dwellings and ancillary buildings will be designed to minimise entry by floodwaters. This will primarily be achieved through the raising of floor levels above the flood level, together with increases in setbacks and decreases in site coverage aligned to hazard flood risk areas. Alternative solutions may be appropriate in limited circumstances provided specific flood proofing measures are included within any design and construction.



GAWLER (CT)
INDEX TO
RESIDENTIAL POLICY AREAS
FIGURE Res/1
 Consolidated - 18 July 2019

PRINCIPLES OF DEVELOPMENT CONTROL

Form of Residential Development

- 1 Residential development should include a range of housing types and styles for a population with diverse needs and in accordance with the policies that apply for the Policy Areas.
- 2 Large housing developments should have a mix of allotments or housing types and sizes.
- 3 Infill development should not occur within the Gawler Rivers Floodplain Area identified on [Figures FI/1 to FI/8](#).

Form of Non-residential Development

- 4 Small shops, health and social services, medical consulting rooms, child care facilities, primary and secondary schools, recreation, open space and similar facilities may be developed, usually where the nature and scale of the use serves a local area function only, and only where the character and amenity of the locality is not detrimentally affected.
- 5 Non-residential development in residential zones should provide adequate protection for residents from air and noise pollution, traffic disturbance and other harmful effects on health or amenity.
- 6 Business uses are only appropriate where indicated in the Policy Areas and their associated Structure Plans Enlargements.
- 7 Scale, bulk and design of non-residential development should be sensitive to the character of the surrounding residential environment.
- 8 Landscaping should be of a high standard and used, where practicable, to minimize the visual impact of development.

Land Division

- 9 Allotments that are less than 450 square metres should be able to include a building envelope that is capable of containing a dwelling private open space and vehicle storage having regard to the principles of development control.

Character of Development

- 10 Land use, siting, design and appearance of development should be in accordance with the desired character for each Policy Areas, or in the absence, the existing character.

Heritage Places (State and Local)

- 11 Places identified in [Table Ga/2](#) or [Table Ga/5](#) should not be demolished, but instead be retained, upgraded and adapted such that:
 - (a) the quality or condition which gives the place its significance is protected or enhanced. This should include elements of the place which have significant cultural associations;
 - (b) the place is put to a use which supports the maintenance and restoration of that building, item, structure or place;
 - (c) the place is only altered so as to retain or restore the original external form, materials, and colours in a manner compatible with its structural visual, material and historic integrity;
 - (d) where internal arrangements and finishes are of significance the place should only be altered in a manner compatible with its structural visual, material and historic integrity.

- 12** Demolition of a place identified in [Table Ga/2](#) or [Table Ga/5](#) should not be undertaken unless the building, item, structure or place is so structurally unsound it represents a risk to public safety and is beyond economic repair. Unsympathetic (later) additions may be demolished if the later addition does not contribute to the historic character of the place.
- 13** Development abutting or in close proximity to a place identified in [Table Ga/2](#) or [Table Ga/5](#) should:
- (a) respect the historic character of the area and the integrity of the particular item of significance and be designed to a high architectural standard; and
 - (b) be compatible in respect of its design, siting, scale, building and roof shape, bulk, height, materials and colours, fences and landscaping and any advertising signs and external illumination with the place.
- 14** Any alteration or addition to a place identified in [Table Ga/2](#) or [Table Ga/5](#) should reinforce and complement the historic character of the place in terms of its design, siting, boundary set-backs, scale, built form and roof shape, bulk, height, materials and external finishes and where applicable:
- (a) additions should minimise their impact on the streetscape by siting them to the rear or the side of existing buildings and not forward of existing buildings;
 - (b) the height of eaves and gutters on additions should be complementary with the existing building;
 - (c) the proportions (vertical and horizontal) of additions visible from the street, should be complementary with those of the existing building;
 - (d) materials may include masonry (stone and brick) render, timber boarding and corrugated steel sheeting (walls and roof);
 - (e) materials such as mirror glass, flat profile steel sheeting, unpainted zincalume sheeting, wide fibre cement sheet planking and polished face concrete panels should not used where they are visible from the street;
 - (f) roof conversions or additional upper floors are not desirable where they compromise the historic integrity of the building or the view of the building from the street;
 - (g) roof conversions are acceptable where the alterations are fully incorporated under the existing roof and dormer style windows are acceptable where they are not visible from the street; and
 - (h) external colour schemes should be contemporary, but should not dramatically contrast with the original colour scheme of the existing place.
- 15** Carports and garages affecting places identified in [Table Ga/2](#) or [Table Ga/5](#) should be located behind the principal facade of the building and:
- (a) may be freestanding;
 - (b) garage doors should not visually dominate the facade of the building when viewed from the street;
 - (c) timber framing for carports is desirable.
- 16** Landscaping which should include fencing at the street boundary, should complement and reinforce the historic character of places identified in [Table Ga/2](#) or [Table Ga/5](#) and the zone generally.

- 17 Development should have fences to define street boundaries that complement the historic fences found in the locality. Existing traditional front fencing should be preserved in the Zone or appropriate Policy Area.
- 18 The design of new fences, or alterations to existing fences associated with places identified in [Table Ga/2](#) or [Table Ga/5](#) should complement and reinforce the historic character of the place as follows:
- (a) front fences should complement historic adjacent fences in height, be timber picket, metal palisade, woven wire mesh between timber posts, hedges or rendered masonry with brick copings or other traditional materials (high brush or corrugated steel fences are not appropriate);
 - (b) side and rear fences should be timber picket or board, corrugated steel sheeting (natural galvanised or painted finish), hedges or rendered masonry with brick copings or other traditional materials;
 - (c) side fences should match the front fence in height for the depth of the front yard.
- 19 Division of an allotment containing a place, or adjoining a place identified in [Table Ga/2](#) or [Table Ga/5](#) should not be undertaken if it may detrimentally affect the heritage significance of the place.

Hazards

The following principles apply to land within the Gawler Rivers Floodplain Area identified on [Figures FI/1 to FI/8](#).

Flooding

- 20 Land division should not result in an additional allotment or allotments unless;
- (a) it is for the purpose of creating a separate allotment for an existing dwelling and all resulting allotments will contain at least one existing dwelling;
 - (b) it involves works designed and certified by a suitably qualified engineer to minimise the subject land and any other land in the area, and where such works would not cause a material adverse impact on other people or property.
- 21 Dwellings, other than detached dwellings, should only be developed where it involves works designed and certified by a suitably qualified engineer to minimise the impact of a 1 in 100 ARI flood event on the subject land and any other land in the area, and where such works would not cause a material adverse impact on other people or property.
- 22 Detached dwellings and residential outbuildings should only be developed on land identified in [Figures FI/1 to FI/8](#) where it involves works designed and certified by a suitably qualified engineer to minimise the impact of a 1 in 100 ARI flood event on the subject land and any other land in the area, and where such works would not cause a material adverse impact on other people or property, or under the following conditions:
- (a) the finished floor level is equal to or greater than 300 millimetres above the predicted level of a 1 in 100 ARI flood event; and
 - (b) in Low hazard flood risk areas:
 - (i) site coverage for all buildings on the allotment does not exceed 50 percent of the area of the allotment; and
 - (ii) set-backs of detached dwellings to side and rear boundaries, but excluding boundaries to a public road, are at least:
 - (A) one metre to side boundaries; and

- (B) four metres to rear boundaries.
 - (c) in Medium hazard flood risk areas:
 - (i) site coverage for all buildings should on the allotment does not exceed 40 percent of the area of the allotment; and
 - (ii) set-backs of detached dwellings to side and rear boundaries, but excluding boundaries to a public road, are at least:
 - (A) two metres to side boundaries; and
 - (B) five metres to rear boundaries.
 - (d) in High hazard flood risk areas:
 - (i) site coverage for all buildings on the allotment does not exceed 30 percent of the area of the allotment; and
 - (ii) set-backs for detached dwellings to side and rear boundaries, but excluding boundaries to a public roads, are at least:
 - (A) three metres to side boundaries; and
 - (B) six metres to rear boundaries.
- 23** Ground floor additions to dwellings should:
- (a) not exceed a 40 square metre increase in floor area;
 - (b) be the same or higher finished floor level as the existing dwelling; and
 - (c) comply with the applicable maximum site coverage and boundary set-back requirements in Principle of Development Control 22.
- 24** Dwelling additions in excess of a 40 square metre increase in the existing floor area should:
- (a) be in the form of upper level additions; or
 - (b) have a finished floor level equal to or greater than 300 millimetres above the predicted level of a 1 in 100 ARI flood event; and
 - (c) comply with the applicable site coverage and boundary setback requirement in Principle of Development Control 22.
- 25** Outbuildings may be constructed at natural ground level providing:
- (a) there is no permanent storage of goods or equipment below 300 millimetres above the predicted level of a 1 in 100 ARI flood event level for the applicable hazard flood risk area; and
 - (b) the maximum site coverage requirements in Principle of Development Control 22 are met.

PROCEDURAL MATTERS

Complying Development

- 26** Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

Non-complying Development

- 27** Development (including building work, a change in the use of land, or division of an allotment) for the following is **non-complying**:

Advertisement and/or advertising hoarding; except land development promotional signs under 10 square metres in size. Approved: consulting rooms, office, or shop or group of shops.

Amusement machine centre

Bulky goods outlet, except where the gross leasable area is less than 250 square metres

Consulting room, except where the total floor area is less than 100 square metres

Crematorium

Demolition

(a) in total; or

(b) of the frontage or side returns visible from a street or other elements visible from a public place of a Place listed in [Table Ga/2](#) or [Table Ga/5](#);

except where the demolition is not total demolition and does not involve the frontage or side return visible from a street or other elements visible from a public place

Dairy

Dwelling on land within the Gawler Rivers Floodplain Area identified in [Figures FI/1 to FI/8](#), except for:

(a) detached dwelling; or

(b) other forms of dwelling where the application includes a report from a suitably qualified engineer expert which states that the proposed dwelling(s) will be located above the predicted level for a 1 in 100 year ARI event and will not exacerbate the risk of flooding on any other land in the surrounding area.

Farming

Fuel depot

General Industry

Horse keeping

Horticulture

Hospital

Hotel

Intensive animal keeping

Land Division in Wheatsheaf Policy Area, except where there is conformity with Principle of Development Control 2 within the Wheatsheaf Policy Area

Motor repair station

Office, except where the total floor area is less than 150 square metres

Petrol filling station

Public service depot

Restaurant

Road transport terminal

Service trade premises

Shop or group of shops, except where the gross leasable area is less than 250 square metres

Special Industry

Stock sales yard

Stock slaughter works

Store

Warehouse

Waste reception, storage, treatment or disposal

Wrecking yard

Public Notification

- 28** Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Evanston Gardens/Evanston South/Hillier Residential Policy Area 4 ([Figure Res/2](#))

OBJECTIVES

- Objective 1:** A predominantly low to medium rise and low to medium density residential area that comprises a range of dwelling types (including affordable housing), designed to integrate with areas of open space, centres or public transport nodes.
- Objective 2:** Development that supports the viability of community services and infrastructure.
- Objective 3:** An orderly sequence of development which allows economic and effective provision of public infrastructure and community services.
- Objective 4:** Sustainable urban development outcomes through innovation in stormwater management, waste minimisation, water conservation, energy efficiency and urban biodiversity.
- Objective 5:** Development that contributes to the desired character of the policy area.

DESIRED CHARACTER

The Policy Area will develop in accordance with [Figure Res/2](#) and Evanston Gardens/Evanston South/Hillier Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement H](#) and be undertaken in an orderly manner that achieves the most efficient use of the land, the extension or expansion of infrastructure services and the timely provision of community facilities, and higher residential densities in designated areas.

The policy area will accommodate a diversity of housing forms with a compatible mix of non residential development in the range of small scale commercial, retail, educational, recreational and community facilities in designated localities.

Development, in particular land division layout needs to respect and enhance the natural attributes of the Policy Area through the retention of significant views, drainage lines and native vegetation, and solar orientation. Innovative and best practice solutions in respect of water reuse and stormwater management will be implemented.

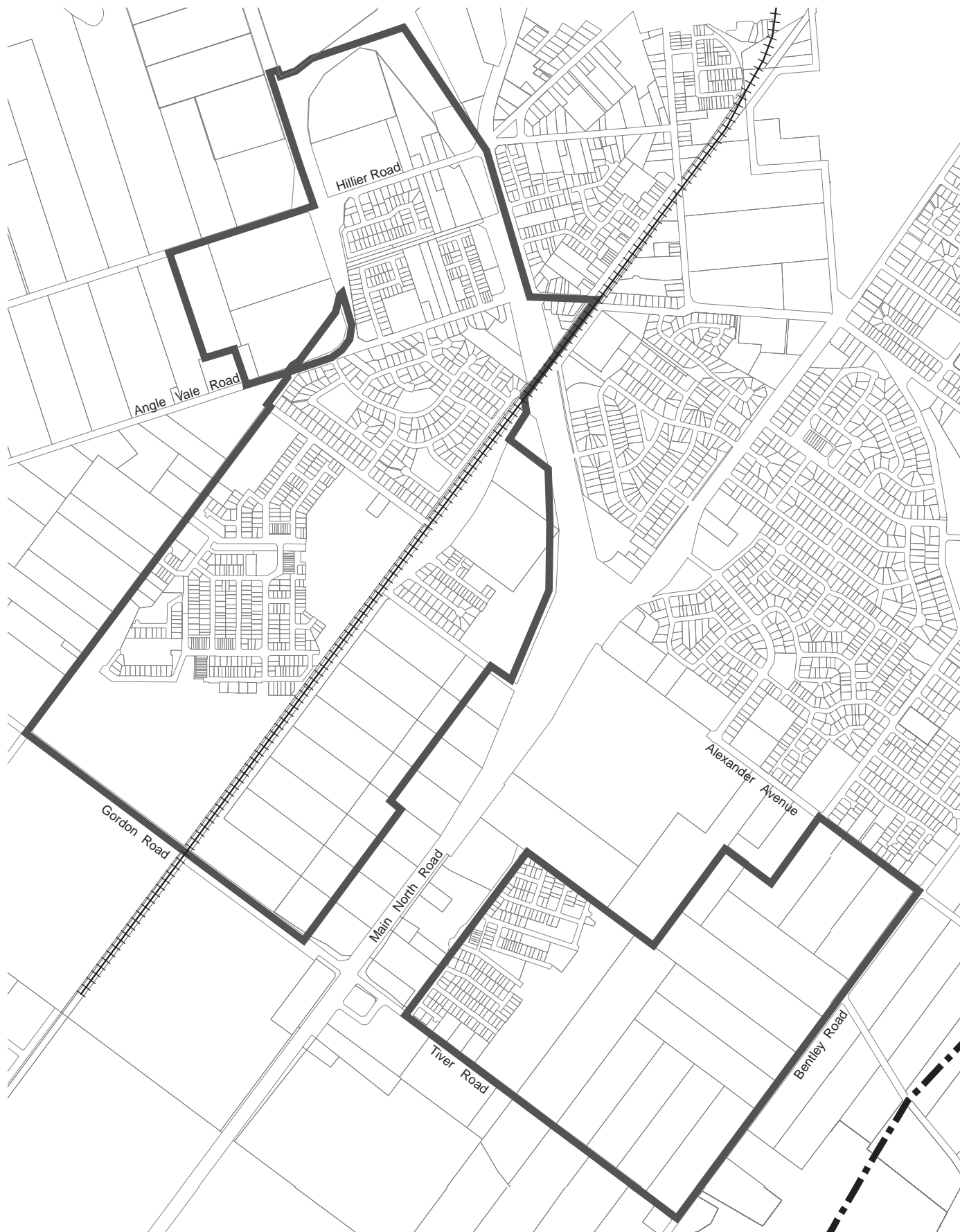
Building height will generally be *low rise* (1 to 3 storeys) but *medium rise* (4+ storeys), such as apartment buildings, will be located near the Tambelin Railway Station and can be integrated with non-residential use.

Housing forms will be simple but include architectural features such as articulation to the street façade and deliver a mix of housing types and forms to provide interesting streetscapes and promote social interaction. This will include the provision of recessed vehicle garaging and the inclusion of front verandahs/porticos and appropriate landscaping.

Street corners will be enlivened by locating small offices or home offices at the junctions as a technique to avoid long blank walls or fences.

The delivery of housing diversity will require innovative solutions for front or rear access and parking. Rear access will be provided in the form of 'service lanes', which support vehicular access requirements at reduced speeds providing a safe pedestrian environment.

Allotment configuration is envisaged to be compact, with building setbacks minimised to assist in facilitating an enclosed and active street. This will incorporate opportunities for multi-storey apartments, terrace and rear lane mews housing that will achieve a range of housing types within a single street.



For more detail refer to MAPS Ga/1 (Overlay 1) Enlargement F and Ga/1 (Overlay 1) Enlargement H

- ++++ Railway
- ▬ Policy Area Boundary

GAWLER (CT)
EVANSTON GARDENS/
EVANSTON SOUTH / HILLIER
RESIDENTIAL POLICY AREA 4
 Consolidated 18 July 2019
FIGURE Res/2

The form and distribution of major open space will be influenced by the need for stormwater detention, treatment and re-use. It will also be influenced by the location of drainage corridors, and the need to integrate with existing corridors, including to the west and the Gawler River which is intended to be an open space system in its own right in the longer term. Public open space areas will need to accommodate both active and passive recreation opportunities, the retention of identified habitat areas of significance and provision for habitat restoration.

Reserves for public purposes such as active and passive recreation and stormwater management, including the provision of associated infrastructure, should be designed to an appropriate standard and level of service suitable for the locality to ensure long term economic, environmental and social sustainability.

A network of connected parks including cohesive pedestrian and bicycle movement corridors and visual links will be established between the new development and adjoining natural creek lines, public recreation areas, local shopping and community services and surrounding road networks.

Portion of the southern boundary of the Policy Area is located adjacent to the productive rural land and light industry. It is essential that development form an appropriate interface with these uses to minimize amenity impacts. The interface will vary in width and comprise a combination of roads deeper than usual building setbacks, paths, public open space and where appropriate areas for stormwater management. Where housing is included in the interface area it is expected that houses will be sited and designed to minimize disturbance.

The collector roads shown as Boulevard style on Evanston Gardens/Evanston South/Hillier Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement H](#) are intended to have that character and comprise a route for public transport (buses) and the key access routes to the Tambelin train station, separate wide footpaths and cycle paths on both sides and substantive street tree plantings. Dwellings will front and address the road with setbacks to contribute to the boulevard character. Accordingly it is expected that vehicles will reverse directly onto the road.

Direct access to roads nominated as distributor roads will be minimized and where required be co-located at the edge of property boundaries to reduce crossing points.

The Policy Area's open space system will be defined by a series of interconnected ephemeral and permanent detention basins and wetlands used for passive recreation and aquifer storage recharge infrastructure.

Gordon Road may ultimately be used as a major collector road and hence require over/underpass of the railway line in which case development should be sited and designed accordingly along the road corridor.

A future extension of the Gawler East Link Road from Potts Road to Tiver Road via part of Bentley Road, east of the high pressure gas transmission pipeline and through new development land is envisaged to accommodate further residential growth in the wider region.

The north-eastern portion of the Policy Area is bounded by Jack Cooper Drive to the west and north, the off-ramp to the Gawler Bypass to the east and Murray Hillier Court to the south. Housing fronting the western and northern boundary (along Jack Cooper Drive) will be sited, designed and constructed to mitigate traffic noise impacts from this road. In addition, housing fronting Jack Cooper Drive will address the road and be enhanced through the street design and the complementary road side planting. Direct vehicular access to Jack Cooper Drive will not be provided.

The eastern boundary of the north-eastern portion of the Policy Area (along the off-ramp to the Gawler Bypass) will require acoustic treatment measures to ensure traffic noise from the off-ramp and Bypass is mitigated to a standard suitable for residential development. Acoustic treatment measures in the form of barriers, of appropriate heights and construction materials, may be required. Direct vehicular access to the off-ramp will not be provided.

Land to the north of Murray Hillier Court, in Certificates of Title 5454/335, 5811/420, 5340/22 and 5405/467, has been identified as potentially being affected by contaminating activities and therefore should not be developed for sensitive uses (i.e. residential) until it is demonstrated that the land is, or will be, suitable for the intended use.

Certificate of Title 5405/467 also contains commercial poultry “hobby farming” activities. Adjacent residential land divisions and development will be designed to ensure an appropriate amenity for residents while minimizing impacts on the poultry farming activities.

The north-western portion of the Policy Area is bounded by Hillier Road to the north, Jack Cooper Drive to the east, Angle Vale Road to the south and the Evanston Gardens Neighbourhood Centre Zone to the west. Housing fronting Jack Cooper Drive, Angle Vale Road and the southern side of Hillier Road will be sited, designed and constructed to mitigate traffic noise and air quality impacts from these roads. In addition, housing adjacent Jack Cooper Drive, Angle Vale Road and the southern side of Hillier Road will front these roads and be enhanced through street design and complementary road side planting. Direct vehicular access to Jack Cooper Drive, Angle Vale Road and Hillier Road will not be provided.

Residential development adjacent to the Neighbourhood Centre Zone will be sited, designed and constructed to minimise potential adverse impacts from that Zone. Given the fall of the land from the north-east near the Hillier Road/Jack Cooper Drive intersection to the south-west corner adjacent the Neighbourhood Centre Zone on Angle Vale Road, the option exists to locate wetlands/stormwater detention basins in this south-western portion in conjunction with an open space area.

Medium density development is envisioned to occur in the south western corner of the ‘Living’ area (Residential Zone adjacent Angle Vale Rd), these developments will have direct access to quality open space as well as education and community facilities.

Preliminary investigations indicate that the potential for site contamination is low. However, further investigations may be required prior to residential development occurring to confirm this.

Railway Station Precinct

The Railway Station Precinct as shown on Evanston Gardens/Evanston South/Hillier Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement H](#) will have a strong urban setting defined by medium residential densities (35+ dwellings per hectare or 3000 to 3500 people) and buildings generally taller than elsewhere in the Policy Area, with well designed public places and spaces. Buildings will be set close to the street and display a relatively cohesive mass and form with a continuous format that is also designed to enliven the public realm and create or accentuate vistas and views. There should be good provision of sustainable design techniques and greening of the urban environment.

Small scale non-residential development will be encouraged at street level particularly near the train station., Office uses may be appropriate on upper floors especially where they provide a buffer between more active ground floor uses and residential uses. The mix of uses will cater for the day to day needs of local residents and workers and will be at its greatest intensity closest to the railway station. Features and activities that attract people and add vitality to the street such as display windows, retail shopfronts and outdoor dining areas are encouraged.

PRINCIPLES OF DEVELOPMENT CONTROL

1 The following forms of development are envisaged in the policy area:

Dwellings, including multi-storey dwellings

Small scale non-residential uses that serve the local community, for example:

- child care facilities
- open space
- recreation areas
- shop, office or consulting room

Supported accommodation

2 Development listed as non-complying is generally inappropriate.

3 Non-residential development such as shops, schools and consulting rooms should be of a nature and scale that:

- (a) serves the local community

- (b) is consistent with the character of the locality
- (c) does not detrimentally impact on the amenity of nearby residents.

4 The use and placement of outbuildings should be ancillary to and in association with a dwelling or dwellings.

Form and Character

- 5 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 6 A residential flat building should provide a variety of dwelling sizes (e.g. bed-sit, one, two and three bedrooms) particularly in larger complexes.
- 7 Medium density development should achieve gross densities of between 23 and 45 dwellings per hectare (which translates to net densities of between 40 and 67 dwellings per hectare).
- 8 Buildings should generally be of one to three storeys in scale. Buildings of four storeys or more may also be established as part of an integrated development scheme within the Railway Station Precinct shown on Evanston Gardens/Evanston South/Hillier Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement H](#).
- 9 Where allotments have direct frontage to a public park, housing should be oriented towards the park.
- 10 Ground floor dwellings and accommodation of multi-level buildings should contribute to the desired streetscape by incorporating either or both of the following:
- (a) front landscaping or terraces
 - (b) individual entries.
- 11 Buildings on corner allotments should address and activate both street frontages and are encouraged for non-residential uses such as home office or small service uses.
- 12 Dwellings adjacent the railway corridor should face the railway line and provide adequate separation and amenity through the use of public road frontage, landscaping and pedestrian/cyclist pathways.
- 13 Residential development in that part of the Policy Area north of Murray Hillier Court should:
- (a) where fronting Jack Cooper Drive be sited, designed and constructed to mitigate traffic noise impacts from that road
 - (b) where adjacent to the Gawler Bypass off- ramp, be screened by acoustic barriers to mitigate traffic noise impacts from the off-ramp and the Gawler Bypass roads.
- 14 Acoustic barriers in that part of the Policy Area north of Murray Hillier Court should be constructed from a combination of materials which incorporate vertical and horizontal elements and other design techniques to create articulation and provide visual interest, avoiding extensive areas of uninterrupted featureless walling facing the street.
- 15 Direct access to Jack Cooper Drive or the off-ramp from the Gawler Bypass should not be provided from the land in that part of the Policy Area north of Murray Hillier Court.
- 16 In that part of the Policy Area north of Murray Hillier Court, a buffer (where warranted by the potential risk of transfer of sprays, dust and noise from nearby horticultural activities) should be:
- (a) established between residential development and the horticultural activity
 - (b) designed to take account of the increased risk associated with prevailing winds and topography

- (c) established prior to a dwelling being erected
 - (d) at least 40 metres in width
 - (e) vegetated to assist with screening of dust and chemical spray drift.
- 17** Development in that part of the Policy Area north of Murray Hillier Court, which is on land subject to inundation in a 1 in 100 year average return interval flood event, should not result in the loss of floodplain storage. This can be achieved by incorporating measures to increase floodplain capacity in the locality through the use of reserves, roadways and swales for temporary flood storage, with works designed and certified by a suitably qualified engineer.
- 18** Residential development in that part of the Policy Area north of Angle Vale Road and west of Jack Cooper Drive should:
- (a) where directly adjacent Jack Cooper Drive, Angle Vale Road and the southern side of Hillier Road, front these roads and be sited, designed and constructed to mitigate noise and air quality impacts from traffic
 - (b) not have direct vehicular access to Jack Cooper Drive, Angle Vale Road and Hillier Road
 - (c) where adjacent to the Neighbourhood Centre (Evanston Gardens) Zone, be sited, designed and constructed to minimise potential adverse impacts from that Zone.
- 19** Dwellings and residential buildings other than within the Railway Station Precinct as shown on Evanston Gardens/Evanston South/Hillier Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement H](#), should be designed within the following parameters:

Parameter	Value
Minimum setback from Primary Street Frontage (excluding collector roads)	- 3.0 metres to front façade - 1.5 metres to verandah/balcony elements - 0.5 metres to entry porch and portico
Minimum setback from Secondary Street Frontage (Corner Lots)	- 1.5 metres to façade where wall height less than 3.0 metres - 2.0 metres where wall height is 3.0 metres or above
Minimum setback from Side Boundary (excluding secondary road frontage)	- 0.0 metres for dwellings with a primary road frontage equal or less than 9.0 metres, other than a garage wall exceeding 8.0 metres in length - 0.9 metres for dwellings with allotments with a primary road frontage greater than 9.0 metres, other than a garage wall exceeding 8.0 metres in length - 2.0 metres where the wall height is in excess of 3.0 metres
Minimum setback from Rear Boundary (other than rear lane)	- 0.9 metres where wall height is equal or less than 3.0 metres - 2.0 metres where wall height in excess of 3.0 metres
Open Space Reserve Frontage	- 5.1 metres where dwellings front the reserve
Single carport/garage	- 5.0 metres from primary street frontage - 0.5 metres from laneway frontage
Double carport/garage	- 5.0 metres from primary street frontage - 0.5 metres for laneway frontage
Garages and carports facing the street (other than access lane)	- 6.0 metres maximum width with no more than 50 percent of the frontage of site

Parameter	Value
Minimum setback from Collector Roads (except Bentley, Tiver and Gordon Roads)	<ul style="list-style-type: none"> - 6.0 metres to front façade - 5.5 metres to carport/garage - 4.0 metres to verandah/balcony
Minimum setback from Bentley, Tiver and Gordon Roads)	- 15.0 metres
Vehicle crossover points on Bentley, Tiver and Gordon Roads	<ul style="list-style-type: none"> - One only - Co-located with a crossover that serves the adjoining property
Minimum setback from Rear Lane (upper level dwelling)	- 0.5 metres for laneway frontage
Maximum building height (from natural ground level)	- 12 metres
Upper level dwellings	- Separate areas are provided for the provision of a rainwater tank and the storage of refuse and recycling bins

- 20 Dwellings, other than within the Railway Station Precinct as shown on Evanston Gardens/Evanston South/Hillier Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement H](#), should include private open space within the following parameters:

Site area	Minimum area of private open space	Value/Provisions
Greater than 250 square metres	60 Square metres	<ul style="list-style-type: none"> - Balconies, roof patios, decks and the like, can comprise part of this area provided the area of each is 8 square metres or greater and has a minimum dimension of 2 metres. - One part of the space should be directly accessible from a living room and have an area of 24 square metres with a minimum dimension of 4 metres and a maximum gradient of 1 in 10.
250 square metres and less	35 square metres	<ul style="list-style-type: none"> - Balconies, roof patios and the like can comprise part of this area provided the area of each is 8 square metres or greater and has a minimum dimension of 2 metres - One part of the space is directly accessible from a living room and has an area of 16 square metres with a minimum dimension of 4 metres and a maximum gradient of 1 in 10.
Upper level dwellings	15 square metres	9 square metres and accessible from a living room with a minimum dimension of 3.0 metres

- 21 Rear lanes should be designed with the following parameters:

Parameter	Value
Road Function	Immediate access (not a thoroughfare)
Reserve width	6.5 metres
Maximum length	90.0 metres
Carriageway width	5.5 metres
Entry width	4.5 – 5.0 metres

Parameter	Value
Protuberances depth function	<ul style="list-style-type: none"> - 1.0 metres - landscaping (tall vertical trees) and lighting - garbage bins

Amenity and Public Space

- 22** Residential development should have regard to existing and possible future noise sources with respect to site layout, orientation, design and construction to ensure a safe and comfortable residential environment and to minimise conflict with existing non-residential activities.
- 23** Front fencing should balance the desire for an open streetscape and passive surveillance with the need for functional privacy. Clear delineation should be provided between public and private spaces, which may incorporate fencing landscaping or a combination of these elements.
- 24** Residential development should provide an area for the storage of waste receptacles that is screened from primary and secondary street frontages.
- 25** Development should include a minimum 15 percent of residential dwellings for affordable housing.
- 26** Affordable housing should be distributed throughout the policy area to avoid over-concentration of similar types of housing in a particular area.

Land Division

- 27** Land Division should be generally in accordance with Evanston Gardens/Evanston South/Hillier Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement H](#).
- 28** Land division should not proceed unless it is established that there is a viable stormwater management system which locates and designs detention and retention basins, and provides for aquifer storage recharge, to be generally consistent with the Evanston Gardens/Evanston South/Hillier Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement H](#) and:
- (a) ensures public health and safety is protected, particularly in regard to high velocity drainage points and access to water bodies
 - (b) minimises potential health risks from exposure to mosquitoes.

High Pressure Gas Pipeline

- 29** Development, including land division, in the vicinity of a high pressure pipeline, as depicted on Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement H](#), should:
- (a) not affect the continued operation of the pipeline infrastructure
 - (b) be located, designed, and developed having regard to the location of the infrastructure and the potential public safety impacts.

Infrastructure Coordination

- 30** Development, including land division, should only occur once required physical and community infrastructure is either provided, or a legally binding agreement has been executed for the provision of the infrastructure items, including the following:
- (a) Gawler East Link Road as illustrated on Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement H](#), and associated traffic interventions (road and intersection upgrades)

- (b) public lighting, pedestrian and bicycle paths and associated landscaping treatments associated with the Gawler East Link Road
- (c) community facilities (including recreation facilities).

Railway Station Precinct

Building Design

- 31** The ground floor frontage of buildings should be transparent or glazed a minimum of 50 percent of the primary frontage to promote active street frontages and maximise passive surveillance.
- 32** All buildings should be within a street set back of 0 to 3 metres which is used for porches, small front yards, rain gardens and designed to positively contribute to and enliven the public realm.
- 33** Building volumes should be articulated as the building rises to differentiate between the base, middle and top of the building. Setbacks at middle and top levels should be used for balconies and roof gardens.
- 34** Building facades should be articulated to unify a building's appearance, add interest, scale and a three dimensional quality. Articulation may be provided through:
 - (a) periodic change in wall plane
 - (b) building material, texture and/or colour, changes in building fenestration and other approaches that create visual interest and/or shadow lines
 - (c) street level facades being distinguished from upper levels.

Urban Design and Public Places

- 35** Development should respect the natural attributes of the area through the preservation and enhancement of significant views including of the Mount Lofty Ranges.
- 36** Blank walls visible from the public domain should include visually interesting treatments of high quality design.

Car Parking

- 37** The majority of parking and service items such as loading areas, mechanical equipment, and garbage storage should be located to the rear of buildings.
- 38** Adequate provision should be made for car parking in the vicinity of the railway station for train users as well as for local residents and businesses.

Evanston/Evanston Park Policy Area ([Figure Res/3](#))

Desired Character

Evanston and Evanston Park are divided into a western sector and eastern sector by Main North Road. Evanston (the western sector) is characterised by public housing or housing designed and constructed by the public housing sector, usually comprising semi-detached and detached housing types. The proximity of Evanston to the full range of urban services is very good and the locality would benefit from ongoing streetscape improvements to complement ongoing renewal of the existing housing stock.

Evanston Park comprises mainly detached dwellings and contains pockets of undeveloped land which needs to be integrated into existing road system.



For more detail refer to MAP Ga/1 (Overlay 1) Enlargement F

- ++++ Railway
- ▬ Policy Area Boundary

GAWLER (CT)
EVANSTON/EVANSTON PARK
RESIDENTIAL POLICY AREA 5

FIGURE Res/3

Consolidated - 18 July 2019

A future extension of the Gawler East Link Road from Potts Road to Tiver Road via part of Bentley Road, east of the high pressure gas transmission pipeline and through new development land is envisaged to accommodate further residential growth in the wider region.

Appropriate Uses are:

Housing
Local Services (recreation areas, education, child care centres, community uses)
Shops on Krieg Road shown on Structure Plan, with gross leasable area not exceeding 450 square metres

OBJECTIVE

Objective 1: A residential area where the majority of existing housing stock is maintained and the number of dwellings is increased through development of large vacant allotments.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development in accordance with Structure Plan, [Map Ga/1 \(Overlay 1\) Enlargement F](#).

The Structure Plan indicates:

- (a) the location and impact of arterial traffic in residential environments
- (b) relationships with business, recreation and public transport, education and visitor accommodation services
- (c) roads which will carry larger traffic flows
- (d) linear open space system
- (e) shops on Krieg Road.

High Pressure Gas Pipeline

- 2 Development, including land division, in the vicinity of a high pressure pipeline, as depicted on Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement F](#), should:
 - (a) not affect the continued operation of the pipeline infrastructure
 - (b) be located, designed, and developed having regard to the location of the infrastructure and the potential public safety impacts.

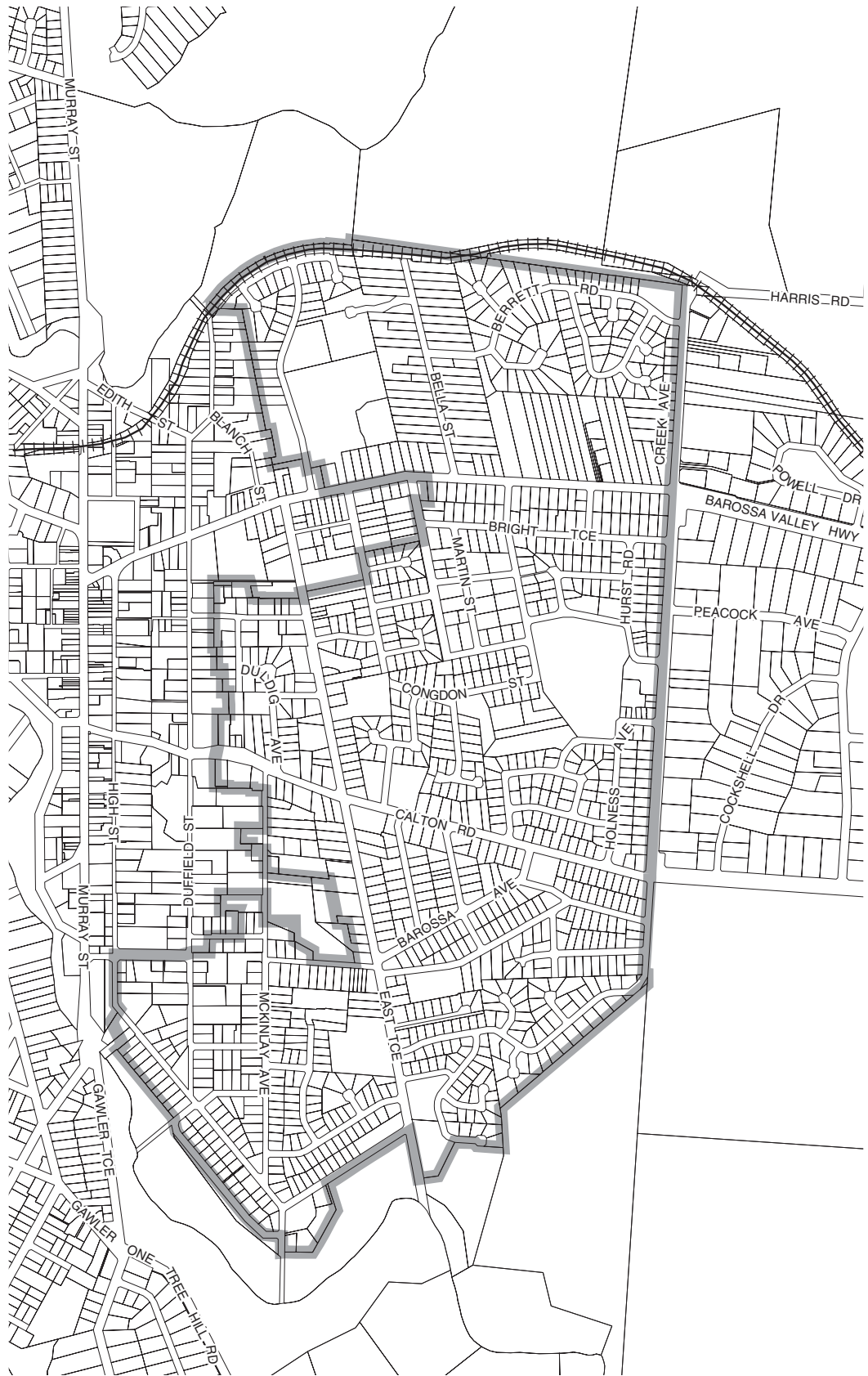
Gawler East Policy Area ([Figure Res/4](#))

Desired Character

Gawler East is well-defined by the North and South Para River Valleys and important traffic routes east and west. The undulating topography of the Policy Area is a significant feature and determinant of the character of this contemporary Gawler suburb.

Any undeveloped land can be expected to be developed for residential purposes, with medium density being located in either convenient proximity of the Town Centre or Cheek Avenue shops.

It will be critical to maximise dwelling set-backs, and design dwellings to mitigate noise impacts from adjoining arterial and major collector roads and minimize site cut and fill on sloping sites.



- ++++ Railway
- █ Policy Area Boundary

GAWLER (CT)
GAWLER EAST
RESIDENTIAL POLICY AREA 6
FIGURE Res/4
 Consolidated - 18 July 2019

Appropriate Uses are:

Housing
Local services (recreation areas, schools, child care centres, community uses)
Gawler Hospital and associated private and public community health services.

OBJECTIVES

- Objective 1:** An area where the existing housing stock is maintained and the number of dwellings is increased through development of vacant large land parcels.
- Objective 2:** Provision for integrated public and private community health services in association with the Gawler Hospital.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development in accordance with Structure Plan, [Map Ga/1 \(Overlay 1\) Enlargement C](#).

The Structure Plan indicates:

- (a) the location and impact of arterial traffic on residential environments
- (b) relationship to centre, recreation and health facilities
- (c) location of aged housing
- (d) roads which will carry major traffic flow.

- 2 Consulting rooms developed only in association with the Gawler Hospital site subject to:
- (a) all vehicular access to health services and consulting rooms being via an integrated car parking and vehicular road system which is accessed off Hutchinson Road
 - (b) no vehicular access off Lyndoch Road
 - (c) retention of the residential building scale, form and landscape setting along Lyndoch Road.

Gawler South Policy Area ([Figure Res/5](#))

Desired Character

Plantings on the eastern escarpment should be developed as part of a linear recreation trail connecting Evanston Park with Dead Mans Pass.

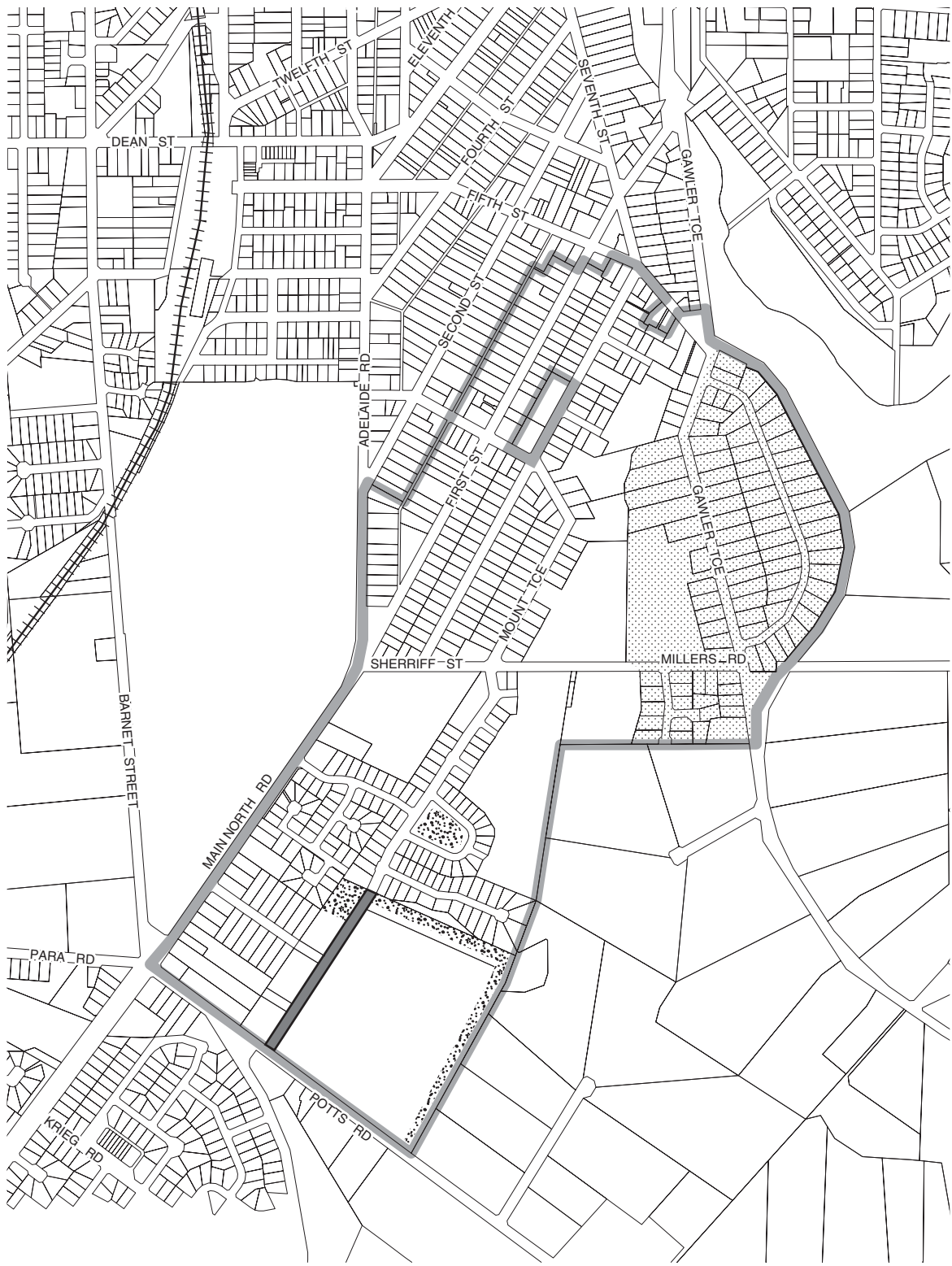
Gawler South residential area stretches from the flat land east of Adelaide Road into the eastern escarpment. The escarpment should be protected from further development and its appearance enhanced through appropriate revegetation.




Appropriate Uses are:

Housing
Local services (recreation areas, schools, child care centres, community uses)

OBJECTIVES

- Objective 1:** A residential area where existing housing stock is maintained and there is substantial infill residential development through utilization of vacant land.



-  Low Density Residential Escarpment
-  Stormwater Management/Public Open space
-  Local Road
-  Railway
-  Policy Area Boundary
-  Collector Road

GAWLER (CT)
GAWLER SOUTH
RESIDENTIAL POLICY AREA 7

FIGURE Res/5
 Consolidated - 18 July 2019

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development in accordance with Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement E](#).

The Structure Plan indicates:

- (a) the location and impact of arterial traffic on residential environments
 - (b) public land reservation to
 - (c) enhance the eastern escarpment and for recreation access
 - (d) vehicular interconnection
 - (e) public open space
 - (f) stormwater management.
- 2 Residential “night cart lanes” should only be used primarily for rear of dwelling vehicular access and the residential densities adjoining the lanes should not be increased.
- 3 The historic alignment and width of residential “night cart lanes”, and their function, should be preserved.
- 4 New allotments on the escarpment shown on [Figure Res/5](#) should have a minimum size of 2000 square metres.

High Pressure Gas Pipeline

- 5 Development, including land division, in the vicinity of a high pressure pipeline, as depicted on Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement E](#), should:
- (a) not affect the continued operation of the pipeline infrastructure
 - (b) be located, designed, and developed having regard to the location of the infrastructure and the potential public safety impacts.

Gawler West Policy Area ([Figure Res/6](#))

Desired Character

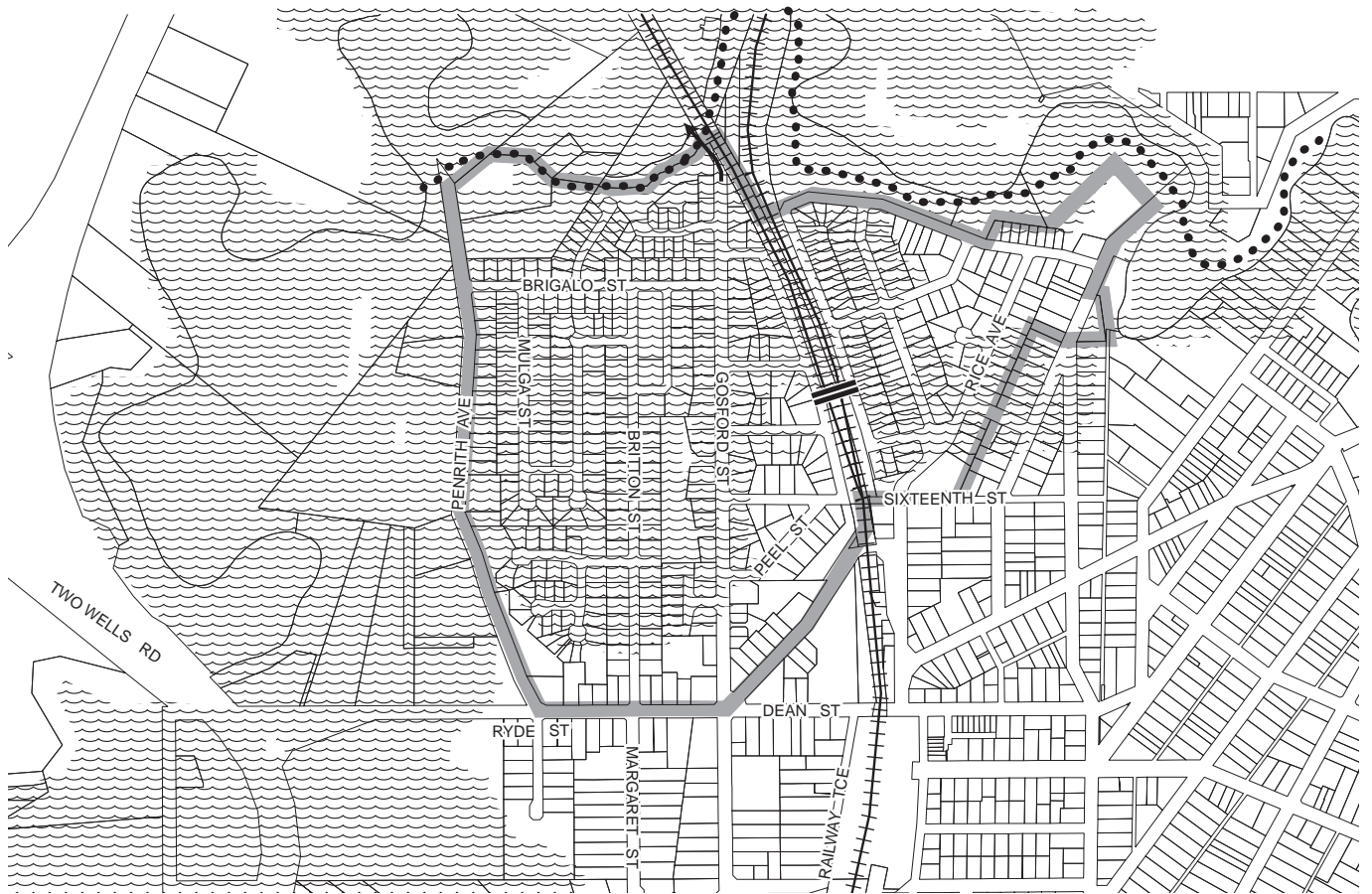
Gawler West will continue to provide a range of housing types, in a locality that is well-located to a range of essential urban services.

General amenity improvements are a high priority to strengthen streetscapes in particular, and integrate these with the attractive riverine landscape along the northern perimeter of Gawler West.

Convenient, safe, high quality pedestrian linkage between the Area’s western and eastern sectors is important to create convenient access for the western sector and public transport services and the town centre. Linkage across the Gawler River, for both vehicles and pedestrians, along the Gosford Street alignment should also be achieved to provide better connection with Gawler’s northern-western sector.






Appropriate Uses are:

- Housing
- Local Services (recreation areas, schools, child care centres, community uses)



Note: See figure FI/1 for flood data definition explanation and disclaimer.



-  Flood Prone Land
-  Cycle/Walking Connection
-  Railway
-  Potential New Roads
-  Policy Area Boundary

GAWLER (CT)
GAWLER WEST
RESIDENTIAL POLICY AREA 8
FIGURE Res/6
 Consolidated - 18 July 2019

OBJECTIVE

Objective 1: A residential area where the majority of the existing housing stock should be maintained and the number of dwellings increased through infill of vacant and underutilized sites outside of flood prone land.

PRINCIPLE OF DEVELOPMENT CONTROL

1 Development in accordance with Structure Plan [Map Ga/1 \(Overlay1\) Enlargement D.](#)

The Structure Plan indicates:

- (a) relationship to recreation and rail services
- (b) roads which will carry higher traffic flows
- (c) intention to upgrade pedestrian access across railway line
- (d) access to public transport.

Hillier Road Policy Area ([Figure Res/7](#))**Desired Character**

Hillier Road's housing types are diverse, with most developed since the 1970's. Despite development occurring in scattered pockets, separated by large undeveloped spaces, the Hillier area features a wide range of urban services with public transport being a key feature.

It is envisaged the diversity in housing will continue, with locations well-located to public transport attracting the highest densities.

Infill of vacant land has the potential to increase Hillier's capacity from 450 dwellings to 500 dwellings (12%). In the localities indicated on the Structure Plan as showing the highest potential for infill land division and residential development, the provision of coordinated vehicular access and urban services will be critical. Development in these locations should not proceed unless adequate provision is made for orderly development and coordination of services.

Land which is subject to inundation by floodwaters should not be developed until required flood mitigation works are installed so as to mitigate the risk of property damage.

Appropriate Uses are:

- Housing
- Local Services (recreation areas, schools, child care centres, community uses)

OBJECTIVE

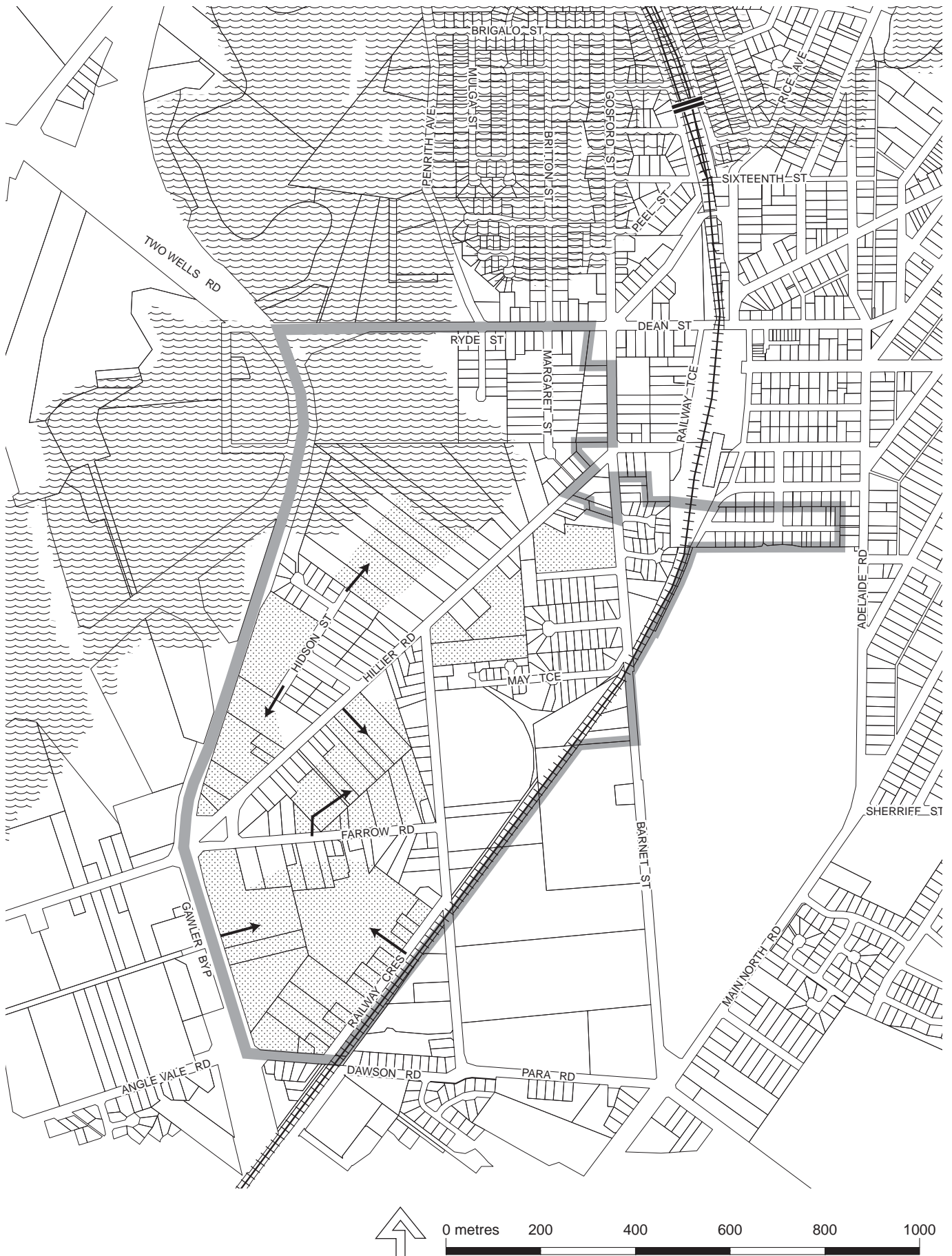
Objective 1: A residential area where the existing housing stock is maintained and there is substantial infill residential through utilisation of vacant land.

PRINCIPLES OF DEVELOPMENT CONTROL






1 Development in accordance with Structure Plan [Map Ga/1 \(Overlay1\) Enlargement D.](#)

The Structure Plan indicates:

- (a) locations with the highest potential for infill residential development and land division
- (b) areas subject to flood inundation



Note: See figure FI/1 for flood data definition explanation and disclaimer.

-  Flood Prone Land
-  Highest Potential for Infill Land Division
-  Railway
-  Potential New Roads
-  Policy Area Boundary

GAWLER (CT)
HILLIER ROAD
RESIDENTIAL POLICY AREA 9
FIGURE Res/7
 Consolidated - 18 July 2019

- (c) potential new roads to service infill residential areas
- (d) relationships with education services
- (e) roads which will carry heavy traffic flows
- (f) the location and impact of arterial traffic on residential environments
- (g) location for potential traffic calming to reduce through traffic
- (h) access to public transport.

Wheatsheaf Policy Area ([Figure Res/8](#))

Desired Character

Wheatsheaf should continue to provide an alternative residential environment as a consequence of its large allotments, deeper than typical building set-backs from front, side and rear boundaries and potential for low-key business uses provided they are associated with residential use of the land. The Barossa Valley Way commences on the northern side of Wheatsheaf and the roadside edges have considerable potential to be upgraded as a gateway to Gawler.

Horse-keeping will be considered providing the management techniques are acceptable and allotments are sufficient in size to not create environmental problems.

The minimum allotment size should be 2000 square metres with the dwelling density at that level. Hammerhead allotments will be considered providing that part of the allotment which is designed for vehicular access has a minimum width of 6 metres.

Wheatsheaf is intended to be used for detached dwellings only, although small dwellings, commonly referred to as *granny flats*, (that are attached to dwellings), will be considered.

Appropriate Uses are:

- Detached dwellings
- Multiple dwellings (comprising a single dwelling and attached flat)
- Horse-keeping providing the area for horse keeping is a minimum area of 4000 square metres
- Small businesses associated with residential use, where residential is the predominant land use
- Local services (recreation areas, child care centres, community uses).

OBJECTIVE

Objective 1: A residential area characterized by large allotments with a semi rural character.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development in accordance with Structure Plan, [Map Ga/1 \(Overlay 1\) Enlargement C](#).

The Structure Plan indicates:

- (a) the location and impact of arterial traffic on residential environments;
- (b) relationships with business, recreation and service infrastructure; and
- (c) roads which will carry major traffic flows.

- 2 New allotments should have a minimum allotment size of 2000 square metres, although this may be varied by up to 20 percent, ie a minimum of 1600 square metres providing the density of residential development in an application is not less than 2000 square metres.

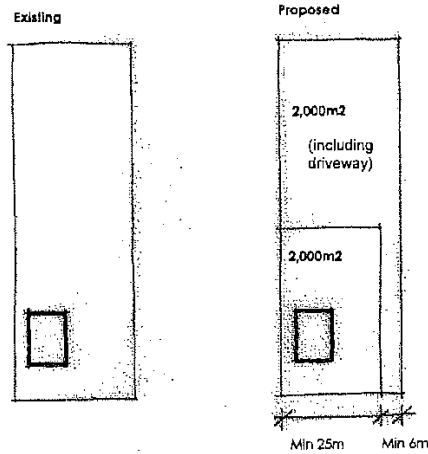


- ++++ Railway
- █ Policy Area Boundary

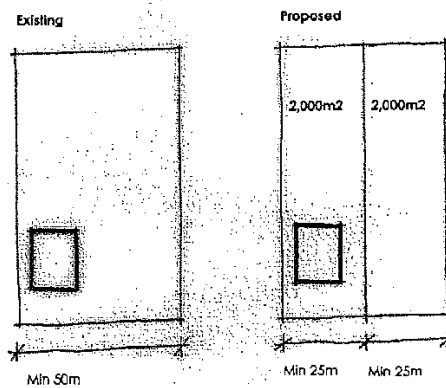
GAWLER (CT)
WHEATSHEAF
RESIDENTIAL POLICY AREA 10
FIGURE Res/8

Consolidated - 18 July 2019

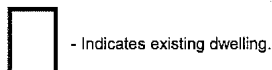
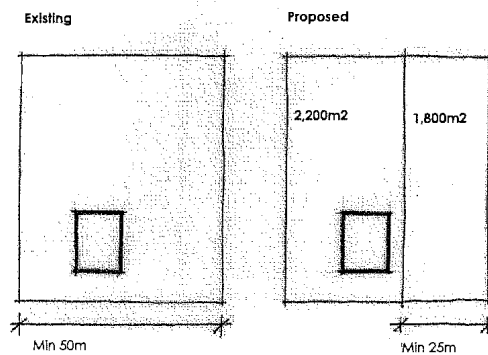
33.1



33.2



33.3



- 3 New allotments should have a minimum frontage to public roads of 25 metres other than a hammerhead shape allotment which should have a width of 6 metres.
- 4 Set-backs for all buildings to be as follows:
 - (a) Road frontage 10 metres
 - (b) Side and rear boundaries for single storey buildings 3 metres
 - (c) Side and rear boundaries for two-storey buildings 6 metres
- 5 Maximum building site coverage should not exceed 40 percent.

Willaston Policy Area ([Figure Res/9](#))

Desired Character

Willaston is tightly defined by the North Para River (which separates it from Gawler), the National Highway and the local government boundary which artificially divides Willaston from Hewett. It comprises approximately 1300 dwellings with no major residential expansion envisaged other than infill of vacant land parcels or residential use of very large gardens or former agricultural plots. These have the potential to increase Willaston's capacity by 200 dwellings or 15 percent. This is in contrast to the suburb of Hewett which is a broadacre residential area that is being progressively transformed to have an ultimate capacity of around 1000 dwellings.

Willaston's townscape comprises a diversity of housing types reflecting the different eras of Willaston's stages of residential development. There are a number of 19th and early 20th Century dwellings on large allotments. These, and their garden settings, should be preserved. Infill development around these dwellings should be sensitively located and designed so as to maintain an appropriate spatial context for the building's scale and architecture.

The large allotments in the Willaston area on [Figure Res/9](#) are ideal for residential infill. In the localities indicated on the Structure Plan as showing the highest potential for infill land division and residential development, provision of coordinated vehicular access and urban services will be critical and development should not proceed unless adequate provision is made for orderly development and coordination of services.

Appropriate Uses are:

Housing
Local Services (recreation areas, schools, child care centres, community uses).

OBJECTIVES

- Objective 1:** A residential area, where the majority of the existing housing stock should be maintained and the number of dwellings increased through infill development of vacant and underutilized sites, particularly areas nominated in Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement A](#).
- Objective 2:** Protection of the character of historic areas through maintenance of the existing allotment pattern and density of residential development.

PRINCIPLE OF DEVELOPMENT CONTROL

- 1 Development in accordance with Structure Plan, [Map Ga/1 \(Overlay 1\) Enlargement A](#).





The Structure Plan indicates:

- (a) the location and associated impact of arterial traffic on residential environments
- (b) locations with the highest potential for infill residential development and land division
- (c) potential new roads to service infill residential areas
- (d) areas subject to flood inundation
- (e) relationships with recreation and centre facilities
- (f) key vehicular interconnection with the suburb of Hewett
- (g) location of aged housing
- (h) roads which will carry major traffic flows.



Note: See figure FI/1 for flood data definition explanation and disclaimer.



-  Flood Prone Land
-  Highest Potential for Infill Land Division
-  Potential New Roads
-  Policy Area Boundary

GAWLER (CT)
WILLASTON
RESIDENTIAL POLICY AREA 11

FIGURE Res/9
 Consolidated - 18 July 2019

RESIDENTIAL (GAWLER EAST) ZONE

Introduction

The Objectives and Principles of Development Control that follow apply in the Residential (Gawler East) Zone shown on Gawler [Maps Ga/5, 6, 9 and 20](#) and Policy Areas [Map Ga/15 and 17](#). They are additional to those expressed for the whole of the Council area.

OBJECTIVES

- Objective 1:** A predominately residential area comprising a range of low and medium-density dwellings, with associated integrated infrastructure, retail, commercial, recreational, educational and community development in master-planned locations in accordance with Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement G](#).
- Objective 2:** Development sensitive to the particular topography of the area and which has minimal visual and environmental impacts.
- Objective 3:** A residential zone comprising a range of dwellings types, including a minimum of 15 percent affordable housing.
- Objective 4:** Increased dwelling densities in close proximity to centres, future public transport routes and public open spaces.
- Objective 5:** Open space systems designed to provide multiple use reserve areas that promote water management, habitat retention and enhancement, and recreational linkages.
- Objective 6:** Development that achieves co-ordination in allotment configuration and physical and community infrastructure provision.
- Objective 7:** Development that contributes to the desired character of the zone.

DESIRED CHARACTER

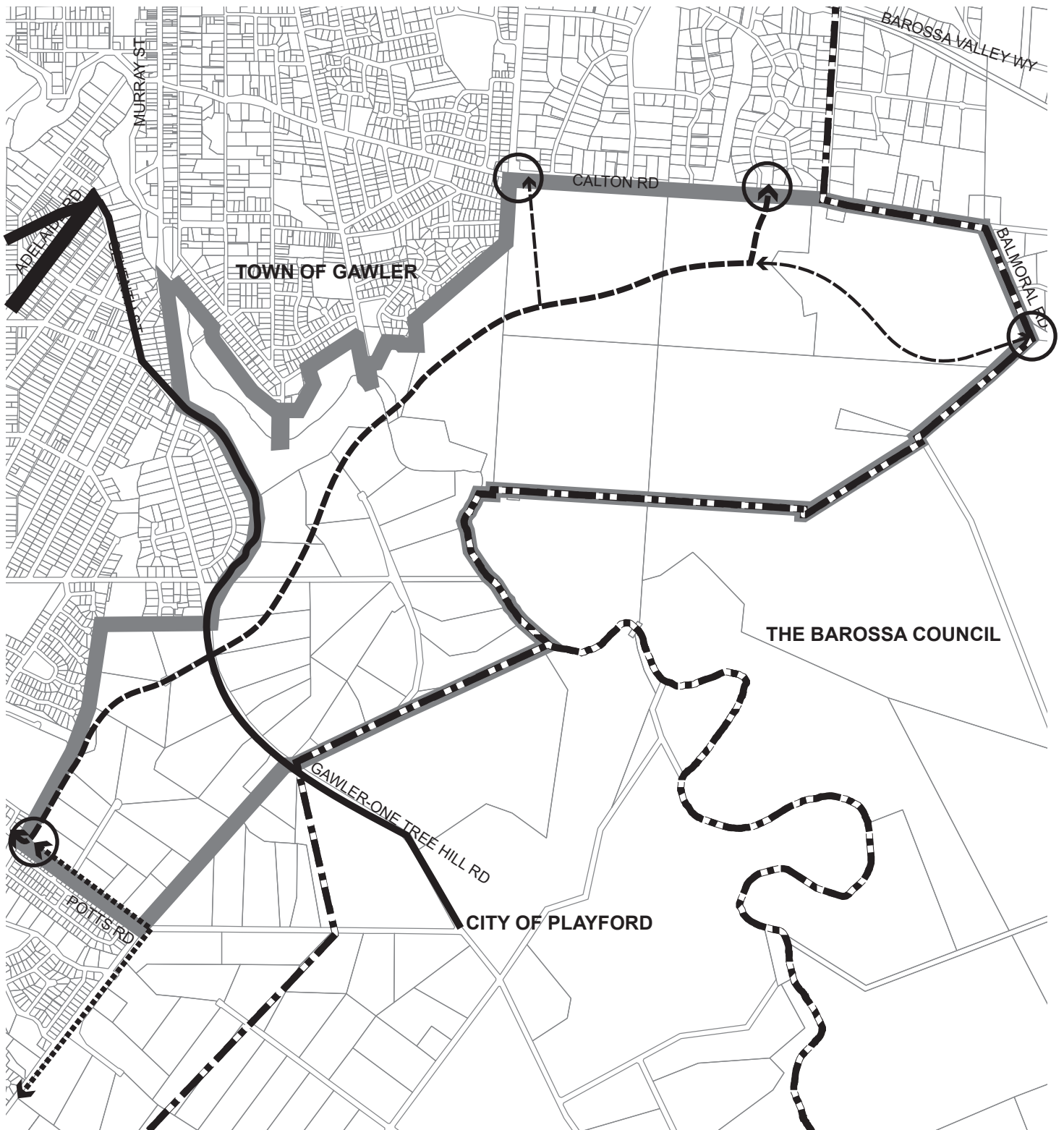
The Residential (Gawler East) Zone is located within both the Town of Gawler and The Barossa Council. The Gawler East area encompasses broad hectare land which is expected to support a population of approximately 10 000 persons.










The zone will develop in accordance with Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement G](#) and be undertaken in an orderly manner that achieves the most efficient use of land, the extension or expansion of infrastructure services and the timely provision of community facilities. No more than 1000 allotments should be created within the area defined by Gawler East Development Constraints Concept Plan [Figure CoP/5](#) until such time as the Gawler East Link Road is complete.

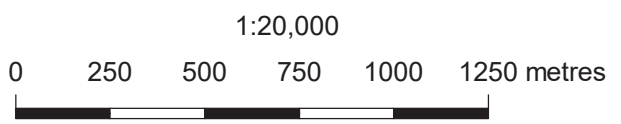
The zone will accommodate a diversity of housing forms. The Mixed Use Centre Policy Area 3 will comprise a mix of residential development and a range of commercial, retail, educational, recreational and community facilities. A smaller range of retail facilities, together with compact housing are anticipated within the Local Centre Policy Area 19.

It is essential that development respects and enhances the natural attributes of the zone through the retention of significant views, creek lines, native vegetation and locations of ecological significance. Innovative and best practice solutions in respect to water reuse, grey water supply and stormwater management will be implemented. Retention of on-site stormwater will be maximised where practical. Any stormwater leaving the site will be dispersed to reduce the risk of erosion to the natural surroundings and impacts to the downstream receiving waters.

Dwellings will range between 1 and 3 storeys in height; however buildings at the interface with adjoining zones other than the Open Space Zone will not exceed 2 storeys.



-  Concept Plan Boundary
-  Primary Arterial Road
-  Secondary Arterial Road
-  Collector Road
-  Road Link
-  Traffic Management
-  Proposed Vehicle access
-  Development Plan Boundary
-  Gawler East Link Road
-  Gawler East Link Road Extension



**GAWLER (CT)
GAWLER EAST
DEVELOPMENT CONSTRAINTS
CONCEPT PLAN FIGURE CoP/5**

Buildings of between 3 and 5 storeys, such as apartment buildings, will be located in the Mixed Use Centre Policy Area 3, the former quarry area, adjacent to open space, and where necessary to frame the end of important or significant vistas. It is important that development achieve a clear transition in building height for a cohesive streetscape.

Housing forms will be simple and incorporate a high degree of articulation to the street façade while delivering a mix of housing types and forms to provide interesting streetscapes and promote social interaction. This will include the provision of recessed of vehicle garaging and the inclusion of front verandas/porticos and appropriate landscaping.

The delivery of housing diversity will require innovative solutions for front or rear access and parking. Rear access will be provided in the form of 'service lanes', which support vehicular access requirements at reduced speeds providing a safe pedestrian environment. At the land division stage, on-street car parking should be provided near areas of proposed medium to high density developments.

Allotment configuration is envisaged to be compact particularly within the Mixed Use Centre Policy Area 3, with building setbacks minimised to assist in facilitating an enclosed and active street. This will incorporate opportunities for multi-storey apartments, terrace and rear lane mews housing that will achieve a range of housing types within a single street.

The slope of the land will dictate the location of particular dwelling types, with some more compact dwelling types located on relatively flat sites, whilst more traditional dwelling types will be located on those portions of the site with moderate to high slope. On topographically steep sites alternative design solutions, such as split level dwellings, that are able to meet desired earthworks and driveway gradient outcomes are appropriate.

The form and distribution of major open space will be influenced by Council Wide Open Space Development Plan provisions and by the need for integrated stormwater detention, treatment and re-use given limitations on the potable water supply for the area. It will also be influenced by the location of natural drainage corridors, and the need to integrate with existing corridors, including the eastern escarpment at Evanston Park. Public open space areas will need to accommodate both active and passive recreation opportunities and the retention of identified conservation areas.

A co-ordinated network of linear parks, including cohesive pedestrian and bicycle movement corridors and visual links, will be established between the new development and adjoining natural creek lines, public recreation areas, local shopping and community services and surrounding road networks.

Steeper environments and watercourses that are proposed to be vested to Council as reserves and/or easements, will be restored to an appropriate natural state in consultation with the Town of Gawler.

Steeper slopes result in higher bushfire risk. Providing vehicle access between the Open Space Zone and residential development together with appropriate buffers assists in managing bushfire risk. Reference to Australian Standard AS3959 – 2009 (incorporating amendments to 2009 and 2011) is recommended to inform and reduce bushfire risk. Any revegetation of areas will take into account the level of bushfire risk, local topography and the need for ongoing management of bushfire risk, ensuring significant hazards are neither created nor exacerbated.

Reserves for public purposes such as active and passive recreation and stormwater management, including the provision of associated infrastructure, will be designed to an appropriate standard and level of service suitable to the locality to ensure long term economic, environmental and social sustainability.

Portion of the eastern boundary of the zone is located adjacent to the Para Woodland Reserve. It is essential that development form an appropriate interface with the Para Woodland Reserve. The interface will act as a buffer between the residential area and the Reserve, balancing access, management of bushfire risk, management of potential invasion by pest plants, minimising the impact of domestic pets on native wildlife and as a provision of open space. The interface will vary in width as appropriate to meet the above criteria and will comprise of a combination of roads, paths, public open space and, where appropriate, areas of natural character for stormwater management. Where housing is included in the interface area it is expected that houses will address the Reserve. The interface area will be planted with locally indigenous species (mainly groundcovers and low shrubs) selected to minimise the bushfire risk by providing an area of reduced fuel hazard.

The north-eastern, eastern and southern boundary of the zone adjoins agricultural, rural and rural living land. It is essential that development provide an appropriate buffer between dwellings and land used for agriculture. Larger allotments together with open space and road networks and increased dwelling setbacks will be established at the peripheries of the zone boundary in order to provide an appropriate low density transition and interface with adjacent rural and rural living land.

The Gawler East Link Road shown on Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement G](#) is intended to have a boulevard character comprising wide footpaths and cycle paths on both sides and substantive street tree plantings. Dwellings will front and address the road with setbacks to contribute to the boulevard character. In addition, dwellings fronting the road will be sited, designed and constructed to mitigate traffic noise impacts.

Infrastructure Corridors have been created in response to the presence of key public infrastructure, namely 132 kV and 275 kV transmission lines. This infrastructure comprises a significant component of the State's high voltage power transmission network. The zone provisions are aimed at protecting this significant public infrastructure from encroachment by incompatible land uses and protecting the infrastructure corridors from being fragmented by land division and therefore ensuring on-going access for maintenance is available and that the security and reliability of the power network is not compromised. No residential allotments should infringe on the corridors or existing easements. These corridors provide the opportunity for co-locating compatible land use activities such as other appropriate infrastructure, at-grade car parking and roads, a linear park or a cycling/walking trail.

Development should have regard to the impact of high noise emission and adverse air quality sources, such as from the high-pressure gas transmission pipeline valve, non-residential development and the Gawler East Link Road. Dwellings fronting this road will be sited, designed and constructed to mitigate traffic noise and adverse air quality impacts.

An Aboriginal heritage survey has been undertaken for the Gawler East area. It indicates that prior to colonial contact the South Para River and its tributaries were associated with long-term seasonal Aboriginal occupation, with adjoining hill crests providing views of culturally significant landscape features and surveillance for the Kurna Nation peoples. While there has since been a high level of ground disturbance in the area, there is the potential for future activities to disturb sub-surface archaeological deposits particularly along the banks of the South Para River. Activities within this area which may damage, disturb or interfere with any Aboriginal site or object need to be undertaken in accordance with the provisions of the *Aboriginal Heritage Act 1988*.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

1 The following forms of development are envisaged in the zone:

- Affordable housing
- Aged persons' accommodation
- Community facilities
- Domestic outbuilding in association with a dwelling
- Domestic structure
- Dwelling
- Dwelling addition
- Dwelling with associated home based business uses
- Land division
- Non-residential use that serves the local community, for example:
 - child care facility
 - health and welfare service
 - open space
 - pre-school
 - primary and secondary school
 - recreation area
 - shop, office or consulting room
- Retirement village
- Supported accommodation

- 2 Development listed as non-complying is generally inappropriate.

Form and Character

- 3 Development should not be undertaken unless it is consistent with the desired character for the zone.
- 4 Development should occur in accordance with Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement G](#).
- 5 Road reserves should be of a width, design and alignment that can:
- (a) provide for safe and convenient movement and parking of vehicles and other users according to projected vehicle volumes, speeds and the character of the road;
 - (b) accommodate bus routes where required;
 - (c) provide for shared, on-street parking bays for nearby residents and visitors wherever practical to achieve unrestricted movement along collector roads;
 - (d) allow vehicles to enter or reverse from an allotment or garage in a single movement, allowing for cars parked on the opposite side of the road (where applicable) or fixed infrastructure on the street;
 - (e) allow for the efficient movement of service and emergency vehicles; and
 - (f) accommodate street planting, landscaping, street furniture and utilities infrastructure.
- 6 **Direct allotment access may be provided to the Gawler East Link Road:**
- (a) where the Road gradient is not more than 8 percent at the point the driveway accesses the Road; or
 - (b) where the Road gradient is more than 8 percent if the allotment has a regular frontage of at least 25 metres.
- 7 Dwellings fronting the Gawler East Link Road should be sited, designed and constructed to mitigate traffic noise and adverse air quality impacts.
- 8 The use and placement of outbuildings should be ancillary to and in association with a dwelling or dwellings.

Land Division

- 9 Land division should facilitate the provision of a broad range of housing options, including affordable housing.
- 10 Land division should accommodate open space and movement networks that provide for strong connections and safe and convenient access to public facilities, public transport and potential future development of adjoining sites.
- 11 Land division:
- (a) should not exceed 1000 allotments until at least the following infrastructure indicated by Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement G](#) has been constructed:
 - (i) the Gawler East Link Road between Calton Road and Gawler - One Tree Hill Road; and
 - (ii) the Gawler East Link Road between Gawler - One Tree Hill Road and Potts Road; and

- (iii) an upgrade of Potts Road and its intersection with Main North Road to accommodate the traffic flows associated with further continued development.
 - (b) should not prejudice the construction of the Gawler East Link Road as detailed in the Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement G](#) and associated pedestrian and bicycle paths, landscaping treatments and public lighting.
 - (c) should facilitate the construction of on and off-site stormwater management systems and community facilities.
- 12** Land division creating reserves containing steeper environments and watercourses, that are proposed to be vested to Council, should be restored to an appropriate natural state through the removal of declared plant species, control of other environmental weeds and the planting and management of native vegetation in order to achieve desired amenity, recreation, conservation, biodiversity, management of bushfire risk and ongoing management outcomes.
- 13** Land division creating reserves for public purposes such as stormwater management and active and passive recreation, including the provision of associated infrastructure, should be designed to an appropriate standard and level of service suitable for the locality to ensure long term economic, environmental and social sustainability.
- 14** Rear lanes should:
- (a) have a minimum reserve width of 6.5 metres;
 - (b) be limited in length to a maximum of 100 metres;
 - (c) have a minimum carriageway width of 5.5 metres;
 - (d) include protuberances to accommodate landscaping and lighting should not exceed 1.0 metre;
 - (e) landscaping should be in the form of tall vertical trees in preference to low level shrubs;
 - (f) be designed to accommodate garbage trucks and emergency service vehicles.
- 15** Public lighting should be provided to all public roads, laneways, paths and open spaces.
- 16** Development with frontage to the eastern side of the Gawler – One Tree Hill scenic road should be established on allotments of no less than 1000 square metres in area and incorporate high quality of building articulation and appropriate landscaping between buildings and the road in order to retain distant vistas, ensure visual interest and a scenic landscape character along this corridor.
- 17** Land located west of the South Para River should not be divided for the purpose of creating additional allotments unless forming part of an integrated development scheme where all infrastructure is delivered to service the land in an orderly and economic manner including potable water supply, grey water supply, waste water disposal, formed all-weather public roads and access, and stormwater disposal.
- 18** Detention and/or retention basins should incorporate good design techniques that:
- (a) allow sediments to settle so as to treat stormwater prior to discharge into watercourses;
 - (b) allow for sediments to be easily removed from the basin and disposed of off-site;
 - (c) ensure human health and safety, particular with respect to high velocity drainage points;
 - (d) ensures the control of mosquitoes and nuisance insects (e.g. midges); and

- (e) ensure detention and retention infrastructure (in conjunction with Water Sensitive Design) is designed to have a capacity that limits outflow to predevelopment flow rates in a 100 year ARI event
- (f) improve water quality through treatment of detained water.

19 Transmission lines should be protected from encroachment through the provision of:

- (a) a 30 metre wide corridor (15 metres each side from the centreline) for the 132kV line;
- (b) a 100 metre wide corridor for the 275kV line as required by relevant authorities.

20 Residential allotments should not be created within the Major Transmission Infrastructure Corridors shown on Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement G](#), or within the existing easements for the 132kV and 275kV transmission lines.

Land Use and Density

21 Housing with an average site area for dwellings less than 250 square metres should be located within the Mixed Use Centre Policy Area and Local Centre Policy Area or within walking distance of public open space, local shops and public facilities.

Built Form/Setbacks

22 Buildings should not exceed the following heights:

- (a) two storey development for properties adjacent to the boundary of adjoining zones other than the Open Space Zone;
- (b) three storeys for the balance of the zone, other than apartment/mixed use buildings within the Mixed Use Centre Policy Area where a 5 storey limit applies.

23 Where allotments have direct frontage to an open space reserve, housing should address the reserve and incorporate a high quality design outcome (i.e. articulation of building facades) and that promotes casual surveillance (i.e. windows and open style fencing that promotes surveillance).

24 Where an allotment immediately adjoins public open space, clear, safe and convenient pedestrian access should be provided to the dwelling.

25 Residential building setbacks should satisfy the minimum dimensions outlined in Table 1 except where a proposed plan of division is accompanied by a building envelope plan that demonstrates that lesser building setbacks will contribute to the achievement of the desired character for the zone:

Table 1

Parameter	Value
Primary Street Frontage (excluding arterial or collector roads forming the zone boundary and the One Tree Hill Scenic Road shown on Map Ga/1 (Overlay 1) Part A and Part B)	3.0 metres to front facade 1.5 metres for dwellings where vehicle access obtained from the rear or side 1.5 metres to veranda/balcony elements 0.5 metres to entry porch and portico
Secondary Street Frontage (Corner Lots)	1.5 metres to façade 0.6 metres for dwellings on allotments with a frontage equal to or less than 9.0 metres 0.5 metres for veranda/balcony elements 0 metres for entry porch/portico

Parameter	Value
Side Boundary (excluding road frontage)	0.0 metres for dwellings on allotments with a road frontage equal to or less than 9.0 metres; 0.9 metres for dwellings on allotments with a frontage greater than 9.0 metres, other than a garage wall with a maximum length of 6.0 metres
Rear Boundary (other than rear lane)	0.9 metres
Open Space Reserve Frontage	1.5 metres where dwellings front the reserve 0.5 metres to entry porch and portico, veranda and balcony elements
Single Carport/Garage	metres from primary street frontage 0.5 metres for laneway frontage
Double Carport/Garage	5.0 metres from primary street frontage 0.5 metres for laneway frontage
Arterial or Collector Roads	5.0 metres to front façade 5.5 metres to carport/garage
Gawler - One Tree Hill Scenic Road shown on Map Ga/1 (Overlay 1) Part A and Part B	15.0 metres to all buildings (excluding minor protrusions such as porches, porticos, eaves, verandas, balconies or similar)
Rear Lane (upper level dwelling)	0.5 metres for laneway frontage

Private Open Space

26 Dwellings should include private open space which conforms to the requirements of Table 2:

Table 2

Site area of dwelling	Minimum area of private open space	Provisions
Greater than 250 square metres	60 square metres (minimum dimension of 2.5 metres)	(a) Balconies, roof patios, decks and the like, can comprise part of this area provided the area of each is 8 square metres or greater and has a minimum dimension of 2.0 metres. (b) One part of the space should be directly accessible from a living room and have an area of 25 square metres with a minimum dimension of 4 metres and a maximum gradient of 1-in-10.
250 square metres and less	35 square metres (minimum dimension of 2.5 metres)	(a) Balconies, roof patios and the like can comprise part of this area provided the area of each is 8 square metres or greater and has a minimum dimension of 2.0 metres. (b) One part of the space is directly accessible from a living room and has an area of 16 square metres with a minimum dimension of 4 metres and a maximum gradient of 1-in-10.
	25 square metres (minimum dimension of 2.5 metres) where:	(a) The dwelling has no more than two bedrooms (or rooms that could reasonably be used as bedrooms) and a total floor area of not more than 110 square metres (b) Separate areas are provided for the provision of a rainwater tank and the storage of refuse and recycling bins.

Site area of dwelling	Minimum area of private open space	Provisions
Upper level dwellings	Minimum area of private open space	(a) 8 square metres and accessible from a living room.

Amenity and Public Spaces

- 27** Residential development should have regard to existing and possible future noise and adverse air quality sources, such as from the high pressure gas transmission pipeline valve, the Gawler East Link Road and non-residential developments, with respect to site layout, orientation, design and construction to ensure a safe and comfortable residential environment and to minimise conflict with existing non-residential activities
- 28** Development should balance the desire for an open streetscape and passive surveillance with the need for functional privacy. Clear delineation should be provided between public and private spaces, which may incorporate fencing, landscaping or a combination of these elements.
- 29** Retaining walls greater than 1.0 metre in height should:
- (a) not be directly visible from a public road or the public realm
 - (b) where located between a building and a road frontage, be stepped in increments of not more than 1.0 metre
 - (c) not cause significant adverse impacts on the amenity of adjacent properties, particularly in relation to overshadowing, cutting off light or views.
- 30** Residential development should provide an area for the storage of waste receptacles that is screened from primary and secondary street frontages.
- 31** Development should be designed and sited to relate to the slope of the land, so that:
- (a) the bulk and scale of the buildings do not dominate the landscape;
 - (b) the amount of cut and fill is minimised.
- 32** Fences should be located to minimise their visual impact and be constructed of materials which are of a low light-reflective nature.

Bushfire Protection

- 33** To protect against bushfire, dwellings should not be sited within 40 metres of a slope greater than 20 degrees, where the length of the slope is greater than 10 metres and covered by unmanaged vegetation.

Separation of Use

- 34** Development should be designed and sited to minimise negative impact on existing and potential future land uses considered appropriate in the locality.
- 35** Residential development adjacent to non-residential zones and land uses should be located, designed and/or sited to protect residents from potential adverse impacts from non-residential activities.

High Pressure Gas Pipeline

- 36** Development, including land division, in the vicinity of a high pressure pipeline, as depicted on Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement G](#), should:

- (a) not affect the continued operation of the pipeline infrastructure
- (b) be located, designed, and developed having regard to the location of the infrastructure and the potential public safety impacts.

Car Parking

- 37** For each dwelling, the maximum width (including the width of any support structure) of any garage or carport opening that faces a street, should be no greater than six metres or 50 percent of the frontage width, whichever is the lesser, except where a site has a frontage of less than 12 metres and the dwelling is:
- (a) two or more storeys; and
 - (b) incorporates protrusions such as verandas, projecting windows, porches, balconies etc. which provide articulation in the building as it presents to the street, in which case garages or carports should have a maximum width of 6 metres or 80 percent of the width of the site, whichever is the lesser.
- 38** No maximum width applies to garage or carport openings where a site has rear vehicular access and from which vehicular access is obtained.
- 39** Development within Mixed Use Centre Policy Area 3 and Local Centre Policy Area 19 should have car parking in accordance with the following table based on calculations relating to net-leasable floor areas:

Form of Development	No. of required car spaces
Apartment	1 space for every unit plus 1 additional space for every 5 2-bedroom units plus 1 additional space for every 3 bedroom unit with a visitor park of 1 space for every 5 units.
Bank	5.5 spaces per 100 square metres
Office	4 spaces per 100 square metres
Shop	5.5 spaces per 100 square metres

Affordable Housing

- 40** Development should include a minimum 15 percent of residential dwellings for affordable housing.
- 41** Affordable housing should be distributed throughout the zone to avoid over-concentration of similar types of housing in a particular area.

Infrastructure Coordination

- 42** Development, including land division, should only occur once required physical and community infrastructure is either provided, or a legally binding agreement has been executed for the provision of the infrastructure items, including the following:
- (a) Gawler East Link Road as illustrated on Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement G](#), and associated traffic interventions (road and intersection upgrades);
 - (b) public lighting, pedestrian and bicycle paths and associated landscaping treatments associated with the Gawler East Link Road;
 - (c) community facilities (including recreation facilities); and
 - (d) stormwater management systems.

PROCEDURAL MATTERS

Complying Development

43 Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

Non-complying Development

44 Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

Crematorium

Development in the form of land division in the area defined by the Gawler East Development Constraints Concept Plan [Figure CoP/5](#) is non-complying if:

- (a) there exist 1000 allotments within the area defined by the Gawler East Development Constraints Concept Plan [Figure CoP/5](#); and
- (b) the following infrastructure has not been completed
 - (i) the Gawler East Link Road between Calton Road and Gawler - One Tree Hill Road;
 - (ii) the Gawler East Link Road between Gawler - One Tree Hill Road and Potts Road;
 - (iii) an upgrade of Potts Road and its intersection with Main North Road to accommodate the traffic flows associated with further continued development.

Fuel Depot

Horticulture

Hospital except where located within Mixed Use Policy Area 3 Industry

Intensive animal keeping Junk Yard

Major public service depot

Office greater than 150 square metres, except where located within Mixed Use Centre Policy Area 3 and/or Local Centre Policy Area 19

Prescribed mining operations Refuse destructor

Shop or group of shops with a gross leasable floor area exceeding 250 square metres, except where located within Mixed Use Centre Policy Area 3 and/or Local Centre Policy Area 19

Telecommunications Facility above 30 metres in height Warehouse

Waste reception, storage, treatment or disposal, except a sewerage treatment plant.

Public Notification

45 Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development (except where the development is non-complying) are designated:

Category 1

Fencing (including a combination of fencing and retaining walls) up to a maximum height of 2.8 metres

Category 2

All development listed within Principle of Development Control 1 of the Residential (Gawler East) Zone, Mixed Use Policy Area 3 and Local Centre Policy Area 19.

Fencing (including a combination of fencing and retaining walls) with a height in excess of 2.8 metres.

Mixed Use Centre Policy Area 3

Introduction

The Objectives and Principles of Development Control that follow apply in the Mixed Use Centre Policy Area 3 of the Residential (Gawler East) Zone shown on Policy Areas [Map Ga/15](#). They are additional to those expressed for the whole of the Council area.

OBJECTIVES

- Objective 1:** A functional and diverse zone accommodating a mix of commercial, retail, recreation, community, residential, office, consulting rooms and educational uses.
- Objective 2:** Development that minimises any adverse impacts upon the amenity of the locality within the policy area.
- Objective 3:** Development that contributes to the desired character of the policy area.

DESIRED CHARACTER

Mixed Use Centre Policy Area 3 will accommodate retail, commercial, community, education and formal recreation facilities and clubrooms to service the local community. It is envisaged that a full range of residential development will form an integral component of this policy area to activate it outside of commercial and retail business hours. Housing forms will include, but not be limited to, mixed use development within a single building where dwellings will typically be established above non-residential land uses.

Low impact, commercial business activities that provide employment opportunities for the local population are envisaged. Such development will need to have particular regard to ensuring that minimal off-site impacts occur with respect to noise, air, water and waste emissions, commercial traffic generation and movement.

A retail facility, not exceeding 10 000 square metres in floor area, will comprise a supermarket and a range of specialty shops to serve the weekly shopping needs of the community. Restaurants, cafes, hotels (incorporating dining) and take away outlets are envisaged and will develop the centre as a destination point and provide uses that extend the hours of the centres operation to promote surveillance and safety of the adjacent recreation and education facilities.

Commercial business activities and retail facilities will be established in the Neighbourhood Centre area indicated on Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement G](#).

It is envisaged that community facilities and additional educational establishments will be established to complement the existing role of the Gawler Township. Future educational establishments will be located in accordance with the Structure Plan [Map Ga/1 \(Overlay 1\)](#). Schools are expected to form a focal point for the new community providing opportunities for the establishment of significant buildings and shared open space.

In terms of urban design and built form, a 'main street' environment will be created, where buildings address the street and car parks are primarily located to the rear. In order to minimise the overall extent of off street parking shared car parking is encouraged, and the convenience, availability and function of on street parking in mixed use environments will be recognised.

Gathering points for formal and informal community events will be established, either by means of a central pedestrian plaza, a village green or series of nodes.

Commercial and mixed use buildings will be established close to the street frontage and incorporate verandas and other protruding elements in order to create a pedestrian-friendly environment and outdoor dining opportunities. These areas will be enhanced by large street trees, high quality paving, lighting and street furniture. Building facades will be designed in a manner to create diversity of interest through the appearance of an aggregation of smaller buildings.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

1 The following forms of development are envisaged in the policy area:

Advertisement
 Bank
 Child care centre
 Community facility
 Consulting room
 Detached dwelling
 Educational establishment
 Group dwelling
 Health centre
 Home activity
 Hospital
 Hotel
 Indoor recreation centre
 Library
 Licensed premises
 Motel
 Nursing home
 Office
 Office and dwelling
 Personal service establishment
 Petrol filling station
 Place of worship
 Pre-school
 Recreation area
 Residential flat building
 Restaurant
 Retail showroom
 Row dwelling
 Semi-detached dwelling
 Serviced accommodation
 Shop
 Shop and dwelling
 Supermarket.

2 Development listed as non-complying is generally inappropriate.

Form and Character

3 Development should be designed to ensure that:

- (a) buildings are designed to address the street frontage with servicing areas located internal to the centre and appropriately screened from public view;
- (b) the establishment of shared car parking areas to the rear of buildings and on-street.

4 Public space established should be activated by uses around its edges.

5 Buildings should have a maximum of 5 storeys in height.

6 Development should not be undertaken unless it is consistent with the desired character for the policy area.

7 Filling of land and retaining walls exceeding 1.0 metre in height are appropriate where they are associated with the remediation and development of the former quarry site.

Local Centre Policy Area 19

Introduction

The Objectives and Principles of Development Control that follow apply in the Local Centre Policy Area 19 of the Residential (Gawler East) Zone shown on Policy Area [Map Ga/17](#). They are additional to those expressed for the whole of the Council area.

OBJECTIVES

- Objective 1:** A policy area accommodating small-scale convenience shopping, office, medical and community facilities to serve the day-to-day needs of the local community.
- Objective 2:** A policy area characterised by a traditional corner store or small groups of shops located within easy walking distance of the population they serve.
- Objective 3:** A policy area accommodating residential development in conjunction with non-residential development.
- Objective 4:** Development that contributes to the desired character of the policy area.

DESIRED CHARACTER

The Local Centre Policy Area 19 will be developed as a mixed use centre providing daily purchase opportunities for the population at the southern end of the zone. A supermarket of 1000 to 1500 square metres is anticipated.

A diversity of housing forms will also be established in the zone, taking advantage of the relatively flat land and access to retail services.

Buildings will be designed to encourage active street frontages and minimise the visibility of car parking from the public realm.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the policy area:

- Advertisement Community facility
- Consulting room
- Detached dwelling
- Group dwelling
- Office
- Office and dwelling
- Residential flat building
- Row dwelling
- Semi-detached dwelling
- Shop
- Shop and dwelling
- Supermarket.

- 2 Development listed as non-complying is generally inappropriate.

Form and Character

- 3 Development should not be undertaken unless it is consistent with the desired character for the policy area.

Land Division

- 4 Land division in the policy area is appropriate provided new allotments are of a size and configuration to ensure the objectives of the policy area can be achieved.

RESIDENTIAL HILLS ZONE

Introduction

The Objectives and Principles of Development Control that follow apply in the Residential Hills Zone shown on Gawler [Maps Ga/5, 6, 9 and 20](#). They are additional to those expressed for the whole of the Council area.

OBJECTIVES

- Objective 1:** A residential zone primarily comprising of detached dwellings at low densities in accordance with Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement G](#).
- Objective 2:** Residential development sensitive to the particular topography of the area and which has minimal visual and environmental impacts.
- Objective 3:** Open space systems designed to provide multiple use reserve areas that promote water management, habitat retention and enhancement, and informal recreational linkages.
- Objective 4:** Development that achieves co-ordination in allotment configuration and physical and community infrastructure provision.
- Objective 5:** Development that contributes to the desired character of the zone.

DESIRED CHARACTER

The Residential Hills Zone comprises both undulating and in parts topographically steep land. Key features of the land are existing interfaces with the South Para River, other natural drainage corridors and remnant native vegetation. The majority of allotments within the zone contain detached dwellings, some of which are associated with low intensity rural uses.

The zone will develop in accordance with Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement G](#) and be undertaken in an orderly manner that achieves the most efficient use of land, the extension or expansion of infrastructure services and the timely provision of community facilities. No more than 1000 allotments should be created within the area defined by Gawler East Development Constraints Concept Plan [Figure CoP/5](#) until such time as the Gawler East Link Road is completed.

Development will comprise predominantly detached dwellings. This is a consequence of the slope of the land, some of which is too steep to readily accommodate compact housing forms.

The southern boundary of the zone adjoins agricultural, rural and rural living land. It is essential that development provide an appropriate buffer between dwellings and land used for agriculture. A range of allotment sizes for residential development will be provided for, with small allotments being established on land which is both central to the zone and on land which is not topographically steep. Larger allotments together with open space and road networks and increased dwelling setbacks will be established at the peripheries of the zone boundary in order to provide an appropriate low density transition and interface with adjacent rural and rural living land.

It is essential that development respects and enhances the natural attributes of the zone through the retention of significant views, creek lines, native vegetation and locations of ecological significance. Innovative and best practice solutions in water reuse, grey water supply and stormwater management will be implemented.

Steeper environments and watercourses that are proposed to be vested to Council as reserves and/or easements, will be restored to an appropriate natural state in consultation with the Town of Gawler.

Steeper slopes result in higher bushfire risk. Providing vehicle access between the Open Space Zone and residential development together with appropriate buffers assists in managing bushfire risk. Reference to Australian Standard AS3959 – 2009 (incorporating amendments to 2009 and 2011) is recommended to inform and reduce bushfire risk. Any revegetation of areas will take into account the

level of bushfire risk, local topography and the need for ongoing management of bushfire risk, ensuring significant hazards are neither created nor exacerbated.

Reserves for public purposes such as active and passive recreation and stormwater management, including the provision of associated infrastructure, will be designed to an appropriate standard and level of service suitable to the locality to ensure long term economic, environmental and social sustainability.

Development will be designed to minimise alteration to the natural landform, including the use of split level dwellings designed to minimise cut and fill. The natural features of the land will be used to minimise the visual dominance of dwellings, outbuildings and retaining walls, particularly when viewed from the street.

Mature trees and extensive landscaping will contribute to the zone's desired character and where practical be located in wide road reserves and public open spaces. All development will need to be designed and managed to ensure adequate fire protection measures can be established and maintained.

Housing forms will be simple and incorporate a high degree of articulation to the street façade while delivering a mix of housing types and forms to provide interesting streetscapes and promote social interaction. This will include the provision of recessed vehicle garaging and inclusion of front verandas/porticos and appropriate landscaping.

The relationship between landscaping contained within road reserves and private gardens will be maintained and reinforced to promote a heavily vegetated character for the area.

The form and distribution of major open space will be influenced Council Wide Open Space Development Plan provisions and by the need for integrated stormwater detention, treatment and re-use given limitations on the potable water supply for the area. It will also be influenced by the location of natural drainage corridors, and the need to integrate with existing corridors, including the eastern escarpment at Evanston Park. Public open space areas will need to accommodate both active and passive recreation opportunities and the retention of identified conservation areas.

A co-ordinated network of linear parks, including cohesive pedestrian and bicycle movement corridors and visual links, will be established between the new development and adjoining natural creek lines, public recreation areas, local shopping and community services and surrounding road networks.

The Gawler East Link Road shown on Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement G](#) is intended to have a boulevard character comprising wide footpaths and cycle paths on both sides and substantive street tree plantings. Dwellings will front and address the road with setbacks to contribute to the boulevard character.

Development should have regard to the impact of high noise emission and adverse air quality sources, such as from the high-pressure gas transmission pipeline valve, non-residential development and the Gawler East Link Road. Dwellings fronting this road will be sited, designed and constructed to mitigate traffic noise and adverse air quality impacts.

An Aboriginal heritage survey has been undertaken for the Gawler East area. It indicates that prior to colonial contact the South Para River and its tributaries were associated with long-term seasonal Aboriginal occupation, with adjoining hill crests providing views of culturally significant landscape features and surveillance for the Kaurna Nation peoples. While there has since been a high level of ground disturbance in the area, there is the potential for future activities to disturb sub-surface archaeological deposits particularly along the banks of the South Para River. Activities within this area which may damage, disturb or interfere with any Aboriginal site or object need to be undertaken in accordance with the provisions of the *Aboriginal Heritage Act 1988*.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the zone:
 - Detached dwelling
 - Domestic outbuilding in association with a dwelling
 - Domestic structure
 - Dwelling addition
 - Dwelling with associated home based business uses
 - Semi-detached dwelling
 - Small scale non-residential use that serves the local community
 - Supported accommodation.
- 2 Development listed as non-complying is generally inappropriate.
- 3 Development should occur in accordance with Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement G](#).
- 4 Road reserves should be of a width, design and alignment that can:
 - (a) provide for safe and convenient movement and parking of vehicles and other users according to projected vehicle volumes, speeds and the character of the road;
 - (b) accommodate bus routes where required;
 - (c) provide for shared, on-street parking bays for nearby residents and visitors wherever practical to achieve unrestricted movement along collector roads;
 - (d) allow vehicles to enter or reverse from an allotment or garage in a single movement, allowing for cars parked on the opposite side of the street (where applicable) or fixed infrastructure on the street;
 - (e) allow for the efficient movement of service and emergency vehicles; and
 - (f) accommodate street tree planting, landscaping, street furniture and utilities infrastructure.
- 5 Vacant or underutilised land should be developed in an efficient and co-ordinated manner to increase housing choice by providing dwellings at densities higher than, but compatible with adjoining residential development.
- 6 Development of semi-detached dwellings and land division for such purposes should only occur where the gradient of each semi-detached dwelling site is no steeper than 1-in-8.
- 7 Non-residential development such as shops, schools and consulting rooms should be of a nature and scale that:
 - (a) serves the local community;
 - (b) is consistent with the character of the locality;
 - (c) does not detrimentally impact on the amenity of nearby residents.
- 8 Development with frontage to the Gawler – One Tree Hill scenic road should be established on allotments no less than 1000 square metres in area and incorporate high quality of building articulation and appropriate landscaping between buildings and the road in order to retain distant vistas, ensure visual interest and a scenic landscape character along this corridor.

- 9 Direct allotment access may be provided to the Gawler East Link Road:
- (a) where the Road gradient is not more than 8 percent at the point the driveway accesses the Road; or
 - (b) where the Road gradient is more than 8 percent if the allotment has a regular frontage of at least 25 metres.

Form and Character

- 10 Development should not be undertaken unless it is consistent with the desired character for the zone.
- 11 Development should be designed and sited to relate to the slope of the land, so that:
- (a) the bulk and scale of the buildings do not dominate the landscape;
 - (b) the amount of cut and fill is minimised through the use of split level dwelling design;
 - (c) the amount of cutting and filling of the natural ground profile is minimized;
 - (d) views from adjoining dwellings and public open spaces are maintained.
- 12 Fences should be located to minimise their visual impact and be constructed of materials which are of a low light-reflective nature.
- 13 Retaining walls greater than 1.0 metre in height should:
- (a) not be directly visible from a public road or the public realm
 - (b) where located between a building and a road frontage, be stepped in increments of not more than 1.0 metre
 - (c) not detract from the amenity of adjacent properties and, in particular, not result in overshadowing, cutting off light or views.
- 14 For each dwelling, the maximum width (including the width of any support structure) of any garage or carport opening that faces a street, should be no greater than six metres or 50 percent of the frontage width, whichever is the lesser except where a site has frontage of less than 12 metres and the dwelling is:
- (a) two or more storeys; and
 - (b) incorporates protrusions such as verandas, projecting windows, porches, balconies etc. which provide articulation in the building as it presents to the street, in which case garages or carports should have a maximum width of 6 metres or 80 percent of the width of the site, whichever is the lesser.
- 15 Dwellings fronting the Gawler East Link Road should be sited, designed and constructed to mitigate traffic noise and adverse air quality impacts.

Building Setbacks

- 16 Buildings should be setback from the allotment boundary on the primary frontage as follows:
- (a) metres where the allotment immediately adjoins a public reserve and has rear vehicular access;
 - (b) no less than of 3.0 metres elsewhere (excluding minor protrusions such as a porch, portico, eave, veranda, balcony or similar).

- 17 Dwellings (other than minor protrusions) should be setback from property boundaries in accordance with the minimum dimensions contained in Table 1 except where a proposed plan of plan that demonstrates that lesser building setbacks will contribute to the achievement of the desired character for the zone.
- 18 Residential building setbacks should satisfy the minimum dimensions outlined in Table 1 except where a proposed plan of division is accompanied by a building envelope plan that demonstrates that lesser building setbacks will contribute to the achievement of the desired character for the zone:

Table 1

Parameter	Value
Primary Street Frontage (excluding arterial or collector roads forming the zone boundary and the One Tree Hill Scenic Road shown on Map Ga/1 (Overlay 1) Part A and Part B)	3.0 metres to front façade 1.5 metres for dwellings where vehicle access obtained from the rear or side 1.5 metres to veranda/balcony elements 0.5 metres to entry porch and portico
Secondary Street Frontage (Corner Lots)	1.5 metres to façade 0.6 metres for dwellings on allotments with a frontage equal to or less than 9.0 metres 0.5 metres for veranda/balcony elements 0 metres for entry porch/portico
Side Boundary (excluding road frontage)	0.0 metres for dwellings on allotments with a road frontage equal to or less than 9.0 metres; 0.9 metres for dwellings on allotments with a frontage greater than 9.0 metres, other than a garage wall with a maximum length of 6.0 metres
Rear Boundary (other than rear lane)	3.0 metres
Open Space Reserve Frontage	1.5 metres where dwellings front the reserve 0.5 metres to entry porch and portico, veranda and balcony elements.
Single Carport/Garage	5.0 metres from primary street frontage 0.5 metres for laneway frontage
Double Carport/Garage	5.0 metres from primary street frontage 0.5 metres for laneway frontage
Arterial or Collector Roads	5.0 metres to front facade 5.5 metres to carport/garage
Gawler - One Tree Hill Scenic Road shown on Map Ga/1 (Overlay 1) Part A and Part B	15.0 metres to all buildings (excluding minor protrusions such as porches, porticos, eaves, verandas, balconies or similar)
Rear Lane (upper level dwelling)	0.5 metres for laneway frontage

Private Open Space

19 Dwellings should include private open space which conforms to the requirements of Table 2:

Table 2

Site area of dwelling	Minimum area of private open space	Provisions
Greater than 250 square metres	60 square metres (minimum dimension of 2.5 metres)	(a) Balconies, roof patios, decks and the like, can comprise part of this area provided the area of each is 8 square metres or greater and has a minimum dimension of 2.0 metres. (b) One part of the space should be directly accessible from a living room and have an area of 25 square metres with a minimum dimension of 4 metres and a maximum gradient of 1-in-10.
250 square metres and less	35 square metres (minimum dimension of 2.5 metres)	(a) Balconies, roof patios and the like can comprise part of this area provided the area of each is 8 square metres or greater and has a minimum dimension of 2.0 metres. (b) One part of the space is directly accessible from a living room and has an area of 16 square metres with a minimum dimension of 4 metres and a maximum gradient of 1-in-10.
	25 square metres (minimum dimension of 2.5 metres) where:	(a) The dwelling has no more than two bedrooms (or rooms that could reasonably be used as bedrooms) and a total floor area of not more than 110 square metres (b) Separate areas are provided for the provision of a rainwater tank and the storage of refuse and recycling bins.
Upper level dwellings	Minimum area of private open space	(a) 8 square metres and accessible from a living room.

20 All buildings should be set-back at least 15 metres from the Gawler - One Tree Hill Scenic Road shown on Gawler (CT) Structure Plan [Map Ga/1 \(Overlay 1\) Part A and Part B](#).

21 Development that would be prominently visible from the Adelaide plains should:

- (a) achieve a profile that blends with the topography of the land;
- (b) avoid the use of bright and highly reflective external materials and finishes;
- (c) incorporate existing vegetation wherever possible and additional landscaping to assist in reducing the apparent bulk and scale of the building and any site works.

22 Development should take account of the height and bulk of the proposed building relative to adjoining dwellings by:

- (a) incorporating stepping in the design in accordance with the slope of the land;
- (b) in the case of two storey dwellings, setting back the upper storey of a dwelling a greater distance from front and side boundaries than the lower storey.

- 23** Dwellings and associated structures should have a site area and frontage distance that is consistent with the provisions of this zone, including, the advancement of:
- (a) low density detached dwellings; and
 - (b) development that is sensitive to the particular topography of its locality and which results in minimal visual and environmental impacts.
- 24** Residential development should provide an area for the storage of waste receptacles that is screened from primary and secondary street frontages.
- 25** Land should not be divided for the purpose of creating additional allotments unless forming part of an integrated development scheme where all infrastructure is delivered to service the land in an orderly and economic manner including potable water supply, grey water supply, waste water disposal, formed all-weather public roads and access, and stormwater disposal.
- 26** Land division:
- (a) should not exceed 1000 allotments until at least the following infrastructure indicated by Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement G](#) has been constructed:
 - (i) the Gawler East Link Road between Calton Road and Gawler - One Tree Hill Road; and
 - (ii) the Gawler East Link Road between Gawler - One Tree Hill Road and Potts Road; and
 - (iii) an upgrade of Potts Road and its intersection with Main North Road to accommodate the traffic flows associated with further continued development.
 - (b) should not prejudice the construction of the Gawler East Link Road as detailed in the Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement G](#) and associated pedestrian and bicycle paths, landscaping treatments and public lighting.
 - (c) should facilitate the construction of on and off-site stormwater management systems and community facilities.
- 27** Land division creating reserves containing steeper environments and watercourses, that are proposed to be vested to Council, should be restored to an appropriate natural state through the removal of declared plant species, control of other environmental weeds and the planting and management of native vegetation in order to achieve desired amenity, recreation, conservation, biodiversity, management of bushfire risk and ongoing management outcomes.
- 28** Land division creating reserves for public purposes such as stormwater management and active and passive recreation, including the provision of associated infrastructure, should be designed to an appropriate standard and level of service suitable to the locality to ensure long term economic, environmental and social sustainability.
- 29** Detention and/or retention basins should incorporate good design techniques that:
- (a) allow sediments to settle so as to treat stormwater prior to discharge into watercourses;
 - (b) ensure human health and safety, particular with respect to high velocity drainage points;
 - (c) ensures the control of mosquitoes and nuisance insects (e.g. midges); and
 - (d) ensure detention and retention infrastructure (in conjunction with Water Sensitive Design) is designed to have a capacity that limits outflow to predevelopment flow rates in a 100 year ARI event.

Bushfire Protection

- 30 To protect against bushfire, dwellings should not be sited within 40 metres of a slope greater than trees, where the length of the slope is greater than 10 metres and covered by unmanaged vegetation.

High Pressure Gas Pipeline

- 31 Development, including land division, in the vicinity of a high pressure pipeline, as depicted on Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement G](#), should:
- (a) not affect the continued operation of the pipeline infrastructure
 - (b) be located, designed, and developed having regard to the location of the infrastructure and the potential public safety impacts.

Infrastructure Coordination

- 32 Development, including land division, should only occur once required physical and community infrastructure is either provided, or a legally binding agreement has been executed for the provision of the infrastructure items, including the following:
- (a) Gawler East Link Road as illustrated on Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement G](#), and associated traffic interventions (road and intersection upgrades);
 - (b) public lighting, pedestrian and bicycle paths and associated landscaping treatments associated with the Gawler East Link Road;
 - (c) community facilities (including recreation facilities); and
 - (d) stormwater management systems.

PROCEDURAL MATTERS

Complying Development

- 33 Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

Non-complying Development

- 34 Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

Advertisement and/or advertising hoarding Amusement machine centre

Consulting room except where:

- (a) the total floor area is less than 100 square metres
- (b) the site does not front an arterial road.

Crematorium

Development in the form of land division in the area defined by the Gawler East Development Constraints Concept Plan [Figure CoP/5](#) is non-complying if:

- (a) there exist 1000 allotments within the area defined by the Gawler East Development Constraints Concept Plan [Figure CoP/5](#); and
- (b) the following infrastructure has not been completed in full:
 - (i) the Gawler East Link Road between Calton Road and Gawler - One Tree Hill Road
 - (ii) the Gawler East Link Road between Gawler - One Tree Hill Road and Potts Road

- (iii) an upgrade of Potts Road and its intersection with Main North Road to accommodate the traffic flows associated with further continued development

Dairy
 Farming
 Fuel depot
 Horse keeping
 Horticulture
 Hospital
 Hotel
 Industry
 Intensive animal keeping
 Motor repair station

Office except where:

- (a) the total floor area is less than 100 square metres
 (b) the site does not front an arterial road.

Petrol filling station
 Public service depot
 Restaurant
 Road transport terminal
 Service trade premises

Shop or group of shops except where:

- (a) the total floor area is less than 250 square metres
 (b) the site does not front an arterial road.

Stock sales yard
 Stock slaughter works
 Store
 Warehouse
 Waste reception, storage, treatment or disposal
 Wrecking yard

Public Notification

- 35** Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development are designated:

Category 1

Fencing (including a combination of fencing and retaining walls) up to a maximum height of 2.8 metres

Category 2

Fencing (including a combination of fencing and retaining walls) with a height in excess of 2.8 metres.

RESIDENTIAL PARK ZONE

Introduction

In addition to the Council Wide policies (including the Gawler Rivers Floodplain Area), the Residential Park Zone applies to the areas shown in:

Residential Park Zone: [Maps Ga/4, 8 and 10](#)

Gawler Rivers Floodplain Area: [Figures FI/1 to FI/8](#)

OBJECTIVES

- Objective 1:** A zone which primarily provides for affordable housing opportunities in a landscaped village setting.
- Objective 2:** A zone accommodating low to medium-scale accommodation predominantly in the form of caravan and camping sites, cabins and transportable dwellings, surrounded by open landscaped areas.
- Objective 3:** Development that contributes to the desired character of the zone.

DESIRED CHARACTER

A zone accommodating a range of affordable housing options, within a landscaped setting, that promotes community interaction through its permeable layout, the orientation of buildings, range of shared facilities and quality communal open space.

The zone accommodates a range of low to medium-scale, long-term and short-term accommodation, including camping sites, caravans, relocatable dwellings and cabins.

Buildings, including dwellings, are designed and constructed to enable their relocation, are a maximum of two storeys in height and generously set back from boundaries to allow for landscaped edges of the park. A landscaped village environment will be created.

New development will be sympathetic to the scale, shape, materials and colours of existing adjoining development and landscapes. Vegetation buffers and landscaping will be important in integrating the residential park into the landscape and adjoining urban areas, providing screening from surrounding land uses, as well as reducing visual and noise impacts and providing privacy for park users. The soft landscaping should dominate over buildings and hard surfaces, with caravans only glimpsed in a leafy environment. The park will be enhanced through the further planting of street trees to provide a more leafy and attractive setting and to conceal the built form when viewed from the approaches to the park.

Pedestrian and vehicle linkages provide safe access to employment, shops, public transport facilities, medical services, education facilities, child care and other community facilities. Circulation and movement within the park should be pedestrian friendly and promote low speed vehicle movement.

The portion of the Zone located at Hillier will develop in accordance with the Evanston Gardens/Evanston South/Hillier Structure Plan Map Ga/1 (Overlay 1) Enlargement H and be undertaken in an orderly manner that achieves the most efficient use of the land.

Development will recognise that the lower areas of land fronting Clifford Road are subject to flood risk hazard. Housing and other accommodation forms will not be located within this area, including all land contained within Certificate of Title 5111/77, with development primarily limited to stormwater management. Housing and other accommodation forms may be located on the upper areas of the land, forming a logical extension to the Hillier Residential Village. Direct vehicle access to Clifford Road will not be provided.

Development which is on land subject to inundation in a 1 in 100 year average return interval flood event will not result in the loss of floodplain storage. This can be achieved by incorporating measures to increase floodplain capacity in the locality through the use of reserves, roadways and swales for temporary flood storage, with works designed and certified by a suitably qualified engineer.

Acoustic measures will be provided along the Clifford Road boundary to minimise noise and air quality impacts from traffic on Jack Cooper Drive. These measures will comprise a continuation of the existing earth mounding, landscaping and setbacks for dwellings already provided along the southern portion of Clifford Road. This mounding and associated landscaping will also provide a significant visual screening effect to the residential village when viewed from Jack Cooper Drive and Clifford Road.

Preliminary investigations indicate that the potential for site contamination is low. However, further investigations may be required prior to residential development occurring to confirm this.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the zone:

- Amenity block, including shower, toilet, laundry and kitchen facilities
- Caravan permanently fixed to land
- Cabin
- Camping ground
- Outbuilding ancillary to a dwelling
- Recreation area
- Residential park
- Resident workshop
- Storage area for recreation vehicle
- Swimming pool
- Transportable dwelling

Form and Character

- 2 Development should not be undertaken unless it is consistent with the desired character for the zone.
- 3 Permanent buildings should be limited to a dwelling (manager's house), shop (in association with and ancillary to a residential park), community or recreational facility and toilets/amenities.
- 4 Recreation facilities should be provided of a scale that is suitable to maintain the open natural character of the area and ancillary to the primary role and function of the park.
- 5 At least 16 square metres of contiguous private space, which may be used as a second car parking space, should be provided on each site intended for residential accommodation.
- 6 Caravan, cabin and dwelling sites should be greater than 100 square metres in area.
- 7 Landscaping should form an integral part of the design and be used to define spaces, reinforce internal networks, screen utility areas and enhance the visual amenity of the area.

Car Parking and Access

- 8 Every caravan, cabin or dwelling site should have parking for at least one vehicle, either located on the site or grouped within the park.

Street and Boundary Setbacks

- 9 Every dwelling, annex, caravan fixed to land, recreational facility or amenities building should be set back a minimum of:
- (a) 1 metre from an internal road (and the internal road surfaces should be surfaced to prevent dust becoming a nuisance)
 - (b) 6 metres from a public road

- (c) 2 metres from the boundary of the caravan park or camping ground.

Natural Hazards

- 10** In areas prone to flooding, bushfire or other natural hazards, buildings and structures (including annexes attached to caravans or caravans fixed to land) should be designed and constructed so that they can be removed in the event of a hazard.

Hazards

The following principles apply to land within the Gawler Rivers Floodplain Area identified on [Figures FI/1 to FI/8](#).

Flooding

- 11** Land division should not result in an additional allotment or allotments.
- 12** Dwellings, other than detached dwellings, should not be developed.
- 13** Detached dwellings should only be developed where:
- (a) the finished floor level is equal to or greater than 300 millimetres above the predicted level of a 1 in 100 ARI flood event; and
 - (b) in **Low** hazard flood risk areas:
 - (i) site coverage for all buildings on the allotment does not exceed 50 percent of the area of the allotment; and
 - (ii) set-backs of detached dwellings to side and rear boundaries, but excluding boundaries to a public road, are at least:
 - (A) one metre to side boundaries; and
 - (B) four metres to rear boundaries.
 - (c) in **Medium** hazard flood risk areas:
 - (i) site coverage for all buildings on the allotment does not exceed 40 percent of the area of the allotment; and
 - (ii) set-backs of detached dwellings to side and rear boundaries, but excluding boundaries to a public road, are at least:
 - (A) two metre to side boundaries; and
 - (B) five metres to rear boundaries.
 - (d) in **High** hazard flood risk areas:
 - (i) site coverage for all buildings on the allotment does not exceed 30 percent of the area of the allotment; and
 - (ii) set-backs of detached dwellings to side and rear boundaries, but excluding boundaries to a public road, are at least:
 - (A) three metre to side boundaries; and
 - (B) six metres to rear boundaries.

- 14** Ground floor additions to dwellings should:
- (a) not exceed a 40 square metre increase in floor area; and
 - (b) be the same or higher finished floor level as the existing dwelling; and
 - (c) comply with the applicable site coverage and boundary setback requirements in Principle of development Control 13.
- 15** Dwelling additions in excess of a 40 square metre increase in the existing floor area should:
- (a) be in the form of upper level additions; or
 - (b) have a finished floor level equal to or greater than 300 millimetres above the predicted level of a 1 in 100 ARI flood event; and
 - (c) comply with the applicable site coverage and boundary setback requirements in Principle of Development Control 13.
- 16** Outbuildings may be constructed at natural ground level providing:
- (a) there is no permanent storage of goods or equipment below 300 millimetres above the predicted level of a 1 in 100 ARI flood event level for the applicable hazard flood risk area; and
 - (b) the maximum site coverage requirements in Principle of Development Control 13.
- 17** Development in that part of the Zone north of Hillier Road at Hillier, which is on land subject to inundation in a 1 in 100 year average return interval flood event or within Certificate of Title 5111/77, should be limited to stormwater management.

Land Division

- 18** No additional allotments should be created wholly or partly within the zone except where a lease or license agreement is made, granted or accepted under the *Residential Parks Act 2007*.

PROCEDURAL MATTERS

Complying Development

- 19** Complying developments are prescribed in schedule 4 of the *Development Regulations 2008*.

Non-complying Development

- 20** The following kinds of development are non-complying in the Residential Park Zone:

- Amusement Machine Centre
- Bus Depot
- Cemetery
- Commercial Forestry
- Community Centre, except where in association with and ancillary to a residential park
- Consulting Room
- Crematorium
- Dairy
- Dam
- Dwelling (except for a manager's residence in association with and ancillary to a residential park)
- Educational Establishment
- Farming
- Fuel Depot
- Horse Keeping
- Horticulture

Hospital
 Hotel
 Indoor Recreation Centre, except where in association with and ancillary to a residential park
 Industry
 Intensive Animal Keeping
 Land Division which results in the creation of an additional allotment(s) either wholly or partly within the Zone, except where a lease or license agreement is made, granted or accepted under the *Residential Parks Act 2007*
 Marina
 Motor Repair Station
 Nursing Home
 Office, except where in association with and ancillary to a residential park
 Petrol Filling Station
 Place of Worship
 Pre-school
 Prescribed Mining Operations
 Public Service Depot
 Residential Flat Building
 Restaurant, except where it is both

- (a) less than 150 square metres in gross floor area
- (b) in association with and ancillary to a residential park

 Road Transport Terminal
 Service Trade Premises
 Shop or group of shops, except where it is both:

- (a) less than 150 square metres in gross floor area
- (b) in association with and ancillary to a residential park

 Stadium
 Stock Sales Yard
 Stock Slaughter Works
 Store
 Warehouse
 Waste reception, storage, treatment or disposal, except where in association with and ancillary to a residential park
 Wrecking Yard

Public Notification

21 For the purpose of Section 38 of the *Development Act 1993* the following kinds of development are assigned as **Category 1 Development** in the Residential Park Zone:

Amenity block, including shower, toilet, laundry and kitchen facilities
 Cabin
 Camping Ground
 Caravan permanently fixed to land
 Outbuilding
 Recreation Area
 Residential Park
 Resident Workshop
 Storage Area for recreation vehicle
 Swimming Pool
 Transportable Dwelling

RESIDENTIAL HISTORIC (CONSERVATION) ZONE

Introduction

In addition to the Council Wide policies, the Residential Historic (Conservation) Zone and Policy Area policies apply to the areas shown on:

Gawler Rivers Floodplain Area: [Figures FI/1 to FI/8](#).

Residential Historic (Conservation) Zone: [Maps Ga/3, 5, 6, 12](#).

Gawler Health Services Helipad: [Figure Hel/1](#).

Development in the Gawler Rivers Floodplain Area will not result in an increase in densities or site coverage. Buildings will be designed to prevent the entry by floodwaters. This will primarily be achieved through the raising of floor levels above the predicted flood level, together with increases in setbacks and decreases in site coverage aligned to hazard flood risk areas. In circumstances where this is likely to impact negatively on the historic character, alternative approaches may be appropriate provided flood proofing measures are included within any design and construction.

OBJECTIVES

Objective 1: The Residential Historic (Conservation) Zone is primarily for residential development together with local facilities that support a healthy and convenient living environment.

Five Policy Areas are identified in [Fig Res H\(C\)/1](#) according to:

- (a) historic significance;
- (b) future character;
- (c) the type and nature of development considered appropriate; and
- (d) other features that differentiate one area from the other.

In some localities, which are specifically nominated in Policy Areas, limited types of business use or mixed residential/business use will be appropriate.

Objective 2: Retention of all places (including landscape elements) which contribute to the historic character of the Zone.

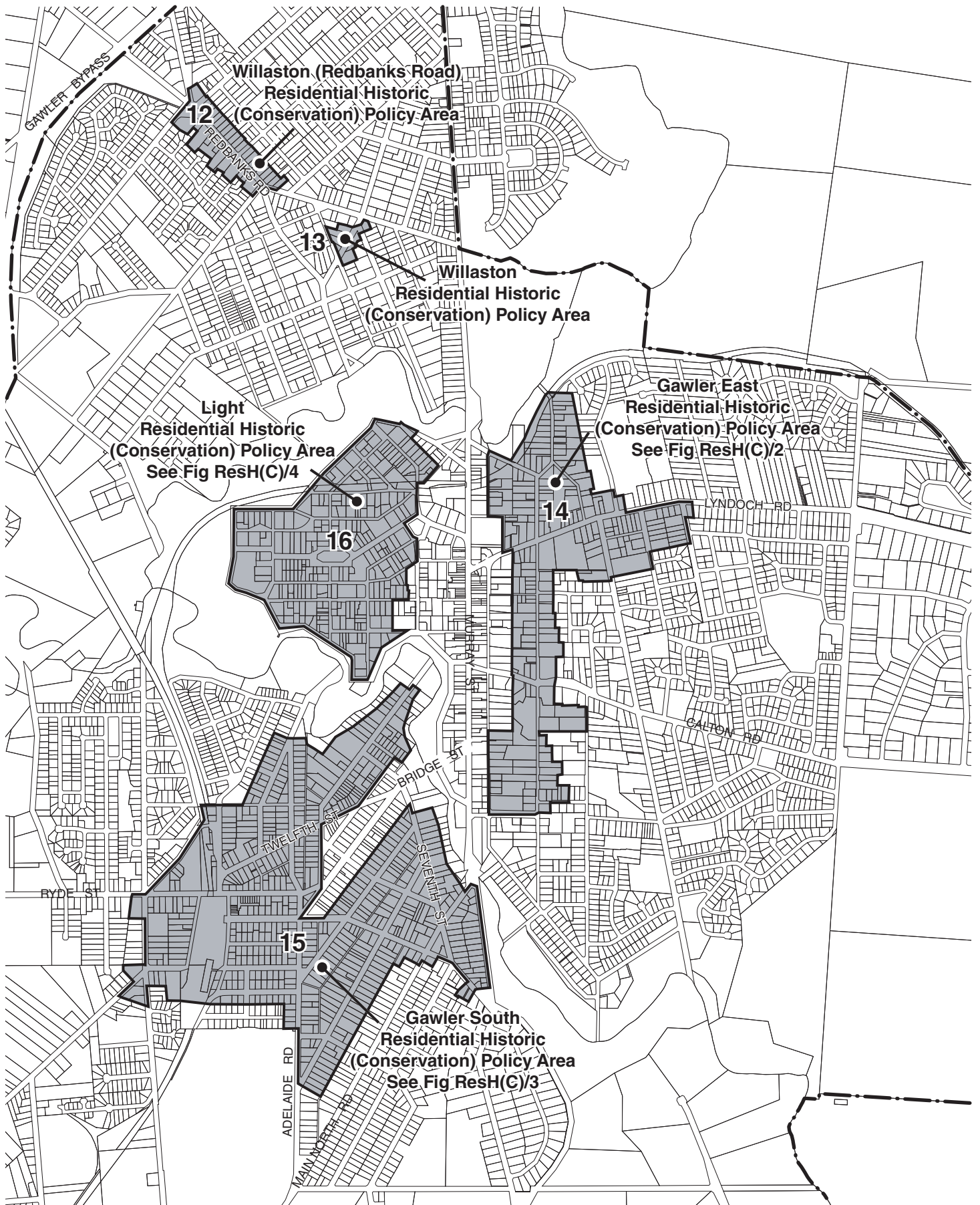
Objective 3: Conservation and enhancement of the historic character of the Zone, through consideration of:



- (a) Streetscape character;
- (b) Subdivision pattern (allotment size and dimensions and street layout);
- (c) Building alignment and set-backs;
- (d) Building form and materials;
- (e) Site layout, landscaping and fencing.

Objective 4: Maintenance of the existing topography and retention of historic townscape vistas and views.

Objective 5: Buildings and structures located and designed to:

- (a) avoid damage from floodwaters; and
- (b) not impede the flow of floodwaters.



-  Residential Historic (Conservation) Policy Area
-  Development Plan Boundary

GAWLER (CT)
INDEX TO RESIDENTIAL
HISTORIC (CONSERVATION)
POLICY AREAS
FIGURE ResH(C)/1

Portions of the zone are within the Gawler Rivers Floodplain Area identified on [Figures FI/1 to FI/8](#). In those areas, increased densities are discouraged and development is limited to detached dwellings at low densities.

Dwellings and ancillary buildings will be designed to minimise entry by floodwaters. This will primarily be achieved through the raising of floor levels above the flood level, together with increases in setbacks and decreases in site coverage aligned to hazard flood risk areas. Alternative solutions may be appropriate in limited circumstances provided specific flood proofing measures are included within any design and construction.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should reinforce and complement the historic character and significance of the area and the integrity of any places and items identified in [Table Ga/2](#), [Table Ga/5](#) or [Table Ga/6](#).
- 2 Vacant land, buildings or sites not having a detrimental affect on the character of the locality should be redeveloped and upgraded in accordance with the historic and future character of the Zone and Policy Areas, and where applicable, the guidelines in [Table Ga/3](#).
- 3 Development should conserve, maintain, enhance and reinforce the existing streetscape character of the Zone and Policy Areas and the historic character of individual buildings, items, structures, and places.
- 4 Development should not adversely impact on the character of an adjacent street, for example by introducing new entrances which require removal of historic slate kerbs, by introducing new entrance surfaces inside the property boundary or on the adjacent street such as concrete or paving which is inconsistent with the historic character of footpaths and street surfaces or by allowing stormwater outlets on streets which due to the placement or materials have a negative effect on the character of the adjacent street.
- 5 Development should enhance and contribute to visual cohesiveness, and any new buildings should be of complementary height, scale, set-back, form and external appearance, and display creative and diverse examples of high standard contemporary architecture.
- 6 Places and items identified in [Table Ga/2](#), [Table Ga/5](#) or [Table Ga/6](#) should not be demolished, but instead be retained, upgraded and adapted such that:
 - (a) the quality or condition which gives the place or item its significance is protected or enhanced. This should include elements of the place which have significant cultural associations;
 - (b) the place or item is put to a use which supports the maintenance and restoration of that building, item, structure or place;
 - (c) the place or item is only altered so as to retain or restore the original external form, materials, and colours in a manner compatible with its structural visual, material and historic integrity;
 - (d) where internal arrangements and finishes are of significance the place or item should only be altered in a manner compatible with its structural visual, material and historic integrity.
- 7 Demolition of a place or item identified in [Table Ga/2](#), [Table Ga/5](#) or [Table Ga/6](#) should not be undertaken unless the building, item, structure or place is so structurally unsound it represents a risk to public safety and is beyond economic repair. Unsympathetic (later) additions may be demolished if the later addition does not contribute to the historic character of the place.
- 8 Development abutting or in close proximity to a place or item identified in [Table Ga/2](#), [Table Ga/5](#) or [Table Ga/6](#) should:

- (a) respect the historic character of the area and the integrity of the particular place or item of significance and be designed to a high architectural standard; and
 - (b) be compatible in respect of its design, siting, scale, building and roof shape, bulk, height, materials and colours, fences and landscaping and any advertising signs and external illumination with the place or item.
- 9** Any alteration or addition to a place or item identified in [Table Ga/2](#), [Table Ga/5](#) or [Table Ga/6](#) should reinforce and complement the historic character of the place in terms of its design, siting, boundary set-backs, scale, built form and roof shape, bulk, height, materials and external finishes and where applicable:
 - (a) additions should minimise their impact on the streetscape by siting them to the rear or the side of existing buildings and not forward of existing buildings;
 - (b) the height of eaves and gutters on additions should be complementary with the existing building;
 - (c) the proportions (vertical and horizontal) of additions visible from the street, should be complementary with those of the existing building;
 - (d) materials may include masonry (stone and brick) render, timber boarding and corrugated steel sheeting (walls and roof);
 - (e) materials such as mirror glass, flat profile steel sheeting, unpainted zincalume sheeting, wide fibre cement sheet planking and polished face concrete panels should not be used where they are visible from the street;
 - (f) roof conversions or additional upper floors are not desirable where they compromise the historic integrity of the building or the view of the building from the street;
 - (g) roof conversions are acceptable where the alterations are fully incorporated under the existing roof and dormer style windows are acceptable where they are not visible from the street; and
 - (h) external colour schemes should be contemporary, but should not dramatically contrast with the original colour scheme of the existing place.
- 10** Carports and garages affecting places or items identified in [Table Ga/2](#), [Table Ga/5](#) or [Table Ga/6](#) should be located behind the principal facade of the building and:
 - (a) may be freestanding;
 - (b) garage doors should not visually dominate the facade of the building when viewed from the street;
 - (c) timber framing for carports is desirable.
- 11** Landscaping which should include fencing at the street boundary, should complement and reinforce the historic character of places or items identified in [Table Ga/2](#), [Table Ga/5](#) or [Table Ga/6](#) and the zone generally.
- 12** Development should have fences to define street boundaries that complement the historic fences found in the locality. Existing traditional front fencing should be preserved in the Zone.
- 13** The design of new fences, or alterations to existing fences should complement and reinforce the historic character of the zone as follows:
 - (a) front fences should complement historic adjacent fences in height, be timber picket, metal palisade, woven wire mesh between timber posts, hedges or rendered masonry with brick copings or other traditional materials (high brush or corrugated steel fences are not appropriate);

- (b) side and rear fences should be timber picket or board, corrugated steel sheeting (natural galvanised or painted finish), hedges or rendered masonry with brick copings or other traditional materials;
 - (c) side fences should match the front fence in height for the depth of the front yard.
- 14 Division of an allotment containing a place or item identified in [Table Ga/2](#), [Table Ga/5](#) or [Table Ga/6](#) should not be undertaken if it may detrimentally affect its heritage significance.
- 15 New allotments should reinforce the integrity of and complement the local historic character.

Hazards

The following principles apply to land within the Gawler Rivers Floodplain Area identified on [Figures FI/1 to FI/8](#).

Flooding

- 16 Land division should not result in an additional allotment or allotments unless;
- (a) it is for the purpose of creating a separate allotment for an existing dwelling and all resulting allotments will contain at least one existing dwelling;
 - (b) it involves works designed and certified by a suitably qualified engineer to minimise the impact of a 1 in 100 ARI flood event on the subject land and any other land in the area, and where such works would not cause a material adverse impact on other people or property.
- 17 Dwellings, other than detached dwellings, should only be developed where it involves works designed and certified by a suitably qualified engineer to minimise the impact of a 1 in 100 ARI flood event on the subject land and any other land in the area, and where such works would not cause a material adverse impact on other people or property.
- 18 Detached dwellings should only be developed where it involves works designed and certified by a suitably qualified engineer to minimise the impact of a 1 in 100 ARI flood event on the subject land and any other land in the area, and where such works would not cause a material adverse impact on other people or property, or where:
- (a) the finished floor level is equal to or greater than 300 millimetres above the predicted level of a 1 in 100 ARI flood event; and
 - (b) in **Low** hazard flood risk areas:
 - (i) site coverage for all buildings on the allotments does not exceed 50 percent of the area of the allotment; and
 - (ii) set-backs of detached dwellings to side and rear boundaries, but excluding boundaries to a public roads, are at least:
 - (A) one metre to side boundaries; and
 - (B) four metres to rear boundaries.
 - (c) in **Medium** hazard flood risk areas:
 - (i) site coverage for all buildings on the allotment does not exceed 40 percent of the area of the allotment; and
 - (ii) set-backs of detached dwellings to side and rear boundaries, but excluding boundaries to a public roads, are at least:

- (A) two metres to side boundaries; and
 - (B) five metres to rear boundaries.
- (d) in **High** hazard flood risk areas:
- (i) site coverage for all buildings on the allotment does not exceed 30 percent of the area of the allotment; and
 - (ii) set-backs of detached dwellings to side and rear boundaries, but excluding boundaries to public roads, are at least:
 - (A) three metres to side boundaries; and
 - (B) six metres to rear boundaries.
- 19** Ground floor additions to dwellings should:
- (a) not exceed a 40 square metre increase in floor area;
 - (b) be the same or higher finished floor level as the existing dwelling; and
 - (c) comply with the applicable site coverage and boundary setback requirements in Principle of Development Control 18.
- 20** Dwelling additions in excess of a 40 square metre increase in the existing floor area should:
- (a) be in the form of upper level additions; or
 - (b) have a finished floor level equal to or greater than 300 millimetres above the predicted level of a 1 in 100 ARI flood event; and
 - (c) comply with the appropriate site coverage and boundary setback requirements in Principle of Development Control 18.
- 21** Outbuildings may be constructed at natural ground level providing:
- (a) there is no permanent storage of goods or equipment below 300 millimetres above the predicted level of a 1 in 100 ARI flood event level for the applicable hazard flood risk area; and
 - (b) the maximum site coverage requirements in Principle of Development Control 18.
- 22** Solid fencing is appropriate provided that the level of flood risk in the vicinity is not significantly altered.

PROCEDURAL MATTERS

Non-complying Development

- 23** The following kinds of development, including expansion of an existing use, are **non-complying** in the Residential Historic (Conservation) Zone:

Demolition:

- (a) in total; or
- (b) of the frontage or side returns visible from a street or other elements visible from a public place of a place listed in [Table Ga/2](#) or [Table Ga/5](#).

Dwelling on land within the Gawler Rivers Floodplain Area identified in [Figures FI/1 to FI/8](#) except for:

- (a) detached dwelling; or
- (b) other forms of dwelling where the application includes a report from a suitably qualified engineer expert which states that the proposed dwelling(s) will be located above the predicted level for a 1 in 100 year ARI event and will not exacerbate the risk of flooding on any other land in the surrounding area.

Fuel Depot
 General Industry
 Horticulture
 Intensive Animal Keeping
 Major Public Service Depot
 Motor Repair Station
 Office with a floorspace in excess of 100 square metres
 Petrol Filling Station
 Retail Showroom with a gross leasable area in excess of 250 square metres
 Service Trade Premises
 Shop with a gross leasable area in excess of 250 square metres
 Special Industry
 Warehouse

Gawler East Policy Area ([Figure Res H\(C\)/2](#))

Desired Character

The first settlement to extend beyond the boundary of the original Gawler Plan took place east of Murray Street. Large allotments were established along High Street and Duffield Street and this area was to become known as "Nob Hill". Further to the east along Lyndoch Road, development progressed on smaller allotments, with 2 to 4 room single storey cottages and villas, mostly constructed towards the end of the 19th century. The cottages in Bishop Street are of particular interest, with unusual proportions. The cottages are two rooms wide by typically two rooms deep, with unusually high eaves, giving the cottages vertical emphasis in proportion.

The residential area on the northern side of Lyndoch Road is a relatively self-contained precinct, located adjoining Light's original township survey and defined by local topography and the original survey layout.

The Policy Area is of historic significance because it contains a large number of high integrity residences, mainly dating from the period 1850-1910, which illustrate the character and early expansion of residential Gawler in the 19th Century.

The Policy Area is located on rising ground and overlooks the town centre. It is divided into two sectors by Lyndoch Road and contains a number of local facilities which have a traditional historic form.

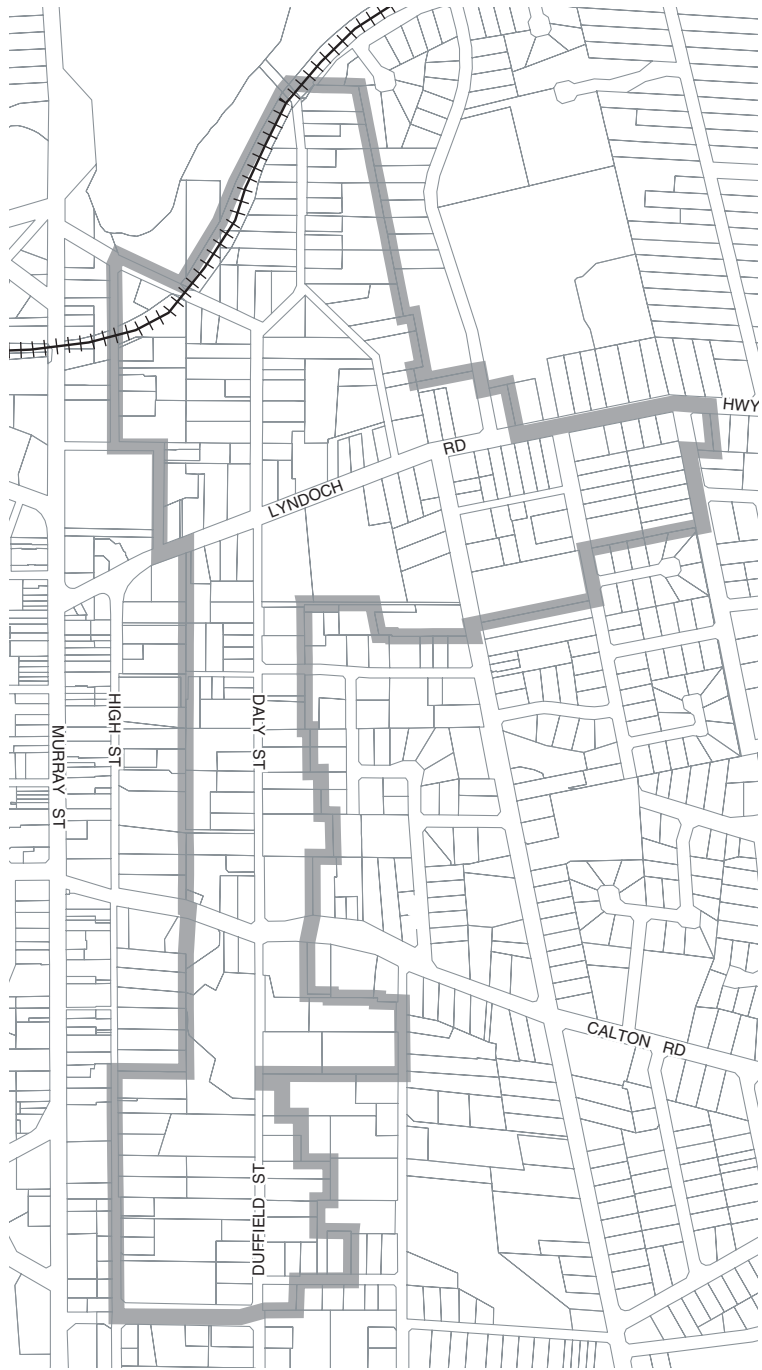
The existing pattern of detached buildings, mainly used as dwellings, which have generous garden/landscaped settings should remain which limited scope for infill development. Fences to define street boundaries are of critical importance to maintain and enhance the streetscape.



Any non-residential development should retain a local function and character to avoid conflict with residential uses.

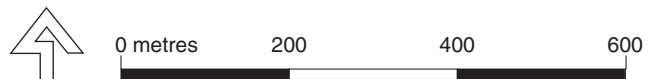
To preserve the area's natural and topographic character, limited development should occur on sloping ground in the Policy Area, particularly on the escarpment above the town centre and adjoining gullies.

Appropriate uses are:

Housing;
 Local Services (recreation areas, schools, childcare centres, community uses).



-  Policy Area Boundary
-  Railway



GAWLER (CT)
GAWLER EAST
RESIDENTIAL HISTORIC
(CONSERVATION) POLICY AREA 14
FIGURE ResH(C)/2
 Consolidated - 18 July 2019

OBJECTIVES

- Objective 1:** Development complementary to the historic character and significance of the Policy Area as expressed in the Future Character.
- Objective 2:** Retention of all places and items (including landscape elements) which contribute to the historic character of the Policy Area.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development in accordance with Structure Plan [Map Ga/1\(Overlay 1\) Enlargement C](#).

The Structure Plan indicates:

- (a) the location and impact of arterial traffic on residential environments;
 - (b) relationship to centre, recreation, health and education facilities;
 - (c) location of aged housing; and
 - (d) roads which will carry major traffic flow.
- 2 Development along Daly/Duffield Street should:
- (a) complement and reinforce the historic character of existing dwellings sited on generous allotments; and
 - (b) be set-back from the street and neighbouring buildings consistent with the historic set-back on the site.
- 3 Further division of existing allotments either resulting in the creation of additional allotments or the substantial realignment of existing allotment boundaries should not occur.
- 4 Development along Bishop Street should:
- (a) complement and reinforce the established historic character of the simple 2 to 4 room single storey gable ended cottages; and
 - (b) have substantial set backs from the street and neighbouring buildings consistent with the prevailing set-backs in the area.
- 5 Development should preserve the existing topography and the relationship of sites to street levels and not involve substantial cut and/or fill of sites.
- 6 The locality on the northern side of Lyndoch Road should be primarily single, detached dwellings not more than one storey high, compatible with the historic character of the Policy Area and conform with the following design guidelines:
- (a) All buildings set-back a minimum of 1.5 metres from the side and rear boundaries of an allotment.
 - (b) Minimum site area for a detached dwelling of 750 square metres with a minimum frontage of 15 metres and maximum site coverage of 40 percent (excluding landscaping structures).
 - (c) Minimum site area of a semi-detached dwelling, row dwelling or group dwelling of 400 square metres with a minimum frontage of nine metres and maximum site coverage of 30 percent (excluding landscaping structures).
 - (d) New dwellings should have a minimum eaves height of three metres and a roof pitch between 25 and 35 degrees.

Gawler South Policy Area ([Figure Res H\(C\)/3](#))

Desired Character

The Policy Area comprises the subdivision of Gawler West; Bassett Town and Gawler South, and is located on relatively flat terrain to the south of the original town. Streets are relatively wide and have been laid out in a grid pattern.

The Gawler Railway Station was established in 1857. The prospects for industry and employment in Gawler West and Bassett Town were immediately increased, and the area was surveyed for suitable subdivisions for a mixture of uses both around the railway yards, and to the other side of the main road to Adelaide (Gawler South). In 1857 and 1858 the local hotels were established, and these buildings were soon followed by industrial and residential buildings.

Gawler South is residential in character, consisting mainly of dwellings dating from the period 1860 - 1910. Dwellings are typically detached and are set back from the street and neighbouring houses.

The area is of historic importance because it contains a significant number of high integrity residences, mainly from the period 1860 - 1910, which illustrate the character and continuing expansion of residential Gawler in the latter half of the nineteenth century.

Part of Nineteenth Street which abuts the Gawler Railway Station Precinct is a locality that is suitable for offices and low-key retail uses. Nineteenth Street, which has a central median and a strong visual axis to the Railway Station buildings, has the potential to be enhanced as a boulevard of considerable style and urban character with the identified precinct for mixed use being developed with two storey buildings positioned close to the road frontage and designed to directly address the street.

The Gawler Railway Station Precinct provides a wide range of services including entertainment, shops, markets, fodder sales; warehousing, offices, car parking and public transport. The station precinct, because of excellent access to services, is ideal for increased residential densities. Two-storey residential development is particularly suited to the edges of the Station precinct. This will occur through redevelopment of buildings and sites of no historic value, or sympathetic reuse of buildings of historic value.

Gardens within the Area should be in scale with existing buildings and large allotments should not be reduced in size if attractive building settings, which are provided by gardens and significant trees, will be compromised or put at risk. Fences to define street boundaries are of critical importance to maintain and enhance the streetscape.

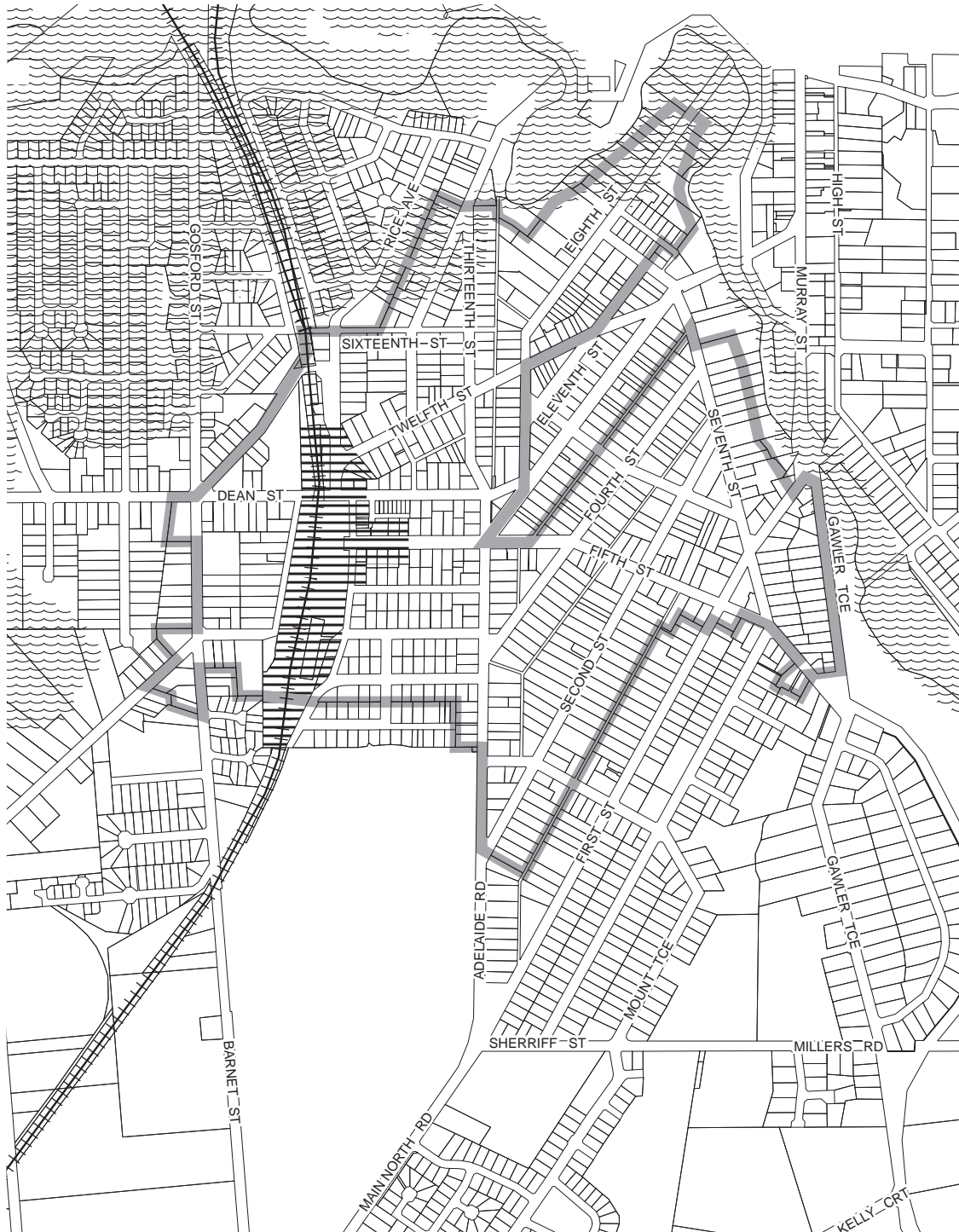
Signage to promote business uses is appropriate, but only where sensitively integrated into the building architecture or located in gardens. The number and scale of signs must be constrained. Where buildings are set back from the road, a single small pylon or free-standing sign is appropriate. For buildings that are close to the street boundary, either a Flat Wall, Projecting or Under verandah signs are appropriate.

Appropriate Uses are:

- Dwellings;
- Two-storey development along the designated part of Nineteenth Street in the Gawler Railway Station Precinct in the form of shops, offices, showrooms and residential use;
- Entertainment, confined to existing hotels in the Gawler Railway Station Precinct;
- Service Trade Premises; Shops; Retail Showrooms; Service Industry; Warehousing; Car parking;
- Public Transport interchange in the Gawler Railway Station Precinct;
- Local Services (Recreation areas, schools, child care centres, community uses).

OBJECTIVES





- Objective 1:** Development complementary to the historic character and significance of the Policy Area as expressed in the Future Character.



Note: See figure FI/1 for flood data definition explanation and disclaimer.



0 metres 200 400 600 800 1000

-  Gawler Railway Station Precinct
-  Flood Prone Land
-  Railway
-  Policy Area Boundary

GAWLER (CT)
GAWLER SOUTH
RESIDENTIAL HISTORIC
(CONSERVATION) POLICY AREA 15

FIGURE ResH(C)/3

Consolidated - 18 July 2019

Objective 2: Retention of all places and items (including structures and landscape elements) which contribute to the historic character of the policy area.

Objective 3: A residential area with locations of:

- (a) historic character which should be retained with limited change in the allotment layout;
- (b) mixed business/residential use in identified localities;
- (c) broadacre land suitable for future division into allotments for residential use; and
- (d) existing residential development suitable for modest infill development.

Objective 4: Conservation and enhancement of the historic character of the policy area, and pattern of development through consideration of subdivision pattern, allotment size, width of streets and the function of residential “night cart lanes” for rear vehicular access.

PRINCIPLES OF DEVELOPMENT CONTROL

1 Development in accordance with Structure Plan [Map Ga/1 \(Overlay\) Enlargement E](#).

The Structure Plan indicates:

- (a) the location and impact of arterial traffic on residential environments;
- (b) relationships with business, recreation and public transport services;
- (c) roads which will carry major traffic flows;
- (d) longer-term requirements for a south-east road connector;
- (e) localities for landscape protection;
- (f) localities for mixed use development in the Gawler Railway Precinct;
- (g) access to public transport.

2 Development should:

- (a) complement and reinforce the historic character of existing dwellings sited on generous allotments; and
- (b) be set-back from the street and neighbouring buildings consistent with the historic set-back on the site.

3 Residential development should not be sited on side allotment boundaries and should be set back from side boundaries to complement adjacent existing development.

4 Residential development generally should be single storey in height unless indicated otherwise.

5 Residential “night cart lanes” should only be used primarily for rear of dwelling vehicular access and the residential densities adjoining the lanes should not be increased.

6 The historic alignment and width of residential “night cart lanes”, and their function, should be preserved.

7 Development in the Gawler Railway Station Precinct should complement and reinforce the historic character of the early commercial development in the area.

8 Land division should occur on the site of the railyards and railway station in a manner which facilitates the retention, enhancement and use of historic buildings.

Light Policy Area ([Figure Res H\(C\)/4](#))

Desired Character

The Policy Area encompasses the original streets and allotments of the town of Gawler, as designed and laid out by Light, Finniss and Co in 1839 and is of historic significance as it still clearly shows the extent of the original town and the principles on which it was laid out.

Gawler's plan is historically significant as a successful example of a grid plan adapted to the topography and natural features of the location. The street pattern reflects the form of the rivers and topography of the hill upon which it is established. The original street pattern, rivers and local hills are still clearly identifiable, with features of note including the public squares around the churches and the terraces and parklands bordering the rivers.

The area is also of historic significance as it contains many of the early buildings of Gawler - residential, commercial, public and ecclesiastical - which illustrate the built form of the town in the 19th Century. The area forms the historic core of Gawler and contains a large number of State heritage, local heritage and contributory items in the areas flanking the Church Hill State Heritage Area.

The Policy Area includes the Church Hill State Heritage Area, which was declared in 1985. The Church Hill State Heritage Area comprises all the elements within its boundaries, including individual dwellings and other buildings, walls, fences, trees and major landscape features such as street trees, roadways, bluestone kerbing and cobblestones and fire hydrants. Development in this area is controlled by the Church Hill Management Plan pursuant to the Heritage Act, 1993.

The areas to the north and south of the Church Hill State Heritage Area are predominantly residential in character, with streets lined with small cottages and villas dating from the latter half of the 19th Century. Houses are typically single storey, detached and have a minimal set back from the street.

The Structure Plan for the centre of Gawler is based on Colonel William Light's original plan for Gawler. The area embraces the North and South Para Rivers and their confluence, rising ground at Church Hill and Gawler East (which flank either side of the Town Centre), and the Town Centre itself which has a linear form that is positioned on a strong north/south axis.

The Church Hill State Heritage Area has a unique residential character due to its distinctive topography, diverse range of nineteenth century architecture (with several different yet cohesive townscapes). The churches should remain as the location's distinctive landmarks, and development should be of a high standard of design and complementary to these important township features.

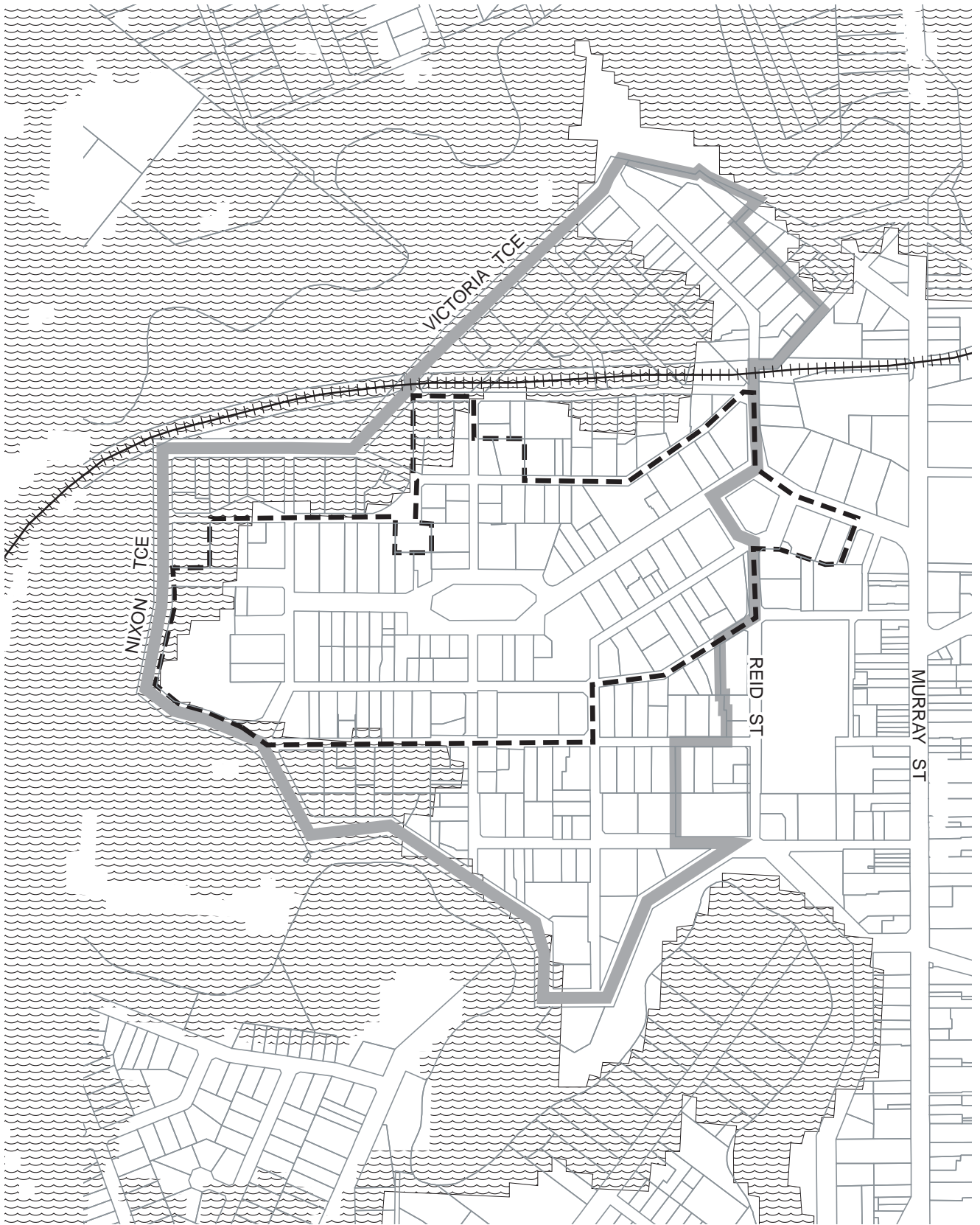
Central Gawler is well-endowed with urban services. As such, apart from some critically important locations such as Church Hill State Heritage Area and the Gawler East escarpment (as viewed from Bridge Street South), the Area is ideal for a wide range of residential forms. Residential buildings however need to be designed to be sympathetic to their setting, with reference in particular to the historic built form which is based on nineteenth and early twentieth century residential building type, topography and the preservation of mature large scale vegetation.

Vehicle movement through the Area, other than along arterial roads, should be maintained predominantly for local traffic and pedestrians with major traffic flows limited to designated streets.

Development should complement the character of individual localities in Central Gawler and have close regard to building scale, massing, composition, siting, subdivision pattern, slope, architectural detailing, materials and colours. Articulation of buildings will be important and use of verandahs and balconies encouraged.

One, one and a half, and two storey buildings will be appropriate, and should be considered for sloping sites to avoid excessive cut and fill.

Gardens within residential locations should be in scale with the buildings and large allotments should not be reduced in size if attractive building settings, which are provided by gardens and significant trees, will be compromised or put at risk. Fences to define street boundaries are of critical importance to maintain and enhance the streetscape.



Note: See figure FI/1 for flood data definition explanation and disclaimer.



- Church Hill State Heritage Area
- █ Policy Area Boundary
- ++++ Railway
- ~~~~~ Flood Prone Land

GAWLER (CT)
LIGHT
RESIDENTIAL HISTORIC
(CONSERVATION) POLICY AREA 16
FIGURE ResH(C)/4
Consolidated - 18 July 2019

Streets, squares and other elements which comprise the original 1839 street layout of Light, Finniss and Co, should not be modified, widened, closed or redirected in a manner that diminishes their historic integrity.

Original streets, currently not used or not paved, should not be modified, widened, closed or redirected and if paved and kerbed this should be in a manner complementary to the detail of other streets in the policy area.

Appropriate uses are:

Housing
Local services (recreation areas, school, childcare centres, community uses).

OBJECTIVES

- Objective 1:** Retention and/or reinstatement of the street layout and subdivision pattern as designed by Light, Finniss & Co.
- Objective 2:** Development complementary to the historic character and significance of the Policy Area as expressed in the Future Character.
- Objective 3:** Conservation and preservation of the collective elements which form the heritage value of Church Hill State Heritage Area.
- Objective 4:** Development which enhances the existing character of the Church Hill State Heritage Area, and those physical elements which collectively create that character.
- Objective 5:** Reinforcement of the historic character of Church Hill by encouraging, as opportunities arise, the replacement and/or adaptation of non-contributory development with development that does not diminish the historic character of the area.
- Objective 6:** Single storey dwellings, ecclesiastical, public and commercial buildings predominating in the Church Hill State Heritage Area, with the exception of existing two storey buildings and where the topography enables the construction of a lower level into the hill face.
- Objective 7:** Non-residential buildings which reflect the traditional character and prominence of this building type and enhance the 19th and early 20th Century character of Church Hill.
- Objective 8:** Land division which retains the historic pattern and layout of allotments in the Church Hill State Heritage Area.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development in accordance with Structure Plan, [Map Ga/1\(Overlay 1\) Enlargement B](#).

The Structure Plan indicates:

- (a) the location and impact of arterial traffic on residential environments;
- (b) localities with limited opportunity for infill residential development and land division;
- (c) relationships with the recreation centre and public transport services;
- (d) location of aged housing;
- (e) roads which will carry major traffic flows;

- (f) access to public transport.
- 2 Streets, squares and other elements which comprise the original 1839 street layout of Light, Finnis and Co, should not be modified, widened, closed or redirected in a manner that diminishes their historic integrity.
- 3 Original streets, currently not used or not paved, should not be modified, widened, closed or redirected and if paved and kerbed this should be in a manner complementary to the detail of other streets in the Policy Area.
- 4 Land division should not be undertaken, unless it can be demonstrated that the integrity of the original street layout of Light, Finnis and Co and all identified heritage and contributory items is not diminished.
- 5 Development should:
 - (a) preserve existing views and vistas; and
 - (b) maintain the existing topography and the relationship of sites to street levels.

The following apply to the Church Hill State Heritage Area, as identified in [Map Ga/1\(Overlay 1\) Enlargement B](#):

- 6 A building, item or structure of State Heritage value, or which contributes to the historic significance of Church Hill, should only be altered so as to retain or restore the original external form, materials and colours, and also the internal arrangements and finishes where these are of heritage value.
- 7 Development should primarily consist of the upgrading of existing buildings in a manner which reinforces and complements the historic character and significance of the building and the area.
- 8 Development should not reproduce individual building styles, except for the replacement of building elements where clear evidence of the original exists.
- 9 Restoration, externally and internally, of buildings to their earlier original appearance should occur.
- 10 Development, including additions to existing buildings, should relate in height, scale, bulk and form to any adjacent places of heritage value.
- 11 Development apart from fencing and landscaping, should not occur between the front street boundary and the facade of existing buildings of heritage value.
- 12 Roof mounted plant and equipment should be located so as to minimise their visibility from any public place.
- 13 The external materials and finishes of extensions to existing structures of heritage value should complement those of the original building.
- 14 The roof pitch of infill development should complement the predominant roof pitches of existing buildings of heritage value in the locality.
- 15 Roofs for rear additions to existing buildings of heritage value may be of skillion construction provided they do not extend greater than 4.0 metres from the main building.
- 16 Original unpainted plaster, brickwork, stonework, or other masonry should be preserved unpainted.
- 17 Front and side fencing to the alignment of the front of the building should be between 1 and 1.4 metres in height above the adjacent footpath level.

- 18 Rear fencing and side fencing behind the alignment of the front of the building should generally not exceed 1.5 metres in height.
- 19 Where new side and rear fencing abuts a public road the height, style and materials should be consistent with the characteristics of the adjacent predominant traditional fencing.
- 20 Corrugated side and rear fencing should be galvanised (unpainted or painted in traditional colours) or pre-coated in traditional colours.
- 21 The existing density, spacing, set-backs and orientation of buildings of historic character should be maintained in any given locality.
- 22 Extensions to existing dwellings and new structures such as garages and outbuildings should be located to the rear of the dwelling or in a location which is not visible from a public place or designed to reinforce historic character of the locality in the case of dual frontage allotments.
- 23 Land division, including the realignment of existing allotment boundaries, should not occur within the area unless it is to correct anomalies to the historic pattern of land division.
- 24 Signage to ecclesiastical buildings should be restricted to self-contained signage panels of a freestanding nature or to smaller panels attached to the building. The area of free standing signage should not be greater than 1 square metre and the area of signage panels should not be greater than 0.75 square metres in total area.
- 25 Signage to commercial buildings should be located in traditional signage panel areas such as parapet walls above verandahs, verandah fascias and infill end panels, and windows. Internally illuminated and neon signs should not occur.

Willaston (Redbanks Road) Policy Area ([Figure Res H\(C\)/1](#))

Desired Character

The Policy Area rises to the north and is residential in character, with dwellings located along both sides of Redbanks Road.

The dwellings on the low (south) side of Redbanks Road typically date from 1890 - 1915 and are well set back from the street and neighbouring houses. The dwellings on the high (north) side of the road typically date from 1860 - 1900 in construction and are located closer to the road and each other than the houses opposite.

The area is historically significant as it contains a number of high integrity residences, dating from 1880 - 1915, which illustrate the residential character and continuing expansion of the town of Willaston from the mid nineteenth century to the early years of the twentieth century.

The Policy Area has no potential for infill residential development and should continue to be used for detached dwellings on large allotments.

OBJECTIVES

- Objective 1:** Development complementary to the historic character and significance of the Policy Area as expressed in the Future Character.
- Objective 2:** Retention of all places and items (including landscape elements) which contribute to the historic character of the policy area.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development in accordance with Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement A](#).

The Structure Plan indicates the relationship of the Policy Area to the arterial road.

- 2 Development should:
 - (a) complement and reinforce the historic character of existing dwellings, sited on generous allotments;
 - (b) be set back from the street and neighbouring buildings consistent with the prevailing set-backs on each side of Redbanks Road.
- 3 Further division of existing allotments either resulting in the creation of additional allotments or the substantial realignment of existing allotment boundaries should not occur.
- 4 Development generally should be single storey in height.
- 5 Development should preserve the existing topography and the relationship of sites to street levels and not involve substantial cut and/or fill of sites.

Willaston Policy Area ([Figure Res H\(C\)/1](#))

Desired Character

Located adjacent the main street of the Willaston township, which was officially laid out on 21 October 1848, the predominately residential buildings in the Willaston Policy Area were amongst the first constructed in the Willaston township, and were therefore central to the town's community fabric. To this day, the historic significance of the Willaston township is evident, with many of the original buildings of the settlement, including the former Willaston Uniting Church and dwellings dating from the period 1848-1900, remaining in use and defining the historic and desired character of built form.

OBJECTIVES

- Objective 1:** Development complementary to the historic character and significance of the Policy Area as expressed in the Desired Character.
- Objective 2:** Retention of all places and items (including landscape elements) which contribute to the historic character of the policy area.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development in accordance with Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement A](#). The Structure Plan indicates the relationship of the Policy Area to the arterial road.
- 2 Development should:
 - (a) complement and reinforce the historic character of the early dwellings;
 - (b) be set back from the street and neighbouring buildings consistent with the historic setback on the site.
- 3 Further division of existing allotments either resulting in the creation of additional allotments or the substantial realignment of existing allotment boundaries should not occur.
- 4 Development generally should be single storey in height.
- 5 Development should preserve the existing topography and the relationship of sites to street levels and not involve substantial cut and/or fill of sites.

RURAL ZONE

Introduction

In addition to the Council Wide policies, the Rural Zone and Gawler Rivers Floodplain Area, policies apply to the areas shown in:

Rural Zone: [Maps Ga/4, 5, 7, 8, 9, 10, 11](#)

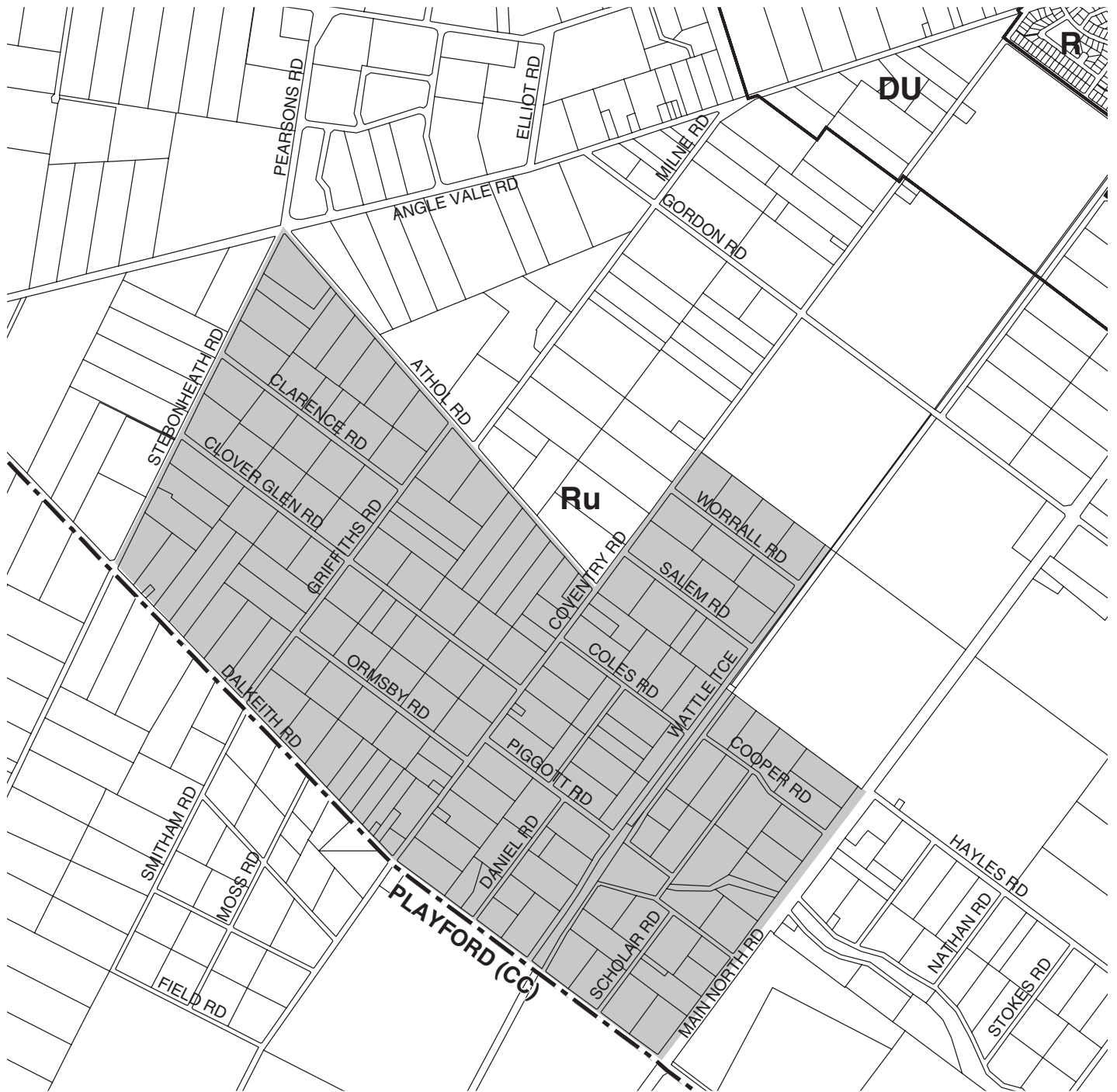
Gawler Rivers Floodplain Area: [Figures FI/1 to FI/8](#)




OBJECTIVE

Objective 1: A zone comprising land to be retained in use primarily for agricultural purposes, but also allowing for the construction of the Gawler East Link Road extension (Bentley – Tiver Road alignment between Potts Road and Alexander Avenue) as shown on Structure Plan [Map Ga/1 \(Overlay 1\) Part B](#) and associated public lighting, pedestrian and bicycle infrastructure and broader stormwater management systems and community facilities.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development undertaken in the Rural Zone should be primarily for agricultural purposes, but also allowing for the construction of the Gawler East Link Road extension (Bentley – Tiver Road alignment between Potts Road and Alexander Avenue) as shown on Structure Plan [Map Ga/1 \(Overlay 1\) Part B](#) and associated public lighting, pedestrian and bicycle infrastructure and broader stormwater management systems and community facilities.
- 2 Land division should create allotments that have a frontage to a public road of at least 25 percent of the average depth of the allotment, and are greater than 4 hectares (except on land identified as 'Affected Area' on [Figure Ru/1](#), where allotments should be greater than 0.9 hectares).
- 3 No new direct vehicular access or egress should be provided to any allotment to or from Main North Road.
- 4 Development within 300 metres of a residential zone, urban zone or Deferred Urban Zone should be compatible with residential development and should:
 - (a) incorporate suitable land management practices to avoid unacceptable impacts on adjacent residential or urban zones;
 - (b) not require aerial spraying within 300 metres of such zones or within 40 metres of such zones where an appropriate vegetation buffer is in place; and
 - (c) not store refuse or organic waste within 150 metres of such zones.
- 5 Development should incorporate landscaping of sufficient width to minimise potential adverse impacts on the boundary where it is adjacent to a residential or urban zone.
- 6 The provision of a landscaped buffer adjacent a residential or urban zone in accordance with principles of development control 3 and 4 should be designed taking into account the nature, source and frequency of potential adverse impacts; prevailing winds in the locality; topography of the area; existing vegetation and the presence of potentially sensitive adjacent uses.
- 7 General industry and light industry may be developed where:
 - (a) it is the extension of an existing industry;
 - (b) it is contained within the allotment on which that industry existed prior to 14 July 1988;
 - (c) the total area of the site occupied by buildings does not exceed 50 percent of the area of the site;



-  Affected Area
-  Zone Boundary
-  Development Plan Boundary

GAWLER (CT) RURAL ZONE FIGURE Ru/1

- (d) a parking space is established on the site at the rate of not less than one car parking space for every 50 square metres of total floor area; and
- (e) a landscaping strip of at least three metres width is established adjacent to the alignment of a road frontage and maintained so as not to be damaged by parked vehicles.

High Pressure Gas Pipeline

- 8** Development, including land division, in the vicinity of a high pressure pipeline, as depicted on Structure Plans [Map Ga/1 \(Overlay 1\) Enlargements F, G and H](#), should:
- (a) not affect the continued operation of the pipeline infrastructure
 - (b) be located, designed, and developed having regard to the location of the infrastructure and the potential public safety impacts.

Hazards

The following principles apply to land within the Gawler Rivers Floodplain Area identified on [Figures FI/1 to FI/8](#).

Flooding

- 9** Land division should not result in an additional allotment or allotments.
- 10** Dwellings, other than detached dwellings, should not be developed.
- 11** Detached dwellings should only be developed where:
- (a) the finished floor level is equal to or greater than 300 millimetres above the predicted level of a 1 in 100 ARI flood event; and
 - (b) access is provided to a public road via an access road or track with a level not less than 300 millimetres above the predicted level of a 1 in 100 ARI flood event level.
- 12** Ground floor additions to dwellings for all hazard flood risk areas should:
- (a) not exceed a 40 square metre increase in floor area; and
 - (b) be the same or higher finished floor level as the existing dwelling.
- 13** Dwelling additions in excess of a 40 square metre increase in the existing floor area should:
- (a) be in the form of upper level additions; or
 - (b) have a finished floor level equal to or greater than 300 millimetres above the predicted level of a 1 in 100 ARI flood event.
- 14** Outbuildings or farm buildings should only be constructed at natural ground level where:
- (a) the floor area does not exceed 80 square metres; and
 - (b) there is no permanent storage of goods or equipment below 300 millimetres above the predicted level of a 1 in 100 ARI flood event level.
- 15** Outbuildings or farm buildings exceeding a floor area of 80 square metres should:
- (a) have a finished floor level equal to or greater than 300 millimetres above the predicted level of a 1 in 100 ARI flood event level applicable to the hazard flood risk area; and

- (b) ensure there is no permanent storage of goods or equipment below 300 millimetres above the predicted level of a 1 in 100 ARI flood event level for the applicable hazard flood risk area.

PROCEDURAL MATTERS

- 16** The following kinds of development are **complying** in the Rural Zone excluding development within the Gawler Rivers Floodplain Area as shown on [Figures FI/1 to FI/8](#), and subject to compliance with conditions comprising, where applicable:

- (a) no building being erected, added to or altered on any land so that any portion of such building is erected, added to or altered nearer to the existing boundary of a road, or to the boundary of any land shown as being required for road widening on the Plan deposited under the provisions of the Metropolitan Adelaide Road Widening Plan Act, 1972-1976.

Agistment and Holding of Stock
 Agriculture
 Airfield
 Golf Course
 Plant Nursery
 Recreation Area
 Stock Saleyard

- 17** The following kinds of development including expansion of an existing use are **non-complying** in the Rural Zone:

Advertising - advertisements which are located within 500 metres of the centreline of any primary, arterial or secondary road, tourist road or scenic road, with the exception of advertisements within an 80 km/hr or less speed rated road.

Amusement Hall
 Amusement Park
 Auction Room
 Bank
 Billiard Saloon
 Boarding House
 Bowling Alley
 Builder's Yard
 Bus Depot
 Bus Station
 Community Centre
 Concert Hall
 Consulting Room
 Dance Hall

Demolition:

- (a) in total; or
 (b) of the frontage or side wall returns visible from a street or other elements visible from a public place

of a place listed in [Table Ga/2](#) or [Table Ga/5](#).

Dog Track
 Drive-in Theatre
 Educational Establishment
 Exhibition Hall
 Fire Station
 Fun Fair
 General Industry
 Golf Driving Range
 Gymnasium
 Harbour Installation

Health Centre
Hospital
Hotel

Land division that would create:

- (a) allotment(s) less than 4 hectares (except within the 'Affected Area' shown on [Figure Ru/1](#));
and
- (b) allotment(s) less than 0.9 hectares within the 'Affected Area' shown on [Figure Ru/1](#).

Landfill that constitutes solid waste disposal required to be licensed as a waste depot under the Environment Protection Act 1993

Library
Light Industry
Meeting Hall
Motel
Motor Race Track
Motor Repair Station
Motor Showroom
Multiple Dwelling
Non-residential Club
Office
Office and Dwelling
Police Station
Post Office
Petrol Filling Station
Place of Worship
Primary School
Private Hotel
Racecourse
Radio or TV Studio
Residential Club
Residential Flat Building
Road Transport Terminal
Row Dwelling
Semi-detached Dwelling
Service Industry
Shop
Shop and Dwelling
Shop or group of shops with a gross leasable area greater than 450 square metres
Show Ground
Skating Rink
Squash Court
Stadium
Store
Theatre
Timber Yard
Used Car Lot
Warehouse
Welfare Institution

In addition, within the Gawler Rivers Floodplain Area, as shown on [Figures FI/1 to FI/8](#), the following kinds of development are non-complying:

All forms of industry development
Dwelling except where the dwelling is set-back a minimum distance of 100 metres from the banks of the Gawler Rivers
Cattle feedlots
Glass houses
Industry involving the storage of bio-chemicals or chemical or hazardous materials or agents located in the Gawler Rivers Floodplain Area identified in [Figures FI/1 to FI/8](#).
Intensive animal keeping
Land based aquaculture

Land division within the Gawler Rivers Floodplain Area identified on [Figures FI/1 to FI/8](#), except where the division:

- (a) does not result in an increase in the number of allotments; and
- (b) provides for public access to the banks of the river in the form of a reserve or easements necessary for the provision of public utility services; and
- (c) facilitates the construction of flood mitigation works.

Materials handling

Materials transportation

Mining and loam extraction

Saleyards

Waste treatment and storage, apart from waste generated on-site.

RURAL LIVING ZONE

Introduction

In addition to the Council Wide policies, the Rural Living Zone policies apply to the areas shown in:

Rural Living Zone: [Maps Ga/4, 5, 6, 9, 12](#)

Gawler Rivers Floodplain Area: [Figures FI/1 to FI/8](#)

OBJECTIVE

Objective 1: A zone primarily accommodating detached dwellings on large allotments set in a semi-rural environment, with a range of agricultural activities, including small hobby farms and some non-agricultural activities, in suitable areas.

Objective 2: Development in accordance with Structure Plan [Map Ga/1 \(Overlay 1\) Part A and B](#).

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development undertaken in the Rural Living Zone should be, primarily, detached dwellings on large allotments set in a semi-rural environment and incorporating a range of agricultural activities including small hobby farms, but some non-agricultural activities that do no conflict with the objective of the zone may be suitable in certain parts of the zone.
- 2 The minimum allotment size should be 4 hectares and the frontage of an allotment to a public road should not be less than $33^{1/3}$ percent of the average depth of that allotment.

High Pressure Gas Pipeline

- 3 Development, including land division, in the vicinity of a high pressure pipeline, as depicted on Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement G](#), should:
 - (a) not affect the continued operation of the pipeline infrastructure
 - (b) be located, designed, and developed having regard to the location of the infrastructure and the potential public safety impacts.

Hazards

The following principles apply to land within the Gawler Rivers Floodplain Area identified on [Figures FI/1 to FI/8](#).

Flooding

- 4 Land division should not result in an additional allotment or allotments.
- 5 Dwellings, other than detached dwellings, should not be developed.
- 6 Detached dwellings should only be developed where:
 - (a) the finished floor level is equal to or greater than 300 millimetres above the predicted level of a 1 in 100 ARI flood event; and
 - (b) access is provided to a public road via an access road or track with a level not less than 300 millimetres above the predicted level of a 1 in 100 ARI flood event level.
- 7 Ground floor additions to dwellings for all hazard flood risk areas should:
 - (a) not exceed a 40 square metre increase in floor area; and
 - (b) be the same or higher finished floor level as the existing dwelling.

- 8** Dwelling additions in excess of a 40 square metre increase in the existing floor area should:
- (a) be in the form of upper level additions; or
 - (b) have a finished floor level equal to or greater than 300 millimetres above the predicted level of a 1 in 100 ARI flood event.
- 9** Outbuildings or farm buildings should only be constructed at natural ground level where:
- (a) the floor area does not exceed 80 square metres; and
 - (b) there is no permanent storage of goods or equipment below 300 millimetres above the predicted level of a 1 in 100 ARI flood event level.
- 10** Outbuildings or farm buildings located on land identified in [Figures FI/1 to FI/8](#) and shown to be at risk of inundation by flood waters exceeding a floor area of 80 square metres should:
- (a) have a finished floor level equal to or greater than 300 millimetres above the predicted level of a 1 in 100 ARI flood event level applicable to the hazard flood risk area; and
 - (b) ensure there is no permanent storage of goods or equipment below 300 millimetres above the predicted level of a 1 in 100 ARI flood event level for the applicable hazard flood risk area.

PROCEDURAL MATTERS

- 11** The following kinds of development including expansion of an existing use are **non-complying** in the Rural Living Zone:

Abattoir

Advertising - Advertisements which are located within 500 metres of the centreline of any primary, arterial or secondary road, tourist road or scenic road, with the exception of advertisements within an 80 km/hr or less speed rated road

Amusement Hall

Amusement Park

Auction Room

Billiard Saloon

Boarding House

Bowling Alley

Builder's Yard

Bus Depot

Bus Station

Community Centre

Concert Hall

Dance Hall

Defence Establishment

Demolition:

- (a) in total; or
- (b) of the frontage or side wall returns visible from a street or other elements visible from a public place of a place listed in [Table Ga/2](#) or [Table Ga/5](#).

Dog Track

Drive-in Theatre

Educational Establishment

Electricity Generating Station

Exhibition Hall

Fire Station

Fun Fair

Gas Holder

General Industry

Golf Course
 Golf Driving Range
 Gymnasium
 Health Centre
 Hospital
 Hotel
 Landfill that constitutes solid waste disposal required to be licensed as a waste depot under the
 Environment Protection Act 1993
 Library
 Light Industry
 Marshalling Yard
 Major Public Service Depot
 Meeting Hall
 Motel
 Motor Race Track
 Motor Repair Station
 Motor Showroom
 Multiple Dwelling
 Non-residential Club
 Office
 Office and Dwelling
 Police Station
 Post Office
 Petrol Filling Station
 Place of Worship
 Primary School
 Private Hotel
 Racecourse
 Radio or TV Studio
 Residential Club
 Residential Flat Building
 Road Transport Terminal
 Row Dwelling
 Semi-detached Dwelling
 Service Industry
 Shop
 Shop and Dwelling
 Shop or group of shops with a gross leasable area greater than 450 square metres
 Show Ground
 Skating Rink
 Squash Court
 Stadium
 Store
 Theatre
 Timber Yard
 Used Car Lot
 Warehouse
 Welfare Institution

In addition, within the Gawler Rivers Floodplain Area, as shown on [Figures FI/1 to FI/8](#), the following kinds of development are non-complying:

Dwelling except where the dwelling is set-back a minimum distance of 100 metres from the banks of the Gawler Rivers

SPECIAL USES ZONE

Introduction

In addition to the Council Wide policies, the Special Uses Zone, Special Uses Historic (Conservation) Policy Area and Special Uses Caravan and Tourist Park Precinct policies apply to the area shown in:

Special Uses Zone: [Map Ga/3, 5, 8, 9, 12](#)

Special Uses Historic (Conservation) Policy Area: [Map Ga/14, 18](#)

Special Uses Caravan and Tourist Park Precinct: [Map Ga/19](#)

Gawler Rivers Floodplain Area: [Figures FI/1 to FI/8](#)

Gawler Health Services Helipad: [Figure Hel/1](#)

Desired Character

The zone comprises a range of identifiable land use precincts including cultural activities, passive recreation, active recreation, spectator sports, tourist uses, and natural or conservation areas, essentially along parts of the North and South Para Rivers.

The prime purpose of the zone is to provide for community use and business use is generally not envisaged.

In addition to accommodating community activities and uses, the zone has a significant amenity function by providing a distinctive setting for urban development and an opportunity for either tall mature trees, such as occurs along the urban river system, or long vistas over open and green spaces.

A large portion of the zone is located within the Gawler Rivers Floodplain Area, particularly the 'High' Hazard Flood Risk Area. Risk to life and property will be minimised through the avoidance of any new structures within these locations, and ensuring that evacuation routes to areas not subject to inundation of floodwaters are available for existing activities within these areas.

Desired Uses:

Active Outdoor Recreation
Agriculture
Car parking
Caravan Park
Community Centre
Community Hall
Conservation Area
Education
Golf Course
Hard Surface Court
Indoor Recreation Facility

Intensive Outdoor Recreation (of playing fields, hardcourts)
Landscaping
Licensed Premise
Meeting Room
Oval
Playground
Stadium
Spectator Stand
Swimming Pool
Recreation Area

OBJECTIVE

Objective 1: A zone accommodating public and private development of an institutional or open character.

PRINCIPLES OF DEVELOPMENT CONTROL

Form of Development

- 1 Development consistent with the Desired Character for the zone.
- 2 Development undertaken in the Special Uses Zone should be public and private development of an institutional or open character.

Signs

- 3 All signs should be developed in accordance with the relevant provisions applying throughout the Council area and with Performance Standards in [Table Ga/4](#).
- 4 The following sign types, advertising and advertising displays are appropriate in the zone:
 - (a) Pylon or Freestanding;
 - (b) Flat Wall;
 - (c) Sandwich Board;
 - (d) First Party Advertising;
 - (e) Projecting;
 - (f) Under Verandah; and
 - (g) Shop Front.
- 5 The following sign types, advertising and advertising displays are inappropriate in the zone:
 - (a) Roof;
 - (b) Bunting;
 - (c) Internally illuminated signs on buildings of heritage significance listed in [Table Ga/2](#) or [Table Ga/5](#);
 - (d) Third Party Advertising; and
 - (e) Mobile.

Hazards

The following principles apply to land within the Gawler Rivers Floodplain Area identified on [Figures FI/1 to FI/8](#).

Flooding

- 6 No new buildings or structures should be established on land identified as being a 'medium' or 'high' hazard risk area. Where this cannot be avoided, any new building or structure should have a finished floor level at least 300 millimetres above the AHD level for a 1 in 100 year ARI flood event. All services should be designed to be disengaged in times of flooding and capped to prevent inundation, contamination or failure.
- 7 Buildings should have a finished floor level equal to or greater than 300 millimetres above the predicted level of a 1 in 100 ARI flood event level applicable to the relevant hazard flood risk area.
- 8 Additions to existing buildings should:
 - (a) be a maximum of 40 square metre increase on the existing floor area; and
 - (b) be at the same or higher finished floor level as the existing building.
- 9 Car parking areas should be provided at natural ground level or with a maximum level increase of 300 millimetres.

PROCEDURAL MATTERS

Non-complying Development

- 10** The following kinds of development including expansion of an existing use are **non-complying** in the Special Uses Zone:

Abattoir
 Amusement Hall
 Auction Room
 Bank
 Billiard Saloon
 Bowling Alley
 Bunting
 Bus Depot
 Bus Station
 Concert Hall
 Consulting Room
 Dance Hall

Demolition:

- (a) in total; or
 (b) of the frontage or side wall returns visible from a street or other elements visible from a public place of a place listed in [Table Ga/2](#) or [Table Ga/5](#).

Detached Dwelling
 Exhibition Hall
 Fire Station
 General Industry
 Group Dwelling
 Gymnasium
 Harbour Installation
 Landfill that constitutes solid waste disposal required to be licensed as a waste depot under the *Environment Protection Act 1993*
 Light Industry
 Mobile Signs
 Motor Repair Station
 Motor Showroom
 Multiple Dwelling
 Office
 Office and Dwelling
 Petrol Filling Station
 Post Office
 Radio or TV Studio
 Refuse Destructor
 Residential Flat Building
 Road Transport Terminal
 Roof Signs
 Row Dwelling
 Semi-detached Dwelling
 Shop or group of shops with a gross leasable area greater than 450 square metres
 Service Industry
 Shop
 Shop and Dwelling
 Skating Rink
 Special Industry
 Squash Court
 Store
 Theatre
 Third Party Advertising
 Timber Yard
 Used Car Lot
 Warehouse

- 11 Further, the following kinds of development are **non-complying** in the Special Uses Zone Caravan and Tourist Park Precinct:

Amusement Machine Centre
 Bus Depot
 Cemetery
 Commercial Forestry
 Community Centre, except where in association with and ancillary to tourist accommodation
 Consulting Room
 Crematorium
 Dairy
 Dam
 Dwelling (except for a manager's residence in association with and ancillary to tourist accommodation)
 Educational Establishment
 Farming
 Fuel Depot
 Horse Keeping
 Horticulture
 Hospital
 Hotel
 Indoor Recreation Centre, except where in association with and ancillary to tourist accommodation
 Industry
 Intensive Animal Keeping
 Land Division which results in the creation of an additional allotment(s) either wholly or partly within the Zone, except where a lease or license agreement is made, granted or accepted under the *Residential Parks Act 2007*
 Marina
 Motor Repair Station
 Nursing Home
 Office, except where in association with and ancillary to tourist accommodation
 Petrol Filling Station
 Place of Worship
 Pre-school
 Prescribed mining operations
 Public Service Depot
 Residential Flat Building
 Restaurant, except where it is both:
 (a) less than 150 square metres in gross floor area
 (b) in association with and ancillary to tourist accommodation.
 Road Transport Terminal
 Service Trade Premises
 Shop, except where it is both:
 (a) less than 150 square metres in gross floor area
 (b) in association with and ancillary to tourist accommodation
 Stadium
 Stock Sales Yard
 Stock Slaughter Works
 Store
 Warehouse
 Waste reception, storage, treatment or disposal
 Wrecking Yard

Public Notification

- 12** The following kinds of development are assigned as **Category 1 Development** in the Special Uses Caravan and Tourist Park Precinct:

Amenity block, including shower, toilet, laundry and kitchen facilities
 Cabin
 Camping ground
 Caravan park
 Caravan permanently fixed to land
 Recreation area
 Swimming pool
 Tourist park

Special Uses Zone Historic (Conservation) Policy Area

Historic Character and Significance

The Historic (Conservation) Policy Area encompasses the town's rivers and parklands adjoining the original streets and allotments of the town of Gawler, as designed and laid out by Light, Finnis and Co in 1839 and is of historic significance as it still clearly illustrates the planning principles of the day and also the initial footprint of the town.

Gawler's plan is historically significant as a successful example of a grid layout plan adapted to the topography and natural features of the location. The street pattern reflects the form of the rivers and topography of the hill upon which it is established. The original street pattern, rivers and local hills are still clearly identifiable, with features of note including the public squares around the churches and the terraces and parklands bordering the rivers.

OBJECTIVES

- Objective 1:** Retention and/or reinstatement of the street layout and subdivision pattern as designed by Light, Finnis & Co.
- Objective 2:** Development complementary to the historic character and significance of the Special Uses Zone Historic (Conservation) Policy Area.
- Objective 3:** Retention of all places and items (including landscape elements) which contribute to the historic character of the Special Uses Zone Historic (Conservation) Policy Area.
- Objective 4:** Conservation and enhancement of the historic character of the Historic (Conservation) Policy Area, through consideration of:
- (a) Streetscape character;
 - (b) Subdivision pattern;
 - (c) Building set-backs;
 - (d) Building form and materials;
 - (e) Site layout, landscaping and fencing.
- Objective 5:** Maintenance of the existing topography.
- Objective 6:** Retention of historic townscape vistas and views.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should reinforce and complement the historic character and significance of the area and the integrity of any places and items identified in [Table Ga/2](#).
- 2 Vacant land, buildings or sites having a detrimental affect on the character of the locality should be redeveloped and upgraded in accordance with the historic character of the Special Uses Zone Historic (Conservation) Policy Area, and where applicable, following the guidelines in [Table Ga/3](#).
- 3 Development should conserve, maintain, enhance and reinforce the existing streetscape character of the Zone and Special Uses Zone Historic (Conservation) Policy Area and the historic character of individual buildings, items, structures, and places.
- 4 Development should enhance and contribute to the visual cohesiveness of the Area and any new buildings should be of complementary height, scale, set-back, form and external appearance, and display creative and diverse examples of high standard contemporary architecture.
- 5 Places and items identified in [Table Ga/2](#), [Table Ga/5](#) or [Table Ga/6](#) should not be demolished, but instead be retained, upgraded and adapted such that:
 - (a) the quality or condition which gives the place its significance is protected or enhanced. This should include elements of the place which have significant cultural associations;
 - (b) the place is put to a use which supports the maintenance and restoration of that building, item, structure or place;
 - (c) the place is only altered so as to retain or restore the original external form, materials, and colours in a manner compatible with its structural visual, material and historic integrity;
 - (d) where internal arrangements and finishes are of significance to places identified in [Table Ga/2](#) the place should only be altered in a manner compatible with its structural visual, material and historic integrity.
 - (e) advertisements or advertising displays, which reflect and complement the character and geometry of the building it is attached to.
- 6 Demolition of a place or item identified in [Table Ga/2](#), [Table Ga/5](#) or [Table Ga/6](#) should not be undertaken unless the building, item, structure or place is so structurally unsound it represents a risk to public safety and is beyond economic repair. Unsympathetic (later) additions may be demolished if the later addition does not contribute to the historic character of the place.
- 7 Development abutting or in close proximity to a place or item identified in [Table Ga/2](#), [Table Ga/5](#) or [Table Ga/6](#) should:
 - (a) respect the historic character of the area and the integrity of the particular item of significance and be designed to a high architectural standard; and
 - (b) be compatible in respect of its design, siting, scale, building and roof shape, bulk, height, materials and colours, fences and landscaping and any advertising signs and external illumination.
- 8 Any alteration or addition to a place or item identified in [Table Ga/2](#), [Table Ga/5](#) or [Table Ga/6](#) should reinforce and complement the historic character of the place in terms of its design, siting, boundary set-backs, scale, built form and roof shape, bulk, height, materials and external finishes and where applicable:
 - (a) additions should minimise their impact on the streetscape by siting to the rear or the side of existing buildings and not forward of existing buildings;
 - (b) the height of eaves and gutters on additions should be complementary with the existing building;

- (c) the proportions (vertical and horizontal) of additions visible from the street, should be complementary with those of the existing building;
 - (d) materials may include masonry (stone and brick) render, timber boarding and corrugated steel sheeting (walls and roof);
 - (e) materials such as mirror glass, flat profile steel sheeting, unpainted zincalume sheeting, wide fibre cement sheet planking and polished face concrete panels should not be used where they are visible from the street;
 - (f) roof conversions or additional upper floors are not desirable where they compromise the historic integrity of the building or the view of the building from the street;
 - (g) roof conversions are acceptable where the alterations are fully incorporated under the existing roof and dormer style windows are acceptable where they are not visible from the street; and
 - (h) external colour schemes should be contemporary, but should not dramatically contrast with the original colour scheme of the existing place.
- 9** Landscaping which should include fencing at the street boundary should be designed to complement and reinforce the historic character of places and items identified in [Table Ga/2](#), [Table Ga/5](#) or [Table Ga/6](#) and the zone generally.
- 10** The design of new fences, or alterations to existing fences associated with places or items identified in [Table Ga/2](#), [Table Ga/5](#) or [Table Ga/6](#) should complement and reinforce the historic character of the place as follows:
- (a) front fences should complement historic adjacent fences in height, be timber picket, metal palisade, woven wire mesh between timber posts, hedges or rendered masonry with brick copings or other traditional materials (high brush or corrugated steel fences are not desired);
 - (b) side and rear fences should be timber picket or board, corrugated steel sheeting (natural galvanised or painted finish), hedges or rendered masonry with brick copings or other traditional materials;
 - (c) side fences should match the front fence in height for the depth of the front yard.
- 11** Division of an allotment containing a place or item identified in [Table Ga/2](#), [Table Ga/5](#) or [Table Ga/6](#) should not be undertaken if it may detrimentally affect its heritage significance.
- 12** Allotments created by the division of land should result in an allotment which reinforces the integrity of and complements the historic character.

Special Uses Zone Caravan and Tourist Park Precinct

The objectives and principles that apply to the Special Uses Zone Caravan and Tourist Park Precinct are additional to those expressed for the whole of the Council area and for the Special Uses Zone and Special Uses Zone Historic (Conservation) Policy Area and take precedence where a conflict occurs between the precinct, policy area and zone provisions.

The following maps apply: [Precinct Map Ga/19](#).

OBJECTIVES

- Objective 1** A precinct primarily for short-term tourist accommodation and associated facilities.

- Objective 2** A precinct accommodating a range of short-term tourist accommodation predominantly in the form of caravan and camping sites, cabins, and transportable dwellings surrounded by open landscaped areas.
- Objective 3** Development that is designed to enhance the natural features of the local environment, including visual amenity, landforms, fauna and flora.
- Objective 4** Development that contributes to the desired character of the precinct.

DESIRED CHARACTER

This precinct primarily accommodates a range of tourist accommodation uses, including camping sites, caravans and cabins. Dwellings and long-term accommodation will not lead to the displacement of existing tourist accommodation in high demand locations.

Buildings will be a maximum height of two storeys and blend in with the natural environment. In rural and natural landscapes, the visual impact of the park will be minimal from scenic vantage points, public lookouts and tourist routes. Vegetation buffers and landscaping will be important in integrating the park into the landscape and providing screening from surrounding land uses, as well as reducing visual and noise impacts and providing privacy for park users.

Circulation and movement within the park will be pedestrian friendly and promote low speed vehicle movement.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the precinct:

- Amenity block, including shower, toilet, laundry and kitchen facilities
- Cabin
- Camping ground
- Caravan park
- Caravan permanently fixed to land
- Recreation area including tennis court, basketball court, playground
- Swimming pool/spa
- Tourist park and other forms of tourist accommodation

Form and Character

- 2 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 3 Permanent buildings should be limited to a dwelling (manager's house), shop (in association with and ancillary to a caravan and tourist park), community or recreational facility and toilets/amenities.
- 4 Recreation facilities should be provided of a scale that is suitable to maintain the open natural character of the area and ancillary to the primary role and function of the park.
- 5 The total number of tourist accommodation sites in the park should be at least 60% of the total number of sites available.
- 6 Landscaping should form an integral part of the design and be used to define spaces, reinforce internal networks, screen utility areas and enhance the visual amenity of the area.
- 7 Every caravan, cabin and dwelling site should be greater than 81 square metres in area.

Car Parking and Access

- 8 Every caravan, cabin or dwelling site should have parking for at least one vehicle, either located on the site or grouped within the park.

Street and Boundary Setbacks

- 9 Every dwelling, annex, caravan fixed to land, recreational facility or amenities building should be set back a minimum of:
- (a) 1 metre from an internal road (and the internal road surfaces should be surfaced to prevent dust becoming a nuisance);
 - (b) 6 metres from a public road;
 - (c) 2 metres from the boundary of the caravan park or camping ground.

Natural Hazards

- 10 In areas prone to flooding, bushfire or other natural hazards, buildings and structures (including annexes attached to caravans or caravans fixed to land) should be designed and constructed so that they can be removed in the event of a hazard.
- 11 Permanent structures or amenities should be located outside of the medium and high hazard flood risk areas shown on [Figures FI/1 to FI/8](#). Where this cannot be avoided, any new building or structure should have a finished floor level at least 300 millimetres above the AHD level for a 1 in 100 year ARI flood event. All services should be designed and constructed in accordance with the appropriate Australian Standard and in consultation with Service Authorities to be disengaged in times of flooding and capped to prevent inundation.

Land Division

- 12 No additional allotment(s) should be created wholly or partly within the precinct except where a lease or license agreement is made, granted or accepted under the *Residential Parks Act 2007*.

TOWN CENTRE HISTORIC (CONSERVATION) ZONE

Introduction

In addition to the Council Wide Policies, the Town Centre Historic (Conservation) Zone and Policy Area policies apply to the areas shown on:

Town Centre Historic (Conservation) Zone: [Map Ga/5, 12](#)
Gawler Health Services Helipad: [Figure Hel/1](#).

OBJECTIVES

Objective 1: The principal centre and focus for retail, business, community and entertainment activities serving the local and broader community and visitors to Gawler.

Two Policy Areas are identified in [Fig TCeH\(C\)/1](#) according to:

- (a) historic significance;
- (b) the future character for each;
- (c) the type and nature of development considered appropriate; and
- (d) other features that differentiate one area from another.

Objective 2: A range and distribution of uses and activities aimed at providing, in convenient and accessible groupings, convenience goods and comparison goods to serve the day-to-day, weekly and tourist needs of the community and those attending the area.

Objective 3: Public and private car parking areas located and designed convenient to the users of the centre.

Objective 4: Facilities to enhance the function and amenity of the centre for pedestrians.

Objective 5: Protection and reinforcement of the historic value, character and significance.

Objective 6: Use of upper building levels of buildings for residential and serviced apartments and hospitality services.

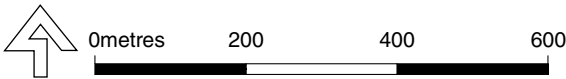
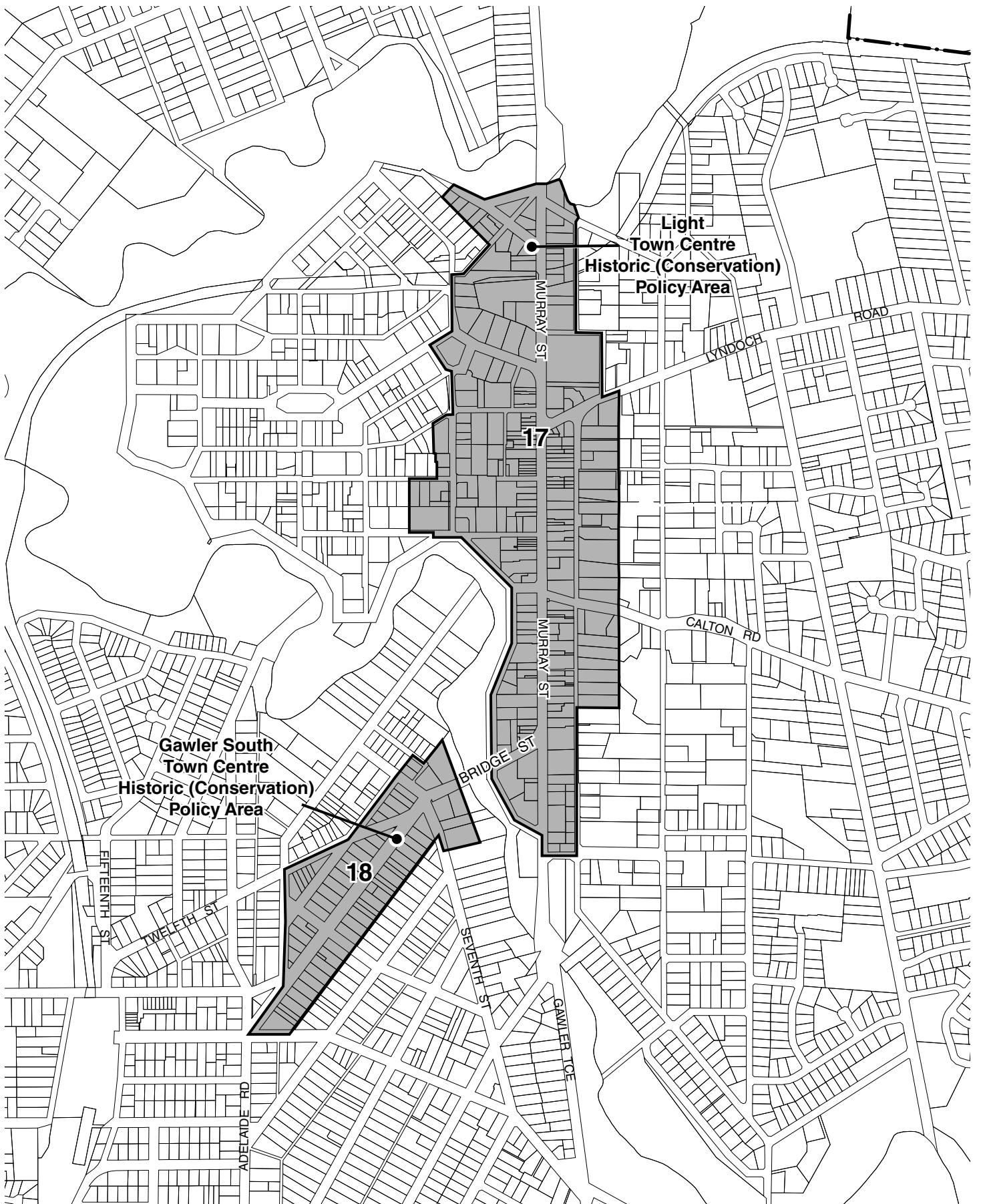
Objective 7: Retention/restoration, adaptation and re-use of desired building stock and maximization of the potential of derelict and underutilised land.

Objective 8: Co-ordinated development of rear car parking areas to:

- (a) optimize available car parking spaces;
- (b) upgrade building frontages with shop fronts;
- (c) establish clearly defined pedestrian links to Murray Street via arcades, sheltered thoroughfares and covered malls; and
- (d) enhance the appeal and public amenity of pedestrian areas/space.

Objective 9: A built character which:

- (a) is individualistic and strongly identified with the zone;
- (b) comprises separate, discrete visual components with common unifying architectural elements;



- Town Centre Historic (Conservation) Policy Area
- Development Plan Boundary

**GAWLER (CT)
TOWN CENTRE
HISTORIC (CONSERVATION)
POLICY AREAS
FIGURE TCEH(C)/1**

- (c) presents an articulated façade to the public street frontage and enhances these public spaces; and
- (d) utilizes the ground slope so as to create multiple building and car parking levels.

Objective 10: Retention and enhancement of the historic character and significance of the Policy Areas based on a consistent patterning of buildings to the street and to their sites, and a preponderance of 19th Century and early 20th Century architecture characterised by:

- (a) buildings developed up to the street alignment with verandahs and/or balconies overhanging the footpath;
- (b) no direct vehicle access from Murray Street and frontages developed with traditional shop fronts;
- (c) orientation of buildings to the street with minimal or no set-backs from side boundaries having regard to traffic safety;
- (d) one and two-storey buildings of a variety of design and detail, but with a scale which does not dominate or overwhelm the streetscape;
- (e) development that does not detract from the prominence of significant townscape items, in particular the main civic and institutional buildings and hotels;
- (f) buildings which create a high degree of individuality and with discrete building facades of generally no more than 25 metres width;
- (g) buildings with a high proportion of “solid” to glass fronts, and where presentation emphasises horizontal rather than vertical elements; and
- (h) simple or orthogonal built-form typical of the character of 19th Century building stock using elements such as pitched, gable (25 to 45°), hip or hip-gable combination roofs, parapets and cornices with traditional materials, particularly stone and rendered masonry and traditional colours.

Objective 11: Retention of all places and items which contribute to the historic character and significance of the Policy Areas as expressed in the Future Character.

Objective 12: Conservation and enhancement of the historic character of the Policy Areas through consideration of:

- (a) Streetscape character;
- (b) Building alignment and set-backs;
- (c) Building form, materials and colours;
- (d) Site layout, landscaping and fencing.

Objective 13: Maintenance of the existing topography.

Objective 14: Retention of historic townscape vistas and views.

Objective 15: A business, shopping, civil and entertainment area with a traditional Main Street Character and a strong pedestrian function that has some variety of scale and building forms, but not of a form or height which would overwhelm or detract from the predominance of significant Murray Street buildings or the elevated and distinctive plateau of the Church Hill State Heritage Area.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should promote and maintain pedestrian connections:
 - (a) to Murray Street;
 - (b) the Centre's principal parking areas and parklands;
 - (c) from Murray Street through to Reid Street
 - (d) Commercial Lane alignment from Cowan Street through to Tod Street.
- 2 Fencing should not impede pedestrian thoroughfares shown on [Figure CoP/1](#).
- 3 Major public entrances to malls (of an integrated centre development) should be clearly signposted and identified with suitable building design elements which reinforce and complement the predominant built character.
- 4 Parking areas and traffic movement should be co-ordinated.
- 5 Commercial Lane should be redeveloped for retail uses.
- 6 Development should incorporate separate commercial servicing and patron access points and manoeuvring areas.
- 7 Off-street parking and vehicle access points should be located such that breaks in the continuity of street front buildings are minimized.
- 8 Redevelopment of surplus rear and service yards should be coordinated between adjoining sites, to maximize off-street parking and development incorporating attractive shop fronts.
- 9 Landscaping should:
 - (a) complement and not mask or overwhelm the building development;
 - (b) define pedestrian boulevards, car park entries and public avenues; and
 - (c) comprise ornamental exotic species characteristically employed in the 19th Century and early 20th Century plantings.
- 10 The built form, site development and trading conditions should promote public access and surveillance and create a feeling of security and personal safety.
- 11 No new industrial, or warehousing development should be undertaken within the area.
- 12 Any redevelopment of an existing industrial or warehousing development should be confined to its existing site and only occur if it reduces the impacts associated with the desired future character and amenity of its locality.

Conservation

- 13 Development should reinforce and complement the historic character and significance of the area and the integrity of any places and items identified in [Table Ga/2](#), [Table Ga/5](#) or [Table Ga/6](#).
- 14 Vacant land, buildings or sites having a detrimental affect on the character of the locality not identified as having any heritage significance should be redeveloped and upgraded in accordance with the historic character of the Policy Area.
- 15 Development should conserve, maintain, enhance and reinforce the existing streetscape character of the Policy Area and the historic character of individual buildings, items, structures, and places.

- 16** Development should enhance and contribute to the visual cohesiveness and historic character of the Policy Area by reference to the external appearance of the building (architectural detail, materials and colours) and any new buildings should be of complementary height, scale, set-back, form and external appearance, and display creative and diverse examples of high standard contemporary architecture.
- 17** Colours of external painted surfaces and materials should be chosen to preserve and enhance the desired future character and historic character of the zone. The use of primary colours should be limited to signage, posts, window frames, door frames, door panels and limited areas on new structures, only where they make an important architectural contribution.
- 18** Primary colours should not be used in decorative mouldings or render.
- 19** Painting on stone work and fluorescent finishes are not appropriate.
- 20** Places and items identified in [Table Ga/2](#), [Table Ga/5](#) or [Table Ga/6](#) should not be demolished, but instead be retained, upgraded and adapted such that:
- (a) the quality or condition which gives the place its significance is protected or enhanced. This should include elements of the place which have significant cultural associations;
 - (b) the place is put to a use which supports the maintenance and restoration of that building, item, structure or place;
 - (c) the place is only altered so as to retain or restore the original external form, materials, and colours in a manner compatible with its structural visual, material and historic integrity;
 - (d) where internal arrangements and finishes are of significance to places and items identified in [Table Ga/2](#), [Table Ga/5](#) or [Table Ga/6](#), the place should only be altered in a manner compatible with its structural visual, material and historic integrity.
- 21** Demolition of a place or item identified in [Table Ga/2](#) should not be undertaken unless the building, item, structure or place is so structurally unsound it represents a risk to public safety and is beyond economic repair. Unsympathetic (later) additions may be demolished if the later addition does not contribute to the historic character of the place.
- 22** Development abutting or in close proximity to a place or item identified in [Table Ga/2](#), [Table Ga/5](#) or [Table Ga/6](#) should:
- (a) respect the historic character of the area and the integrity of the particular place or item of significance and be designed to a high architectural standard; and
 - (b) be compatible in respect of its design, siting, scale, building and roof shape, bulk, height, materials and colours, fences and landscaping and any advertising signs and external illumination.
- 23** Any alteration or addition to a place identified in [Table Ga/2](#), [Table Ga/5](#) or [Table Ga/6](#) should reinforce and complement the historic character of the place in terms of its design, siting, boundary set-backs, scale, built form and roof shape, bulk, height, materials and external finishes and where applicable:
- (a) should minimise their impact on the streetscape by siting to the rear or the side of existing buildings and not forward of existing buildings;
 - (b) the height of eaves and gutters on additions should be complementary with the existing building;
 - (c) the proportions (vertical and horizontal) of additions visible from the street, should be complementary with those of the existing building;

- (d) materials may include masonry (stone and brick) render, timber boarding and corrugated steel sheeting (walls and roof);
- (e) materials such as mirror glass, flat profile steel sheeting, unpainted zincalume sheeting, wide fibre cement sheet planking and polished face concrete panels should not be used where they are visible from the street;
- (f) roof conversions or additional upper floors are not desirable where they compromise the historic integrity of the building or the view of the building from the street;
- (g) roof conversions are acceptable where the alterations are fully incorporated under the existing roof and dormer style windows are acceptable where they are not visible from the street; and
- (h) external colour schemes should be contemporary, but should not dramatically contrast with the original colour scheme of the existing place.

24 Fencing (new and alteration to existing) should:

- (a) complement and reinforce the historic character of places identified in [Table Ga/2](#), [Table Ga/5](#) or [Table Ga/6](#), and the zone generally; and
- (b) facilitate desired pedestrian paths and interconnection between adjoining uses.

25 Division of an allotment containing a place or item identified in [Table Ga/2](#), [Table Ga/5](#) or [Table Ga/6](#) should not be undertaken if it may detrimentally affect its heritage significance, particularly if it would result in fragmentation of the ownership of a single building and its site.

Car Parking

26 Development should provide car parking spaces in accordance with the requirements of [Table Ga/1](#), either on the site of the development, or another convenient and accessible site in the locality, providing permanent ongoing arrangements for the parking shortfall are provided on another site

27 The provision of a lesser amount of car parking on site than that prescribed in [Table Ga/1](#) may be allowed in order to:

- (a) retain existing buildings of heritage interest; or
- (b) achieve the desired built form

provided however a financial contribution at the specified rate per car park is paid to the Council Car park Fund for any underprovision with respect to development in the designated area.

Note: Generally substitution of the total obligation to provide on-site car parking for a financial contribution will be relevant to smaller floorspace developments of less than 450 square metres total floor area or gross leasable area.

28 Roof top, undercroft or multi-level car parking may be developed in a manner that:

- (a) provides for convenient and safe pedestrian access and movement;
- (b) is compatible with the desired built character and is integrated with buildings and land form and screened with vegetation;
- (c) promotes personal and property safety (eg lighting and minimisation of entrapment areas); and
- (d) provides lighting that is designed to meet performance requirements while minimising energy requirements.

29 Onsite car parking shall incorporate:

- (a) a level of provision that meets the requirements of [Table Ga/1](#).

Where appropriate, the amount of any car parking shortfall (calculated with reference to [Table Ga/1](#)) may be made-up by a financial contribution to the Council Car Park Fund in accordance with the rate specified for a designated area.

Note: Car parking contribution in lieu of on-site provision will be more suitable in the traditional main street precincts of the town centre and for small to medium scale business developments, ie less than 450 square metres of total floor area or gross leasable area as appropriate;

- (b) a design that meets the current Australian Design Standards for car park layout, commercial vehicle loading, disabled parking, and bicycle parking;
 - (c) car parking spaces for people with disabilities in location which are convenient and accessible to the building/or use on the site;
 - (d) secure and convenient spaces for bicycles;
 - (e) landscaped design to shade vehicles and enhance the appearance of the car park;
 - (f) opportunities for interconnection of access driveways with adjoining sites or car parks to facilitate shared use;
 - (g) limited number of crossovers points onto public roads;
 - (h) design elements that promote personal and property safety (eg lighting, surveillance and minimisation of entrapment areas); and
 - (i) lighting designed to meet performance requirements while minimising energy requirements.
- 30** Car parking should be coordinated with other parking sites and preferably located at the rear of buildings which incorporate clearly defined pedestrian links.

Signs

- 31** All signs should be developed in accordance with the relevant provisions applying throughout the Council area and with the Performance Standards in [Table Ga/4](#).

32 The following sign types are appropriate in the zone:

- (a) Pylon or Freestanding (internal or external illumination);
- (b) Flat Wall;
- (c) Projecting (other than Town Centre Gawler South Policy Area);
- (d) Verandah Fascia (other than Town Centre Gawler South Policy Area);
- (e) Under Verandah (other than Town Centre Gawler South Policy Area);
- (f) Shop-Front (other than the Town Centre Gawler South Policy Area);
- (g) Sandwich Board;
- (h) First Party Advertising;

Where buildings on the site are set back less than 3.0 metres from street frontage:

- (i) Pylon or Freestanding;
- (j) Projecting;
- (k) Flat Wall;
- (l) Under Verandah;
- (m) Shop Front;
- (n) Sandwich Board; and
- (o) First Party Advertising.

33 The following sign types are inappropriate in the zone:

- (a) Above-Verandah;
- (b) Bunting;
- (c) Mobile;
- (d) Roof;
- (e) Internally illuminated signs on buildings of heritage significance listed in [Table Ga/2](#) or [Table Ga/5](#).
- (f) Third Party Advertising;
- (g) Banner.

Vending Machines and Automatic Teller Machines

34 Siting of vending machines and automatic teller machines should have regard to:

- (a) consistency with the relevant zone provisions;
- (b) Maintenance of character and continuity of activity along street frontages;
- (c) maintenance of unimpeded pedestrian movement. Where necessary sufficient set-back should be provided from front boundaries to accommodate queues.

35 Advertising displays relating to vending machines and automatic teller machines should be restrained in size.

Industrial Development

36 No new industrial or warehousing development should be undertaken within the zone. Any redevelopment of an existing industrial or warehousing development should be confined to its existing site and only occur if it reduces the impacts associated with the future character and amenity of its locality.

Hazards

The following principles apply to land within the Gawler Rivers Floodplain Area identified on [Figure FI/1 to FI/8](#).

Flooding

37 All new buildings should:

- (a) have a finished floor level equal to or greater than 300 millimetres above the predicted level of a 1 in 100 ARI flood event level applicable to the relevant hazard zone; and
- (b) ensure there is no permanent storage of goods or equipment below 300 millimetres above the predicted level of a 1 in 100 ARI flood event level for the applicable hazard zone.

38 Additions to existing buildings should:

- (a) be a maximum of 40 square metre increase on the existing floor area; and
- (b) be at the same or higher finished floor level as the existing building.

39 Car parking areas should be provided at natural ground level or with a maximum level increase of 300 millimetres.

PROCEDURAL MATTERS

Complying Development

40 Complying developments are prescribed in schedule 4 of the *Development Regulations 2008*. In addition, the following forms of development (except where the development is non-complying) are complying:

A change of use to a shop, office, consulting room or any combination of these uses where all of the following are achieved:

- (a) the area to be occupied by the proposed development is located in an existing building and is currently used as a shop, office, consulting room or any combination of these uses;
- (b) the building is not a State heritage place;
- (c) it will not involve any alterations or additions to the external appearance of a local heritage place as viewed from a public road or public space;
- (d) if the proposed change of use is for a shop that primarily involves the handling and sale of foodstuffs, it achieves either (i) or (ii):
 - (i) all of the following:
 - (A) areas used for the storage and collection of refuse are sited at least 10 metres from any Residential Zone boundary or a dwelling (other than a dwelling directly associated with the proposed shop);
 - (B) if the shop involves the heating and cooking of foodstuffs in a commercial kitchen and is within 30 metres of any Residential Zone boundary or a dwelling (other than a dwelling directly associated with the proposed shop), an exhaust duct and stack (chimney) exists or is capable of being installed for discharging exhaust emissions;
 - (ii) the development is the same or substantially the same as a development, which has previously been granted development approval under the *Development Act 1993* or any subsequent Act and Regulations, and the development is to be undertaken and operated in accordance with the conditions attached to the previously approved development;
- (e) if the change in use is for a shop with a gross leasable floor area greater than 250 square metres and has direct frontage to an arterial road, it achieves either (i) or (ii):

- (i) the primary vehicle access (being the access where the majority of vehicles access/ egress the site of the proposed development) is from a road that is not an arterial road;
 - (ii) the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared;
- (f) off-street vehicular parking is provided in accordance with the rate(s) specified in [Table Ga/1](#) - Car Parking Requirements to the nearest whole number, except in any one or more of the following circumstances:
- (i) the building is a local heritage place;
 - (ii) the development is the same or substantially the same as a development, which has previously been granted development approval under the *Development Act 1993* or any subsequent Act and Regulations, and the number and location of parking spaces is the same or substantially the same as that which was previously approved;
 - (iii) the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared.

Non-complying Development

- 41** The following kinds of development, including expansion of an existing use, are **non-complying** in the Town Centre Historic (Conservation) Zone:

Above-Canopy Signs
 Amusement Park
 Builder's Yard
 Bunting

Demolition:

- (i) in total; or
- (ii) of the frontage or side wall returns visible from a street or other elements visible from a public place

of a place listed in [Table Ga/2](#) or [Table Ga/5](#).

Industry other than Service Industry
 Junk Yard
 Major Public Service Depot
 Mobile Sign
 Motor Repair Station
 Motor Showroom
 Petrol Filling Station
 Refuse Destructor
 Road Transport Terminal
 Roof Sign
 Warehouse

Town Centre Gawler South Policy Area ([Figure TCeH\(C\)/1](#))

Desired Character

The official plan of the township of Gawler South was deposited in the Lands Titles Office in February 1858. The Policy Area was part of an area to the south of Dead Mans Pass, and the east of the new industrial area, railway yards and residential areas on the other side of Adelaide Road which became a popular and affordable area to live.

Although largely used now for business purposes, the area has a residential form and pattern of development, consisting of dwellings mainly dating from the period 1860–1910. Dwellings are typically detached and are set back from the street and neighbouring houses.

The area is of historic importance because it contains a significant number of high integrity buildings, originally used as residences, mainly dating from the period 1860–1910, which illustrate the character and continuing expansion of residential Gawler in the latter half of the nineteenth century.

The Policy Area includes Adelaide Road which is the principal commercial arterial road entrance. The character is derived from a former residential function, which should be retained. Conversion or use of existing buildings and new development should not occur at the expense of the residential character with reference to the traditional pattern, building set-back, form and rhythm of the built form, and intensification of business activity should be accommodated through the adaptation and renovation of existing buildings or addition of compatible high quality new buildings. Ultimately high canopy trees should line the street and complement the landscaped surrounds of the majority of existing buildings.

Signs are appropriate in the area but should be constrained in number. Signs should have a minor impact on the townscape and should be developed in scale with the low rise nature of both the built form and the street's overall townscape. Where buildings are set-back from the road, a Pylon or Free Standing sign, together with a single Wall Sign, are appropriate. For buildings set-back between zero and three metres, Flat Wall Projecting and Under Verandah Signs are appropriate.

Appropriate Uses are:

- Community Use
- Consulting Room
- Educational Establishment
- Hospitality Services
- Office
- Place of Worship
- Shop

OBJECTIVES

- Objective 1:** A zone developed primarily for office, consulting rooms, community use specialized small-scale retailing and hospitality services;
- Objective 2:** A scale and intensity of development which is secondary to, and complements that in the Town Centre Light Policy Area, and which limits the intrusion of associated traffic and noise on residential development in abutting residential zones.

PRINCIPLES OF DEVELOPMENT CONTROL

Form of Development

- 1 Development in accordance with the Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement B](#).
- 2 Business uses which front Twelfth Street should:
 - (a) be limited to offices and consulting rooms;
 - (b) re-use existing residential buildings and adapt them to maintain their residential form and scale;
 - (c) share car parking on Eleventh Street;
 - (d) retain a front garden setting and front fencing of a residential style;
 - (e) use minimal outdoor signage with no signage on buildings.
- 3 Car parking should be co-ordinated with neighbouring development and provided at the rear of the buildings fronting Adelaide Road.

- 4 Centre development which abuts residential areas via a common boundary should:
- (a) incorporate a complementary scale of building development;
 - (b) be set-back to afford visual relief from such buildings and to buffer any associated nuisance;
 - (c) incorporate landscaping along common boundaries to screen service yards and areas liable to become unsightly.

Town Centre Light Policy Area ([Figure TCeH\(C\)/1](#))

Desired Character

This area encompasses part the original streets and allotments of the town of Gawler, as designed and laid out by Light, Finniss and Co in 1839 and is of historic significance as it still clearly illustrates the planning principles of the day and also the initial footprint of the town.

Colonel William Light first visited the Gawler area in December 1837, at which time he explored the Para Valley travelling past the North and South Para Rivers to Lyndoch and the Barossa Valley. He returned to the area in January 1839, camping for many days at Dead Mans Pass, an area named when Light and Finniss located a corpse in a River Red Gum tree. While at Dead Mans Pass, Light walked over the area now known as the town of Gawler, carrying out initial inspections.

In early 1839, Light Finniss and Co. recommended that this area would be suitable for a town, and in the following month it was visited by Reid and Murray with a view to establishing a Special Survey at the site. They found the site suitable, and their application for a special survey was approved. Various members of the firm of Light Finniss and Co. worked on the plans for the town. The firm's surveyor William Jacob, maintained that Light was responsible for the position and the overall plan of the town, with the plan itself being drawn up by RG Thomas and the town being pegged out by Jacob and Nixon.

Gawler's plan is historically significant as a successful example of a grid layout plan adapted to the topography and natural features of the location. The street pattern reflects the form of the rivers and topography of the hill upon which it is established. The original street pattern, rivers and local hills are still clearly identifiable, with features of note including the public squares around the churches and the terraces and parklands bordering the rivers.

The area is also of historic significance as it contains many of the early buildings of Gawler - Murray Street is the primary commercial street of Gawler and is lined with 1 to 2 storey commercial premises dating from 1850 – present. Of note are several freestanding "Italianate revival" style bank buildings, 2 storey terraces of shops and the landmark civic buildings of the Town Hall and Telegraph office.

The Area should be developed as the principal retail area for convenience and comparison goods in the Council area and adjoining areas.

Continuous retail frontages including eating places such as cafes, restaurants and entertainment facilities which support major retailing in the form of supermarkets or department stores should be characteristic of Jacob Street, Cowan Street and Commercial Lane frontages. These frontages should comprise small retail tenancies opening onto the street and retail arcades. Offices and consulting rooms are only appropriate at ground floor level if minor in scale with no disruption to retail shopfront continuity. Public parking at ground level other than in multi-storey developments is discouraged.

Use of upper floor levels for car parking, offices or residential development is appropriate.

Murray Street should retain its distinctive historical, commercial and civic character of older style ground floor shop fronts and professional rooms or residential use above. Development should acknowledge and respect the significant built form derived from the zone's commercial function and should retain a consistent building scale equivalent to two levels (8.0 metres) and traditional architectural forms.

Although Murray Street services a critical role in the movement and circulation of traffic in Gawler and the surrounding region, Murray Street should have a pedestrian focus and orientation, characterised by verandah covered footpaths, seating, high quality pavements, road crossing points and protection from the elements.

Within the Policy Area the emphasis on use of land should be specialist and major retailing, civic and public administration north of Calton Road and offices, tourist, entertainment and recreation facilities south of Calton Road.

North of the railway line, the existing character, derived from a former residential function, should be retained. Conversion or use of existing buildings and new development should not occur at the expense of the residential character with reference to the traditional patterns, building set-back, form and rhythm of the built form, and intensification of business activity should be accommodated through the adaptation and renovation of existing buildings or addition of compatible high quality new buildings. Signs are appropriate in this precinct but should be constrained in number. Signs should have a minor impact and should be developed in scale with the low rise nature of both the built form and the street's overall townscape. Where buildings are set-back from the road, a Pylon or Free Standing sign, together with a single Wall Sign, are appropriate. For buildings set-back between zero and three metres, Flat Wall Projecting and Under Verandah Signs are appropriate.

Development in the Policy Area should comply with the Concept Plan, [Fig CoP/1](#) to achieve an integrated movement pattern (pedestrian and vehicles), access points and shared car parking.

Signage of various types will be appropriate providing it achieves a high level of integration and respect for the architecture or individual buildings where signs are attached. For buildings recognised as heritage items, detached signage will be an appropriate technique for promoting goods and services. Clutter should be avoided, particularly so far as shop front signage is concerned where coverage should be restrained and colours chosen carefully to contrast but not clash with building colours and detailing.

Signage should concentrate on major symbols and logos and corporate signs in the form of Flat Wall Signs, Freestanding or Pylon signs which is directed to vehicular traffic. These should be used strategically and in scale with building architecture to identify major land uses, and may include directory boards, using generic descriptions rather than business names. Signage at the pedestrian scale should be limited to shop front, under verandah, verandah fascia, verandah blinds and sandwich boards.

The topography east of High Street contributes significantly to the setting of the town centre. Development for commercial and retail purposes in this locality should be sensitive to maintaining the natural land form of the eastern edge of the Centre. Examples of preferred approaches are shown in [Fig CoP/2](#).

Appropriate Uses are:

- Advertising Display
- Bank
- Café and Restaurant
- Car parking undercroft or above street level retail uses (apart from heritage places)
- Child care centre
- Civic community and cultural use
- Dwellings on 1st floor or upper levels (apart from north of the railway line)
- Education facilities
- Hotel (apart from north of the railway line)
- Office
- Office and Consulting rooms on upper levels
- Public Transport Interchange
- Leisure studio
- Shops including supermarket, department store, discount department store (apart from north of the railway line)
- Service trades in identified locations
- Residential uses
- Tourist Accommodation

OBJECTIVES

Objective 1: Development of a public transport interchange at the Gawler Central Railway Station.

PRINCIPLES OF DEVELOPMENT CONTROL

1 Development in accordance with Concept Plan [Figures CoP/1, CoP/2, CoP/3, Co/P4](#);

Concept Plan [Fig CoP/1](#) indicates:

- (a) locations for enhanced street frontage;
- (b) locations for potential decked car parking and coordinated parking
- (c) proposed pedestrian thoroughfares;
- (d) heavy vehicle routes;
- (e) through vehicle routes;
- (f) traffic controls;
- (g) retail areas identified for improved pedestrian treatment.

Concept Plan [Fig CoP/2](#) indicates:

- (a) preferred building set-backs;
- (b) locations for elevated buildings to protect landform;
- (c) ways of integrating walls into building forms;
- (d) methods for protecting existing vegetation;
- (e) preferred uses.

Concept Plan [Fig CoP/3](#) indicates preferred built form along High Street.

Concept Plan [Fig CoP/4](#) indicates:

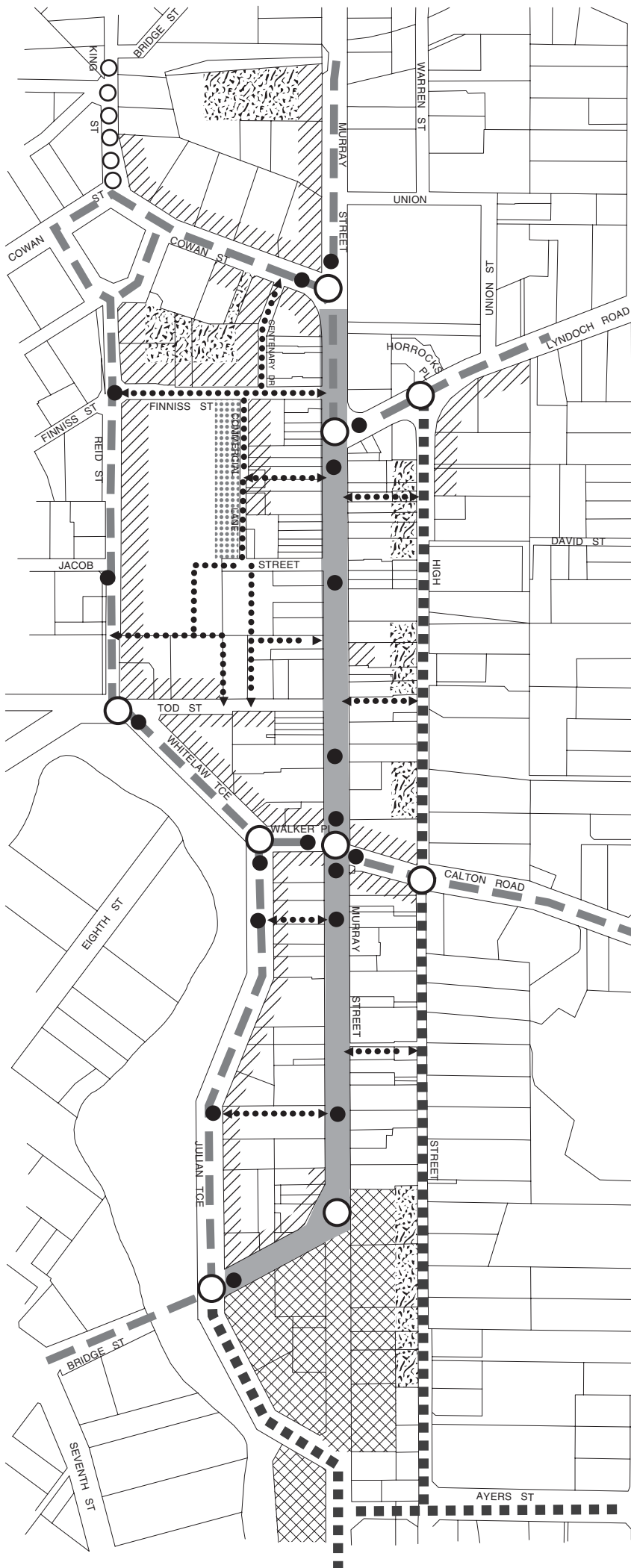
- (a) the preferred layout of a train/vehicle interchange;
- (b) preferred traffic flows;
- (c) access points;
- (d) pedestrian pathways.

2 Development in accordance with Structure Plan [Map Ga/1 \(Overlay 1\) Enlargement B](#).

The Structure Plan indicates:

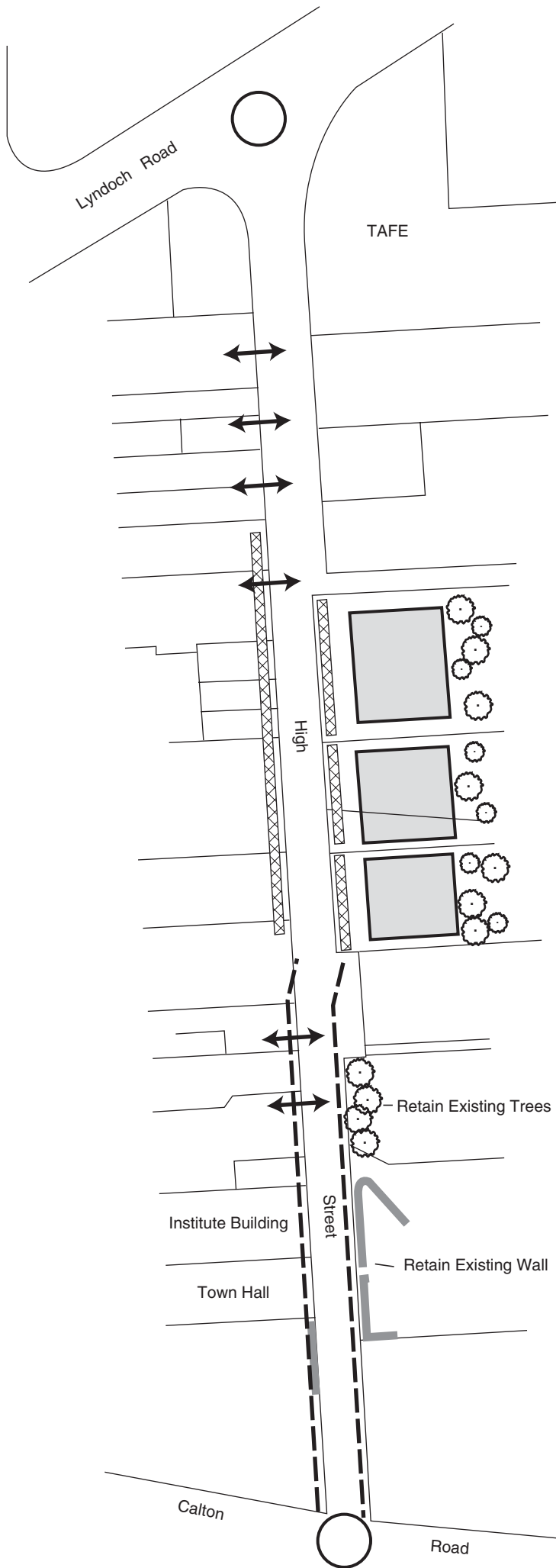
- (a) status and function of various roads;
- (b) extent of Church Hill State Heritage Area;
- (c) location of public transport, rail line and interchange;
- (d) relationship to recreation areas;








- (e) future road diversion;
 - (f) critical vistas.
- 3** No development should be carried out on land adjacent to the Gawler Central Railway Station which would prejudice the establishment of a public transport interchange as shown on [Fig CoP/3](#).



- — — Through Traffic and Heavy Vehicles
 - ■ ■ Through Traffic
 - Traffic Management with Pedestrian Priority
 - ○ ○ Residential Street
 - Primary Retail Street
 - ▨ Entertainment / Tourist Accommodation
 - Traffic Management
 - ▨ Enhanced Street Frontage/ Active Fronts
 - ▨ Potential for Decked Parking
 - ▨ Co-ordinated Parking
 - ◄ ● ● ● ► Pedestrian Thoroughfare
- ↑
- 0 100 200 300m

**GAWLER TOWN CENTRE
CONCEPT PLAN
Fig CoP/1**



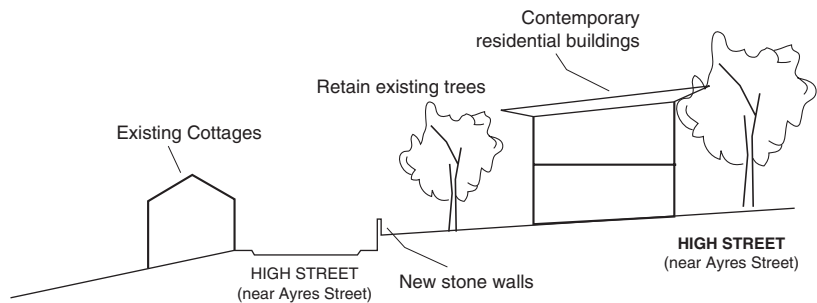
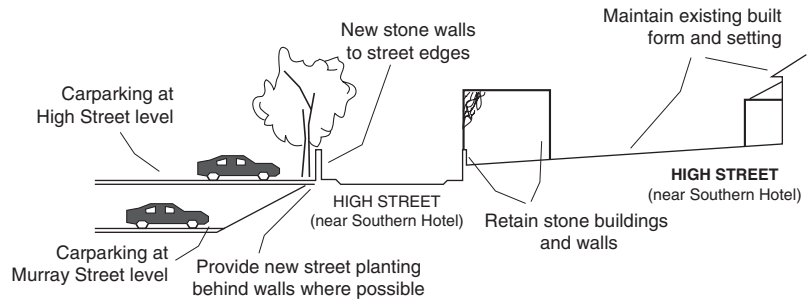
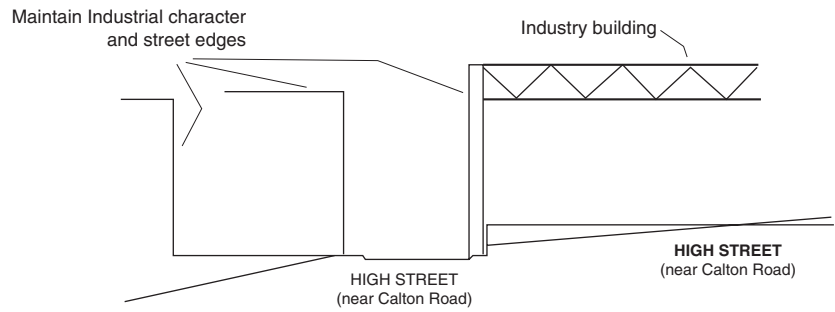
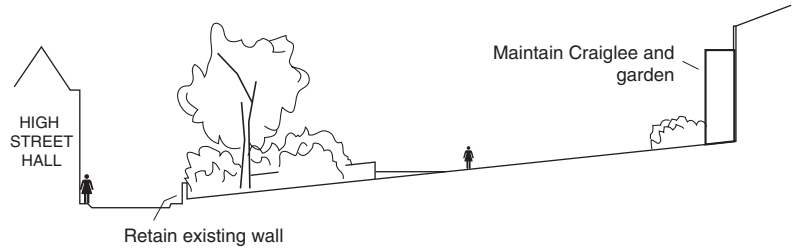
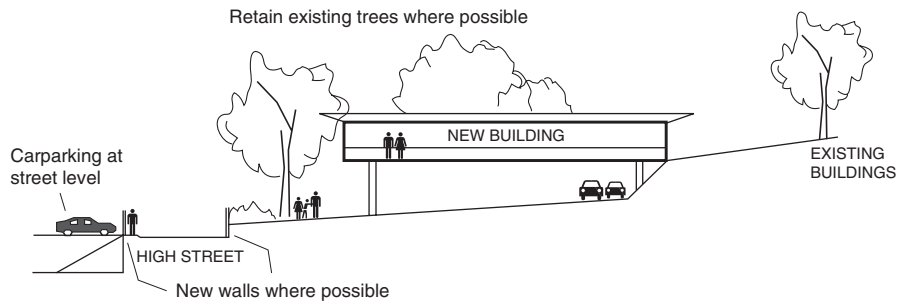
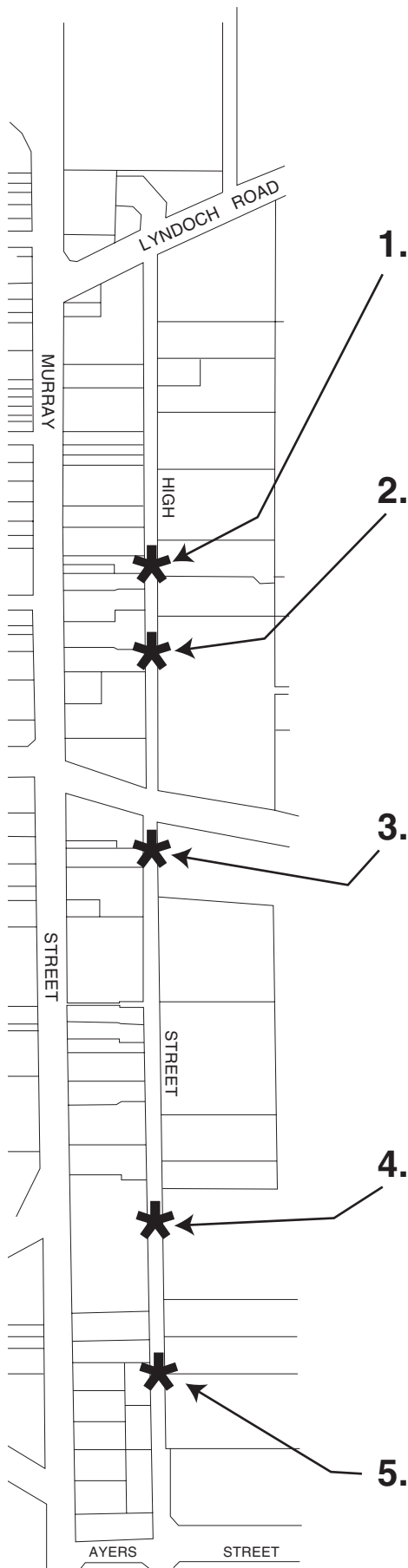
-  Indicative Building Envelope
-  New Stone Wall
-  Retain Existing Wall
-  New Road Alignment
-  Vehicle Access
-  Traffic Management
-  Landscape Setting

not to scale

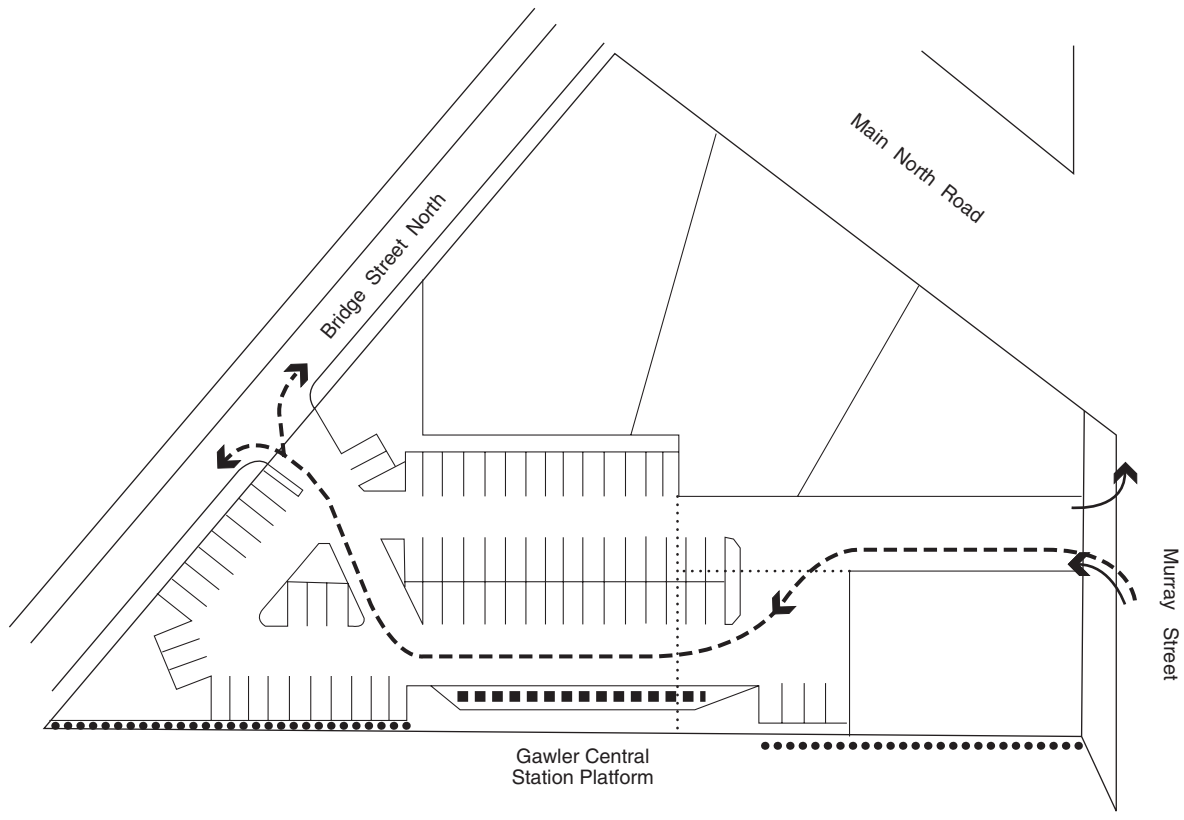


GAWLER TOWN CENTRE CONCEPT PLAN Fig CoP/2

Consolidated - 18 July 2019



GAWLER TOWN CENTRE CONCEPT PLAN



- Existing Property Boundaries
- Kiss and Ride and Bus Stop
- Preferred Bus Route
- Pedestrian Path
- > Vehicle Entry / Exit Movement



0 metres 20 40

GAWLER TOWN CENTRE CONCEPT PLAN Fig CoP/4

TABLE Ga/1**Car parking Requirements**

Car parking that should be provided on the site of the proposed development or on another related site where shared use is permanently secured as scheduled below.

In accordance with Section 50 A of the Development Act, any shortfall in car parking in designated areas allows the option by the planning authority to allow payment to the Town of Gawler Car-Park Fund of a financial contribution calculated at the specified rate multiplied by the amount of the shortfall.

Land Use	Car Park Rate
ACCOMMODATION	
Aged Care Retirement Homes	1 space per unit plus employees and visitors
Display Home	Assess on needs basis
Guesthouse/Hostel	1 space per 3 beds
Motel	1 space per room plus 1 space per employee
Serviced Apartments	1 space per room plus 1 space per employee
Tourist Accommodation (Bed & Breakfast)	1 space per guest room plus 1 space per employee
COMMERCIAL	
Auction Depot	1 space per 100m ² plus 2 spaces
Bank	6 spaces per 100m ²
Call Centre	Assess on needs basis (employee based)
Hardware and other Retail Showrooms	4 spaces per 100m ²
Motor Repair Station	2 spaces per 100m ²
Motor Showroom	2 spaces per 100m ²
Office	4 spaces per 100m ²
Petrol Filling Station	1 space per service bay plus 5 spaces per 100m ² retail floor space
Post Office	7 spaces per 100m ²
Retail Showroom (Bulky Goods)	4 spaces per 100m ²
Service Trade Premises	4 spaces per 100m ²
Shop	7 spaces per 100m ²
Used Car Lot/Vehicle Sales Yard	2 spaces per 100m ²
Video Store	6 spaces per 100m ²

Land Use	Car Park Rate
COMMUNITY/CIVIC	
Child Care	1 space per 4 children
Civic Administration Offices	4 spaces per 100m ²
Community Centre	10 spaces per 100m ²
Education Institutions (long term and short term) - Pre-school - Primary school - Middle school - Secondary school	(refer below) 1 space per employee plus 1 space per 2 children 1 space per employee plus 1 space per 6 students 1 space per employee plus 1 space per 12 students 1.2 spaces per employee plus 1 space per 20 students
Tertiary Institution	0.6 space for full time employee plus 0.2 space for part-time student
Library	0.75 space per user
Meeting Hall	1 space per 3 seats
Place of Worship	1 space per 3 seats
ENTERTAINMENT	
Amusement Machine Centre	7 spaces per 100m ²
Bowling Club	10 spaces per bowling green
Cinema Complex	85th percentile of projected attendance level
Concert Hall/Theatre	85th percentile of projected attendance level
Conference Facility	1 space per 3 seats
Entertainment Complex	Assess on needs basis
Exhibition Hall	Assess on needs basis
Hotels & Taverns - Public Bar - Lounge or beer garden - Dining room - Gaming room - Accommodation	1 space per 2m ² 1 space per 6m ² 1 space 3 seats 1 space per 2 machines 1 space per Room
Indoor Recreation/Gymnasium	Assess on needs basis
Night Clubs/Late Night Venues	1 space per 2.5 people
Non-Residential Club	Assess on needs basis
Restaurant (traditional)	1 space per 3 seats

Land Use	Car Park Rate
Restaurant (fast food/family) <ul style="list-style-type: none"> - without dine-in and drive through facilities - with dine-in facilities but no drive through - with dine-in and drive through facilities⁽¹⁾ 	12 spaces per 100m ² greater of 1 space per 5 seats (internal and external seating) or 1 space per 2 seats (internal seating) greater of 1 space per 3 seats (internal and external seating) or 1 space per 2 seats (internal seating)
Squash/Tennis Courts	4 spaces per court
TAB Facility	9.5 spaces per 100m ²
INDUSTRY & WAREHOUSE	
Office Component PLUS Non-Office Component	4 spaces per 100m ²
<ul style="list-style-type: none"> - Up to 200m² - Plus 200 - 2000m² - Plus greater than 2000m² 	2 spaces per 100m ² 1.33 spaces per 100m ² 0.67 spaces per 100m ²
MEDICAL	
Consulting Room	9.5 spaces per 100m ²
Hospital	2.5 spaces per bed
Medical Centre/Day Surgery	10 spaces per 100m ²
Nursing Home	1 spaces per 4 beds
OTHER	
Funeral Parlour	4 spaces per 100m ² plus 1 space per 4 seats (chapel)
Interchange/Transport Station	Assess on needs basis
Radio & TV Studio	5 spaces per 100m ²

Area relates to **total floor area** (including areas not within a building or roofed areas), except for **retail** which is gross leasable area and for used car lot/vehicle sales yard is total area of the site.

Where large multi-use complexes are involved, car parking requirements may need to be calculated according to the individual components.

⁽¹⁾ A car queuing area for a maximum of 12 cars is also required. There should be at least 4 spaces adjoining the ordering point for waiting purposes.

TABLE Ga/2

State Heritage Places

An indicative location for each place is shown on [Figures Ga\(HPCI\)/1 to 12.](#)

Property Address	Description and/or Extent of Listed Place	Lot No. or Part Section	Plan No.	Certificate of Title	Sec 16 Criteria	Heritage Branch ID
1 Calton Road, Gawler East	Ice Works & Office (Phoenix Foundry)			CT 5239/553, 5253/932, 5253/933		12158
1 Cameron Street	Dwelling	Lot 10	126107	CT 5232/32		
18 Cameron Street	Dwelling	Lot 70	154371	CT 5568/348		
20 Cameron Street	Dwelling	Lot 28	154429	CT 5483/824		
22 Cameron Street	Dwelling	Sec 24 Lot 33	154434	CT 5693/756		
24 Cameron Street	Dwelling	Lot 2	104532	CT 5147/872		
28 Cameron Street	Dwelling	Sec 24 Lot 38	154439	CT 5359/869		
3 Cameron Street	Dwelling	Lot 37	154438	CT 5357/943		
32 Cameron Street	Dwelling	Lot 253	25211	CT 5667/130		
Cowan Street	Sunday School	LOT: 99 SEC: 1	154400	CT 5831/268		
Cowan Street	St George's Anglican Church	Sec 24 Lot 35	154436	CT 5544/714		
15 Cowan Street	Funeral Parlour	LOT: 66 TYP: A SEC: 24	154467	CT 5798/568		
16Cowan Street	Reserve	Lot 213		CT 1304/74		
17 Cowan Street	Office	Sec 1	3681	CT 5033/319		
17 Cowan Street		Sec 1	3681	CT 5033/319		
17 Cowan Street	Home Unit	Sec 2	3681	CT 5033/320		
17 Cowan Street		Sec 2	3681	CT 5033/320		
18 Cowan Street	Dwelling	LOT: 49 TYP: A SEC: 24	154450	CT 5798/535		
19 Cowan Street	Dwelling	Lot 21	102537	CT 5124/511		
20 Cowan Street	Dwelling	Lot 48	154449	CT 5436/272		
21 Cowan Street	Dwelling	Sec 24 Lot 846	211632	CT 5690/500		
22 Cowan Street	Dwelling	LOT: 47 TYP: A SEC: 24	154448	CT 5835/98		
23 Cowan Street	Court House	Lot 2	60238	CT 5879/930		
24 Cowan Street	Kindergarten	LOT: 42 SEC: 42	154443	CT 5823/119		
25 Cowan Street	Police Station	Lot 1	60238	CT 5879/929		
26 Cowan Street	St George's Anglican Church Hall	Sec 24 Lot 7	154408	CT 5535/936		
28 Cowan Street	Dwelling	Lot 100	154401	CT 5369/738		
29 Cowan Street	Dwelling	LOT: 50 TYP: A SEC: 24	154451	CT 5788/472		
30 Cowan Street	Dwelling	LOT: 1 TYP: A SEC: 24	154402	CT 5808/147		
31 Cowan Street	Dwelling	Sec 24 Lot 51	154452	CT 5519/916		
32 Cowan Street	Dwelling	Sec 24 Lot 36	154437	CT 5602/979		

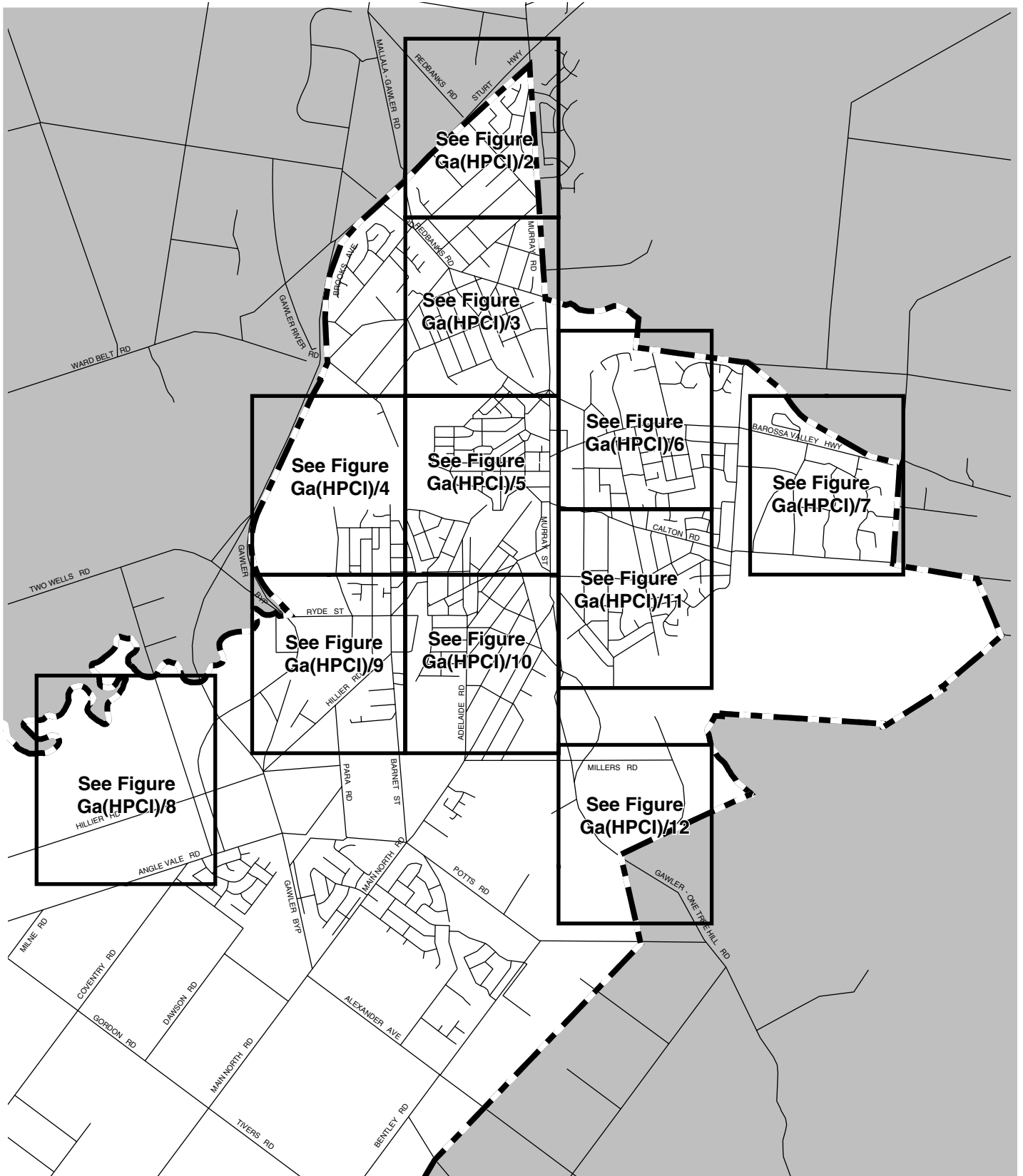
Property Address	Description and/or Extent of Listed Place	Lot No. or Part Section	Plan No.	Certificate of Title	Sec 16 Criteria	Heritage Branch ID
32 Cowan Street	Land	Sec 24 Lot 34	154435	CT 5721/98		
32 Cowan Street	Land	Lot 25	154426	CT 5565/791		
33 Cowan Street	Dwelling	Lot 52	154453	CT 5563/578		
34 Cowan Street	Dwelling	LOT: 24 TYP: A SEC: 24	154425	CT 5778/824		
35 Cowan Street	Dwelling	Sec 24 Lot 19	100562	CT 5102/812		
36 Cowan Street	Dwelling	Sec 24 Lot 22	154423	CT 5600/354		
36 Cowan Street		Sec 24 Lot 23	154424	CT 5600/353		
36 Cowan Street		Sec 24 Lot 27	154428	CT 5600/352		
37 Cowan Street	Dwelling	LOT: 53 TYP: A SEC: 24	154454	CT 5770/938		
38 Cowan Street	Dwelling	Sec 24 Lot 21	154422	CT 5642/871		
40 Cowan Street	Dwelling	LOT: 4 TYP: A SEC: 24	153523	CT 5279/286		
41 Cowan Street	Tennis Courts	Lot 128		CT 2459/133		
42 Cowan Street	Dwelling	Sec 24 Lot 19	154420	CT 5671/374		
42 Cowan Street		Sec 24 Lot 20	154421	CT 5716/813		
45 Cowan Street	Playground	Sec 24 Lot 39	154440	CT 5742/12		
49 Cowan Street	Dwelling	Sec 24 Lot 29	154430	CT 5630/549		
51 Cowan Street	Dwelling	Lot 30	154431	CT 5421/396		
53 Cowan Street	Dwelling	Lot 13	38846	CT 5171/97		
55 Cowan Street	Dwelling	Lot 12	38846	CT 5171/96		
57 Cowan Street	Dwelling	Lot 31	154432	CT 5441/800		
59 Cowan Street	Dwelling	Lot 136		CT 4064/323		
61 Cowan Street	Dwelling	Lot 137		CT 1332/179		
8 Cowan Street	Former St Andrew's Presbyterian Church	Lot 3, Sec 24 Hd Mudla Wirra		CT 5220/169		
10 Cowan Street	Old Bushman Hotel			CT 5215/59		10380
11 Dawkins Avenue, Willaston	Willaston Cemetery	Sec 463 HD Mudla Wirra		CT 1816/1		
1-13 Deland Avenue, Gawler East	Trevu House			CT 4195/147		12153
1 Duffield Street, Gawler East	Martindale			CT 2107/17		10376
9 Dundas Street	Dwelling	Lot 3	154404	CT 5357/514		
25-27 Eighteenth Street, Gawler South	Railway Hotel			CT 3663/91		12136
13 Finniss Street	Dwelling	Lot 33	4348	CT 4138/730		
15 Finniss Street	Dwelling	Sec 24 Lot 64	154465	CT 5722/817		
20 Finniss Street	Dwelling	Lot 97	154398	CT 5422/765		
22 Finniss Street	Dwelling	Sec 24 Lot 98	154399	CT 5549/656		
23 Finniss Street	Church	LOT: 99 SEC: 1	154400	CT 5831/268		
26 Finniss Street	Dwelling	LOT: 3 TYP: A	101272	CT 5105/724		
28 Finniss Street	Dwelling	Lot 69	154370	CT 5420/716		
32 Finniss Street	Dwelling	LOT: 3 TYP: A SEC: 24	107187	CT 5181/241		

Property Address	Description and/or Extent of Listed Place	Lot No. or Part Section	Plan No.	Certificate of Title	Sec 16 Criteria	Heritage Branch ID
35 Finniss Street	Dwelling	Sec 24 Lot 61	154362	CT 5676/472		
36 Finniss Street	Dwelling	Sec 24	11312	CT 5017/762		
36 Finniss Street	Dwelling	Sec 2	11312	CT 5017/763		
36 Finniss Street	Dwelling	Sec 3	11312	CT 5017/764		
37 Finniss Street	Dwelling	Sec 24 Lot 62	154363	CT 5698/432		
38 Finniss Street	Dwelling	LOT: 54 TYP: A SEC: 24	154355	CT 5811/499		
39 Finniss Street	Dwelling	Lot 63	154364	CT 5289/210		
39 Finniss Street		Lot 26	154427	CT 5289/211		
40 Finniss Street	Dwelling	Lot 55	154356	CT 5293/471		
41 Finniss Street	Dwelling	Sec 24 Lot 2	20241	CT 5126/513		
42 Finniss Street	Dwelling	LOT: 17 TYP: A	100428	CT 5101/55		
43 Finniss Street	Dwelling	Sec 24 Lot 1	20241	CT 5231/35		
44 Finniss Street	Dwelling	LOT: 60 TYP: A SEC: 24	154361	CT 5416/520		
45 Finniss Street	Dwelling	Sec 24 Lot 64	154365	CT 5680/236		
46 Finniss Street	Dwelling	Sec 24 Lot 57	154358	CT 5512/726		
47 Finniss Street	Dwelling	Sec 24 Lot 65	154366	CT 5720/98		
48 Finniss Street	Dwelling	Sec 24 Lot 58	154359	CT 5670/654		
49 Finniss Street	Dwelling	LOT: 66 TYP: A SEC: 24	154367	CT 5831/498		
50 Finniss Street	Dwelling	LOT: 59 TYP: A SEC: 24	154360	CT 5775/3		
21a High Street	Office (Phoenix Foundry)			CTs 5239/553, 5253/932, 5253/933		12158
43 Jacob Street	Dwelling	Lot 68	154369	CT 5283/923		
53 Jacob Street	Dwelling	Lot 56	154357	CT 5424/600		
20 Jerningham Street	Dwelling	Sec 24 Lot 2	49127	CT 5595/858		
22 Jerningham Street	Land	LOT: 1 SEC: 24	49127	CT: 5595/857		
24 Jerningham Street	Dwelling	Sec 24 Lot 252	25211	CT 5662/383		
Julian Terrace/ Bridge Street South	Gawler Mill (former Union Mill and Office)	Lot 3-4		CT 5604/666		10378
12 King Street, Gawler	"Hemingby" Cottage			CT 4229/296		10374
16 King Street, Gawler	"Hemingby" Cottage			CT 5087/34		10374
16a King Street, Gawler	"Hemingby" Cottage			CT 5156/257		10374
Light Square	Stone Barn adjacent Old Bushman Hotel Property	Lot 5		CT 5215/58		
1 Moore Street	Dwelling	Lot 2	131901	CT 5242/464		
10 Moore Street	Dwelling	Lot 2	154403	CT 5359/34		
1a Moore Street	Baptist Church, former Congregational Church	Lot 111, Sec 24 Hd Mudla Wirra		CT 2559/44		

Property Address	Description and/or Extent of Listed Place	Lot No. or Part Section	Plan No.	Certificate of Title	Sec 16 Criteria	Heritage Branch ID
1a Moore Street	Former Congregational Church (1851)	Lot 111, Sec 24 Hd Mudla Wirra		CT 2559/44		
2 Moore Street	Dwelling	Lot 65	154466	CT 5508/210		
3 Moore Street	Dwelling	Sec 24 Lot 44	154445	CT 5688/10		
4 Moore Street	Dwelling	Sec 24 Lot 61	154462	CT 5575/102		
5 Moore Street	Dwelling	Lot 1	149506	CT 5274/774		
5 Moore Street		Lot 3	149508	CT 5274/788		
6 Moore Street	Dwelling	Lot 43	154444	CT 5409/720		
6 Moore Street	Dwelling	Sec 24 Lot 60	154461	CT 5409/866		
7 Moore Street	Dwelling	Sec 24 Lot 45	154446	CT 5716/952		
8 Moore Street	Dwelling	Lot 2	128661	CT 5233/279		
8 Moore Street		Lot 3	128661	CT 5233/279		
9 Moore Street	Dwelling	Lot 46	154447	CT 5285/411		
Murray Street, Gawler	McKinlay Memorial (Monument & Garden - McKinlay Memorial & Pioneer Park)					10381
146/150 Murray Street, Gawler	Shops			CTs 4308/333, 3132/91		12164
152 Murray Street, Gawler	Shop			CT 3132/94		12164
154 Murray Street, Gawler	Shop			CT 3132/92		12164
18 Murray Street, Gawler	Baptist Church			CT 667/87		10377
2 Murray Street, Gawler	E & WS Depot			CT GM 26/538		12161
25 Murray Street, Gawler	Professional Offices			CTs 4236/83, 92/44		12142
30-32 Murray Street, Gawler	Kingsford Hotel			CT 5140/762		10390
33 Murray Street, Gawler	Bank			CT 3099/103		10371
59 Murray Street, Gawler	Telegraph Station (museum)					10370
61 Murray Street, Gawler	Post Office Building			CT 5357/564		10386
66 Murray Street, Gawler	Bank			CTs 3620/194, 3744/55		10385
77-79 Murray Street, Gawler	Old Spot Hotel			CT 5223/250		12126
89 Murray Street, Gawler	Town Hall			CT 275/57		10383
91 Murray Street, Gawler	Institute			CT 275/57		10382
93 Murray Street, Gawler	Bank			CT 2428/197		10384
98-100 Murray Street, Gawler	Essex House			CT 2581/133		11049
1-13 Nixon Terrace, Gawler	Primary School			CT 5454/821		12162
1 Parnell Street	Dwelling	Lot 15	154416	CT 5358/382		

Property Address	Description and/or Extent of Listed Place	Lot No. or Part Section	Plan No.	Certificate of Title	Sec 16 Criteria	Heritage Branch ID
1 Parnell Street		Lot 16	154417	CT 5358/369		
1-11 Parnell Street	Roman Catholic presbytery, outbuildings & walls	Lots 138, 139 Sec 24 Hd Mudla Wirra		CT 5358/354		
Penrith Avenue, Gawler West	House "Para Para" and Outbuildings			CT 5299/959		10057
3 Porter Street		Lot 91	200551	CT 5358/804		
4-6 Porter Street	Dwelling	Sec 24 Lot 11	19294	CT 5454/975		
4-6 Porter Street	Former Good Samaritan Convent The historic form and fabric of the former Convent but not including attached carport and outbuildings.	Lot 10 Sec 24 Hd Mudla Wirra		CT 5454/974	(a, b, f)	
6a Porter Street	Former St Joseph's Convent The historic form and fabric of the former Convent.	Lot 10, Sec 24 Hd Mudla Wirra		CT 5454/974	(a, b, e)	
3-7 Porter Street	Former Roman Catholic school	Lot 67, Sec 24 Hd Mudla Wirra		CT 5358/161		
8 Porter Street	St Peter & St Paul Roman Catholic Church	Sec 24 Hd Mudla Wirra	518	CT 5358/434		
10 Queen Street	Dwelling	Lot 1	13199	CT 5209/168		
12 Queen Street	Dwelling	Sec 24 Lot 54	154455	CT 5549/558		
14 Queen Street	Dwelling	LOT: 2 TYP: A SEC: 24	125706	CT 5225/5		
15 Queen Street	Dwelling	Lot 3	121555	CT 5214/758		
17 Queen Street	Dwelling	Lot 1	137797	CT 5250/977		
4 Queen Street	Dwelling	Lot 56	154457	CT 5444/164		
6 Queen Street	Dwelling	LOT: 55 TYP: A SEC: 24	154456	CT 5779/526		
8 Queen Street	Dwelling	Lot 2	13199	CT 5280/106		
11 Railway Terrace, Gawler West	House			CTs 5244/561, 5244/560		12172
1 Scheibener Terrace, Gawler	House			CT 5233/432		10387
8-10 Seventh Street, Gawler South	Former Gas Works			CT 269/244		12141
6 Sixteenth Street, Gawler South				CT 5141/87		14429
2-6 Sunnysdale Avenue, Gawler East	Wheatsheaf Hotel			CT 5215/615		12280
2 Sutton Street	Dwelling	Lot 1	104477	CT 5148/55		
1 Tod Street	Shop			CT 5361/477		12154
Tod Street, Gawler	Uniting Church			CT 2321/85		10388
12 Tod Street, Gawler	Tortola House			CT 5180/445		10375
1 Turner Street, Gawler East	House			CT 4003/767		10373
Twenty-third Street, Gawler South	Railway Structure including Station, Parcel Office, Train Shed, Goods Shed & Signal Box	Lot 6		CT 5155/143		10379

Property Address	Description and/or Extent of Listed Place	Lot No. or Part Section	Plan No.	Certificate of Title	Sec 16 Criteria	Heritage Branch ID
1/25 Twenty-third Street, Gawler South	Wheat Store			CT 5102/586		12159
2/25 Twenty-third Street, Gawler South	Wheat Store			CT 5102/587		12159
3/25 Twenty-third Street, Gawler South	Wheat Store			CT 5102/588		12159
4/25 Twenty-third Street, Gawler South	Wheat Store			CT 5102/589		12159



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--- Development Plan Boundary

**GAWLER (CT)
HERITAGE PLACES AND
CONTRIBUTORY ITEMS
INDEX
FIGURE Ga(HPCI)/1**

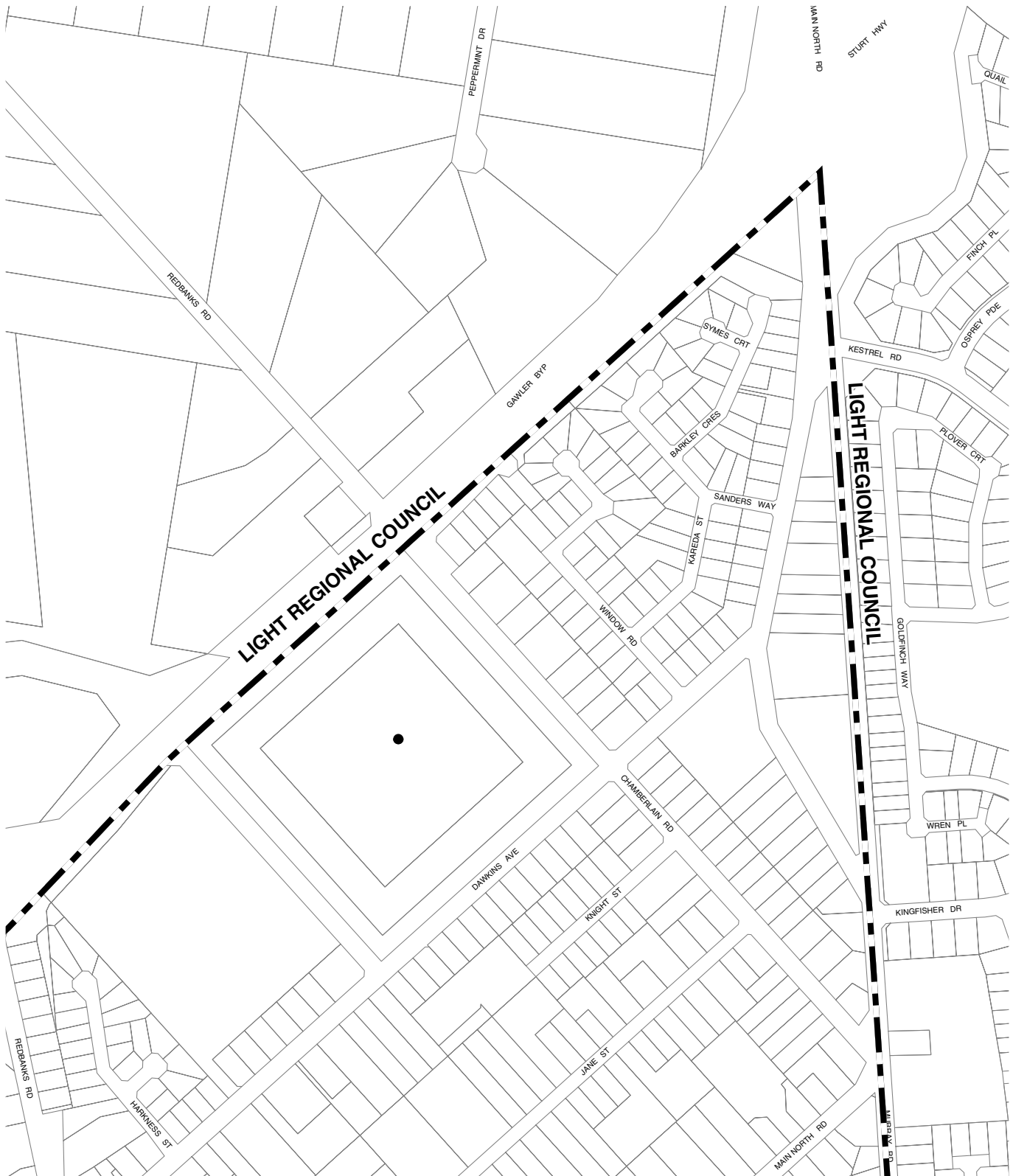


FIGURE Ga(HPCI)/3 ADJOINS



1:6,000



- State Heritage Places
- Local Heritage Places
- Contributory Items
- Development Plan Boundary

GAWLER (CT) HERITAGE PLACES AND CONTRIBUTORY ITEMS

FIGURE Ga(HPCI)/2
Consolidated - 18 July 2019

FIGURE Ga(HPCI)/2 ADJOINS



FIGURE Ga(HPCI)/6 ADJOINS

FIGURE Ga(HPCI)/5 ADJOINS



1:6,000



- State Heritage Places
- Local Heritage Places
- Contributory Items
- Development Plan Boundary

GAWLER (CT) HERITAGE PLACES AND CONTRIBUTORY ITEMS

FIGURE Ga(HPCI)/3
Consolidated - 18 July 2019



FIGURE Ga(HPCI)/5 ADJOINS

FIGURE Ga(HPCI)/9 ADJOINS



1:6,000



- State Heritage Places
- ◐ Local Heritage Places
- Contributory Items
- Development Plan Boundary

GAWLER (CT) HERITAGE PLACES AND CONTRIBUTORY ITEMS

FIGURE Ga(HPCI)/4
Consolidated - 18 July 2019

FIGURE Ga(HPCI)/3 ADJOINS

FIGURE Ga(HPCI)/6 ADJOINS

FIGURE Ga(HPCI)/11 ADJOINS

FIGURE Ga(HPCI)/4 ADJOINS



FIGURE Ga(HPCI)/10 ADJOINS

1:6,000



- State Heritage Places
- Local Heritage Places
- Contributory Items
- ⊗ Places within Church Hill State Heritage Area

- - - - Church Hill State Heritage Area Boundary
- — — — Development Plan Boundary

**GAWLER (CT)
HERITAGE PLACES AND
CONTRIBUTORY ITEMS
FIGURE Ga(HPCI)/5**
Consolidated - 18 July 2019

FIGURE Ga(HPCI)/3 ADJOINS

FIGURE Ga(HPCI)/5 ADJOINS



FIGURE Ga(HPCI)/11 ADJOINS

1:6,000



- State Heritage Places
- Local Heritage Places
- Contributory Items
- Development Plan Boundary

GAWLER (CT) HERITAGE PLACES AND CONTRIBUTORY ITEMS

FIGURE Ga(HPCI)/6
Consolidated - 18 July 2019



1:6,000



- State Heritage Places
- Local Heritage Places
- Contributory Items

--- Development Plan Boundary

GAWLER (CT) HERITAGE PLACES AND CONTRIBUTORY ITEMS

FIGURE Ga(HPCI)/7
Consolidated - 18 July 2019



1:7,000



- State Heritage Places
- ◐ Local Heritage Places
- Contributory Items

--- Development Plan Boundary

GAWLER (CT) HERITAGE PLACES AND CONTRIBUTORY ITEMS

FIGURE Ga(HPCI)/8

Consolidated - 18 July 2019

FIGURE Ga(HPCI)/4 ADJOINS



FIGURE Ga(HPCI)/10 ADJOINS



1:6,000



- State Heritage Places
- Local Heritage Places
- Contributory Items

--- Development Plan Boundary

GAWLER (CT) HERITAGE PLACES AND CONTRIBUTORY ITEMS

FIGURE Ga(HPCI)/9
Consolidated - 18 July 2019

FIGURE Ga(HPCI)/5 ADJOINS



FIGURE Ga(HPCI)/9 ADJOINS

FIGURE Ga(HPCI)/11 ADJOINS

FIGURE Ga(HPCI)/12 ADJOINS



1:6,000



- State Heritage Places
- ◐ Local Heritage Places
- Contributory Items
- Development Plan Boundary

**GAWLER (CT)
HERITAGE PLACES AND
CONTRIBUTORY ITEMS
FIGURE Ga(HPCI)/10**
Consolidated - 18 July 2019

FIGURE Ga(HPCI)/6 ADJOINS

FIGURE Ga(HPCI)/5 ADJOINS

FIGURE Ga(HPCI)/10 ADJOINS



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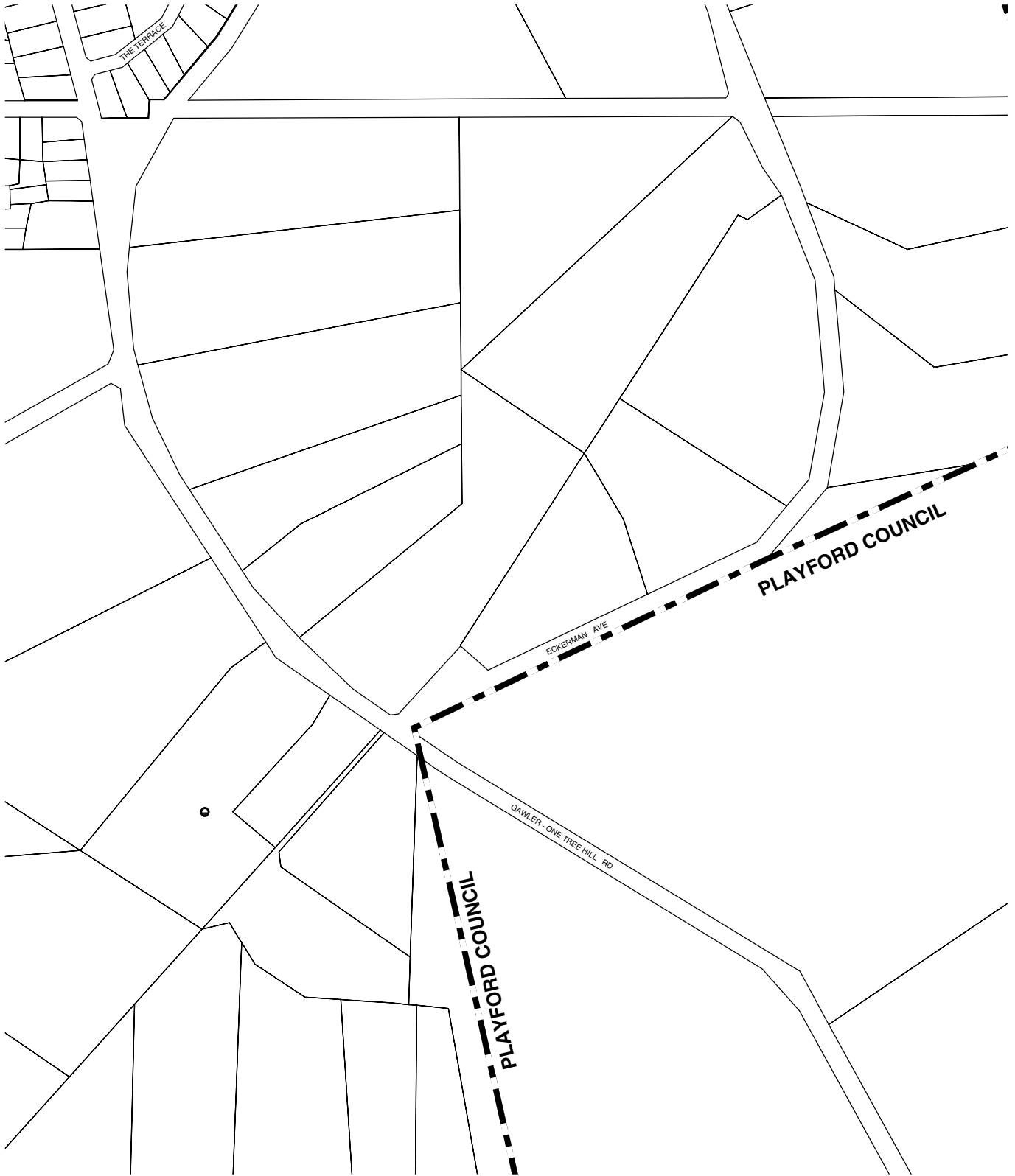


- State Heritage Places
- Local Heritage Places
- Contributory Items
- Development Plan Boundary

**GAWLER (CT)
HERITAGE PLACES AND
CONTRIBUTORY ITEMS**

FIGURE Ga(HPCI)/11
Consolidated - 18 July 2019

FIGURE Ga(HPCI)/10 ADJOINS



1:6,000



- State Heritage Places
- Local Heritage Places
- Contributory Items
- Development Plan Boundary

**GAWLER (CT)
HERITAGE PLACES AND
CONTRIBUTORY ITEMS
FIGURE Ga(HPCI)/12**
Consolidated - 18 July 2019

TABLE Ga/3

Infill Development Design Guidelines

INTRODUCTION

The guidelines are directed at utilisation of infill sites within Historic (Conservation) Policy Areas and the Historic Conservation Zone.

Such heritage areas and the zone are defined with the purpose of retaining and strengthening existing historic character. To achieve this, development must be responsive to site and locality conditions and the Desired Character. This means that off-the-shelf building products are unlikely to be suitable forms of development for infill sites.

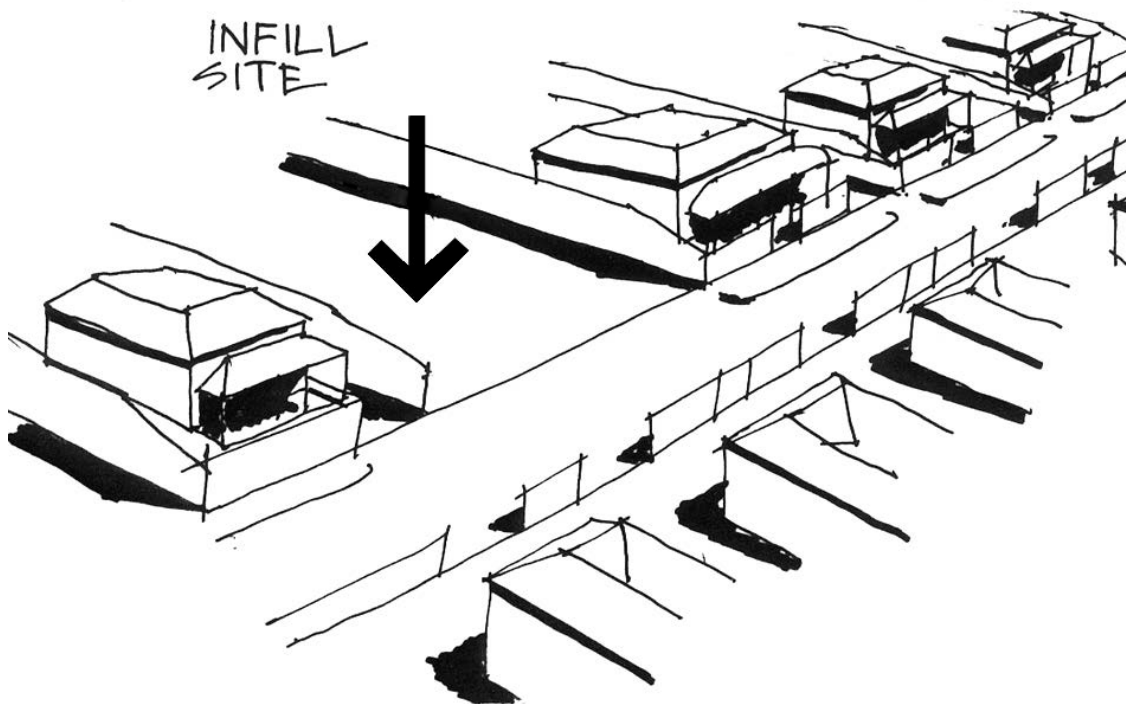
OVERVIEW

The objective is development, which is compatible with and sympathetic to the:

- desired character;
- adjacent buildings; and
- streetscapes.

This will be achieved through sympathetic contemporary design without copying historic architectural detailing or decoration, and specifically through:

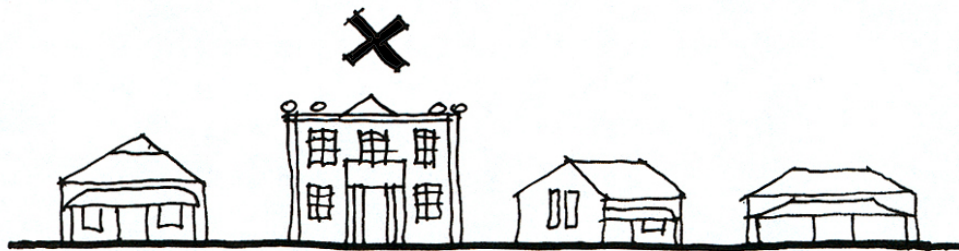
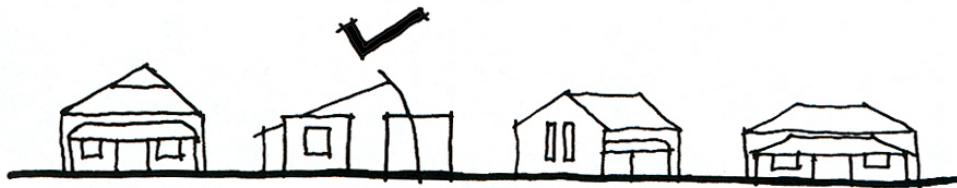
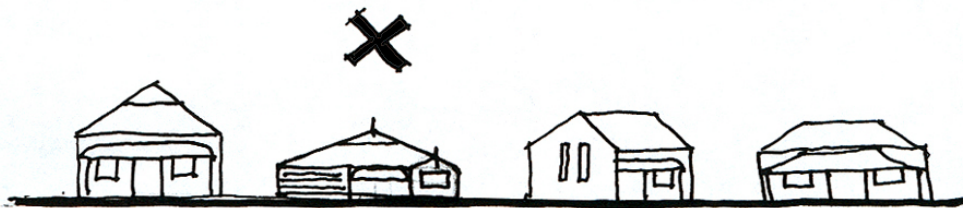
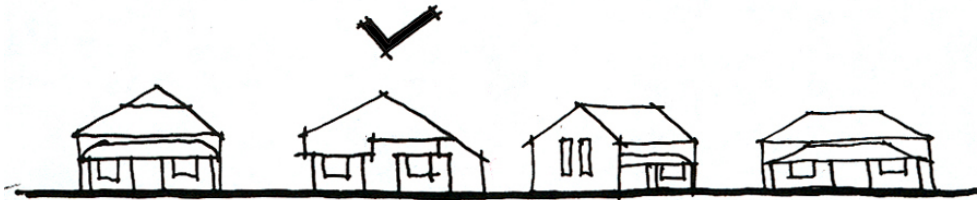
- reinforcing streetscape patterns by appropriate building location, plan and roof forms and front fence design;
- development designed to follow topography and which directly addresses the street;
- detached garages that are set behind dwellings;
- wall heights and building elements which match heritage buildings in the locality; and
- use of simple materials.



APPLICATION

Development compatible with and sympathetic to the Desired Character, adjacent buildings and streetscape.

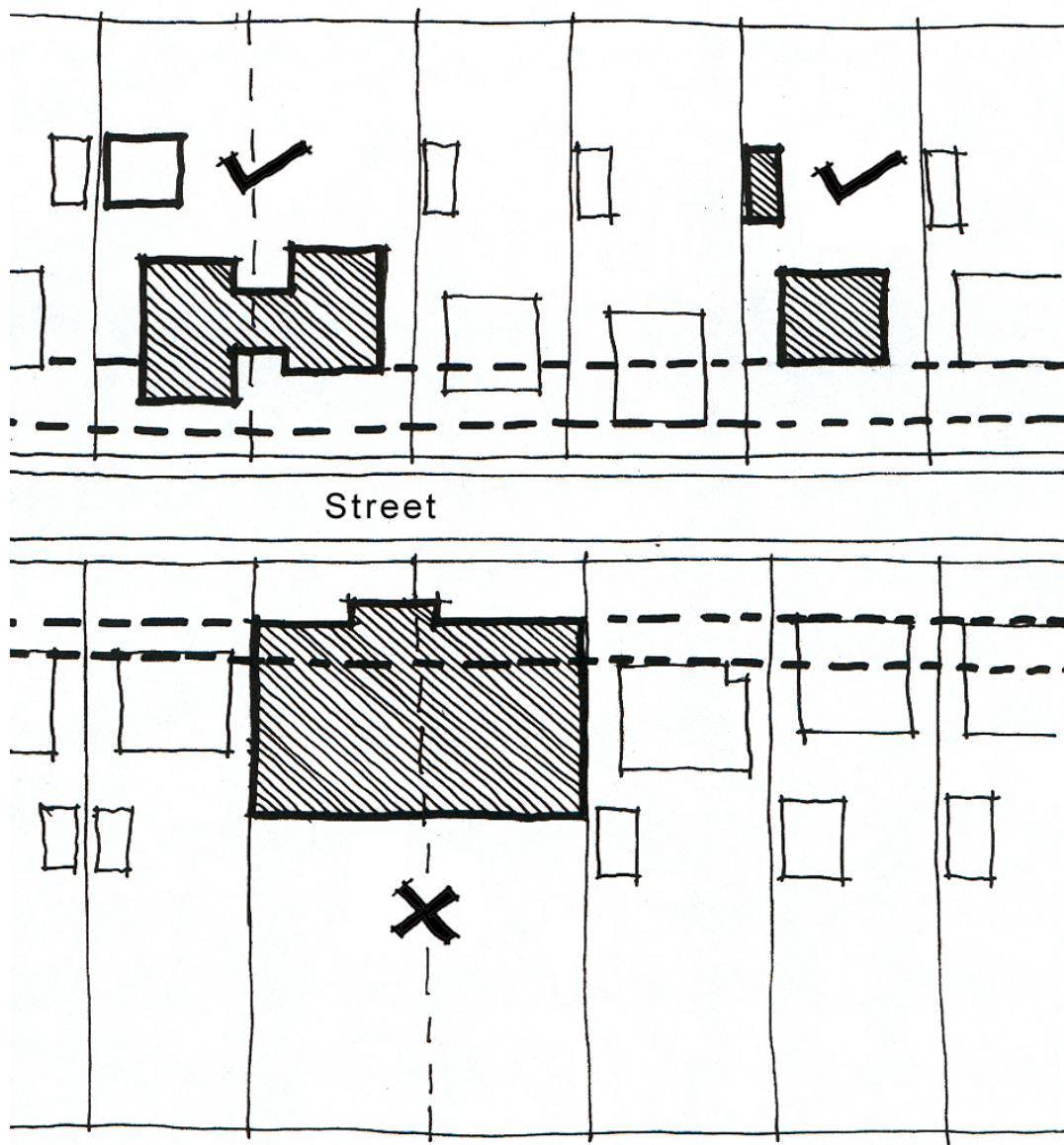
Sympathetic contemporary design is encouraged. Copying of architectural detailing should not occur.



STREETSCAPE PATTERN

Locate development to maintain the streetscape pattern by:

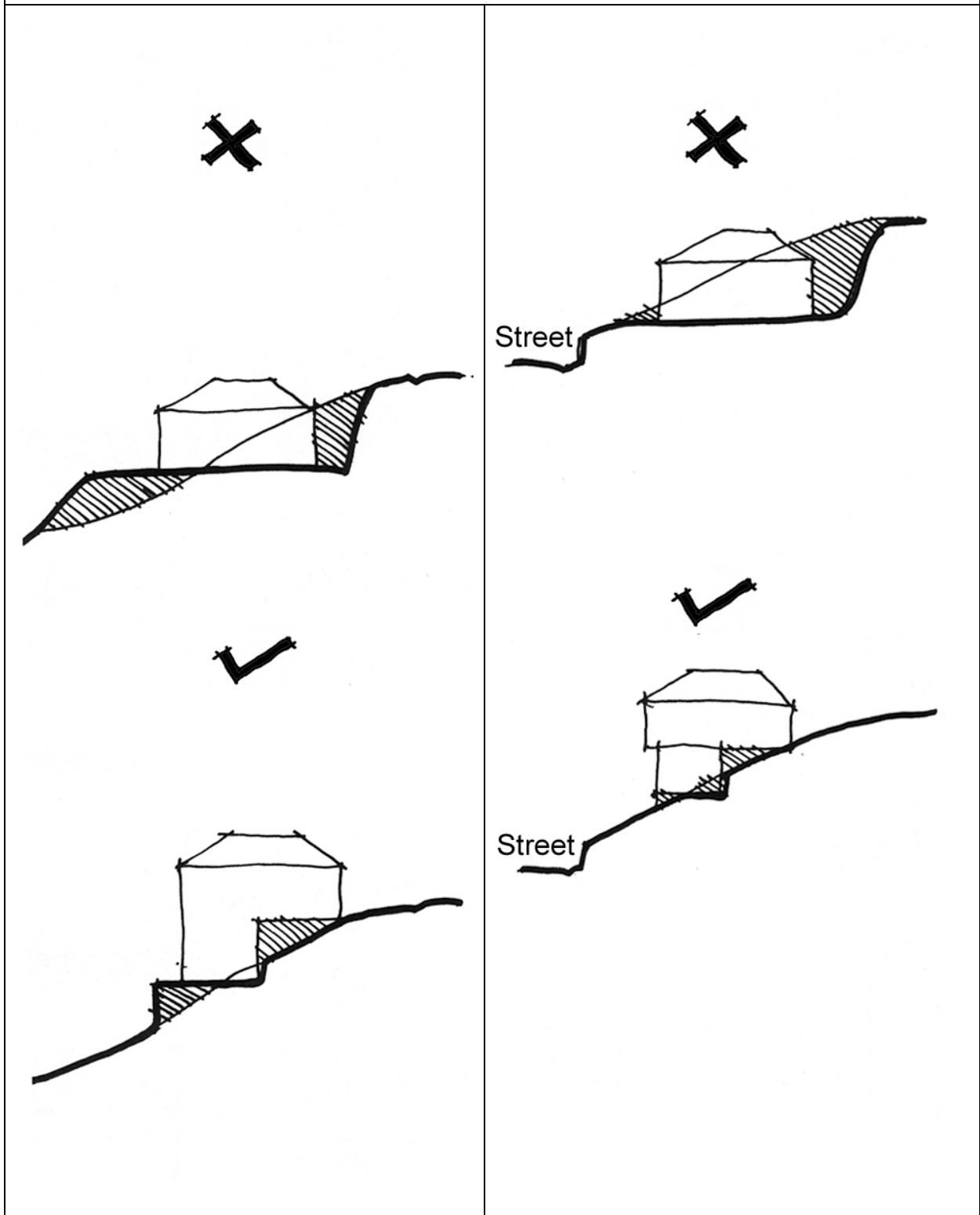
- reflecting existing set backs to front and side boundaries;
- using building modules typical of the locality;
- a direct relationship between front door and the front fence gate;
- by replicating the rhythm of fence openings found in the locality and providing a clear distinction between pedestrian and vehicular entrance-ways; and
- by setting the height of the front fence in proportion to the wall height of the infill development.



TOPOGRAPHY

Ensure infill development addresses the street by:

- principal floor levels being at or slightly above street level; and
- variation in floor levels to reduce the extent of cut and fill.

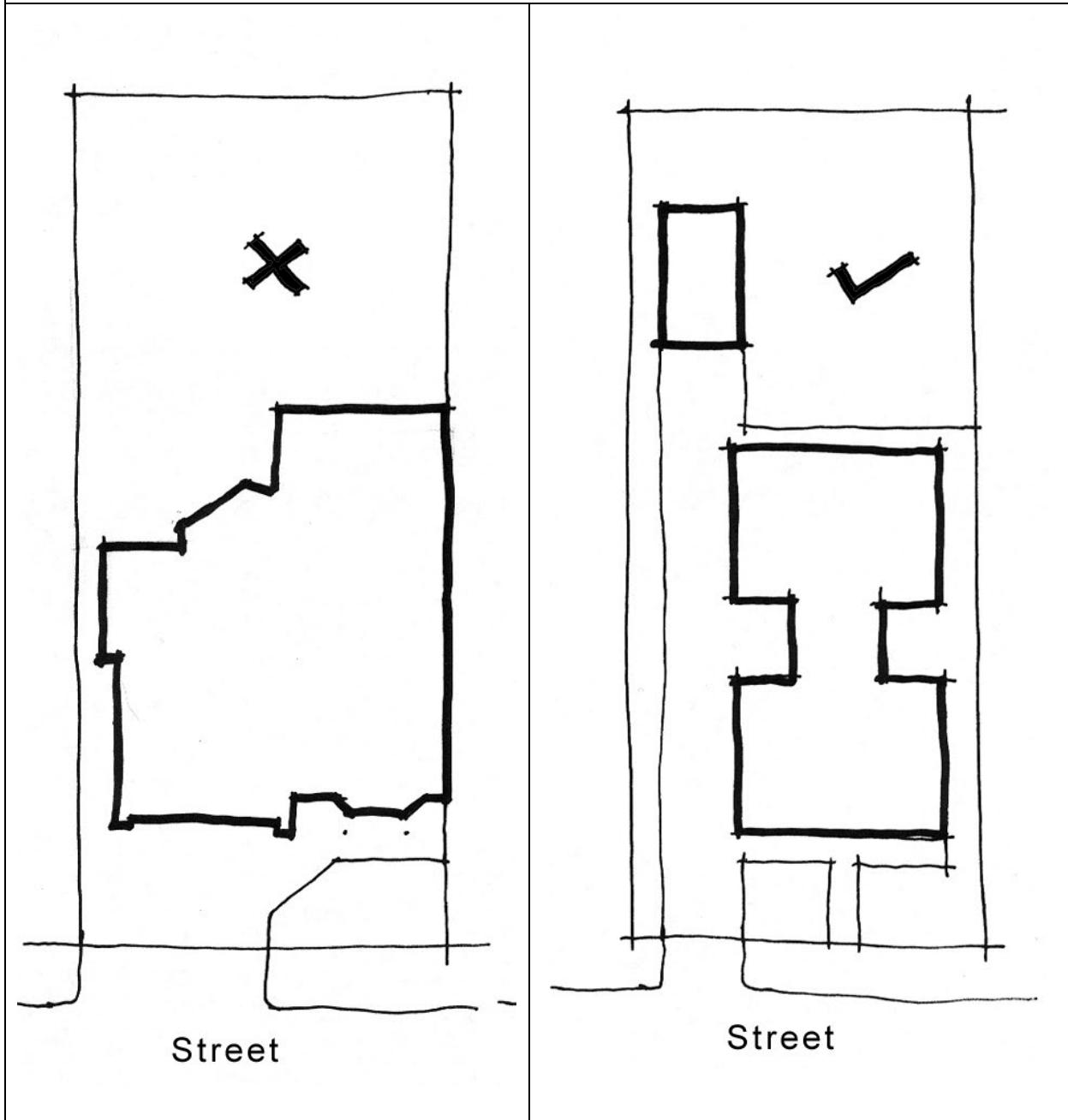


PLAN FORM

Adopt a simple plan form.

A simple plan does not only mean a small building floor area. A large floor area can be achieved by utilising a series of simple, connected plan forms.

It is critical to design the plan form as a response to site conditions and street character.



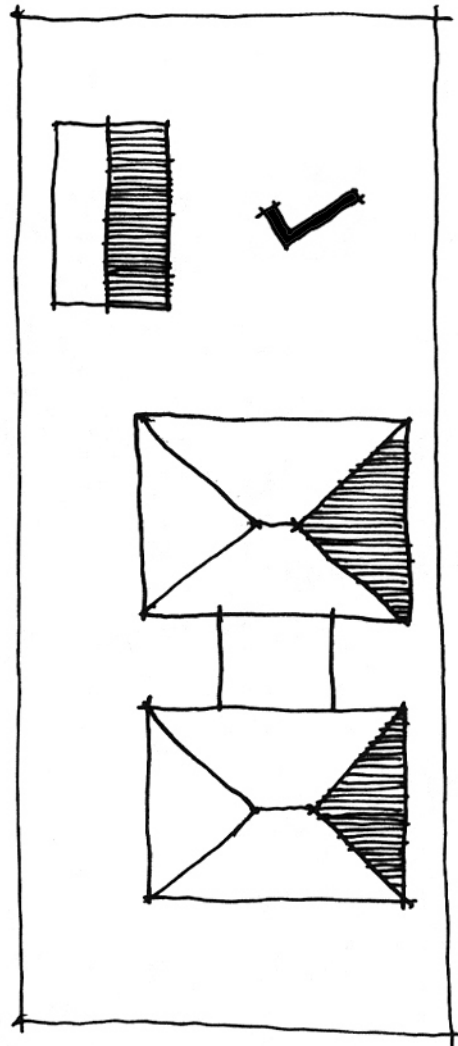
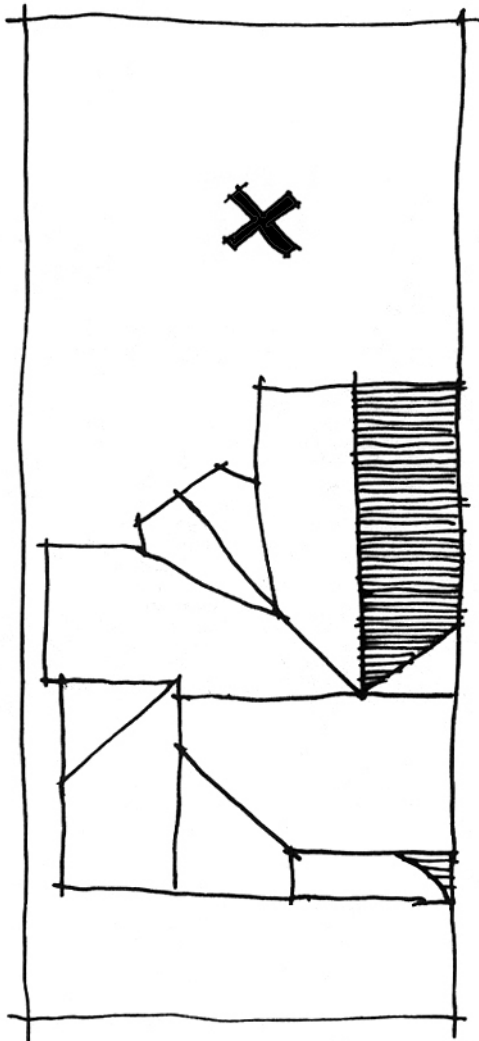
ROOFS

A simple plan form leads to a simple roof form.

Use a series of small simple connected roof forms.

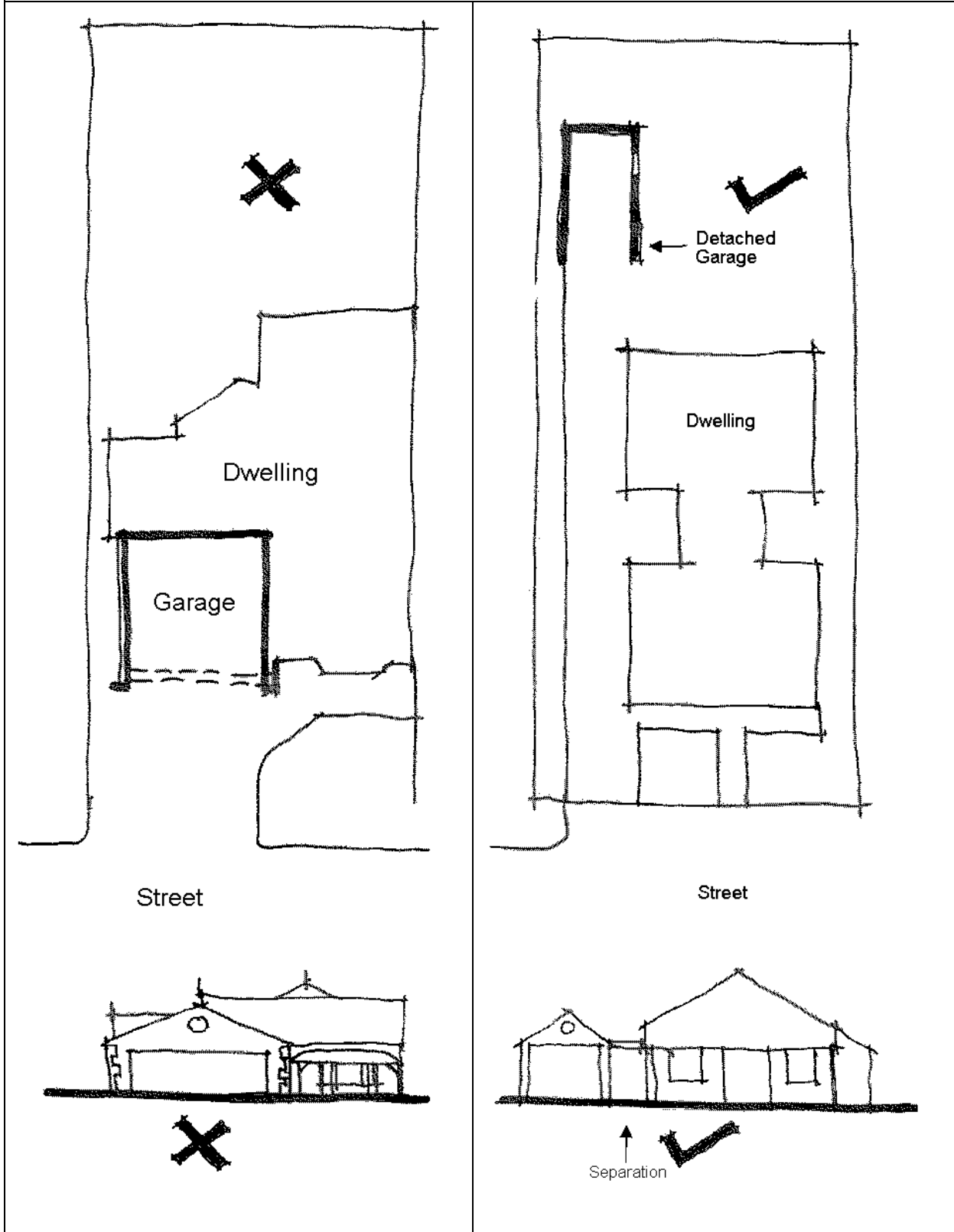
Select a roof pitch in proportion to plan form and wall height.

Gable and hipped roof forms are preferred.



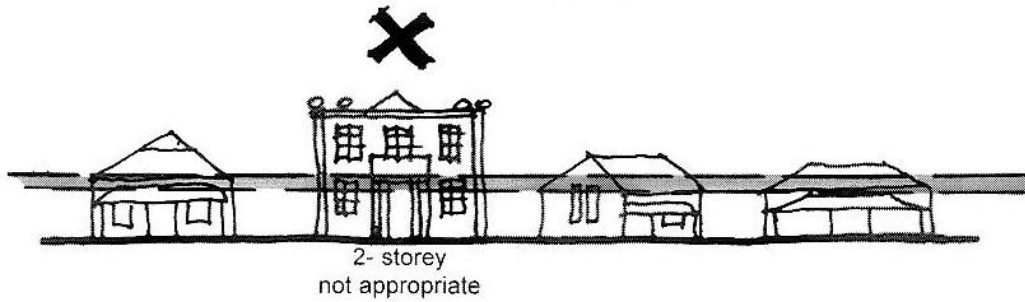
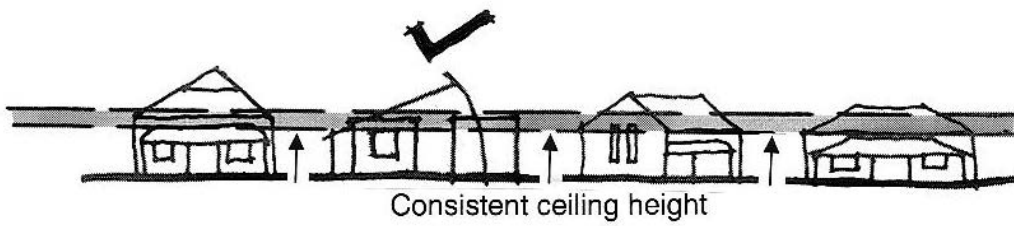
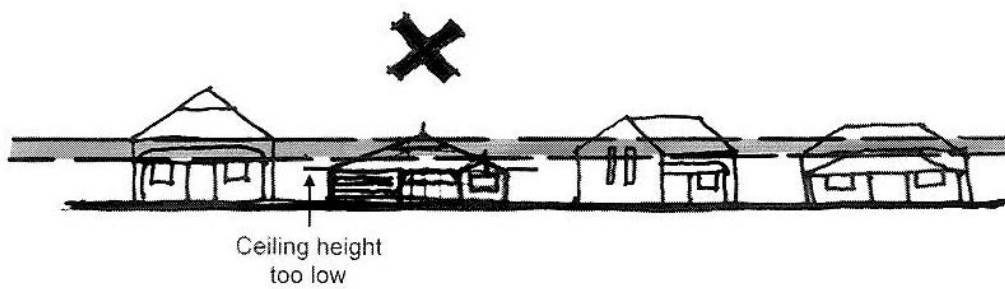
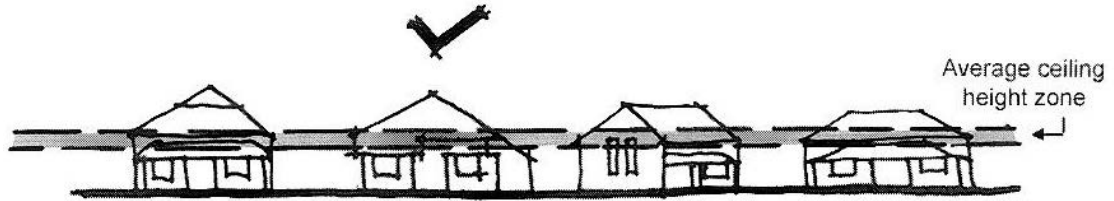
GARAGES AND CARPORTS

Garages should be detached or set back from the front of the dwelling. Garage roof forms should be visually separated from the main road.



WALL HEIGHT

Development should reflect the height of adjacent buildings. This can be achieved by matching the gutter height of Heritage Places in the locality.

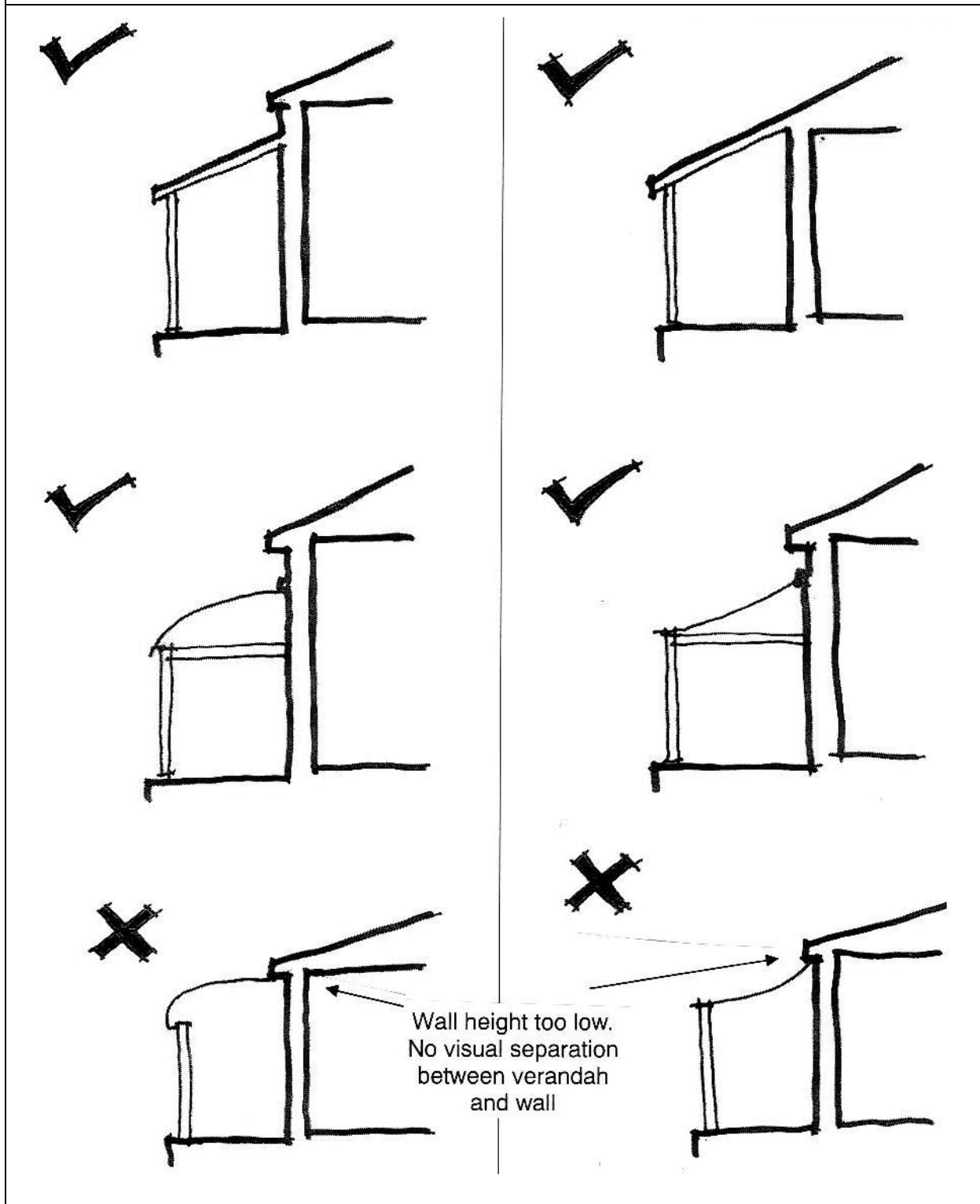


VERANDAHS AND CANOPIES

Functional verandahs, canopies and overhangs are encouraged.

Verandah extensions of the main roof are the most suitable approach for lower wall heights.

Verandah form and pitch should be designed in proportion to wall height.

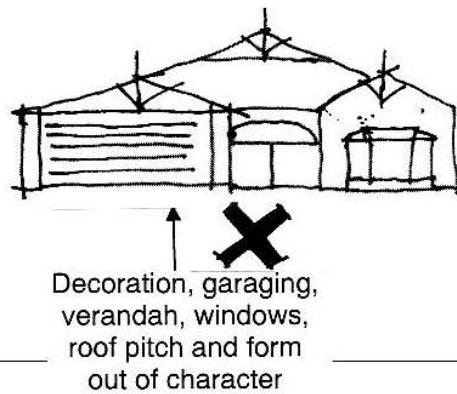
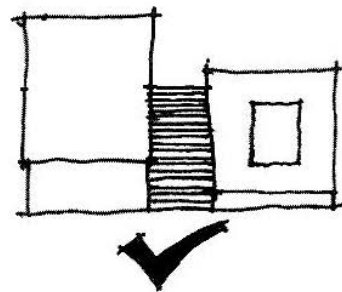
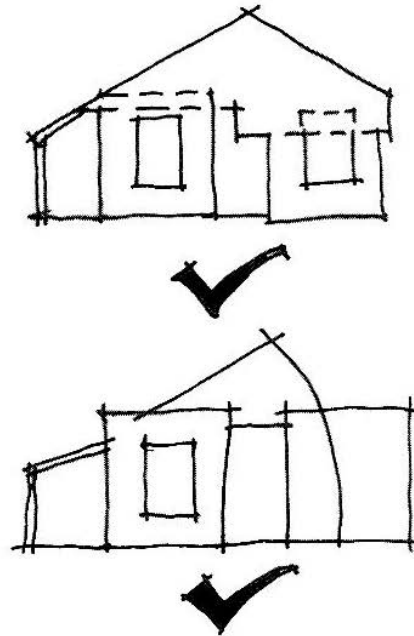
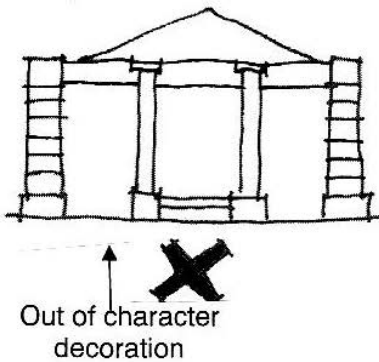
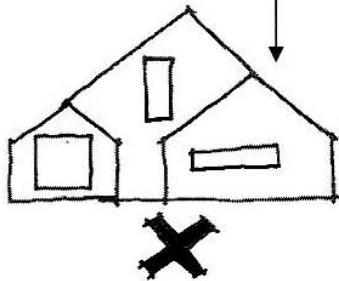


PROPORTIONS OF BUILDING AND BUILDING ELEMENTS

Use proportions of buildings and building elements, such as doors and windows, that reflect those of Heritage Places in the locality.



Windows, roof form and Building Modules out of proportion

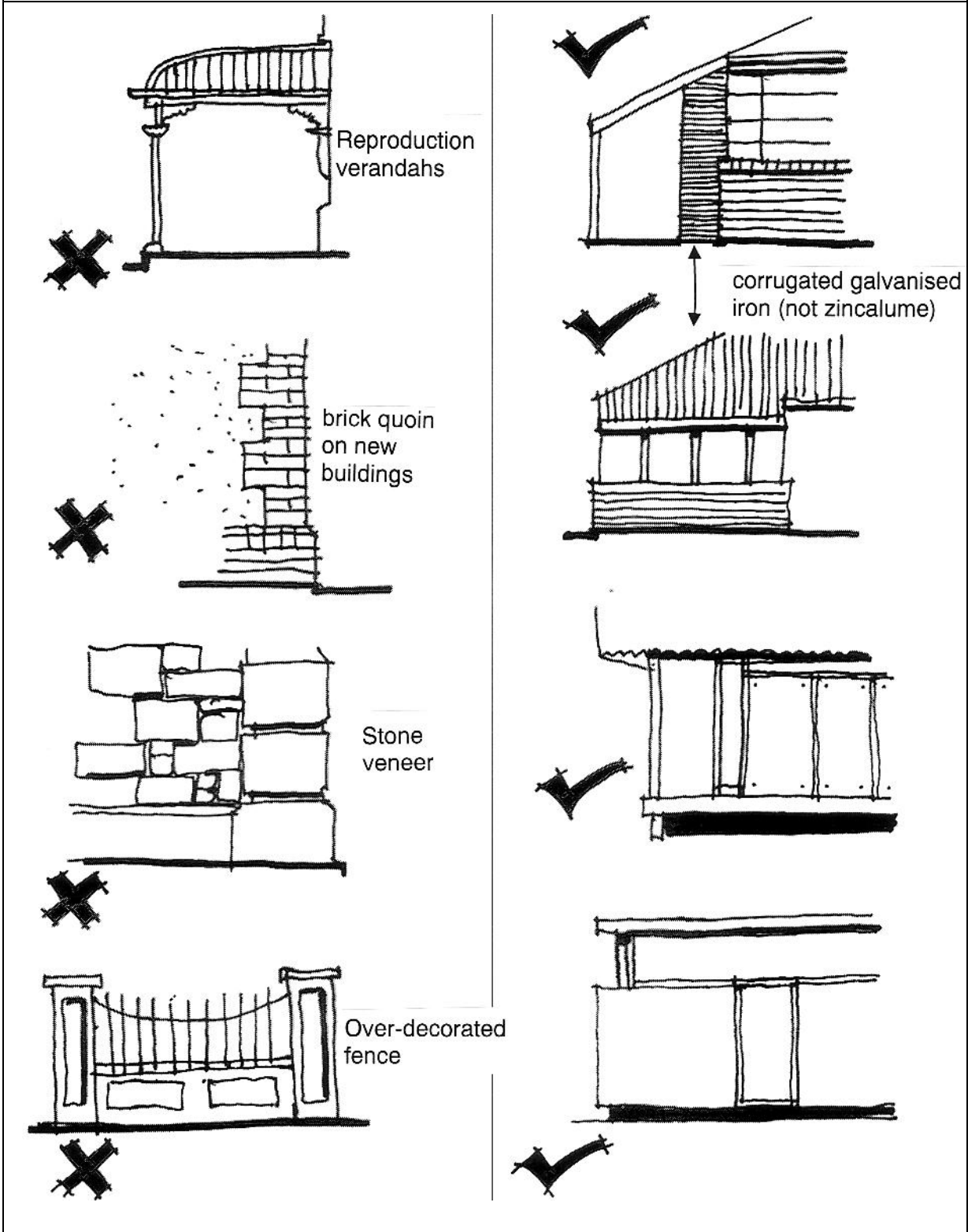


MATERIALS

Use simple materials such as render, brick, or corrugated galvanised iron (not zincalume).

Materials can be combined to introduce pattern and texture.

Stone veneer is not appropriate.



DECORATION

Traditional decorative elements such as finials, decorative timber, lacework, OG gutters, toothed quoins and bay windows should not be used.

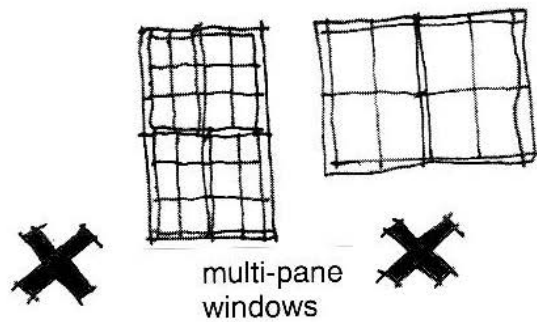
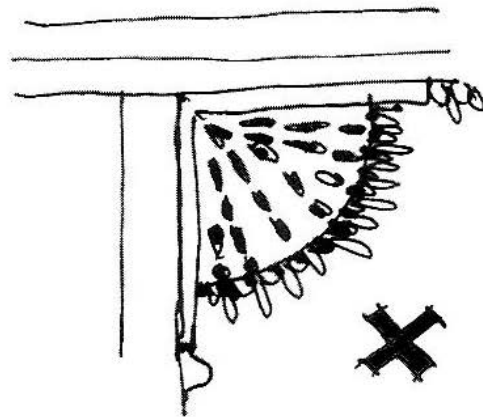
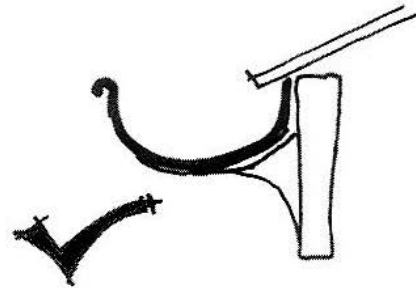
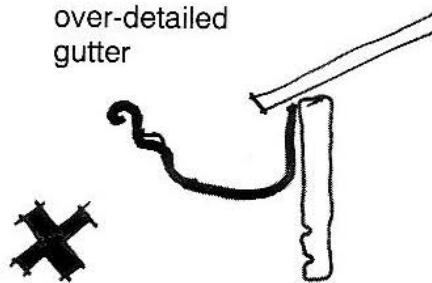
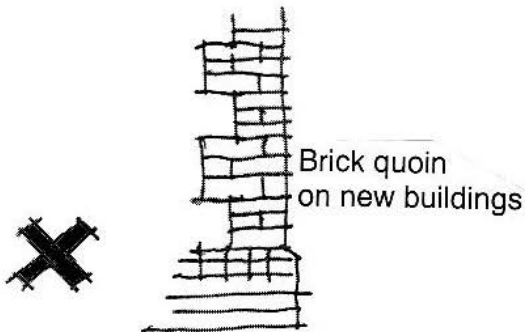
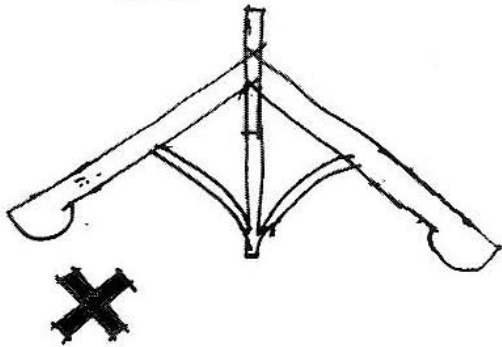
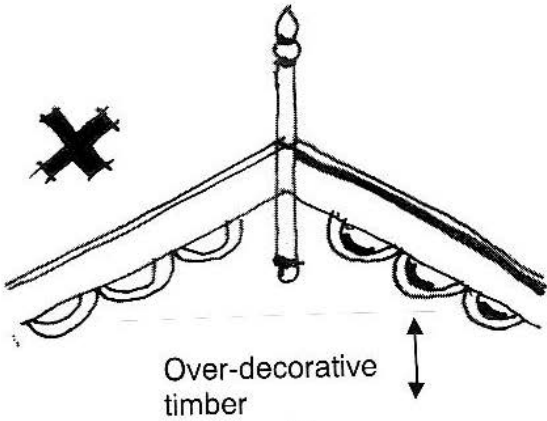
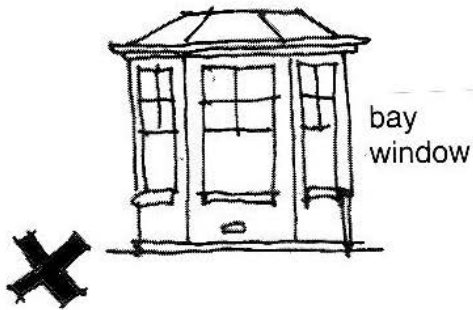
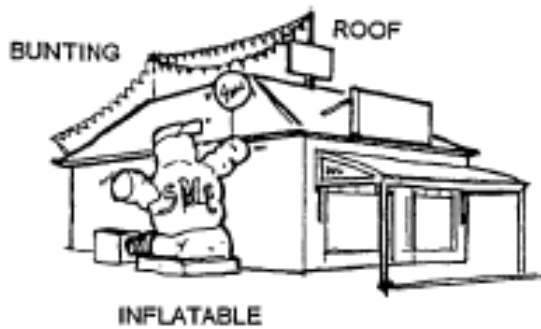
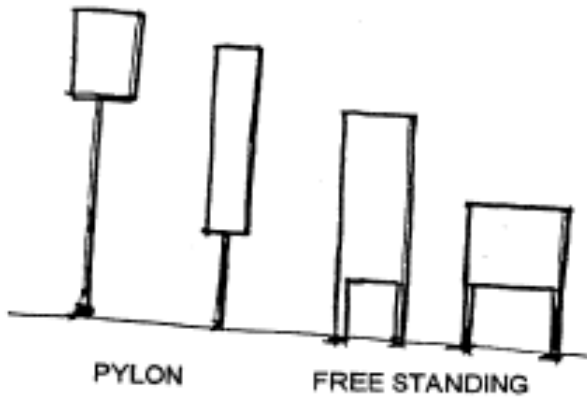
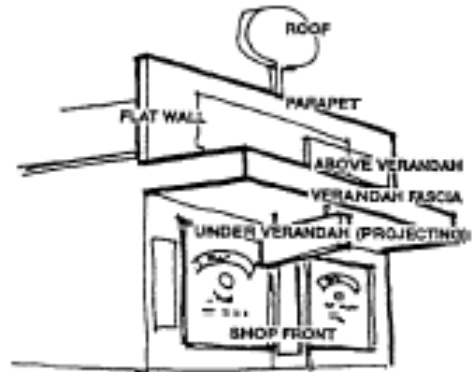
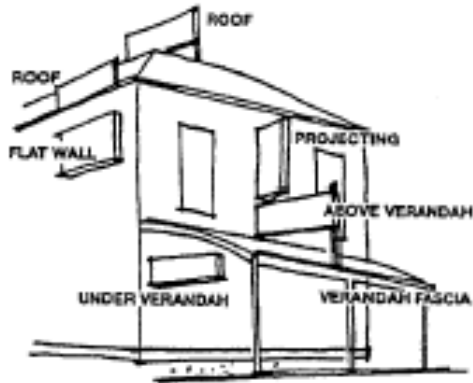


TABLE Ga/4

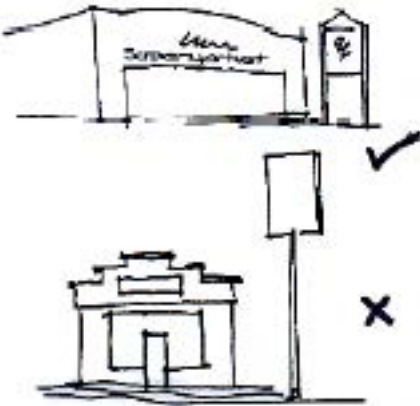
Advertising and Advertising Displays


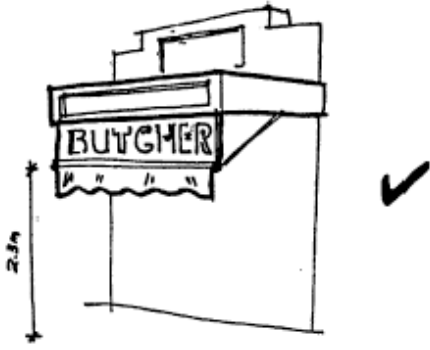
Definitions and Design Criteria

1.0 DEFINITIONS OF SIGN TYPES

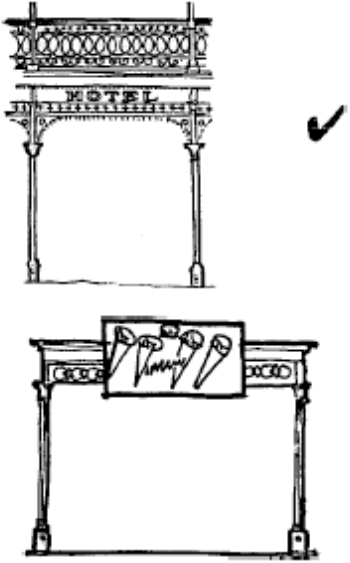
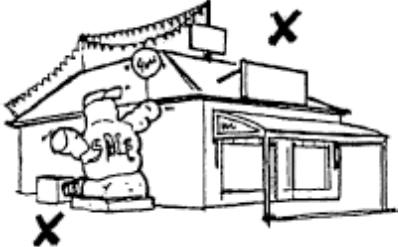
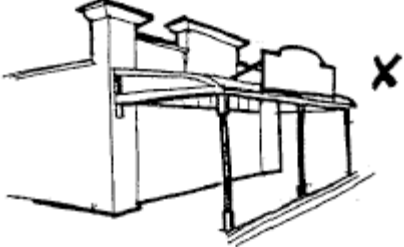



DESIGN RULES TO BE FOLLOWED:

Sign Type	Criteria	
PYLON OR FREESTANDING SIGN	<p>No higher than the buildings on the site, up to maximum height of 6.0m Maximum sign face area: 3.0m² Maximum number for per site: 1, except where otherwise specified below:</p> <p>Supermarket: (regardless of height of building) maximum height 6.0m. Maximum sign face area: 5.0m²</p> <p>Town Centre Historic Zone: Maximum sign face area 2.2m²</p> <p>Petrol Filling Station: maximum number of pylon or freestanding signs limited to 1, with a maximum height of 7.0m and a maximum sign area of 12.0m² per face. All other signs being consistent with Development Plan provisions.</p> <p>The site on the south-western corner of the Main North Road and Gordon/Tiver Road intersection: maximum number of pylon or freestanding signs limited to 4 with a maximum height of 7.0m and a maximum sign area of 12.0m² per face. All other signs being consistent with Development Plan provisions.</p>	

Sign Type	Criteria	
<p>FLAT WALL SIGN</p>	<p>Maximum number: 1 per side wall 1 per rear wall</p> <p>Placement and size of signs in scale and integrated with the architectural features and elements of the building gable wall.</p> <p>Positioned not to conceal architectural features or detailing.</p>	
<p>VERANDAH BLIND</p>	<p>Minimum clearance of any fixed blind: 2.3m</p>	

Sign Type	Criteria	
PROJECTING SIGN	<p>Maximum width: 1.2m</p> <p>Maximum height: 1.8m</p> <p>Maximum number per site: 1</p> <p>Location below Parapet</p>	
UNDER VERANDAH SIGN	<p>Maximum number: 1 per tenancy</p> <p>Minimum clearance to pavement: 2.3m</p> <p>Maximum width: 1.8m</p> <p>Maximum height: 0.3m</p>	
SHOP FRONT, PETROL FILLING STATION CONTROL BUILDING OR SHOWROOM	<p>Maximum coverage: (including windows) 30 percent</p> <p>Placement integrated with architectural features or detailing</p>	

Sign Type	Criteria	
VERANDAH FASCIA SIGN	Placement and size of signs in scale and integrated with the architectural features and elements of the building	
ROOF AND BUNTING	Not appropriate	
ABOVE VERANDAH	Not appropriate	
MOBILE SIGN	Not appropriate	

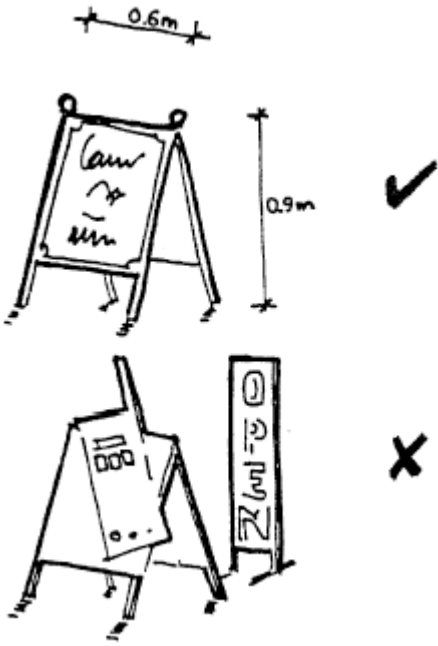
Sign Type	Criteria	
SANDWICH BOARD	<p>Maximum number: 1 per site</p> <p>Maximum height: 0.9m</p> <p>Maximum width: 0.6m</p>	 <p>The diagram illustrates two sandwich board configurations. The top configuration shows a sandwich board with a width of 0.6m and a height of 0.9m, marked with a checkmark (✓). The bottom configuration shows a sandwich board with a height exceeding 0.9m and a width exceeding 0.6m, marked with an X (✗).</p>

TABLE Ga/5

Local Heritage Places

An indicative location for each place is shown on [Figures GA\(HPCI\)/1 to 12](#)

Property Address	Description and/or extent of Listed Place	Lot No. or Part Section	Plan No.	Certificate of Title	Section 23(4) Criteria	Heritage Branch ID
35 Adelaide Road	Church of the Transfiguration The whole form and fabric of the Church building.	Lot 733-5, Sec 3 Hd Mudla Wirra			(a, c, d, f)	
35 Adelaide Road	Gawler South War Memorial The War Memorial structure and flag pole. The original cast iron fence surround has been removed.	Adjacent Lot 733-5, Sec 3 Hd Mudla Wirra			(a, c, e, f)	
3-5 Adelaide Road	former Church of Christ The historic form and fabric of the former Church of Christ including the rear Church additions but not including the new office building and structural signs.	Lot 22, Sec 3, Hd Mudla Wirra		CT 5316/555	(a, c)	
4-6 Adelaide Road	Surgery, former Mill Inn The historic form and fabric of the former Mill Inn but not including modern additions on the eastern side or concrete additions on the western road front or the altered roof line.	Lot 103, Sec 3, Hd Mudla Wirra		CT 4043/470	(a, c)	
9 Adelaide Road	Former Gawler South District Council Office The historic form and fabric of the former Gawler South Council Office but not including the southern additions, structural signs or built in parts of the north verandah.	Lot 42-3, Sec 3, Hd Mudla Wirra		CT 3226/196	(a, c, e)	
Adelaide/Bridge Street	South Para River Bridge The historic form and fabric of the bridge including cast iron pillars with lamp posts and coat of arms of Governor Gawler.	Hd Mudla Wirra			(a, c, d, f)	
Angle Vale Road	Evanston Gardens School & Schoolhouse The historic form and fabric of the former Gawler Blocks school and schoolhouse but not including verandah additions on the north and west sides of the school and brick extensions on the north and east of the schoolhouse.	Lot 29, Sec 115 Hd Munno Para		CT 3094/155	(a, c, f)	
Angle Vale Road	Evanston Gardens Memorial Hall The historic form and fabric of the Hall and rear porch.	Lot 28, Sec 115 Hd Munno Para		CT 1958/186	(a, c, f)	
79 Angle Vale Road	Former Gawler Blocks Chapel The historic form and fabric of the former Gawler Blocks Chapel but not including additions on the north side.	Lot 30, Sec 115, Hd Munno Para		CT 766/40	(a, c, f)	
2 Ayers Street	Mars Hill, Dwelling & Coachhouse The historic form and fabric of the dwelling and coach house and the stone and cast iron entrance gates.	Lot 87, Sec 4 Hd Nuriootpa		CT 3764/49	(a, b, d, e)	

Property Address	Description and/or extent of Listed Place	Lot No. or Part Section	Plan No.	Certificate of Title	Section 23(4) Criteria	Heritage Branch ID
56 Barnet Street	Racecourse building The historic form and fabric of the principally bluestone building but not including the southern brick additions.	Sec 3246 Hd Munno Para		CT 4347/471	(a, e)	
2 Blanch Street	Yenda, dwelling & Outbuilding The historic form and fabric of the dwelling and two stone outbuildings but not including the eastern concrete block additions of the dwelling or the western iron frame additions of the main outbuilding.	Lot 2, Sec 6 Hd Nuriootpa		CT5259/144	(a, d, e)	
7 Bray Street	Bluestone quarries (also 10 Mount Terrace) The exposed bluestone quarry sites.	Lots 2, 5, 3, 17, Sec 319, 3 Hd Nuriootpa		CTs 5067/220, 5135/721 & 5465/260	(a, f)	
Lot 72 Cheek Avenue North	St George's Anglican Cemetery The principle heritage interest is the historic form and fabric of older grave monuments and grave surrounds, including cast iron fences but not including more recent grave sites except those of significant local persons.	Lot 72, Sec 3077 Hd Barossa		CT 4248/313	(a, d, e)	
Sec 52 Clifford Road	Former Hillier farmhouse The historic form and fabric of the farmhouse, stone barn, stone tank and water trough and stables with original wooden posts and iron roof but not including concrete shed, steel framed shed and other modern buildings.	Lot 62, Sec 52 Hd Munno Para		CT 3152/185	(a, b, e)	
Commercial Lane	River Red Gum The whole of the tree but not including parts of the tree required to be pruned as part of appropriate tree management.	Sec 24 Hd Mudla Wirra		CT 1139/141	(a, c, e, f)	
10-16 Daly Street	Row of four cottages The historic form and fabric of the row of cottages.	Lot 30, Sec 4 Hd Nuriootpa		CT 5069/301	(a, b)	
1B Dawes Avenue, Gawler East	Dwelling The historic form and fabric of the building but not including the modern additions and alterations on the southern side	Lot 31, Sec 4	52130	CT 5675/255	(a, d, e)	
10 Dundas Street	Dwelling The historic form and fabric of the dwelling but not including roof alterations.	Lot 38, Sec 24 Hd Mudla Wirra		CT 1361/188	(a, b, e)	
12 Dundas Street	Dwelling The historic form and fabric of the building including attached stone building and wall	Lot 89, Sec 24 Hd Mudla Wirra		CT 5692/513	(a, e)	
14 Dundas Street	Dwelling, former Victoria Mill Office The historic form and fabric of the building including attached stone building and wall.	Lot 89, Sec 24 Hd Mudla Wirra		CT 2138/172	(a, e)	
4-6 Dundas Street	Attached cottages (marked 2, 2a) The historic form and fabric of the former row of four cottages but not including southern carport, alterations to former eastern door openings and northern additions.	Lot 38, Sec 24 Hd Mudla Wirra		CT 4294/300	(a, b)	
23 East Terrace	Dance Academy, former barn The historic form and fabric of the building including stone wall but not including the northern road front addition.	Lot 6, Sec 4 Hd Nuriootpa		CT 5290/488	(a, b)	

Property Address	Description and/or extent of Listed Place	Lot No. or Part Section	Plan No.	Certificate of Title	Section 23(4) Criteria	Heritage Branch ID
6-8 East Terrace	Former Hutchinson Hospital The historic form and fabric of the early Hospital buildings but not including various post World War II additions.	Lots 11, 81, 16 Sec 3500 Hd Barossa		CTs 2370/81, 4144/16	(a, c, e)	
6 Eucalypt Drive	Former Korff barn The historic form and fabric of the former barn.	Lot 36, Sec 3079 Hd Barossa		CT 3564/179	(a, b, e)	
7 Eucalypt Drive	Former Korff farmhouse & attached stable The historic form and fabric of the former farmhouse and attached stables including the cobblestone floor of the stables.	Lot 100, Sec 3079 Hd Barossa		CT 5299/586	(a, b, d, e)	
7 Eucalypt Drive	Former Korff farmhouse The historic form and fabric of the former farmhouse but not including recent western additions.	Lot 100, Sec 3079 Hd Barossa		CT 5299/586	(a, b, e)	
10 Fifteenth Street	Fitness Centre, former Duffield grain store The historic form and fabric of the whole former grain store building but not including the rendered finish on all except western walls, structural signs or recent minor entrance alterations.	Lots 93-96 Sec 2 Hd Mudla Wirra		CT 1399/54	(a, b, e, f)	
31 Finnis Street	Zion Lutheran Church & wall The historic form and fabric of the Church and stone walls to street front and street entrance.	Lot 104, Sec 24 Hd Mudla Wirra		CT 2601/99	(a, b, c, f)	
2-4 Fourteenth Street	former Gawler West Uniting Church The historic form and fabric of the Church but not including the southern concrete block additions.	Lot 2, Sec 8 Hd Mudla Wirra		CTs 1726/42, LGT/14 & 16	(a, c, f)	
Gawler River	Railway bridge, Roseworthy line The historic form and fabric of the bridge including stone piers and embankments.	Lot 8 Hd Mudla Wirra		CT 3570/61	(a, f)	
25 High Street	Craiglee, house barn & stone wall The historic form and fabric of the house, outbuildings and boundary stone wall as well as several trees forming a significant landscape feature but not including the cast iron lacework on the western balcony.	Lot 237-8, Sec 4 Hd Nuriootpa		CT 4387/863	(a, b, d, e, f)	
22 Hillier Road, Evanston	Former Osmington farmhouse The historic form and fabric of the former Osmington farmhouse both original pise and later western additions but not including the rendered cover over the pise.	Lot 2, Sec 3221 Hd Munno Para		CT 4215/835	(a, b, d, e)	
* Julian Terrace	Avenue of Moreton Bay Fig Trees The whole of the double planted avenue of Moreton bay fig trees but not including parts of the trees required to be pruned as part of appropriate tree management.	Hd Mudla Wirra		CT GM43/319	(a, c, d, e, f)	
23-25 King Street	Former Eagle Foundry & fence The historic form and fabric of the stone and galvanised iron former Eagle Foundry building (but not including the northern brick additions) and the stone and cast iron street fence on King Street.	Lot 177, Sec 24 Hd Mudla Wirra		CT 28/246	(a, b, d)	

Property Address	Description and/or extent of Listed Place	Lot No. or Part Section	Plan No.	Certificate of Title	Section 23(4) Criteria	Heritage Branch ID
27-29 King Street	Dwelling, former private hospital The historic form and fabric of the building.	Lot 176, Sec 24 Hd Mudla Wirra,		CT 2366/83	(a, c)	
7 King Street	Dwelling, former grain store The historic form and fabric of the building and stone wall with cast iron gate but not including the northern masonry wall or alterations to the north west part of the building.	Lot 6, Sec 24 Hd Mudla Wirra		CT 5114/534	(a, b, d)	
2 Lally Drive	Dwelling, former chaff mill & barn The historic form and fabric of the building including the internal chaff mill features.	Lot 53, Sec 3080 Hd Barossa		CT 5064/826	(a, b, f)	
11 Lyndoch Road	Immanuel Lutheran School The historic form and fabric of the former High School and School of Mines building and stone wall on Lyndoch Road but not including the other more modern buildings and additions.	Lot 348, Sec 4 Hd Nuriootpa		CT 4027/653	(a, c)	
4 Lyndoch Road	Masonic Lodge The historic form and fabric of the building but not including the northern concrete block addition.	Lot 249, Sec 4 Hd Nuriootpa		CT 711/83	(a, c, d, f)	
Lyndoch/Hemaford	Stone culvert under Lyndoch Road The historic form and fabric of the stone culvert but not including the more recent road construction over it.	Sec 3079 Hd Barossa			(a, d)	
37 Main North Road, Willaston	former Willaston Uniting Church The historic form and fabric of the Church.	Lot 48, Sec 1 Hd Mudla Wirra		CT LTD/92	(a, c, f)	
19-21 Main Street, Willaston	Attached dwellings The historic form and fabric of the building and stone outbuilding.	Lot 10, Sec 1 Hd Mudla Wirra		CT 1330/55	(a)	
2 Main Street, Willaston	Dwelling, former Willaston Hotel The historic form and fabric of the building.	Lot 12, Sec 1 Hd Mudla Wirra		CT 5345/496	(a, c)	
31-35 Main Street, Willaston	Willaston Hotel, former Victoria Hotel The historic form and fabric of the building and stone outbuilding but not including structural signs or the additions on the southern and northern sides or the bottle shop.	Lot 15, Sec 1 Hd Mudla Wirra		CT 5141/96	(a, c, f)	
5 Main Street, Willaston	Willaston Post Office The historic form and fabric of the building but not including structural signs or the dwelling at rear of lot.	Sec 704 Hd Mudla Wirra		CT 5300/704	(a, c, e)	
6-8 Main Street, Willaston	Costin's Shop & shed former wheelwrights The historic form and fabric of both the front and rear stone buildings including the slate verandah but not including brick façade, structural signs or verandah alterations on the front building or the various attached iron sheds or structures.	Lot 3, Sec 1 Hd Mudla Wirra		CT 4236/602	(a, c)	
8 McKinlay Avenue	Oaklands The historic form and fabric of the building but not including the modern additions and alterations on the southern side.	Lot 80, Sec 4 Plan 154781 Hd Nuriootpa		CT 5682/664	(a, d, e)	

Property Address	Description and/or extent of Listed Place	Lot No. or Part Section	Plan No.	Certificate of Title	Section 23(4) Criteria	Heritage Branch ID
2 & 10 Mount Terrace	Bluestone quarries See 7 Bray Street					
Murray Road	Clonlea The river environment and indigenous vegetation and open space areas including the reconstructed limestone shed and National Trust plaque but not including the entrance gate, toilets, or recently provided recreation structures.	Sec 7 Hd Nuriootpa		CT 2341/24	(a, c, d, e, f)	
102 Murray Street	Gawler Arms Hotel The historic form and fabric of the Hotel but not including modern entrance and alterations to the rear of the building.	Lot 47, Sec 24 Hd Mudla Wirra		CT 4099/199	(a, c)	
106-116 Murray Street	Shops, Wilcox Buildings The historic form and fabric of the buildings but not including alterations to the parapet and verandah of 114, additions in the south west of 106, modern brick additions on western side and structural signs.	Lots 84, 85 Sec 24 Hd Mudla Wirra		CTs 2314/172, 5169/593, 4202/766, 5211/186, 4120/338	(a, c, e, f)	
109 Murray Street	Prince Albert Hotel & outbuildings The historic form and fabric of the Hotel and rear stone outbuildings and wall but not including rear additions or front verandah addition of posts and chain.	Lot 197, Sec 24 Hd Nuriootpa		CT 5123/636	(a, c, f)	
118-122 Murray Street	Bunyip Press & adjacent shop The historic form and fabric of the buildings but not including the rear masonry brick and iron shed.	Lot 85, Sec 24 Hd Mudla Wirra		CT 3765/188	(a, c, e, f)	
155-157 Murray Street	Exchange Hotel The historic form and fabric of the Hotel including the slate footpath.	Lot 194, Sec 24 Hd Nuriootpa		CT 4273/1	(a, c, d, f)	
155-157 Murray Street	Exchange Hotel bottle shop, former grain store The historic form and fabric of the building but not including alterations to the parapet and front of building.	Lot 194, Sec 4 Hd Nuriootpa		CT 5350/582	(a, b)	
21-23 Murray Street	South End Hotel & Outbuildings The historic form and fabric of the Hotel and stone outbuildings but not including structural signs.	Lot 205, Sec 228 Hd Nuriootpa		CT 5265/228	(a, c, d, f)	
27 Murray Street	Shop & former Austral Theatre The historic form and fabric of the shop and stone former cinema building to rear.	Sec 24 Hd Nuriootpa		CT 4028/328	(a, c)	
39 Murray Street	Office The historic form and fabric of the building but not including northern verandah or recent eastern additions.	Lot 99, Sec 24 Hd Nuriootpa		CT 5327/249	(a, d)	
63 Murray Street	Former Oddfellows Hall The historic form and fabric of the building but not including cantilever verandah or concrete façade additions.	Lot 201, Sec 24 Hd Nuriootpa		CT 3920/148	(a, c, d, e)	
67-73a Murray Street	Former Regal Cinema and shops The historic form and fabric of the building.	Lot 200, Sec 4, 24 Hd Nuriootpa		CT 5122/724	(a, c, f)	

Property Address	Description and/or extent of Listed Place	Lot No. or Part Section	Plan No.	Certificate of Title	Section 23(4) Criteria	Heritage Branch ID
Murray/First Streets	Dead Mans Pass & Ford The river environment and indigenous vegetation and open space areas including the original ford area but not including the toilets, horse jumps or recently constructed bridge.	Hd Nuriootpa		CT GM43/319	(a, c, e, f)	
18-20 Nineteenth Street	Criterion Hotel and stone shed The historic form and fabric of the Hotel and rear stone outbuildings but not including rear additions or the bottle shop additions.	Sec 8 Hd Mudla Wirra		CT 5180/242	(a, c, f)	
Sec 24 Nixon Terrace	Exhibition Building, Parklands The historic form and fabric of the Exhibition Building but not including 20th Century additions.	Sec 24 Hd Mudla Wirra		CT GM43/319	(a, c, d, e, f)	
One Tree Hill Road	Bentley, dwelling and outbuildings The historic form and fabric of the dwelling, previous school and various historic outbuildings.	Lot 80, Sec 3217 Hd Munno Para		CT 5098/618	(a, b, e)	
Paternoster Road	Two munitions bunkers The historic form and fabric of the munitions bunkers including sliding doors but not including any modern doors.	Lot 50 Hd Mudla Wirra		CT 5359/959	(a, e)	
Paternoster Road	Three munitions bunkers, Gawler Dump The historic form and fabric of the munitions bunkers including sliding doors.	Lot 1002 Hd Mudla Wirra		CT 5274/569	(a, e)	
* Paternoster Road	Air-raid shelter The historic form and fabric of the air raid shelter including the concrete underground building and stairways and above ground iron sheds and concrete and iron hatches.	Lot 50, Sec 1 Hd Mudla Wirra		CT 5706/991	(a, c, d, e)	
Paxton Street, Willaston	Former brick kilns The historic form and fabric of the former brick kilns including chimney, arch and brick tunnels but not including Council waste depot facilities.	Lot 100, Sec 1 Hd Mudla Wirra		CT 5285/462	(a, f)	
Sec 2 Penrith Avenue	29-31 Penrith Avenue The historic form and fabric of the former Para Para lodge including the stone wall and entrance but not including iron outbuildings or the metal roof tiling or wall rendering.	Sec 2 Hd Mudla Wirra		CT 3943/82	(a, e)	
13 Railway Terrace	Gawler hybrid bottlebrush The whole of the tree but not including parts of the tree required to be pruned as part of appropriate tree management.	Lot 138, Sec 8 Hd Mudla Wirra		CT 1337/79	(a, d, e)	
8-12 Redbanks Road, Willaston	Willaston Memorial Hall The historic form and fabric of the former school and hall but not including northern yellow brick additions or new CFS buildings.	Lot 5, Sec 5 Hd Mudla Wirra		CT 526/124	(a, c)	

Property Address	Description and/or extent of Listed Place	Lot No. or Part Section	Plan No.	Certificate of Title	Section 23(4) Criteria	Heritage Branch ID
32 Seventh Street	Dwelling, former shop, stone out-buildings which relate to dairy industry The historic form and fabric of the dwelling including front stone portico and walls, cast iron verandah materials and stone outbuildings which relate to dairy industry, but not including the brick verandah infill.	Lot 40 Sec 3 Hd Mudla Wirra		CT 4389/520	(a, d, e)	
South Para River	Railway Bridge, Angaston Line (1910) The historic form and fabric of the bridge including stone piers and embankments.	Lot 3, Sec 24 Hd Mudla Wirra		CT1767/25	(a, d, f)	
10 Tod Street, Gawler	Uniting Church Hall, former Wesleyan Chapel The historic form and fabric of the former Church and western two storey extension and of the street front stone wall.	Lot 2 Sec 24 Hd Mudla Wirra			(a, b, c, d, f)	
13 Twenty-first Street	Former Bassett house The historic form and fabric of the dwelling but not including the eastern verandah addition.	Lot 67, Sec 8 Hd Mudla Wirra		CT 2347/99	(a, e)	
22-24 Twenty- second Street	Former Seventh Day Adventist Church The historic form and fabric of the building including internal timber features.	Sec 8 Hd Munno Para		CT 4383/642	(a, c)	
2 Twenty-third Street	Former Dawson grain store The historic form and fabric of the bluestone building but not including the northern and southern iron shed additions.	Lots 95, 96 Sec 8 Hd Munno Para		CT 4244/632	(a, b, e, f)	
12 Union Street	Coachhouse, stable (Formally 1 Warren Street) The historic form and fabric of the attached buildings but not including the entrance alterations.	Lot 11, Sec 4 Plan No 66001 Hd Nuriootpa		CT 5758/867	(a, d)	
2 Walker Place	Former James Martin shed The historic form and fabric of the building but not including the northern concrete parapet and entrance alterations.	Lot 1, Sec 24 Hd Mudla Wirra		CT 5300/474	(a, e)	
3 Warren Street	Attached Cottage The historic form and fabric of the attached building but not including the entrance alterations	Lot 32		CT 5725/315	(a, d)	
7 Warren Street	Dwelling, fr North Gawler Lutheran Church The historic form and fabric of the building including the porch.	Lot 255, Sec 4 Hd Nuriootpa		CT 4031/156	(a, c)	
Whitelaw Terrace	James Martin Monument The historic form and fabric of the monument. The monument has been moved from its original location.	Sec 24 Hd Mudla Wirra		CT GM 43/319	(a, e, f)	

TABLE Ga/6**Contributory Items**

An indicative location for these items is shown in [Figures GA\(HPCI\)/1 to 12](#)

Property Address	Description of Contributory Item	Certificate of Title
1 Adelaide Road	Former Dwelling	
10 Adelaide Road	Former Dwelling	
11-11a Adelaide Road	Former Dwelling	
15 Adelaide Road	Former Dwelling	
16 Adelaide Road	Former Dwelling	
18a & b Adelaide Road	Offices - former dwelling	
2 Adelaide Road	Dwelling	
2 Adelaide Road	Stone stables "Joseph's Place"	
20 Adelaide Road	Dwelling	
21 Adelaide Road	Former Dwelling	
22a & b Adelaide Road	Dwelling	
23 Adelaide Road	Former Dwelling	
24 Adelaide Road	Former Dwelling	
25 Adelaide Road	Former Dwelling	
27 Adelaide Road	Former Dwelling	
28 Adelaide Road	Former Dwelling	
30 Adelaide Road	Shop	
31, 31a 31b Adelaide Road	Shops	
32 Adelaide Road	Dwelling	
36 Adelaide Road	No 1 store	
37 Adelaide Road	Dwelling	
38a Adelaide Road	Solicitor's Office	
39 Adelaide Road	Dwelling	
41 Adelaide Road	Corner store and cottages	
43 Adelaide Road	Dwelling	
44 Adelaide Road	Former Dwelling	
47 Adelaide Road	Corner store and cottage/dwelling	
50 Adelaide Road	former Anglican Church Hall	
51 Adelaide Road	Dwelling	
56-56a Adelaide Road	Dwelling	
57 Adelaide Road	Dwelling	
58 Adelaide Road	Dwelling	
59 Adelaide Road	Dwelling	
60 Adelaide Road	Office	
62 Adelaide Road	Dwelling	

Property Address	Description of Contributory Item	Certificate of Title
63 Adelaide Road	Dwelling	
64 Adelaide Road	Dwelling	
66 Adelaide Road	Dwelling	
70 Adelaide Road	Dwelling	
72 Adelaide Road	Dwelling	
74 Adelaide Road	Dwelling	
78 Adelaide Road	Dwelling	
8 Adelaide Road	Former Dwelling	
80 Adelaide Road	Workshop	
82 Adelaide Road	Dwelling	
1 Barnet Street	Dwelling	
15 Barnet Street	Dwelling (May Bros)	
17 Barnet Street	Dwelling	
19 Barnet Street	Dwelling	
21 Barnet Street	Dwelling	
2 Bishop Street	Dwelling	
3 Bishop Street	Dwelling	
4 Bishop Street	Dwelling	
6 Bishop Street	Dwelling	
8 Bishop Street	Dwelling	
10 Blanch Street	Dwelling	
11 Blanch Street	Dwelling	
12 Blanch Street	Dwelling	
14 Blanch Street	Dwelling	
15 Blanch Street	Dwelling	
16 Blanch Street	Dwelling	
17 Blanch Street	Dwelling	
18 Blanch Street	Dwelling	
20 Blanch Street	Dwelling	
22 Blanch Street	Dwelling	
23 Blanch Street	Dwelling	
24 Blanch Street	Dwelling	
27 Blanch Street	Dwelling	
29 Blanch Street	Dwelling	
31 Blanch Street	Dwelling	
33 Blanch Street	Dwelling	
35 Blanch Street	Dwelling	
37 Blanch Street	Dwelling	
7-9 Blanch Street	Dwelling	

Property Address	Description of Contributory Item	Certificate of Title
Bridge Street North	Wooden Railway Building	
1 Bridge Street Nth	Dwelling	
3 Bridge Street Nth	Dwelling	
2 Burrows Street	Dwelling	
2 Calton Road	Dwelling	
4 Calton Road	Dwelling	
6 Calton Road	Dwelling	
10 Cameron Street	Dwelling	
12 Cameron Street	Dwelling	
14 Cameron Street	Dwelling	
16 Cameron Street	Dwelling	
2 Cameron Street	Dwelling	
4-6Cameron Street	Attached dwellings	
8 Cameron Street	Dwelling	
1 Cowan Street	Offices, former station master's house	
11 Crown Street	Dwelling	
3 Crown Street	Dwelling	
5 Crown Street	Dwelling	
7 Crown Street	Dwelling	
1 Daly Street	Dwelling	
13 Daly Street	Dwelling	
17 Daly Street	Dwelling	
18 Daly Street	Dwelling	
22 Daly Street	Dwelling	
26 Daly Street	Dwelling	
3 Daly Street	Dwelling	
38 Daly Street	Dwelling	
5 Daly Street	Dwelling	
6 Daly Street	Dwelling	
8 Daly Street	Dwelling	
9 Daly Street	Dwelling	
2 David Street	Dwelling	
1-3 Dean Street	Cottages	
5 Dean Street	Dwelling	
6 Dean Street	Railway Cottage	
7 Dean Street	Dwelling	
8 Dean Street	Railway Cottage	
Duffield Street	Entrance Stairs	
Duffield Street	Western stone wall	

Property Address	Description of Contributory Item	Certificate of Title
Duffield Street	Western stone wall	
10 Duffield Street	Coach House	
18 Duffield Street	Dwelling	
20 Duffield Street	Dwelling	
22 Duffield Street	Dwelling	
5 Duffield Street	Dwelling & stone wall along High Street	
7 Duffield Street	Dwelling	
9 Duffield Street	Dwelling	
8 Dundas Street	Dwelling	
1 East Terrace	Dwelling	
2 East Terrace	Dwelling	
3 East Terrace	Dwelling	
4 East Terrace	Dwelling	
5 East Terrace	Dwelling	
7 East Terrace	Dwelling	
1 Edith Street	Dwelling	
10 Edith Street	Dwelling	
11 Edith Street	Dwelling	
12 Edith Street	Dwelling	
14 Edith Street	Dwelling	
15 Edith Street	Dwelling	
16 Edith Street	Dwelling	
18 Edith Street	Dwelling	
3 Edith Street	Dwelling	
4 Edith Street	Dwelling (May Bros)	
6 Edith Street	Dwelling	
7 Edith Street	Dwelling	
8 Edith Street	Dwelling	
1 Eighteenth Street	Dwelling	
11 Eighteenth Street	Dwelling	
12 Eighteenth Street	Dwelling	
15 Eighteenth Street	Dwelling	
19 Eighteenth Street	Former store	
3 Eighteenth Street	Dwelling	
5 Eighteenth Street	Dwelling (corner 13th)	
7 Eighteenth Street	Dwelling	
9 Eighteenth Street	Dwelling	
10 Eighth Street	Dwelling	
12 Eighth Street	Dwelling	

Property Address	Description of Contributory Item	Certificate of Title
14 Eighth Street	Dwelling	
16 Eighth Street	Dwelling	
18 Eighth Street	Dwelling	
19-21 Eighth Street	Dwelling	
20 Eighth Street	Dwelling	
22 Eighth Street	Dwelling	
23 Eighth Street	Dwelling	
24 Eighth Street	Dwelling	
26 Eighth Street	Dwelling	
30 Eighth Street	Dwelling	
31 Eighth Street	Dwelling	
34 Eighth Street	Dwelling	
35 Eighth Street	Dwelling	
4 Eighth Street	Dwelling	
52 Eighth Street	Dwelling	
54 Eighth Street	Dwelling	
56 Eighth Street	Dwelling	
3 Eleventh Street	Dwelling	
9 Eleventh Street	Dwelling	
1 Elizabeth Street	Dwelling	
7 Elizabeth Street	Dwelling	
2 Fifteenth Street	Dwelling	
5 Fifteenth Street	Dwelling	
7 Fifteenth Street	Dwelling	
1 Fifth Street	Dwelling	
10 Fifth Street	Dwelling	
11 Fifth Street	Dwelling	
13 Fifth Street	Dwelling	
14 Fifth Street	Dwelling	
15 Fifth Street	Dwelling	
16 Fifth Street	Dwelling	
17 Fifth Street	Dwelling	
18 Fifth Street	Dwelling	
19 Fifth Street	Dwelling	
20 Fifth Street	Dwelling	
22 Fifth Street	Dwelling	
24 Fifth Street	Dwelling	
3 Fifth Street	Dwelling	
4 Fifth Street	Dwelling	

Property Address	Description of Contributory Item	Certificate of Title
5 Fifth Street	Dwelling	
6 Fifth Street	Dwelling	
7-7a Fifth Street	Dwelling	
8 Fifth Street	Dwelling	
9 Fifth Street	Dwelling	
12 Finniss Street	Dwelling	
1 First Street	Dwelling	
10 First Street	Dwelling	
12 First Street	Dwelling	
2 First Street	Dwelling	
3 First Street	Dwelling	
5 First Street	Dwelling	
6 First Street	Dwelling	
7 First Street	Dwelling	
10 Fourteenth Street	Dwelling	
11 Fourteenth Street	Dwelling	
13 Fourteenth Street	Dwelling	
14 Fourteenth Street	Dwelling	
16 Fourteenth Street	Dwelling	
17 Fourteenth Street	Dwelling	
20 Fourteenth Street	Dwelling	
21 Fourteenth Street	Dwelling	
28 Fourteenth Street	Dwelling	
3 Fourteenth Street	Dwelling	
30 Fourteenth Street	Dwelling	
32 Fourteenth Street	Dwelling	
34 Fourteenth Street	Dwelling	
36 Fourteenth Street	Dwelling	
38 Fourteenth Street	Dwelling	
40 Fourteenth Street	Dwelling	
5 Fourteenth Street	Dwelling	
7 Fourteenth Street	Dwelling	
9 Fourteenth Street	Dwelling	
1 Fourth Street	Dwelling	
10 Fourth Street	Dwelling	
11 Fourth Street	Dwelling	
12 Fourth Street	Dwelling	
13 Fourth Street	Dwelling	
14 Fourth Street	Dwelling	

Property Address	Description of Contributory Item	Certificate of Title
15 Fourth Street	Dwelling	
17 Fourth Street	Dwelling	
19 Fourth Street	Dwelling	
2 Fourth Street	Dwelling	
22 Fourth Street	Dwelling	
24 Fourth Street	Dwelling	
25 Fourth Street	Dwelling	
26 Fourth Street	Dwelling	
27 Fourth Street	Stone Shed (rear new dwelling)	
28 Fourth Street	Dwelling	
30 Fourth Street	Dwelling	
31 Fourth Street	Dwelling	
32 Fourth Street	Dwelling	
33 Fourth Street	Dwelling	
34 Fourth Street	Dwelling	
35 Fourth Street	Dwelling	
36 Fourth Street	Dwelling	
37 Fourth Street	Dwelling	
38 Fourth Street	Dwelling	
39 Fourth Street	Dwelling	
4 Fourth Street	Dwelling	
40 Fourth Street	Dwelling	
42 Fourth Street	Dwelling	
44 Fourth Street	Dwelling	
46 Fourth Street	Dwelling	
5 Fourth Street	Dwelling	
52 Fourth Street	Dwelling	
54 Fourth Street	Dwelling	
58 Fourth Street	Dwelling	
6 Fourth Street	Dwelling	
60 Fourth Street	Dwelling	
7 Fourth Street	Dwelling	
8 Fourth Street	Dwelling	
9 Fourth Street	Dwelling	
13 Gawler Terrace	Dwelling	
15 Gawler Terrace	Dwelling	
17 Gawler Terrace	Dwelling	
23 Gawler Terrace	Dwelling	
25 Gawler Terrace	Dwelling	

Property Address	Description of Contributory Item	Certificate of Title
7 Gawler Terrace	Dwelling	
9 Gawler Terrace	Dwelling	
1 Glenelg Lane	Dwelling	
15 Glenelg Lane	Building rear of new dwelling	
2 High Street	Dwelling	
4 High Street	Dwelling	
5 High Street	Boundary stone wall	
5a High Street	Boundary stone wall	
7 High Street	Dwelling and stone wall	
High Street/ 15 Duffield Street	Boundary stone wall	
High Street/ 7 Duffield Street	Entrance stairs Dwelling	
1 High Street	Dwelling	CT 5454/415
13 Howard Street	Dwelling	
4 Howard Street	House	
19 Jacob Street	Community Centre, former Dwelling	
20 Jacob Street	Dwelling	
21 Jacob Street	Dwelling	
22 Jacob Street	Dwelling	
23 Jacob Street	Dwelling	
24 Jacob Street	Dwelling	
25 Jacob Street	Dwelling	
26 Jacob Street	Dwelling	
27 Jacob Street	Dwelling	
28 Jacob Street	Dwelling	
29 Jacob Street	Dwelling	
30 Jacob Street	Dwelling	
31 Jacob Street	Dwelling	
36 Jacob Street	Dwelling and wall	
40-42 Jacob Street	Attached dwellings (Brick)	
44 Jacob Street	Dwelling	
46 Jacob Street	Dwelling	
10 Jerningham Street	Dwelling	
14 Jerningham Street	Dwelling	
6 Jerningham Street	Dwelling	
8 Jerningham Street	Dwelling	
10 King Street	Dwelling	
11 King Street	Dwelling	
12 King Street	Dwelling	

Property Address	Description of Contributory Item	Certificate of Title
13 King Street	Dwelling	
15 King Street	Dwelling	
17 King Street	Dwelling	
21 King Street	Dwelling	
24 King Street	Dwelling	
26 King Street	Dwelling	
28 King Street	Dwelling	
32 King Street	Dwelling	
34 King Street	Dwelling	
5 King Street	Offices, former station master's house	
6 King Street	Dwelling	
8 King Street	Dwelling	
9 King Street	Dwelling	
10 Lyndoch Road	Dwelling & stone wall	
11a Lyndoch Road	Dwelling	
12 Lyndoch Road	Euke, dwelling (Frederick May)	
14 Lyndoch Road	Dwelling	
15 Lyndoch Road	Dwelling	
16 Lyndoch Road	Dwelling	
17 Lyndoch Road	Dwelling	
18 Lyndoch Road	Dwelling	
2 Lyndoch Road	Drinking fountain, adjacent Tourist Centre	
20 Lyndoch Road	Dwelling	
21 Lyndoch Road	Dwelling	
22 Lyndoch Road	Dwelling	
23 Lyndoch Road	Dwelling	
24 Lyndoch Road	Dwelling	
25 Lyndoch Road	Dwelling	
31 Lyndoch Road	Dwelling	
33 Lyndoch Road	Dwelling	
35 Lyndoch Road	Dwelling	
45 Lyndoch Road	Dwelling	
47 Lyndoch Road	Dwelling	
6-8 Lyndoch Road	Dwelling & stone wall	
7 Lyndoch Road	Dwelling	
9 Lyndoch Road	Dwelling	
10 Main North Road	House	
12 Main North Road	House	
2 Main North Road	Dwelling	

Property Address	Description of Contributory Item	Certificate of Title
36 Main North Road	Dwelling	
4 Main North Road	Dwelling	
6 Main North Road	House	
6a Main North Road	House, former barn	
8 Main North Road	House	
32 Main North Road, Willaston	Dwelling	
34 Main North Road, Willaston	Dwelling	
36-38 Main North Road, Willaston	Dwelling	
37 Main North Road, Willaston	former Uniting Church Hall	
39-41 Main North Road, Willaston	Dwelling	
40 Main North Road, Willaston	Dwelling	
42 Main North Road, Willaston	Dwelling	
46 Main North Road, Willaston	Dwelling	
48 Main North Road, Willaston	Dwelling	
Main Street, Willaston	Willaston Bridge	
1 Main Street, Willaston	Butchers shop, outbuildings and stone well	
10 Main Street, Willaston	Stone shed and walls	
12-14 Main Street, Willaston	Coolup, dwelling & stone shed	
13 Main Street, Willaston	Dwelling	
20 Main Street, Willaston	Dwelling	
22 Main Street, Willaston	Dwelling	
23 Main Street, Willaston	Dwelling	
25-29 Main Street, Willaston	Shops	
5 Main Street, Willaston	Dwelling (rear of Post Office) and Hitching post outside Post Office	
6-8 Main Street, Willaston	Hitching post outside Costin's shop	
7-9 Main Street, Willaston	Dwelling, rear dwelling & stone wall	
1 Murray Street	Office, former dwelling	CT 2428/122
103-107 Murray Street	Stone wall to rear of shops	
111-115 Murray Street	Shops	
124-126 Murray Street	Stone shop to street front of arcade	
13 Murray Street	Dwelling	
134 Murray Street	Shop (Humphries)	
136-138 Murray Street	Shops	
140 Murray Street	Shops	
144 Murray Street	Shop/office (2 Jacob St – 1864)	
147-151 Murray Street	Office, Building Society/Bank	CTs 886/30 5133/671, 5133/672
153 Murray Street	Workshop	CT 5169/905
155 Murray Street	Slate footpath at front of Exchange Hotel	

Property Address	Description of Contributory Item	Certificate of Title
161-163 Murray Street	House and garden	
165 Murray Street	Dwelling	
166 Murray Street	House	
167 Murray Street	Office	
170 Murray Street	Dwelling	
171 Murray Street	Dwelling	
172 Murray Street	Dwelling	
173 Murray Street	Dwelling	
34-36 Murray Street	Shop (Hardware) original wells	
38 Murray Street	Restaurant	
41-53 Murray Street	Shops	
55a Murray Street	Gawler Fish Café	
62-64 Murray Street	Shops (former Eudunda Farmers)	CT 3737/189
65 Murray Street	Shop and flat	CT 5102/104
68-76 Murray Street	Shops	
78-88 Murray Street	Shops	
8 Murray Street	Dwelling/office	CT 4396/474
81-87 Murray Street	Shops and rear stone shed	CT 1814/115
77-79 Murray Street, Gawler	Wall adjacent Old Spot Hotel Car park	
1 Nineteenth Street	Dwelling	
10 Nineteenth Street	Dwelling	
15-17 Nineteenth Street	Dwelling	
2 Nineteenth Street	Dwelling	
4 Nineteenth Street	Dwelling	
5 Nineteenth Street	Dwelling	
7 Nineteenth Street	Dwelling	
9-11 Nineteenth Street	Dwelling	
10 Ninth Street	Dwelling	
12 Ninth Street	Dwelling	
15 Ninth Street	Dwelling	
16 Ninth Street	Dwelling	
2 Ninth Street	Dwelling	
4 Ninth Street	Dwelling	
6 Ninth Street	Dwelling	
8 Ninth Street	Dwelling	
1 Overway Bridge Rd	Railway Cottage	
3 Overway Bridge Rd	Railway Cottage	
5 Overway Bridge Rd	Railway Cottage	
3-5 Paterson Terrace	House	

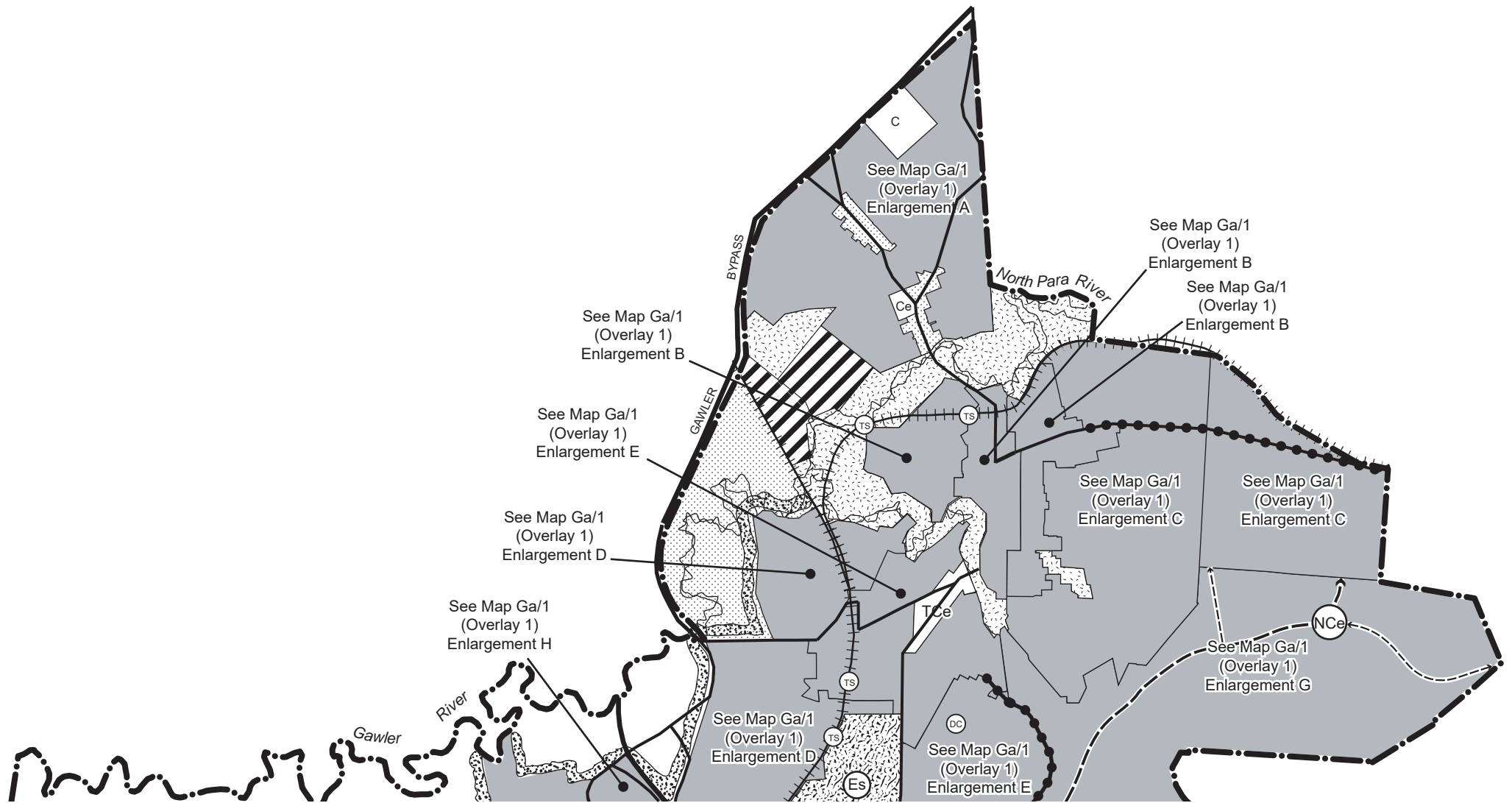
Property Address	Description of Contributory Item	Certificate of Title
49 Paxton Street	Dwelling	
52 Paxton Street	Dwelling	
4 Princess Street	Dwelling	
1 Queen Street	Cottage	
20 Queen Street	Dwelling	
1 Railway Terrace	Dwelling	
7 Railway Terrace	Dwelling	
9 Railway Terrace	Dwelling	
25 Redbanks Road	Dwelling	
27 Redbanks Road	Dwelling	
31 Redbanks Road	Dwelling	
32 Redbanks Road	Dwelling	
33 Redbanks Road	Dwelling	
37 Redbanks Road	Dwelling	
39 Redbanks Road	Dwelling	
40 Redbanks Road	Dwelling	
41 Redbanks Road	Boundary stone wall	
43 Redbanks Road	Dwelling	
44 Redbanks Road	Dwelling	
46 Redbanks Road	Dwelling	
47 Redbanks Road	Dwelling	
50 Redbanks Road	Dwelling	
51 Redbanks Road	Dwelling	
55 Redbanks Road	Dwelling	
56 Redbanks Road	Dwelling & outbuildings	
57 Redbanks Road	Dwelling	
59 Redbanks Road	Dwelling	
61 Redbanks Road	Dwelling	
Reid Street	Trees and Baker's Park	
2 Rudall Street	Dwelling	
8 Rudall Street	Dwelling	
7 Scheibener Tce	Dwelling	
1 Second Street	Dwelling	
10 Second Street	Dwelling	
12 Second Street	Dwelling	
13 Second Street	Dwelling	
14 Second Street	Dwelling	
15 Second Street	Dwelling	
16 Second Street	Dwelling	

Property Address	Description of Contributory Item	Certificate of Title
17 Second Street	Dwelling	
18 Second Street	Dwelling	
19 Second Street	Dwelling	
20 Second Street	Dwelling	
21 Second Street	Dwelling	
23 Second Street	Dwelling	
24 Second Street	Dwelling	
26 Second Street	Dwelling	
27 Second Street	Dwelling	
29 Second Street	Dwelling	
3 Second Street	Dwelling	
31 Second Street	Dwelling	
33 Second Street	Dwelling	
38 Second Street	Dwelling	
39 Second Street	Dwelling	
4 Second Street	Dwelling	
5 Second Street	Dwelling	
50 Second Street	Dwelling	
54 Second Street	Dwelling	
6 Second Street	Dwelling	
60-62 Second Street	Dwelling	
64 Second Street	Dwelling	
66 Second Street	Dwelling	
8 Second Street	Dwelling	
2 Seventeenth Street	Dwelling	
1 Seventh Street	Dwelling	
12 Seventh Street	Dwelling	
15 Seventh Street	Dwelling	
19 Seventh Street	Dwelling	
20 Seventh Street	Dwelling	
22 Seventh Street	Dwelling	
24 Seventh Street	Dwelling	
27 Seventh Street	Dwelling	
29 Seventh Street	Dwelling	
3 Seventh Street	Dwelling	
38 Seventh Street	Dwelling	
40 Seventh Street	Dwelling	
42 Seventh Street	Dwelling	
44 Seventh Street	Dwelling	

Property Address	Description of Contributory Item	Certificate of Title
46 Seventh Street	Dwelling	
48 Seventh Street	Dwelling	
50 Seventh Street	Dwelling	
52 Seventh Street	Dwelling	
54 Seventh Street	Dwelling	
58 Seventh Street	Dwelling	
2 Short Street	Dwelling	
4 Short Street	Dwelling	
1 Sixteenth Street	Dwelling	
2 Sixteenth Street	Dwelling	
3 Sixteenth Street	Dwelling	
4 Sixteenth Street	Dwelling	
3 Sixth Street	Dwelling	
5 Sixth Street	Dwelling	
7 Sixth Street	Dwelling	
Station Lane	Stone Shed	
11 Tenth Street	Dwelling	
13 Tenth Street	Dwelling	
2 Tenth Street	Dwelling	
5 Tenth Street	Dwelling	
4-6 Tenth Street	Dwelling	
7 Tenth Street	Dwelling	
8 Tenth Street	Dwelling	
9 Tenth Street	Dwelling	
10 Thirteenth Street	Dwelling	
12 Thirteenth Street	Dwelling	
14 Thirteenth Street	Dwelling	
15 Thirteenth Street	Dwelling	
16 Thirteenth Street	Dwelling	
17 Thirteenth Street	Dwelling	
18 Thirteenth Street	Dwelling	
19 Thirteenth Street	Dwelling	
2 Thirteenth Street	Dwelling	
20 Thirteenth Street	Dwelling	
21 Thirteenth Street	Dwelling	
22 Thirteenth Street	Dwelling	
4-4a Thirteenth Street	Dwellings	
5 Thirteenth Street	Dwelling	
6 Thirteenth Street	Dwelling	

Property Address	Description of Contributory Item	Certificate of Title
7 Thirteenth Street	Dwelling	
9 Thirteenth Street	Dwelling	
1 Thomas Terrace	House	
11 Thomas Terrace	Dwelling	
13 Thomas Terrace	Dwelling	
17 Thomas Terrace	Dwelling	
19 Thomas Terrace	House	
5 Thomas Terrace	Dwelling	
7 Thomas Terrace	Dwelling/converted barn	
12 Tod Street	Attached cottages	
14 Tod Street	Dwelling	
18 Tod Street	Dwelling & domestic outbuilding	
3 Turner Street	Dwelling, barn & western perimeter walls	
10 Twelfth Street	Dwelling	
11 Twelfth Street	Dwelling	
12 Twelfth Street	Dwelling	
14 Twelfth Street	Dwelling	
17-17a Twelfth Street	Dwelling	
18 Twelfth Street	Dwelling	
19-19a Twelfth Street	Dwelling	
2 Twelfth Street	Dwelling	
20 Twelfth Street	Dwelling	
21 Twelfth Street	Dwelling	
22 Twelfth Street	Dwelling	
24 Twelfth Street	Dwelling	
25 Twelfth Street	Dwelling	
26 Twelfth Street	Dwelling & Stone Shed	
28 Twelfth Street	Dwelling	
3 Twelfth Street	Dwelling	
32 Twelfth Street	Dwelling	
4 Twelfth Street	Dwelling	
5 Twelfth Street	Dwelling	
6 Twelfth Street	Dwelling	
7 Twelfth Street	Dwelling	
8 Twelfth Street	Dwelling	
9 Twelfth Street	Dwelling	
1 Twentieth Street	Dwelling	
11 Twentieth Street	Dwelling	
14 Twentieth Street	Dwelling	

Property Address	Description of Contributory Item	Certificate of Title
16 Twentieth Street	Dwelling	
20 Twentieth Street	Dwelling (corner 23rd)	
6 Twentieth Street	Dwelling	
7 Twentieth Street	Dwelling	
10 Twenty-first Street	Dwelling	
15-15a Twenty-first Street	Dwelling, former Engine & Driver	
17 Twenty-first Street	Dwelling	
2 Twenty-first Street	Dwelling	
3 Twenty-first Street	Dwelling	
4 Twenty-first Street	Dwelling	
5 Twenty-first Street	Dwelling	
6 Twenty-first Street	Dwelling	
7 Twenty-first Street	Dwelling	
8 Twenty-first Street	Dwelling	
9 Twenty-first Street	Dwelling	
10 Twenty-second St	Dwelling	
12 Twenty-second St	Dwelling	
18 Twenty-second St	Dwelling	
1 Twenty-third Street	Dwelling	
Twenty-third Street, Gawler South	Train no 245 and tramlines	
4 Union Street	Dwelling	
8 Union Street	former Dwelling	
14 Union Street	Dwelling	CT 5758/867
1 Victoria Terrace	Dwelling, Park View	
7 Victoria Terrace	House	
3 Warren Street	Dwelling	
5 Warren Street	Dwelling	
Willaston Bridge		



Map Ga/1 (Overlay 1) Part B Adjoins

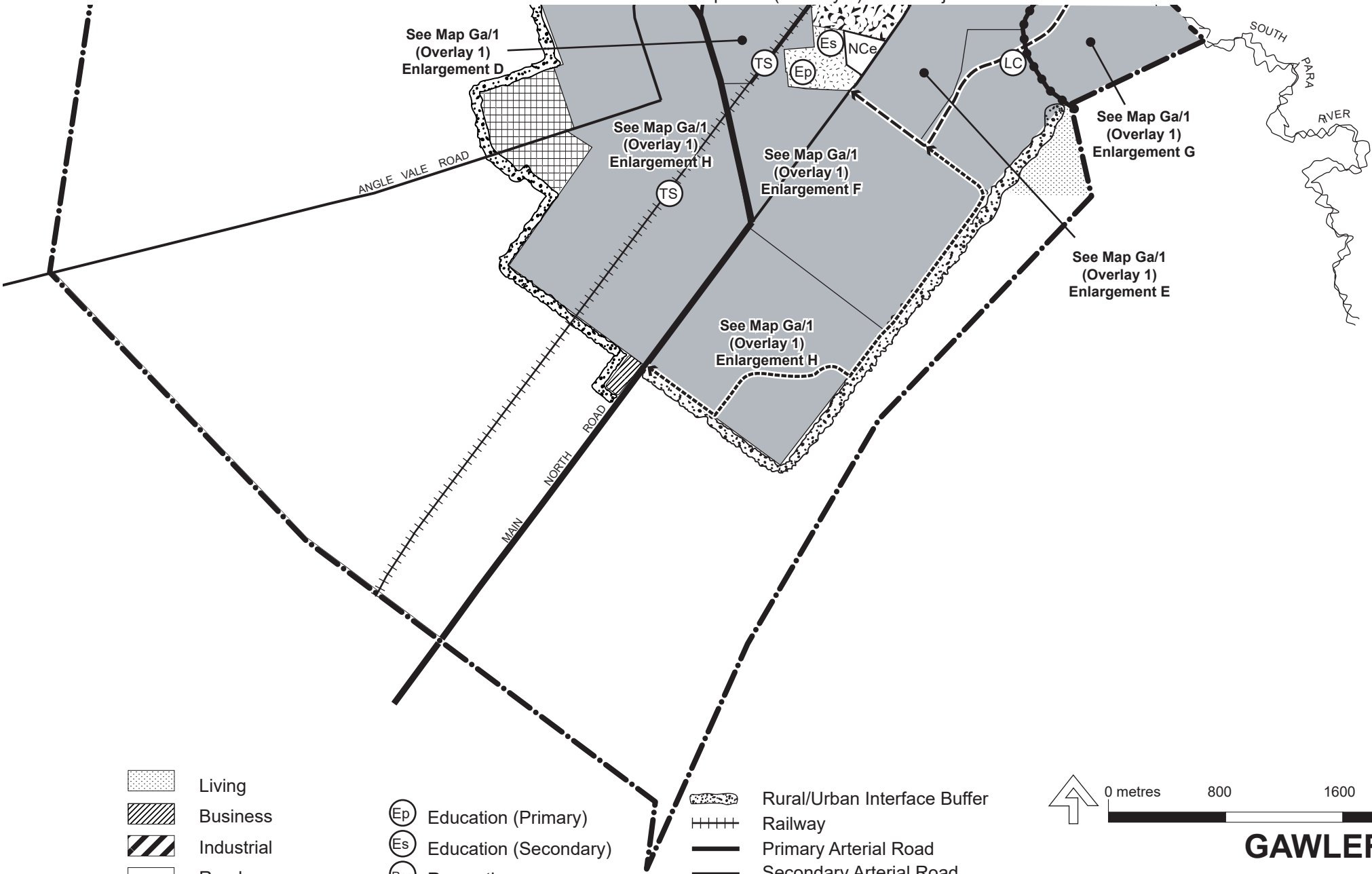
- | | | | | | |
|--|--------------|--|-----------------------|--|------------------------------|
| | Living | | Town Centre | | Rural/Urban Interface Buffer |
| | Industrial | | Education (Secondary) | | Railway |
| | Recreation | | Train Station | | Primary Arterial Road |
| | Rural | | Centre | | Secondary Arterial Road |
| | Special Uses | | Neighbourhood Centre | | Scenic Road |
| | Open Space | | Cemetery | | Future Collector Road |
| | Future Urban | | District Commercial | | Gawler East Link Road |
| | | | | | Road Link |
| | | | | | Development Plan Boundary |



GAWLER (CT) STRUCTURE PLAN MAP Ga/1 (Overlay 1) Part A









Consolidated - 18 July 2019

Map Ga/1 (Overlay 1) Part A Adjoins



-  Living
-  Business
-  Industrial
-  Rural
-  Special Uses
-  Open Space
-  Future Urban

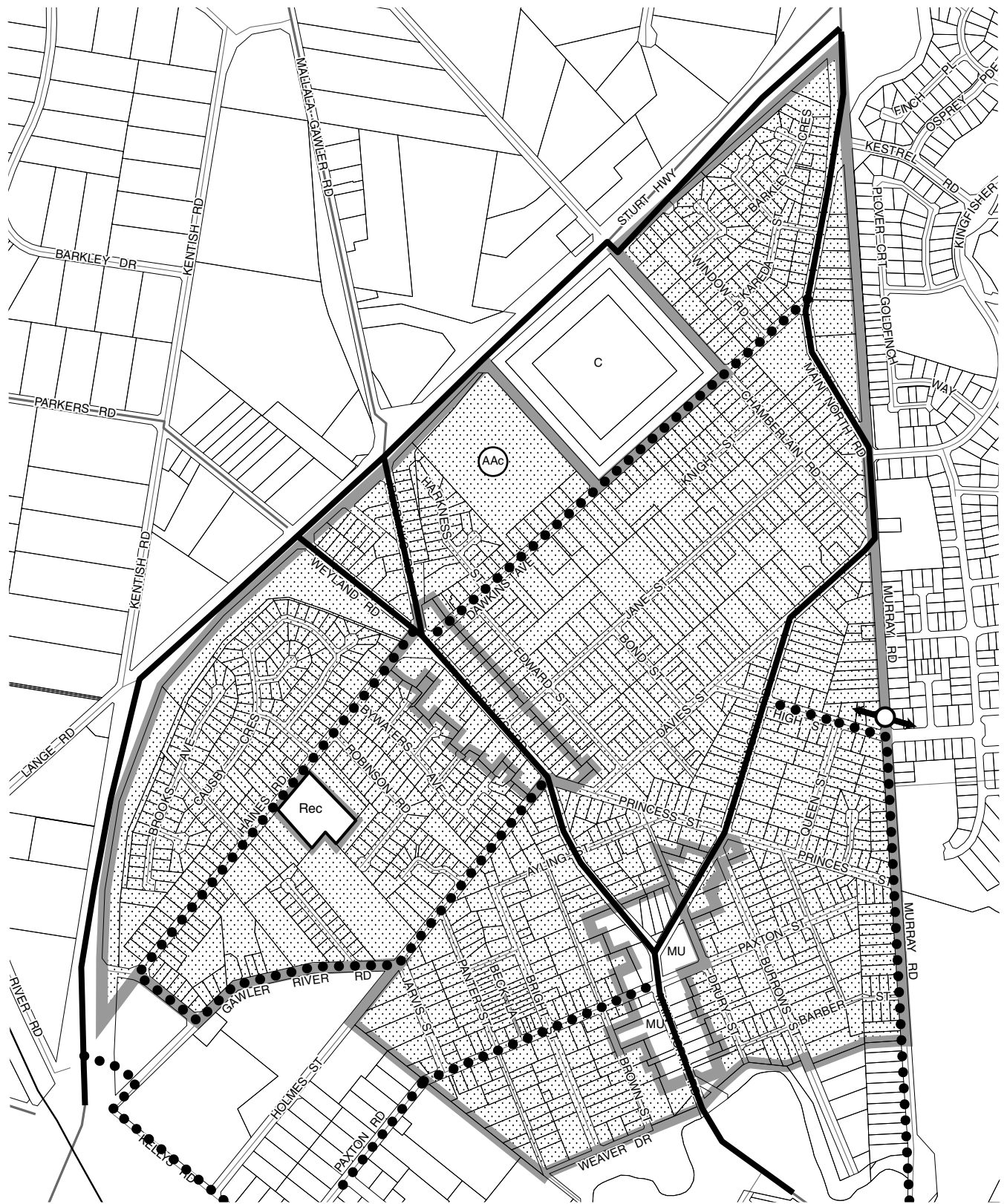
-  Education (Primary)
-  Education (Secondary)
-  Recreation
-  Train Station
-  Neighbourhood Centre
-  Local Centre










-  Rural/Urban Interface Buffer
-  Railway
-  Primary Arterial Road
-  Secondary Arterial Road
-  Scenic Road
-  Gawler East Link Road
-  Gawler East Link Road Extension
-  Development Plan Boundary

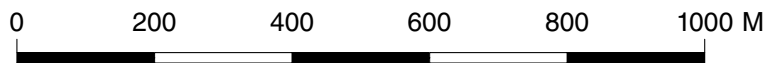


**GAWLER (CT)
STRUCTURE PLAN
MAP Ga/1 (Overlay 1)
Part B**

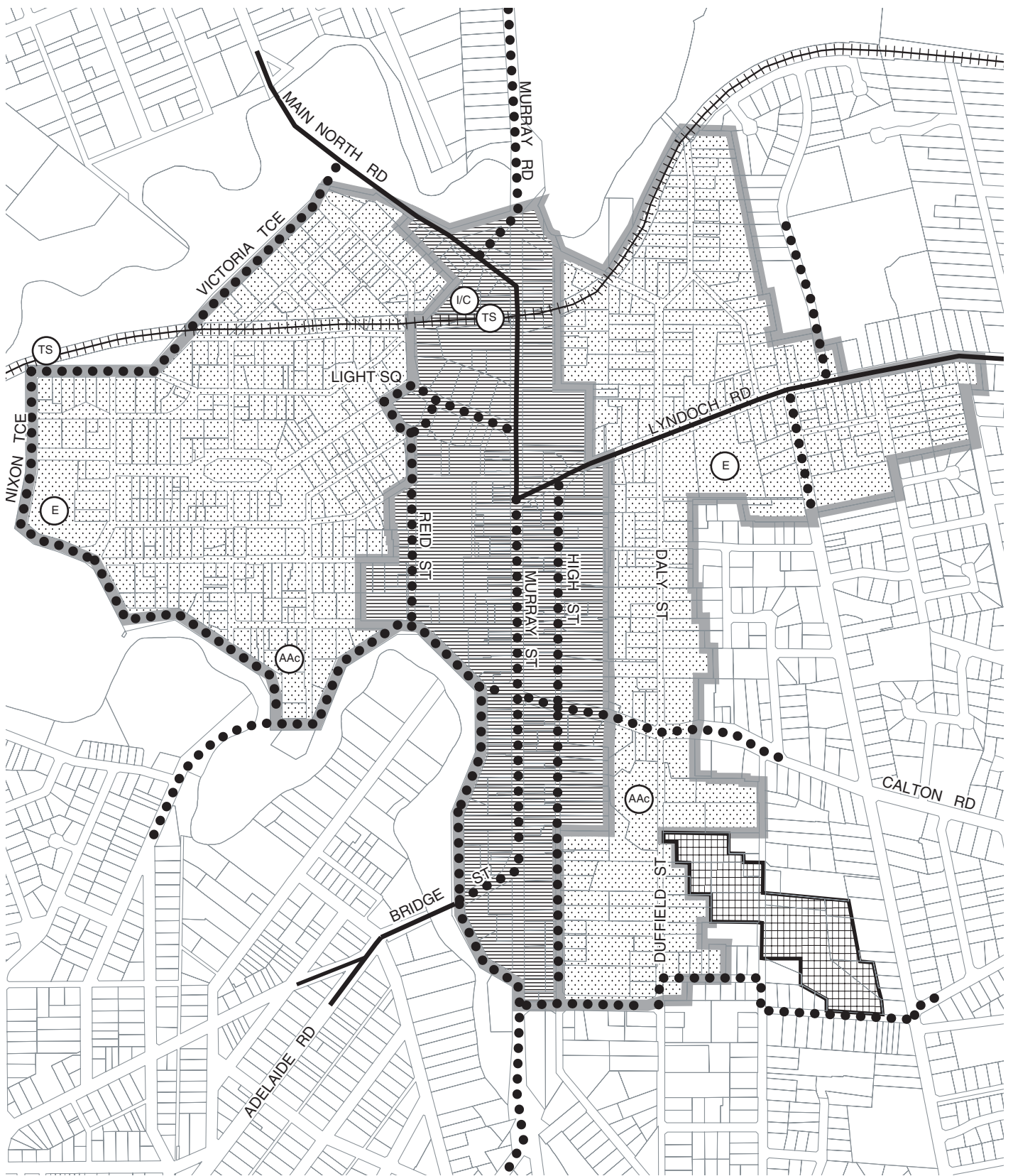
Consolidated - 18 July 2019














-  Living
-  Major Traffic Flows
-  Major Interconnection Hewett and Willaston
-  Primary Arterial Road
-  Policy Area Boundary
-  Aged Accommodation
-  Cemetery
-  Mixed Use
-  Recreation



GAWLER (CT)
WILLASTON
STRUCTURE PLAN
MAP Ga/1 (Overlay 1)
Enlargement A

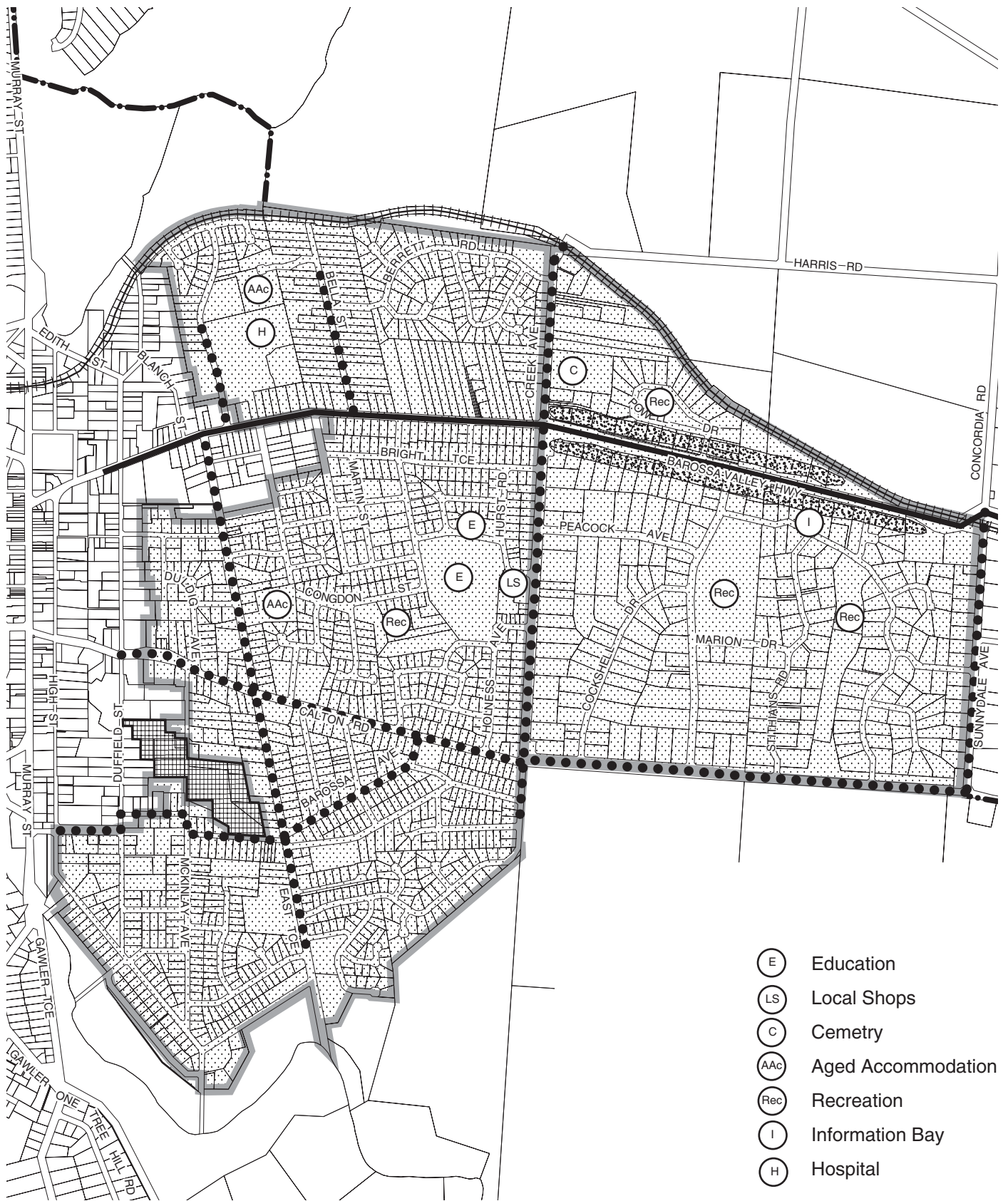


-  Living
-  Town Centre
-  Green Gully
-  Major Traffic Flows
-  Primary Arterial Road
-  Policy Area Boundary
-  Railway

-  Education
-  Aged Accommodation
-  Train Station
-  Interchange

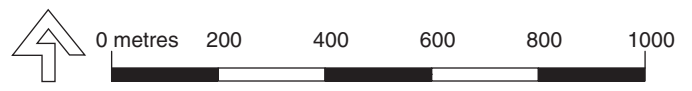


GAWLER (CT)
CENTRAL GAWLER
STRUCTURE PLAN
MAP Ga/1 (Overlay 1)
Enlargement B

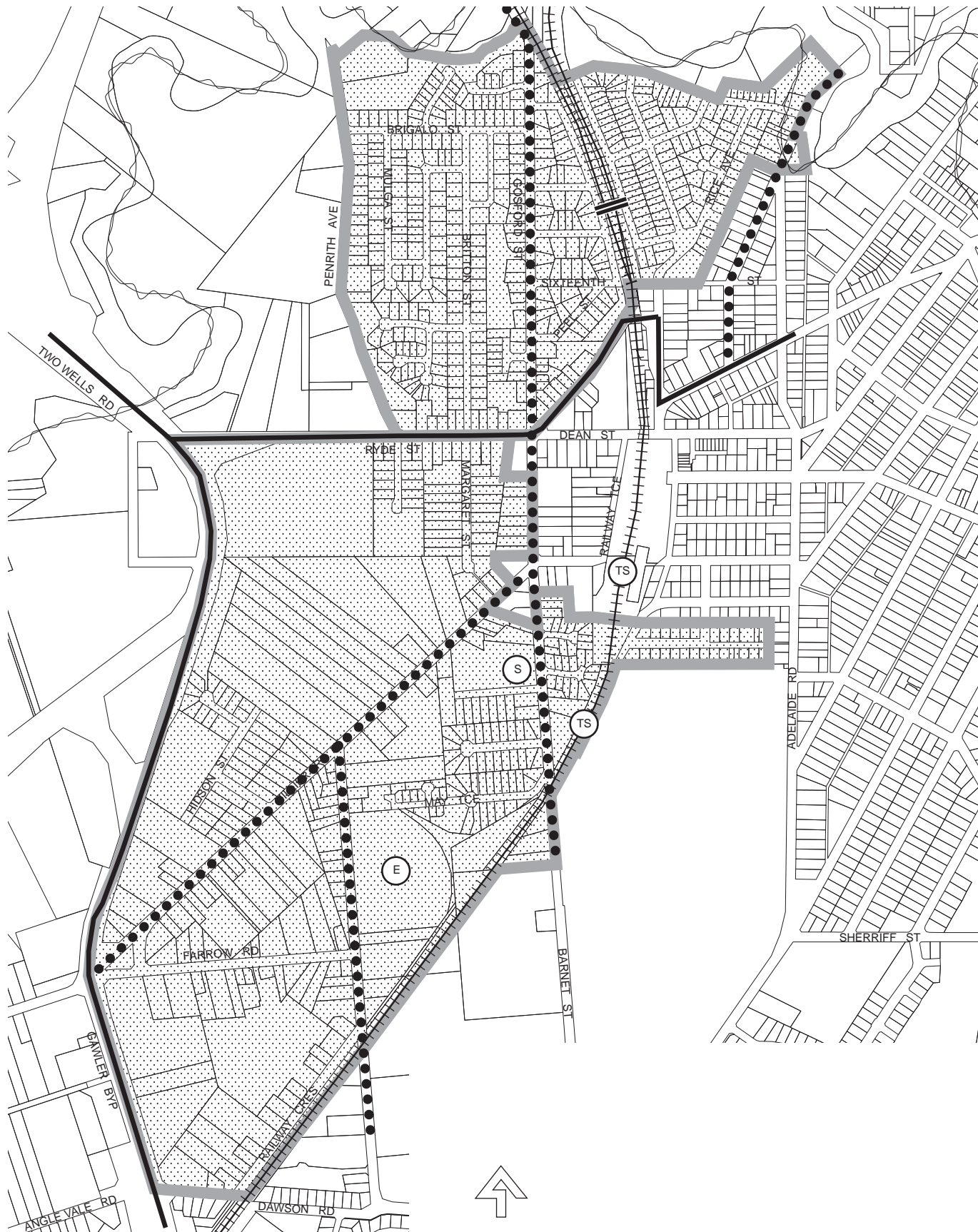











- (E) Education
- (LS) Local Shops
- (C) Cemetery
- (AAc) Aged Accommodation
- (Rec) Recreation
- (I) Information Bay
- (H) Hospital

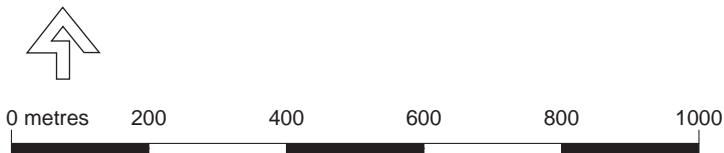
- Living
- Green Gully
- Major Traffic Flows
- Scenic Route
- Primary Arterial Road
- Policy Area Boundary
- Railway
- Gateway Landscape Treatment



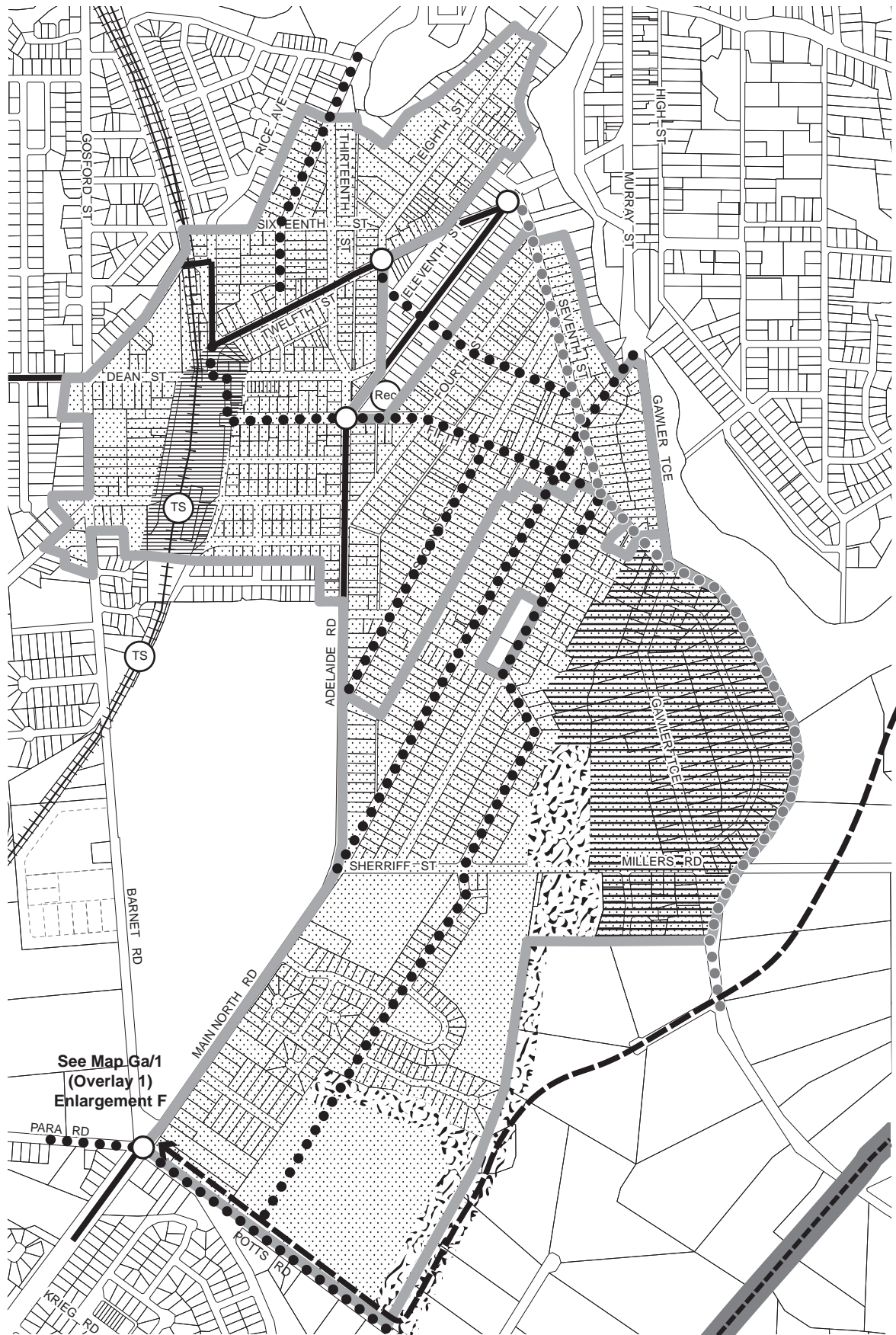
GAWLER (CT)
GAWLER EAST & WHEATSHEAF
STRUCTURE PLAN
MAP Ga/1 (Overlay 1)
Enlargement ©



-  Living
-  Major Traffic Flows
-  Primary Arterial Road
-  Policy Area Boundary
-  Improved Pedestrian Rail Crossing
-  Railway
-  Train Station
-  Education
-  Shopping



GAWLER (CT)
GAWLER WEST AND HILLIER
STRUCTURE PLAN
MAP Ga/1 (Overlay 1)
Enlargement D

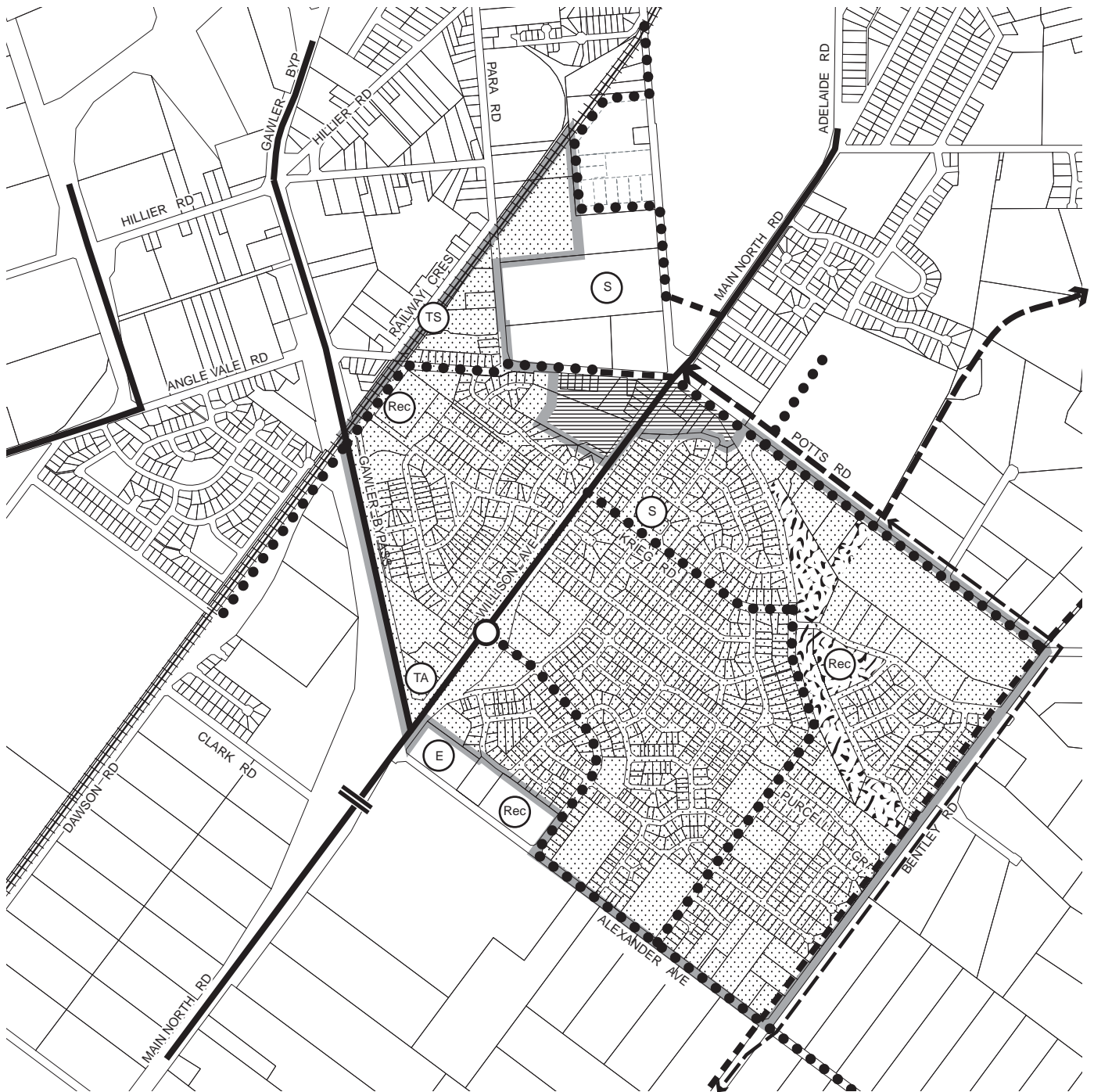



















- | | | | |
|--|---|--|---------------|
| | Living | | Railway |
| | Low Density Residential | | Train Station |
| | Mixed Residential / Business Use / Gawler Rail Station Precinct | | Recreation |
| | Open Space | | Gas Pipeline |
| | Major Traffic Flows | | |
| | Primary Arterial Road | | |
| | Gawler East Link Road | | |
| | Policy Area Boundary | | |
| | Traffic Control | | |
| | Scenic Road | | |

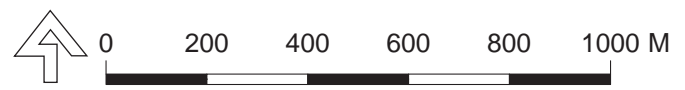


**GAWLER (CT)
GAWLER SOUTH
STRUCTURE PLAN
MAP Ga/1 (Overlay 1)
Enlargement E**
Consolidated - 18 July 2019

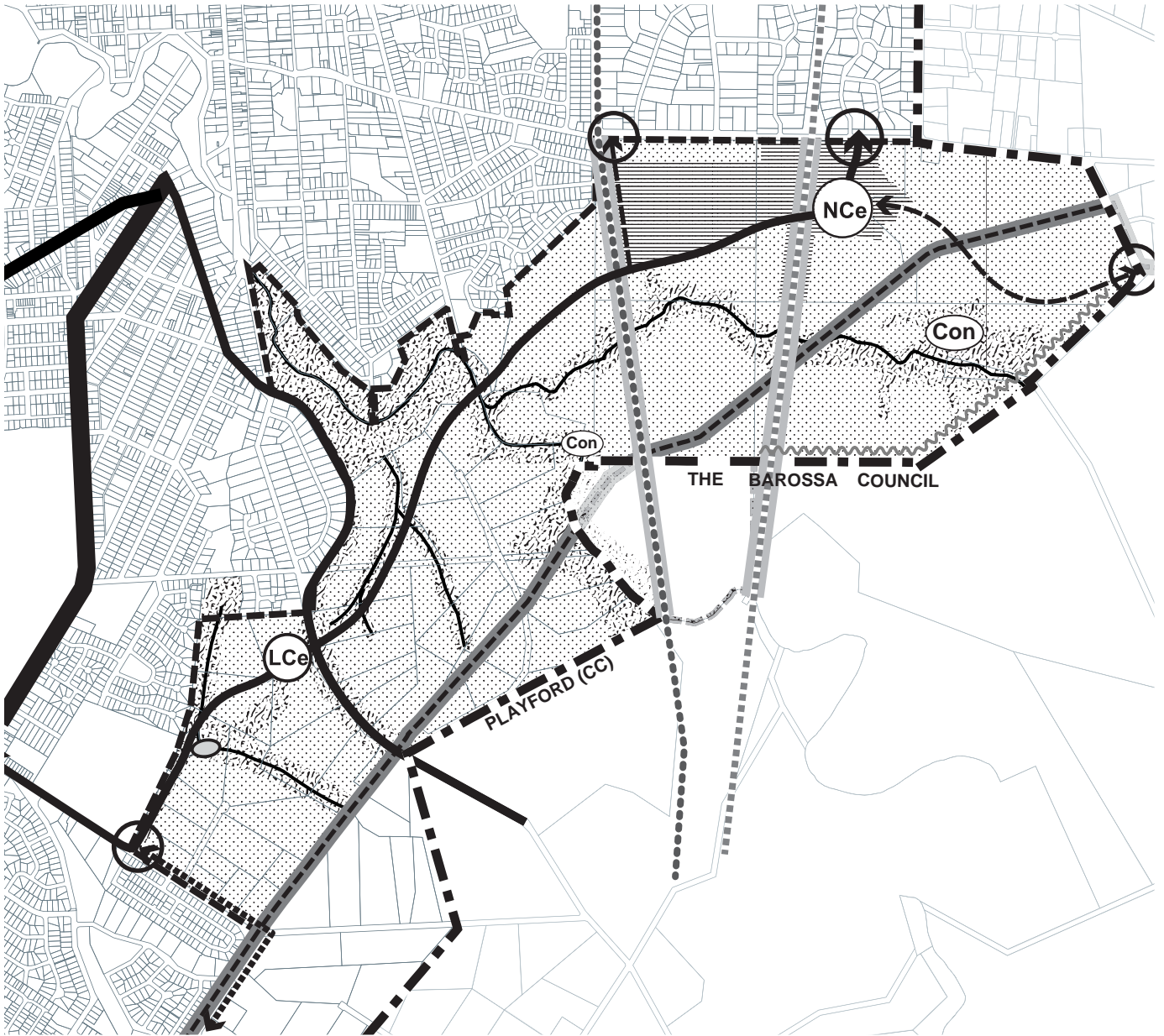
See Map Ga/1
(Overlay 1)
Enlargement F



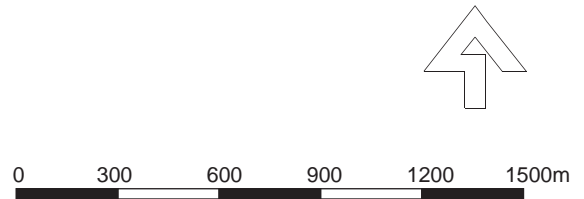
-  Living
-  Business
-  Open Space
-  Major Traffic Flows
-  Primary Arterial Road
-  Gawler East Link Road
-  Gawler East Link Road Extension
-  Traffic Control
-  Pedestrian Crossing (Underpass)
-  Policy Area Boundary
-  Railway
-  Train Station
-  Tourist Accommodation
-  Education
-  Shopping
-  Recreation
-  Gas Pipeline



GAWLER (CT)
EVANSTON/EVANSTON PARK
STRUCTURE PLAN
MAP Ga/1 (Overlay 1)
Enlargement F
 Consolidated - 18 July 2019

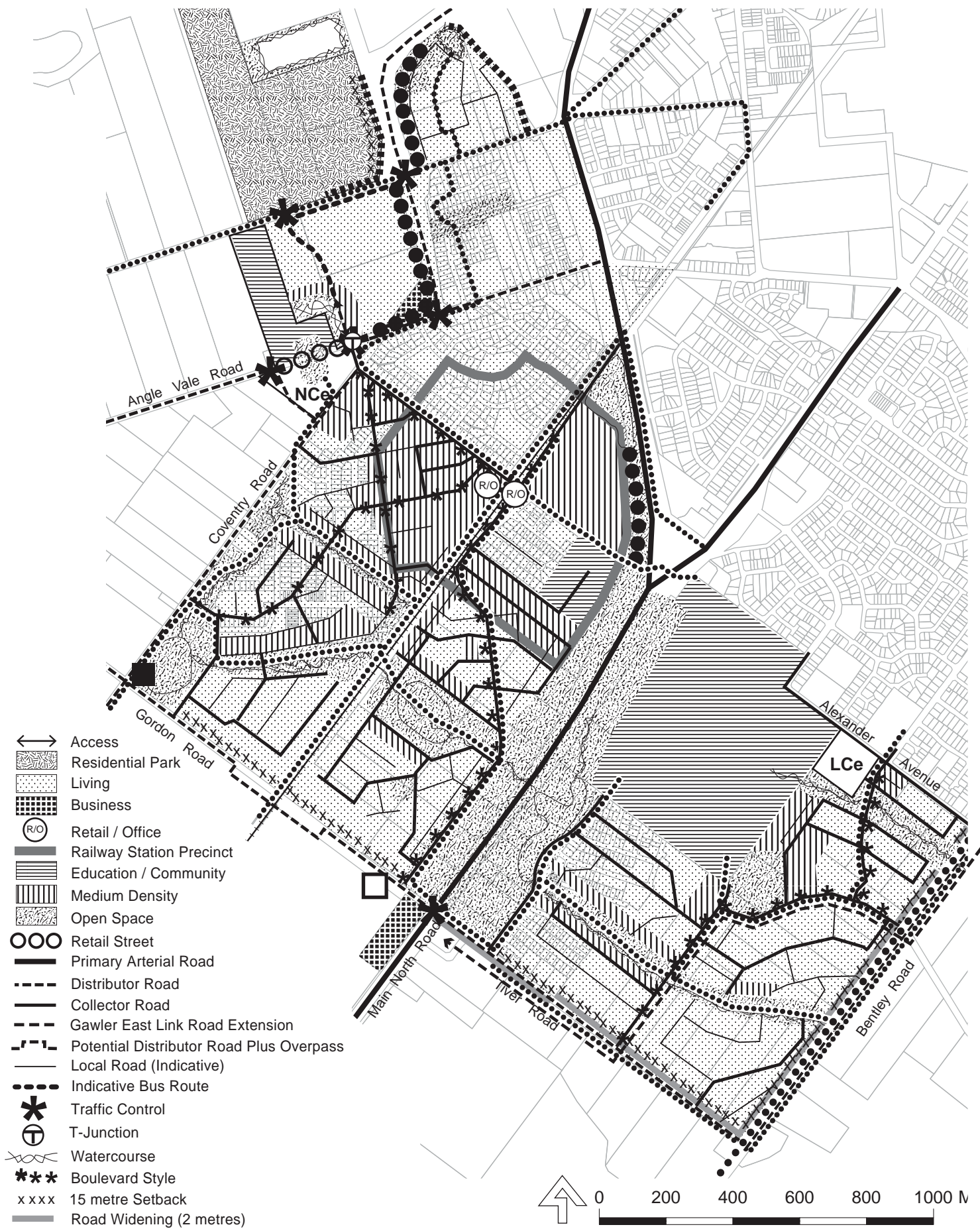


- | | |
|--|--|
| Neighbourhood Centre | Primary Arterial Road |
| Local Centre | Secondary Arterial Road |
| Mixed Use Centre | Collector Road |
| Living | Gawler East Link Road |
| Creek Alignment incorporating stormwater detention basin | Gawler East Link Road Extension |
| Open Space | Proposed Link Road |
| Conservation Area | Traffic Management |
| Development Plan Boundary | Major Transmission Infrastructure (132kV) Corridor (30 Metres) |
| Structure Plan Boundary | Major Transmission Infrastructure (275kV) Corridor (50 Metres) |
| Buffer | Gas Pipeline |

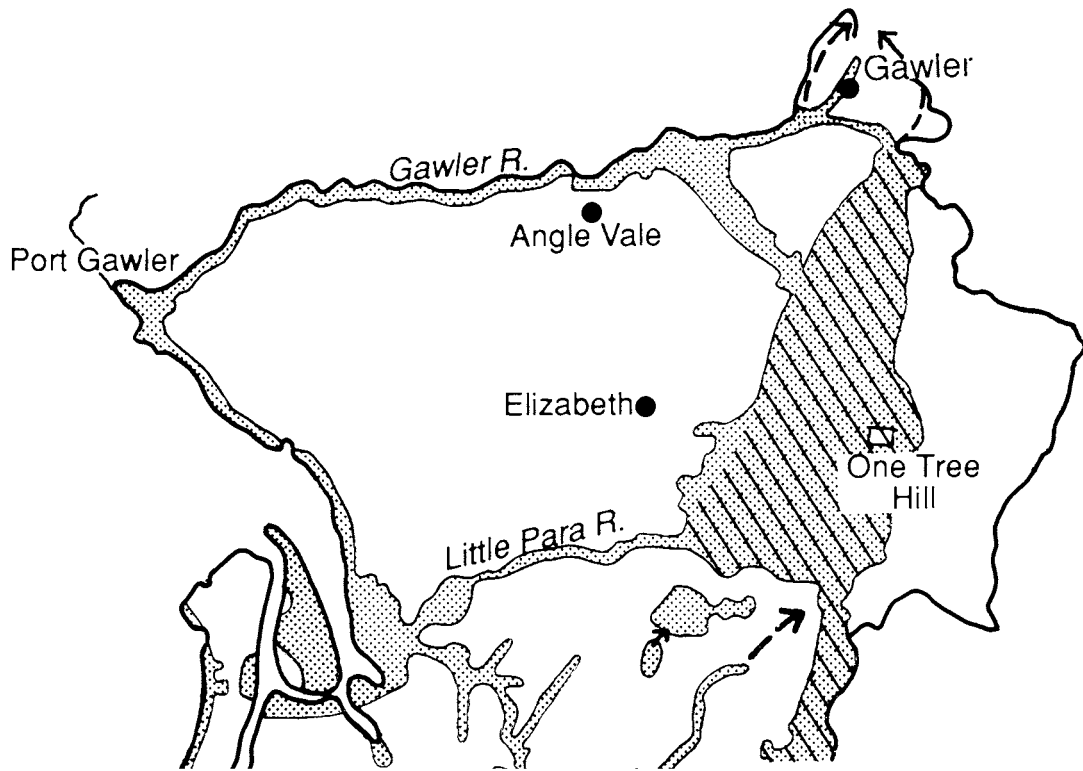


GAWLER (CT) GAWLER EAST STRUCTURE PLAN MAP Ga/1 (Overlay 1) Enlargement G



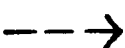
Consolidated - 18 July 2019



GAWLER (CT)
EVANSTON GARDENS /
EVANSTON SOUTH / HILLIER
STRUCTURE PLAN
MAP Ga/1 (Overlay 1)
Enlargement H



NOTE: This Map is indicative only. The State Government and Councils will undertake studies of each area resulting in detailed zoning maps to designate the boundary of MOSS and the policies relating to various areas (The inclusion of private land in MOSS does not indicate an intention to purchase that land).

-  Metropolitan Open Space System
-  Metropolitan Open Space System / Hills Face Zone
-  Metropolitan Open Space System - Proposed



Kilometres 5 10 15 20





GAWLER (CT) METROPOLITAN OPEN SPACE SYSTEM MAP Ga/1 (Overlay 2)

LIGHT REGIONAL COUNCIL



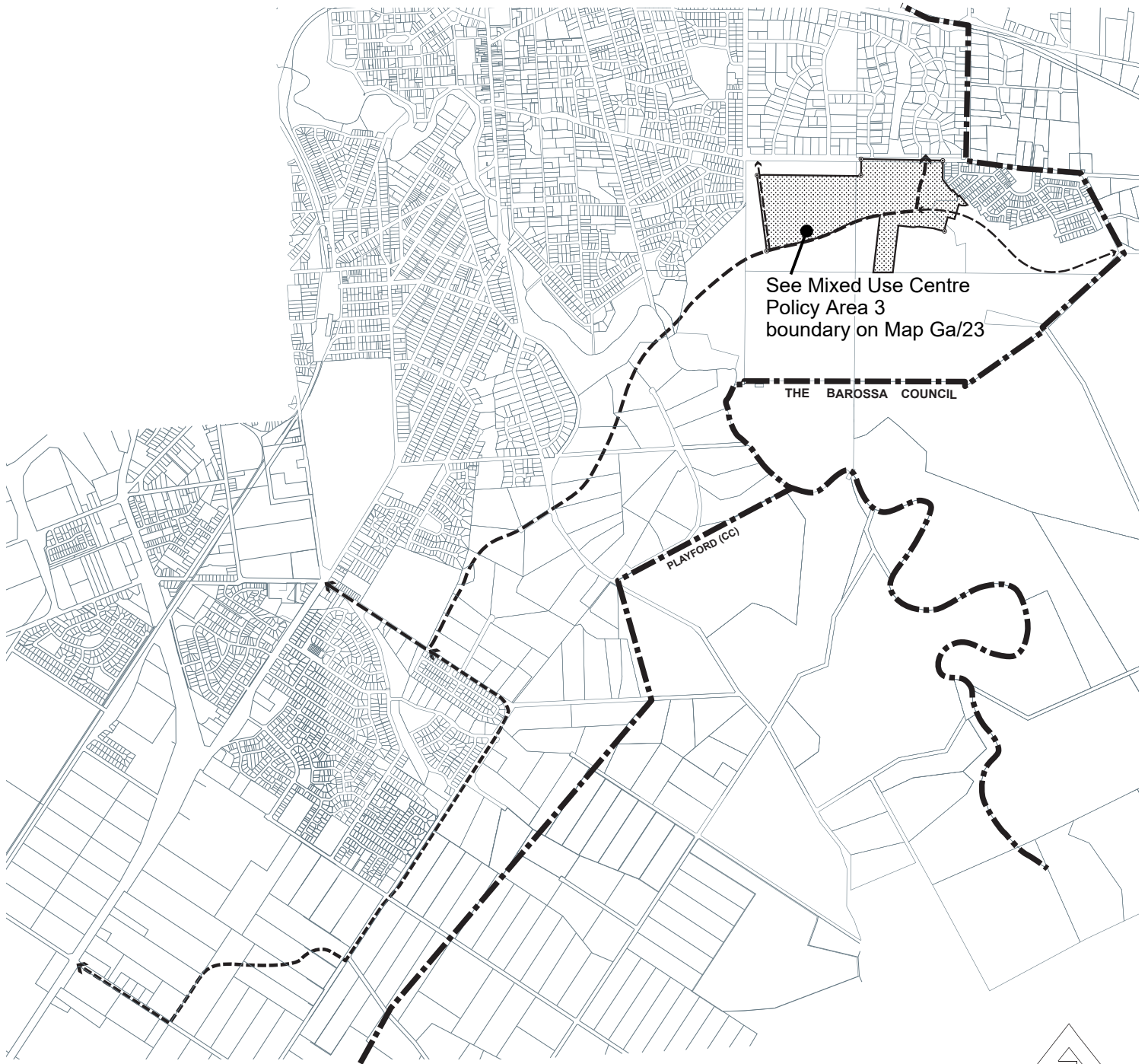
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






-  Noise and Air Emissions Designated Area
-  Designated Road : Type A Road
-  Railway Line
-  Development Plan Boundary

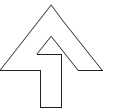
GAWLER (CT) NOISE AND AIR EMISSIONS MAP Ga/1 (Overlay 3)

Consolidated - 18 July 2019

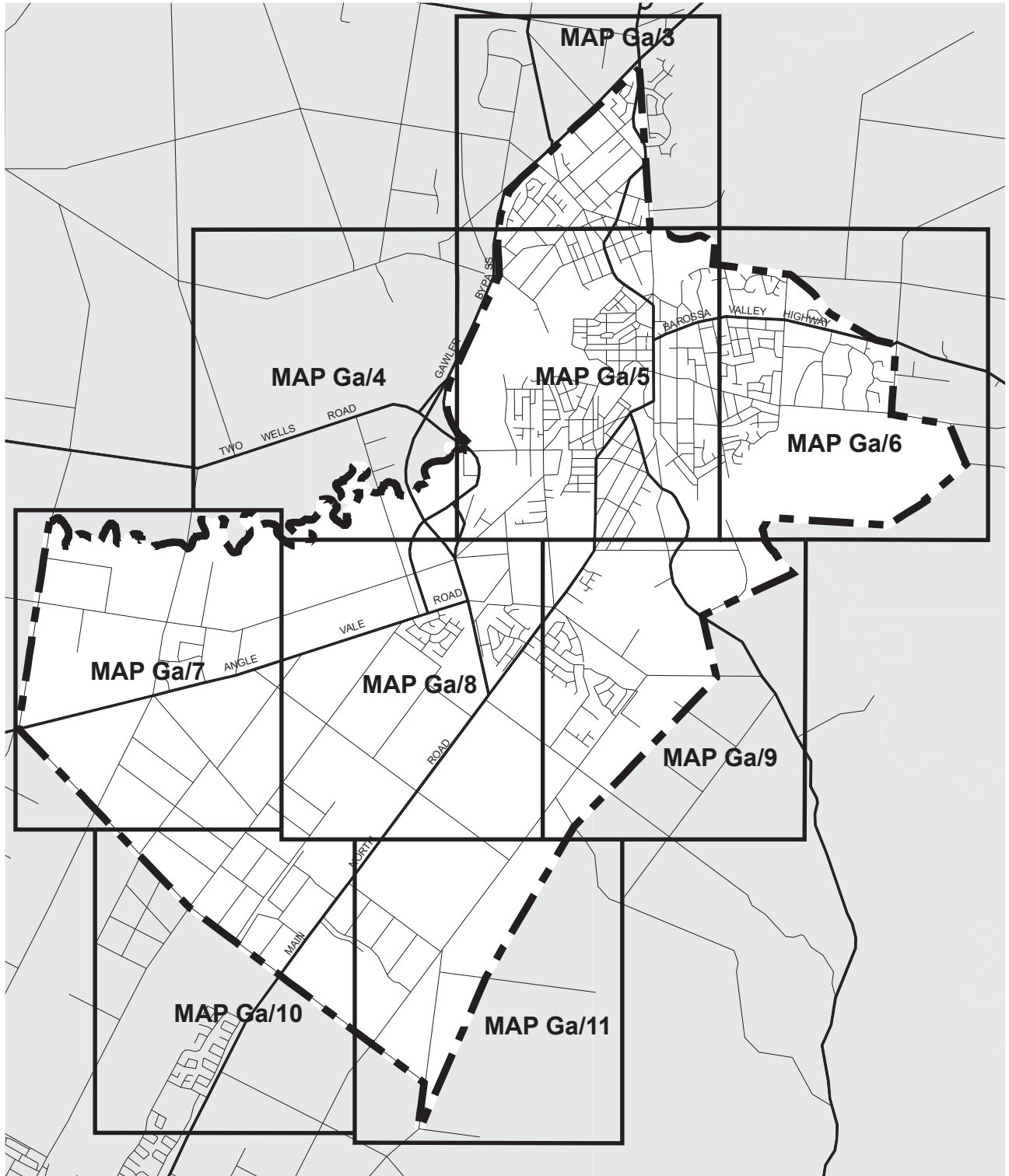


-  Noise and Air Emissions Designated Area
-  Development Plan Boundary
-  Gawler East Link Road
-  Gawler East Link Road Extension
-  Road Link

0 300 600 900 1200 1500m



GAWLER (CT) GAWLER EAST NOISE AND AIR EMISSIONS MAP Ga/1 (Overlay 4)



For the purposes of the Development Plan unless otherwise clearly indicated, the zone/policy area boundaries depicted on or intended to be fixed by Maps Ga/3 to Ga/23 inclusive shall be read as conforming in all respects (as the case may require) to the land division boundaries, to the centre line of roads or drain reserves or to the title boundaries, or to imaginary straight lines joining the positions defined by survey or by the measurements shown on the said maps against which the said zone/policy area boundaries are shown or otherwise as indicated.



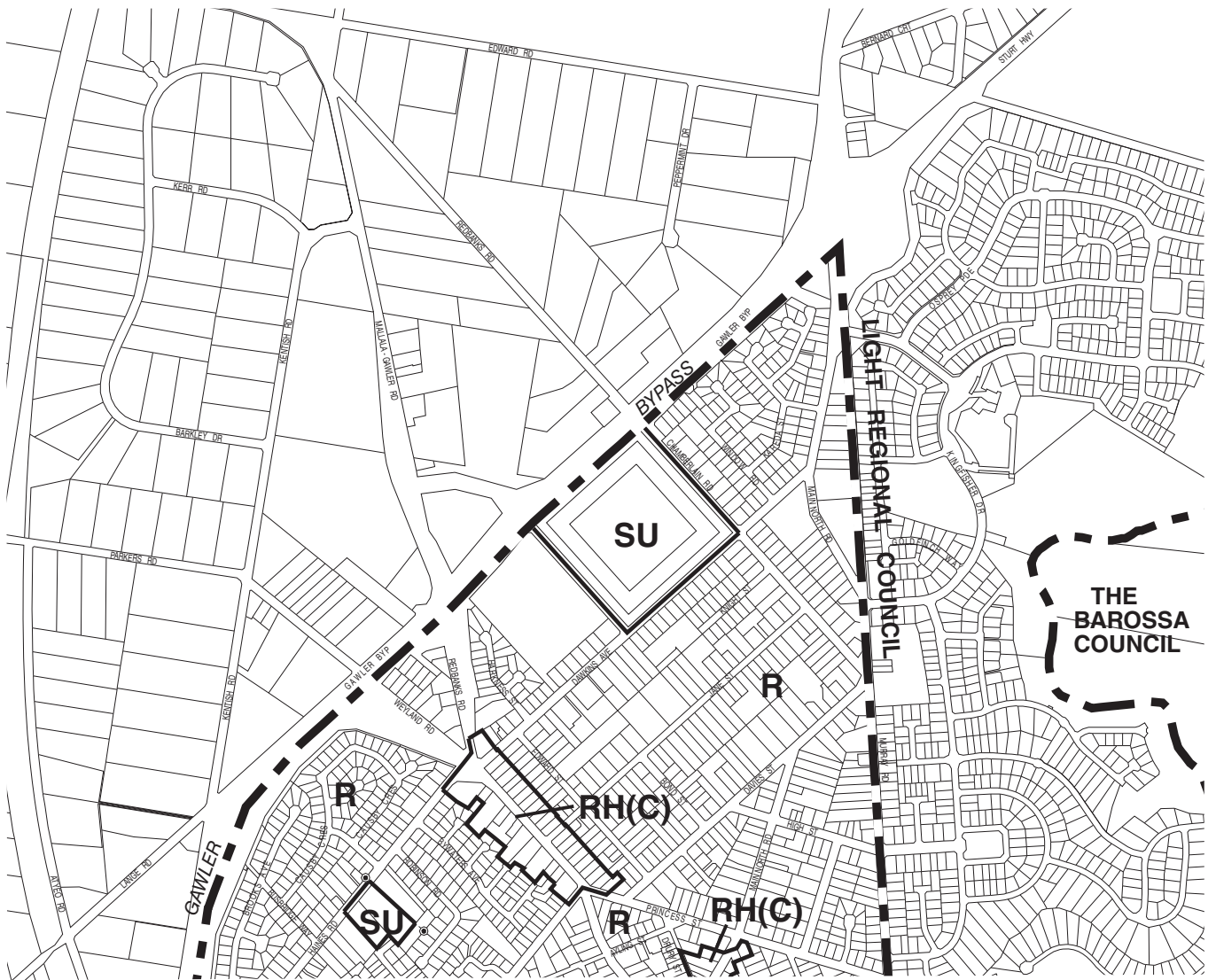
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GAWLER (CT) INDEX TO ZONES MAP Ga/2

— — — — — Development Plan Boundary

Consolidated - 18 July 2019



MAP Ga/5 ADJOINS

NOTE : For Policy Areas See MAP Ga/13

- R Residential
- RH(C) Residential Historic (Conservation)
- SU Special Uses



Scale 1:15000



- Zone Boundary
- - - Development Plan Boundary

GAWLER (CT) ZONES MAP Ga/3

Consolidated - 18 July 2019



NOTE : For Policy Areas See MAP Ga/22

Ru Rural
 RuL Rural Living
 DU Deferred Urban
 R Residential
 RP Residential Park

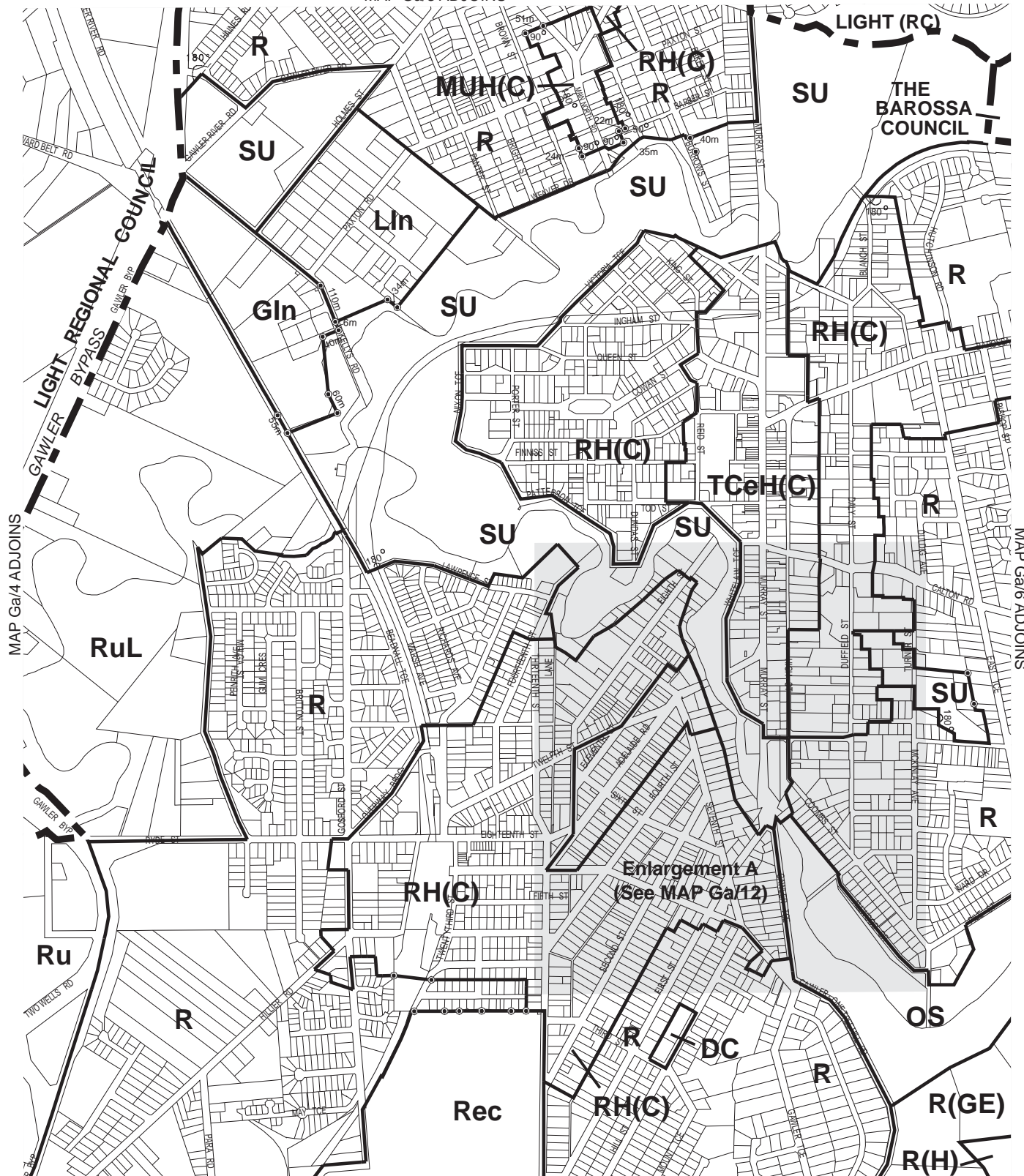
— Zone Boundary
 - - - Development Plan Boundary

Scale 1:15000



GAWLER (CT) ZONES MAP Ga/4

Consolidated - 18 July 2019



MAP Ga/4 ADJOINS

MAP Ga/6 ADJOINS

MAP Ga/8 ADJOINS

MAP Ga/9 ADJOINS

NOTE : For Policy Areas See MAP Ga/14
NOTE : For Precincts See MAP Ga/19

- DC District Commercial
- Gln General Industry
- LIn Light Industry
- MUH(C) Mixed Use Historic(Conservation)
- OS Open Space
- Rec Recreation
- R Residential
- R(GE) Residential (Gawler East)
- R(H) Residential (Hills)
- RH(C) Residential Historic (Conservation)
- Ru Rural
- RuL Rural Living
- SU Special Use
- TCeH(C) Town Centre Historic (Conservation)

- Zone Boundary
- — — Development Plan Boundary

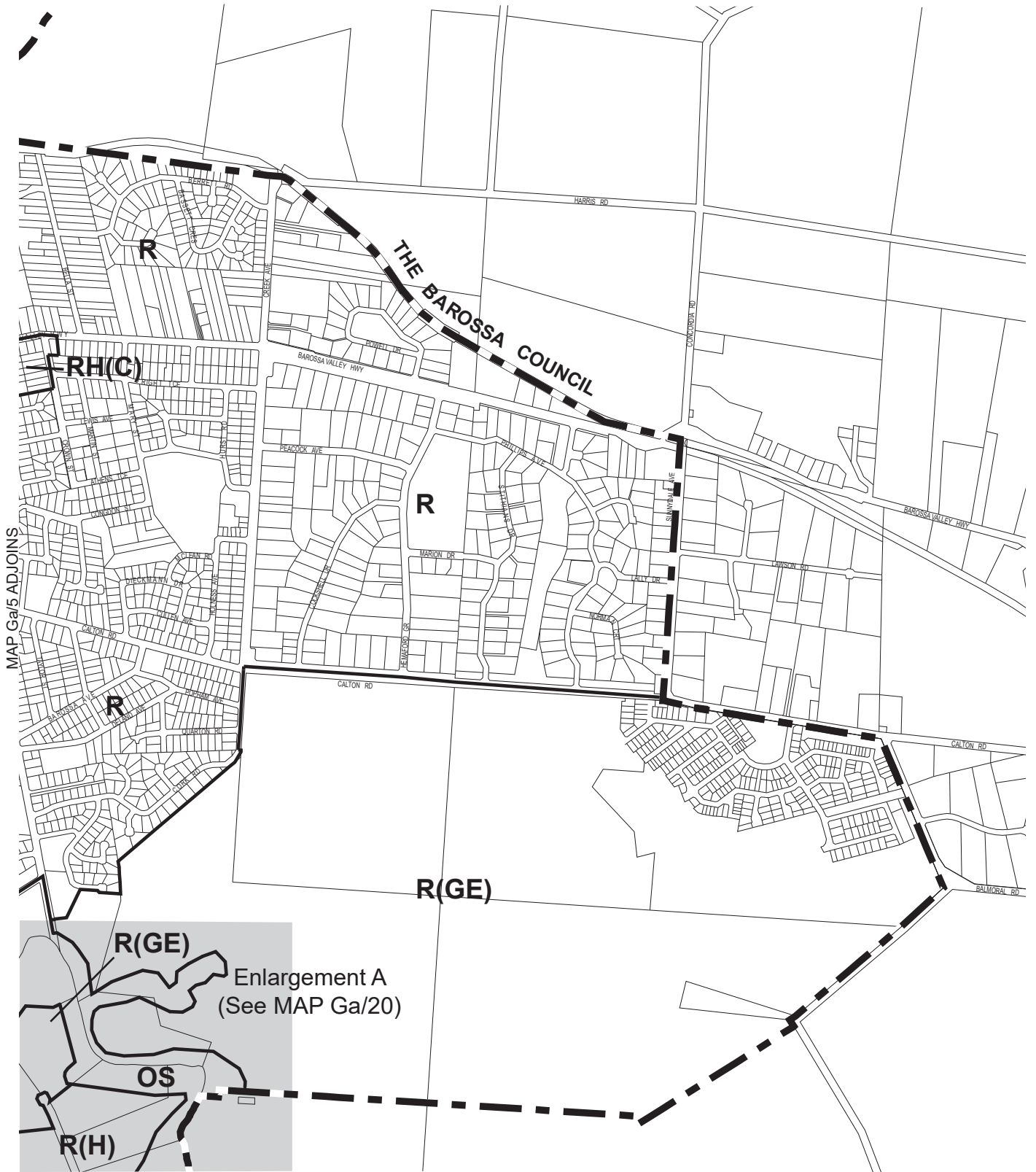
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GAWLER (CT) ZONES

MAP Ga/5

Consolidated - 18 July 2019



MAP Ga/5 ADJOINS

MAP Ga/9 ADJOINS

NOTE : For Policy Areas See MAP Ga/15

- R Residential
- R(GE) Residential (Gawler East)
- R(H) Residential (Hills)
- RH(C) Residential Historic (Conservation)
- RuL Rural Living
- OS Open Space

- Zone Boundary
- - - Development Plan Boundary





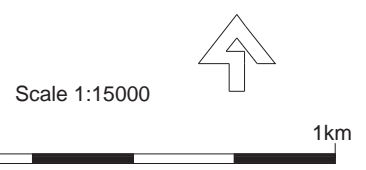
GAWLER (CT) ZONES MAP Ga/6



Ru Rural

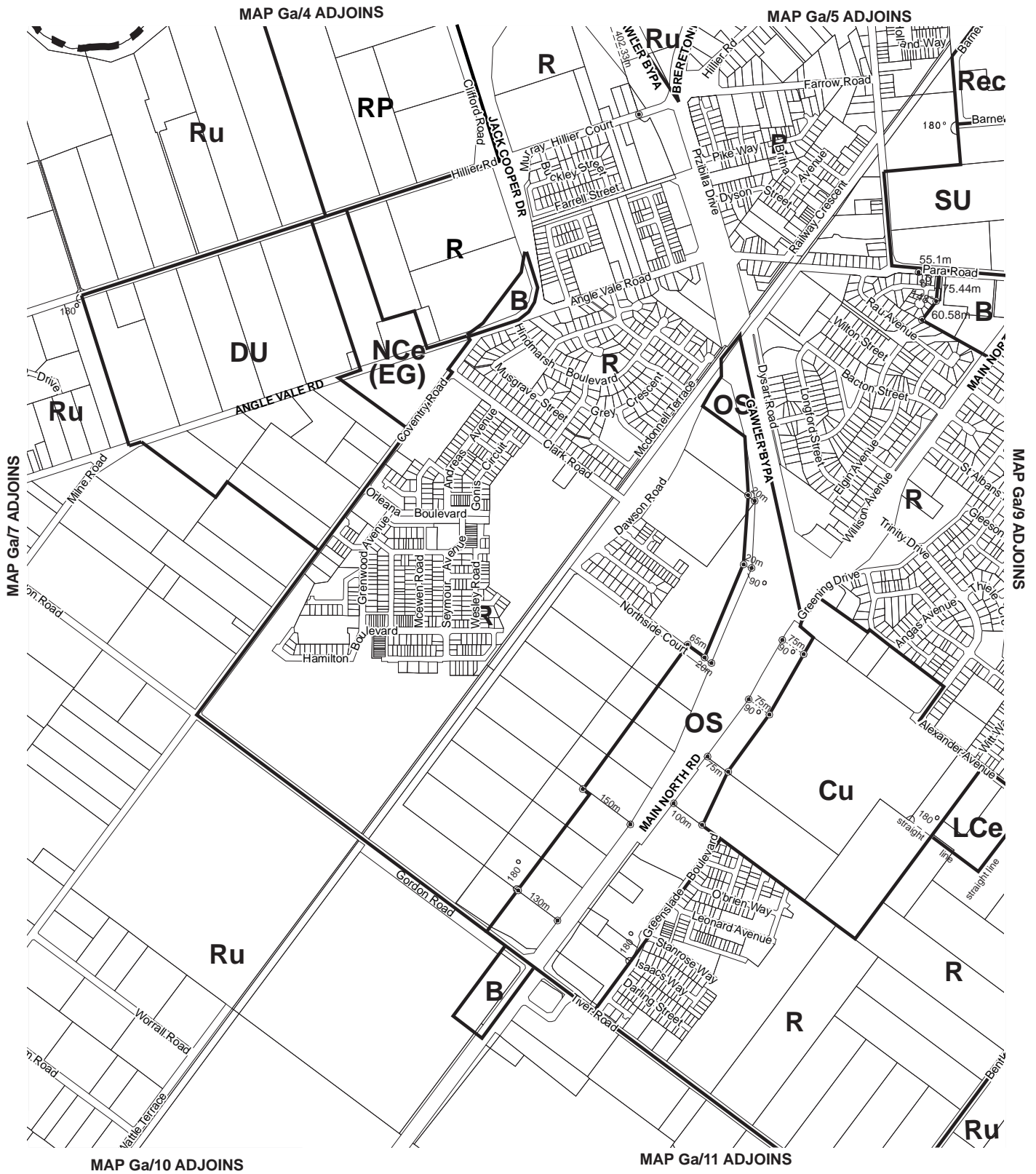
MAP Ga/10 ADJOINS

 Zone Boundary
 Development Plan Boundary



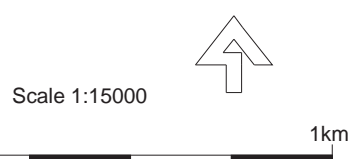
**GAWLER (CT)
ZONES
MAP Ga/7**

Consolidated - 18 July 2019

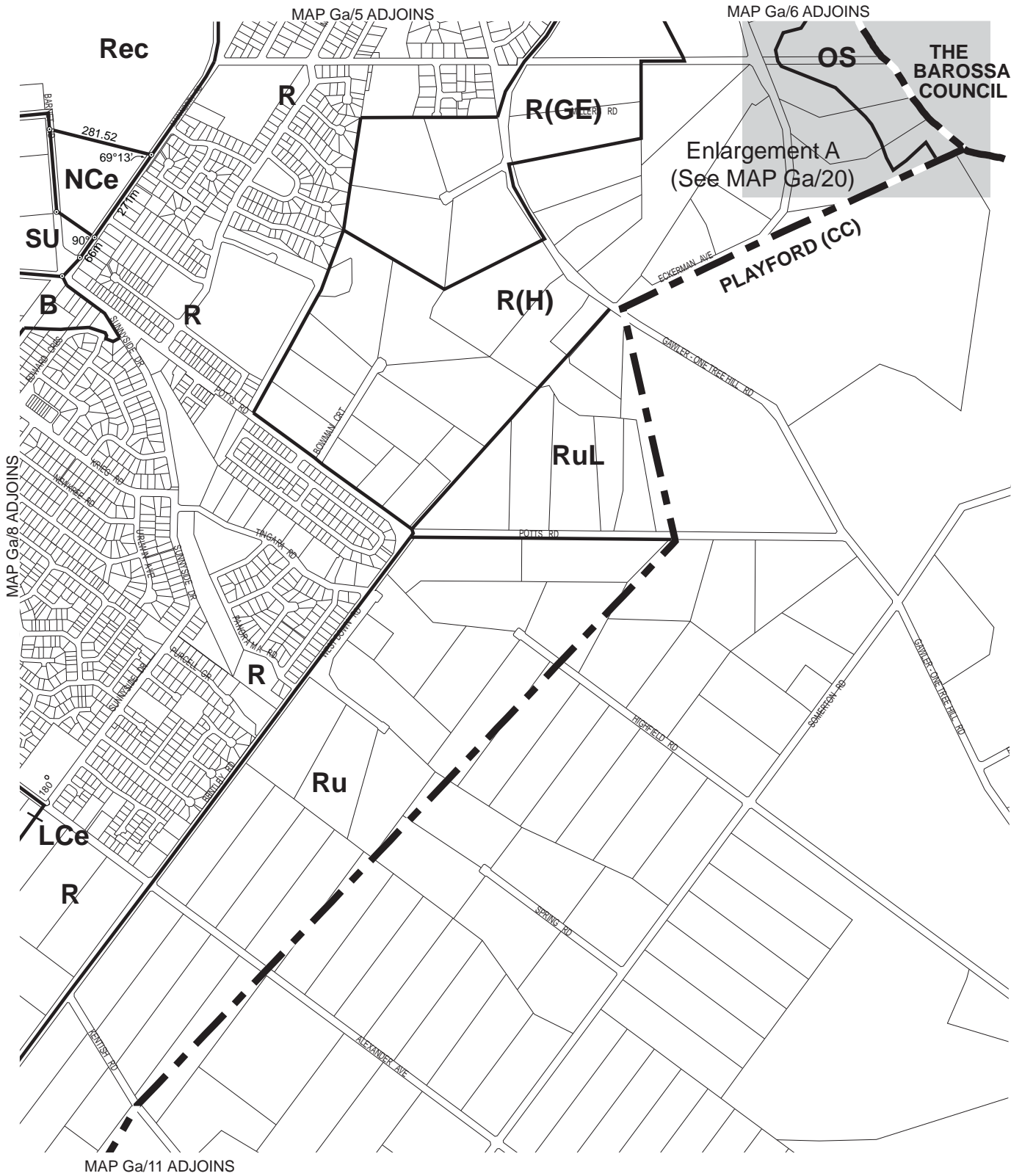


- NOTE : For Policy Areas See MAP Ga/16**
- B** Business
 - Cu** Community
 - DU** Deferred Urban
 - LCe** Local Centre
 - NCE(EG)** Neighbourhood Centre (Evanston Gardens)
 - OS** Open Space
 - Rec** Recreation
 - R** Residential
 - RP** Residential Park
 - Ru** Rural
 - SU** Special Uses

- Zone Boundary
- Development Plan Boundary





GAWLER (CT) ZONES MAP Ga/8



NOTE : For Policy Areas See MAP Ga/17

- | | |
|--------------|---------------------------|
| B | Business |
| R | Residential |
| NCe | Neighbourhood Centre |
| LCe | Local Centre |
| DU | Deferred Urban |
| Rec | Recreation |
| OS | Open Space |
| R(GE) | Residential (Gawler East) |
| R(H) | Residential (Hills) |
| Ru | Rural |
| RuL | Rural Living |
| SU | Special Uses |

- | | |
|---|---------------------------|
|  | Zone Boundary |
|  | Development Plan Boundary |

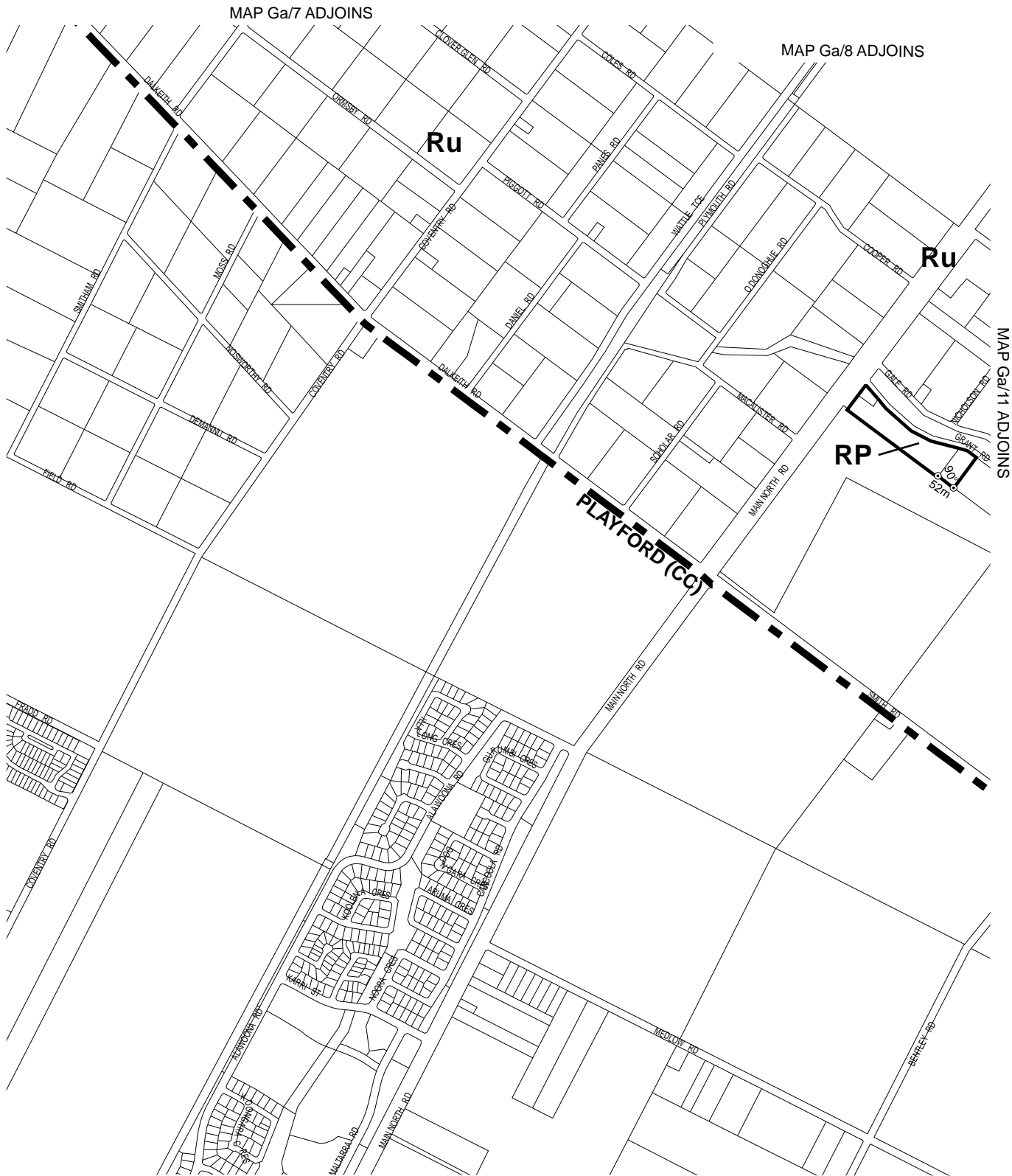


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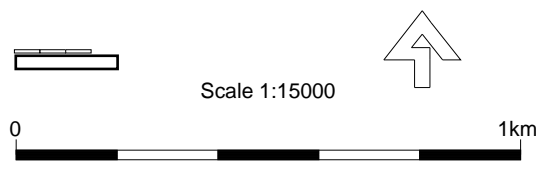
GAWLER (CT) ZONES MAP Ga/9

Consolidated - 18 July 2019

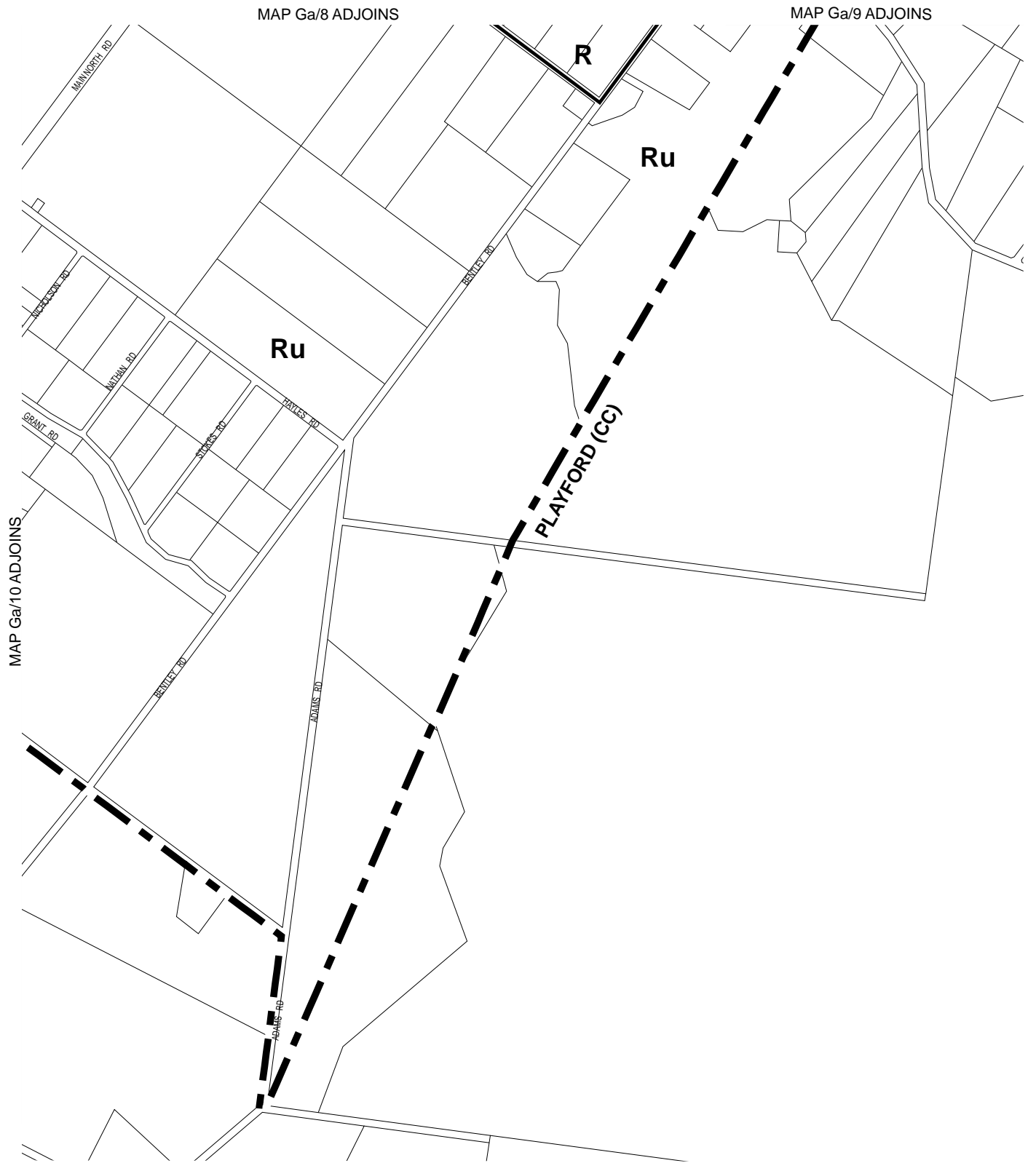


RP Residential Park
 Ru Rural

— Zone Boundary
 - - - Development Plan Boundary



GAWLER (CT) ZONES MAP Ga/10



NOTE : For Policy Areas See MAP Ga/21

R Residential
 Ru Rural

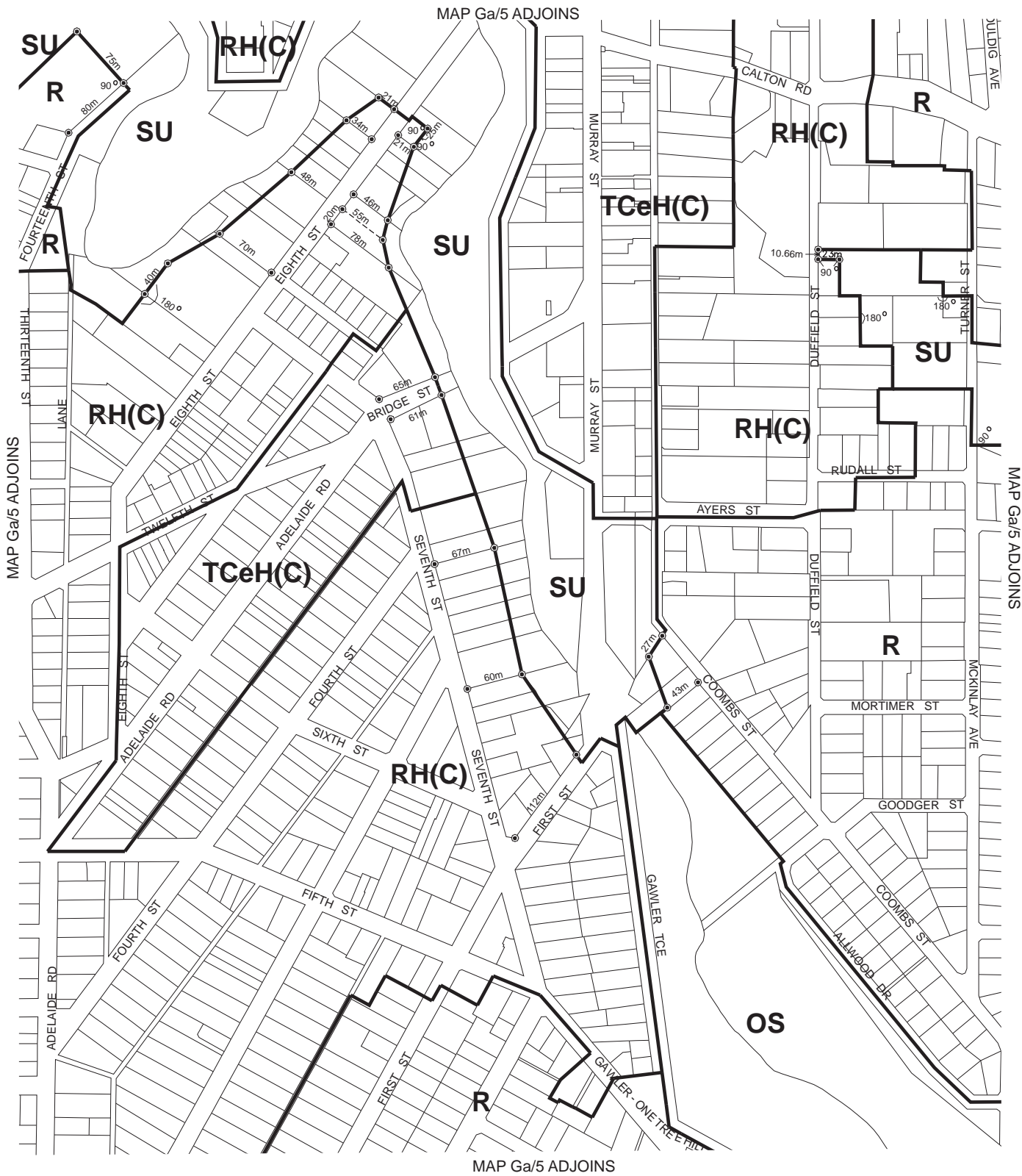


Scale 1:15000



**GAWLER (CT)
 ZONES
 MAP Ga/11**

— Zone Boundary
 - - - Development Plan Boundary



ENLARGEMENT A

- OS Open Space
- R Residential
- RH(C) Residential Historic (Conservation)
- RuL Rural Living
- SU Special Uses
- TCeH(C) Town Centre Historic (Conservation)

-  Zone Boundary
-  Development Plan Boundary

Scale 1:6000



**GAWLER (CT)
ZONES
ENLARGEMENT
MAP Ga/12
Consolidated - 18 July 2019**



MAP Ga/14 ADJOINS

- 11 Willaston Residential
- 12 Willaston (Redbanks Road) Residential Historic (Conservation)
- 13 Willaston Residential Historic (Conservation)

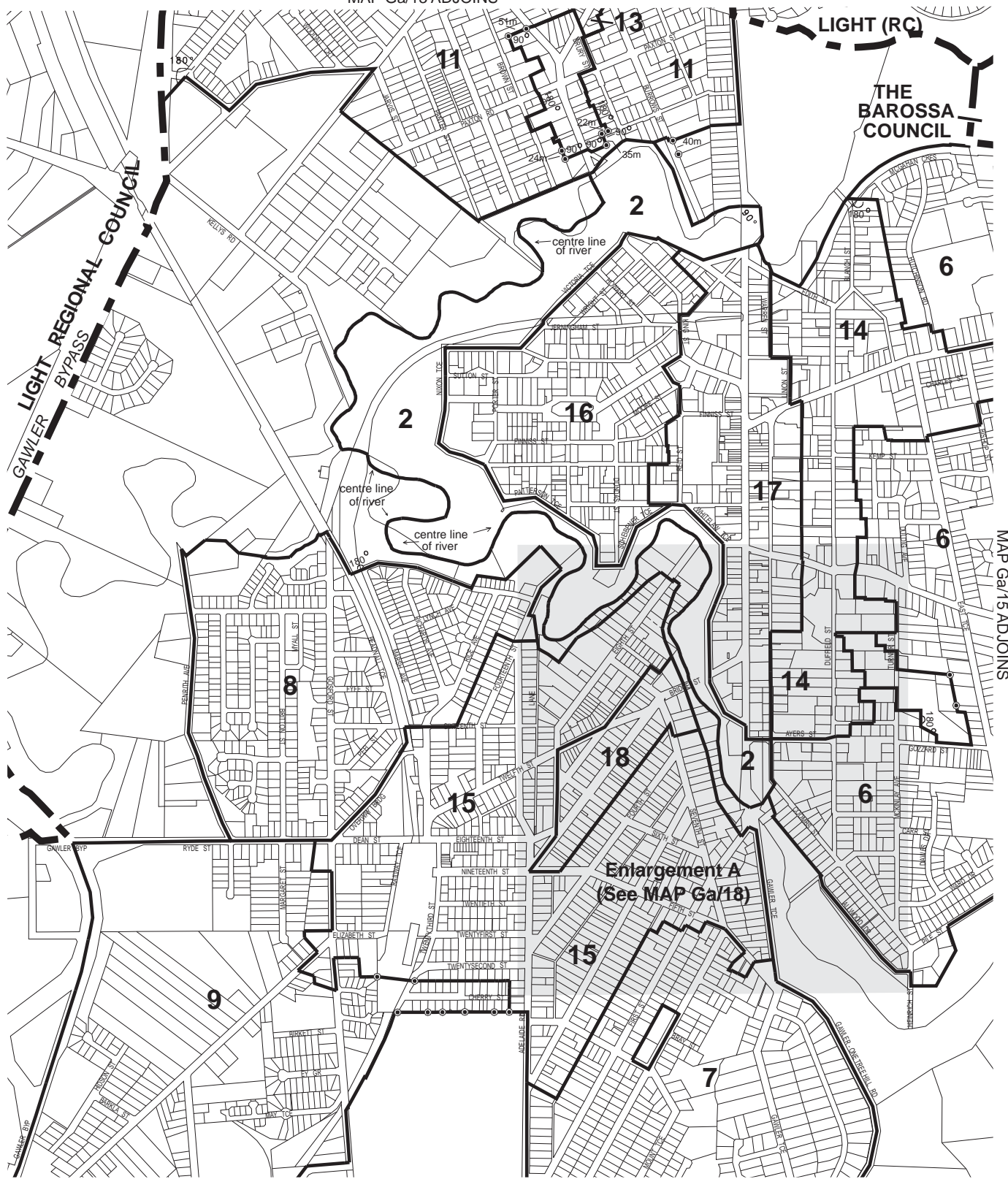
Scale 1:15000



GAWLER (CT) POLICY AREAS MAP Ga/13

-  Policy Area Boundary
-  Development Plan Boundary

Consolidated - 18 July 2019



- 1 District Shopping zone Historic (Conservation)
- 2 Special Use zone Historic (Conservation)
- 6 Gawler East Residential
- 7 Gawler South Residential
- 8 Gawler West Residential
- 9 Hillier Road Residential
- 11 Willaston Residential
- 13 Willaston Residential Historic (Conservation)
- 14 Gawler East Residential Historic (Conservation)
- 15 Gawler South Residential Historic (Conservation)

- 16 Light Residential Historic (Conservation)
- 17 Light Town Centre Historic (Conservation)
- 18 Gawler South Town Centre Historic (Conservation)

Scale 1:15000



Policy Area Boundary
 Development Plan Boundary

GAWLER (CT) POLICY AREAS MAP Ga/14



- 3 Mixed Use Centre
- 6 Gawler East Residential
- 10 Wheatsheaf Residential
- 14 Gawler East Residential Historic (Conservation)

- Policy Area Boundary
- - - Development Plan Boundary

GAWLER (CT) POLICY AREAS MAP Ga/15

Consolidated - 18 July 2019

MAP Ga/22 ADJOINS

MAP Ga/14 ADJOINS

MAP Ga/17 ADJOINS

MAP Ga/17 ADJOINS



MAP Ga/21 ADJOINS

- 4 Evanston Gardens / Evanston South / Hillier Residential
- 5 Evanston/Evanston Park Residential
- 9 Hillier Road Residential



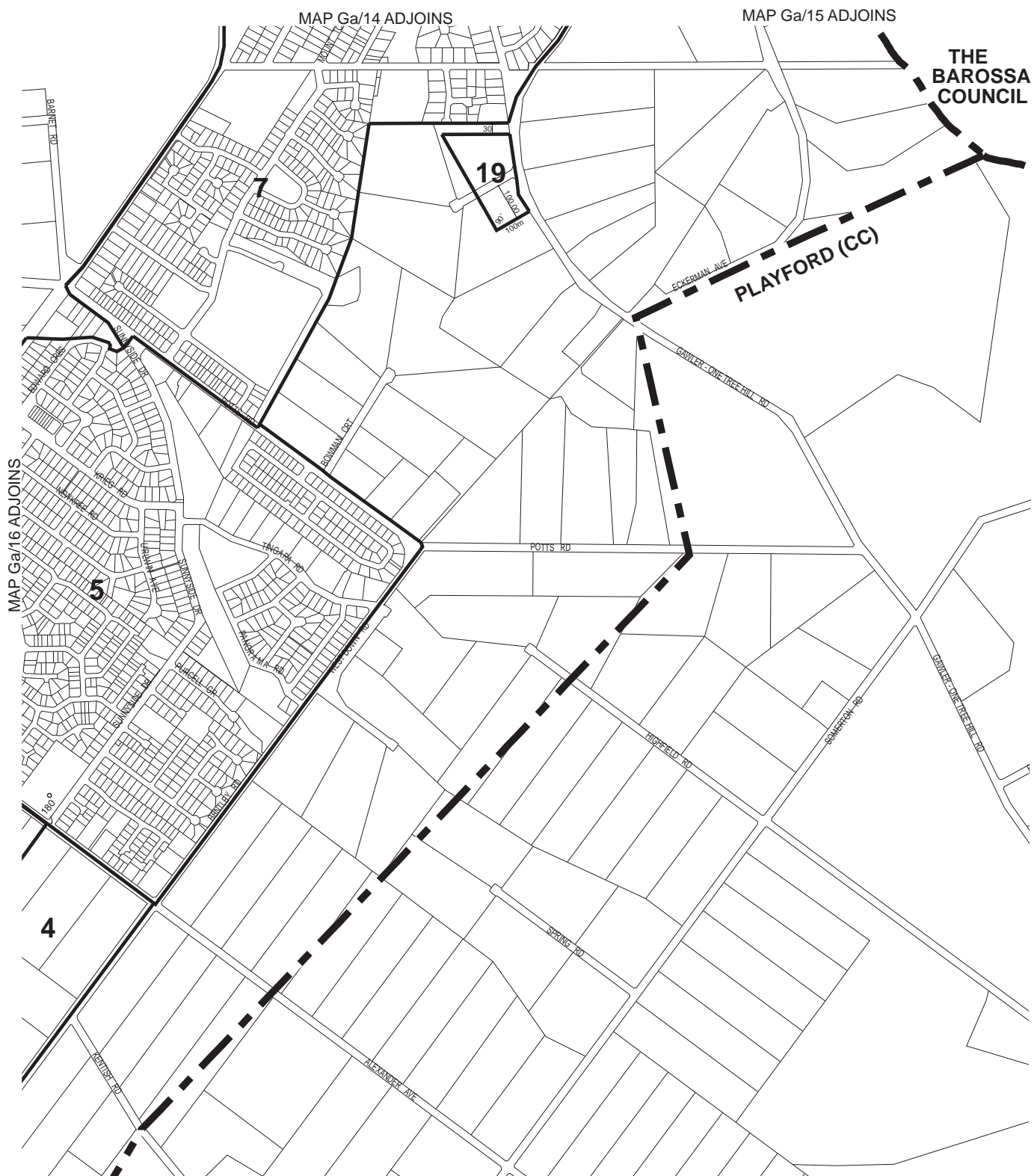
Scale 1:15000



GAWLER (CT) POLICY AREAS MAP Ga/16

- Policy Area Boundary
- Development Plan Boundary

Consolidated - 18 July 2019



- 4 Evanston Gardens / Evanston South / Hillier Residential
- 5 Evanston/Evanston Park Residential
- 7 Gawler South Residential
- 19 Local Centre Policy Area

Scale 1:15000

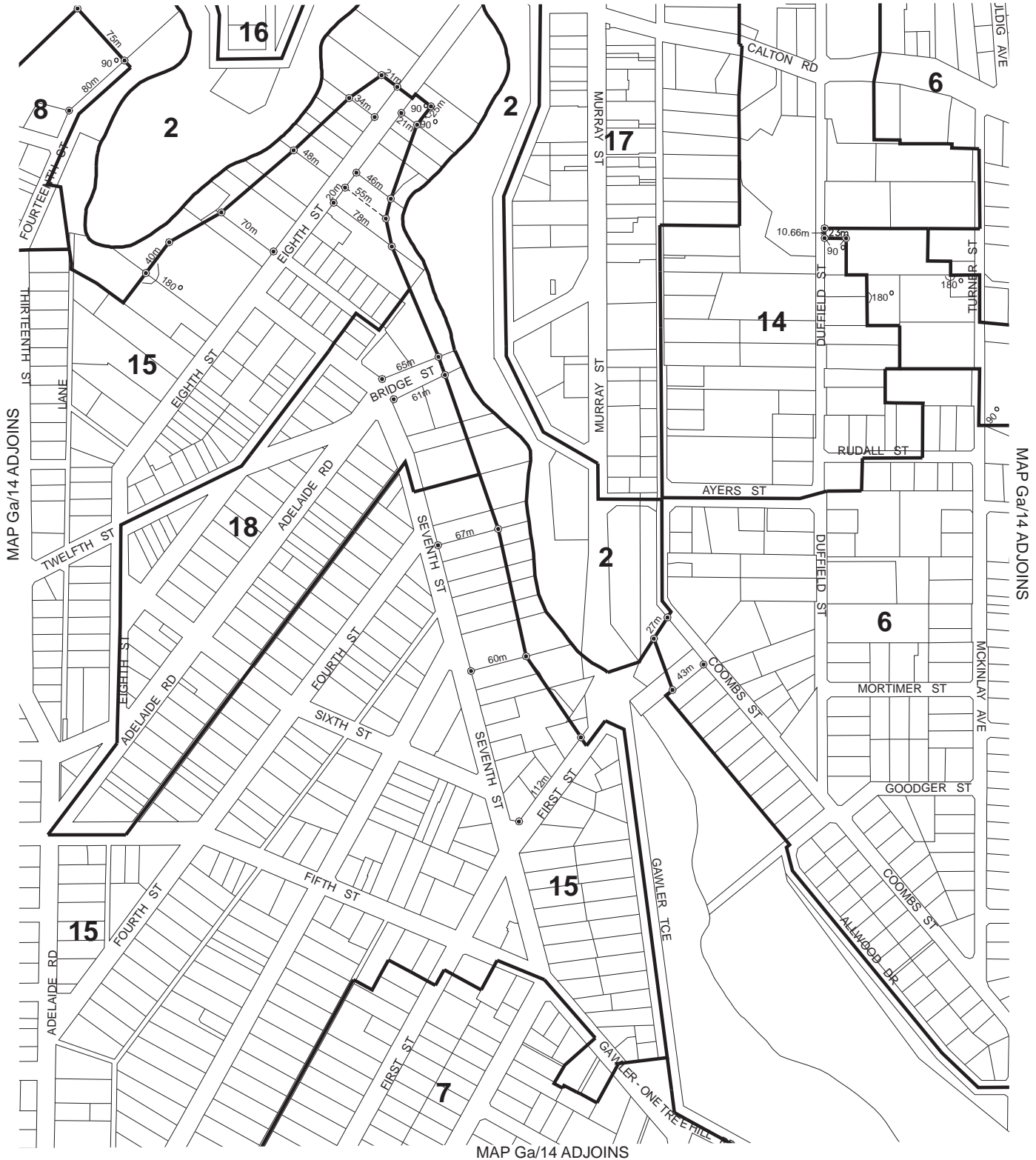


GAWLER (CT) POLICY AREAS MAP Ga/17

- Policy Area Boundary
- Development Plan Boundary

Consolidated - 18 July 2019

MAP Ga/14 ADJOINS



MAP Ga/14 ADJOINS

ENLARGEMENT A

- 2 Special Use zone Historic (Conservation)
- 6 Gawler East Residential
- 7 Gawler South Residential
- 8 Gawler West Residential
- 14 Gawler East Residential Historic (Conservation)
- 15 Gawler South Residential Historic (Conservation)
- 16 Light Residential Historic (Conservation)
- 17 Light Town Centre Historic (Conservation)
- 18 Gawler South Town Centre Historic (Conservation)

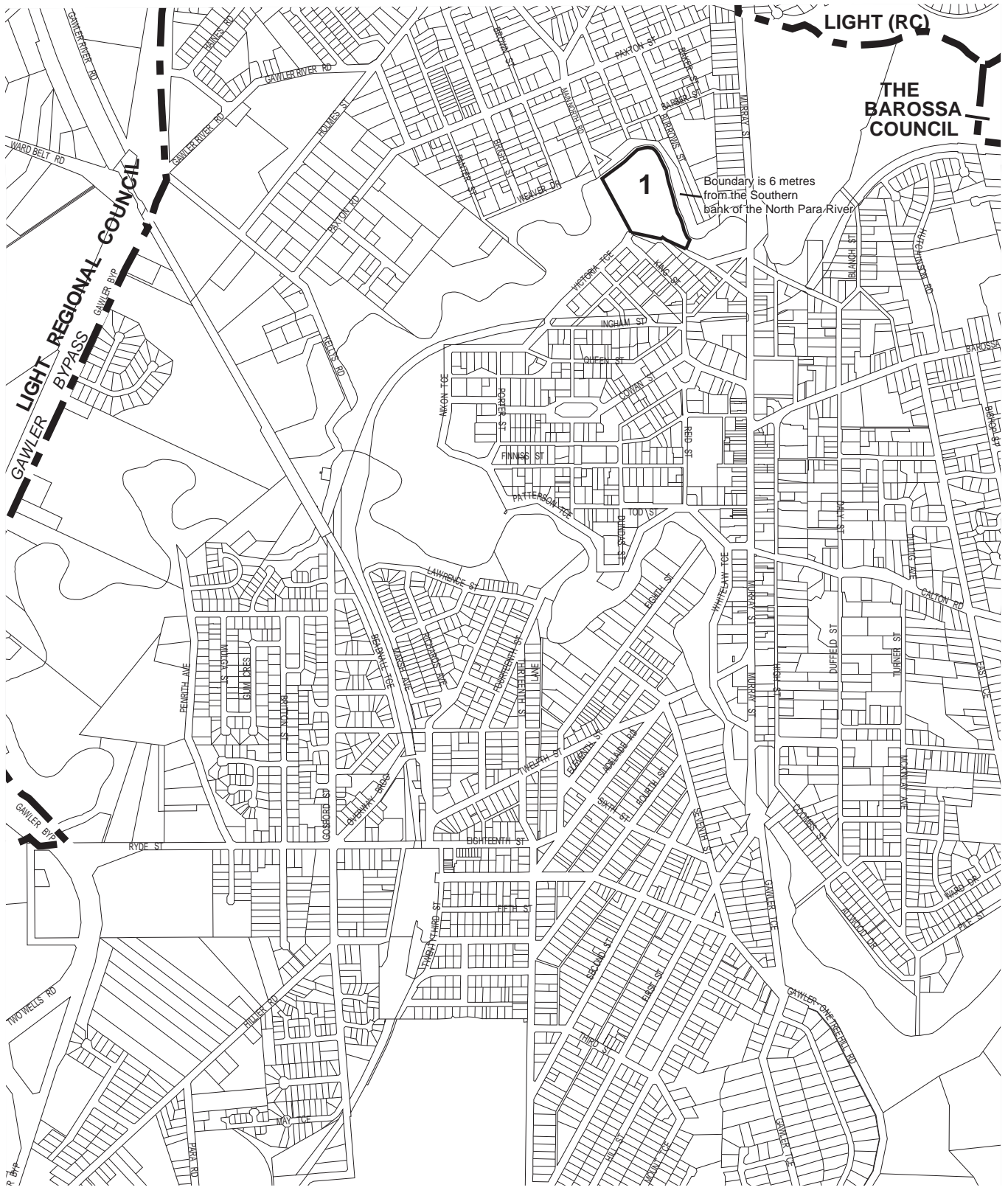
-  Policy Area Boundary
-  Development Plan Boundary

Scale 1:6000



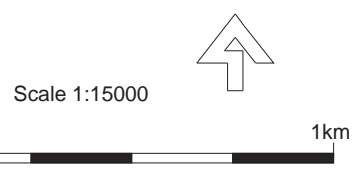
GAWLER (CT) POLICY AREAS ENLARGEMENT

MAP Ga/18
Consultation - 10/10/18



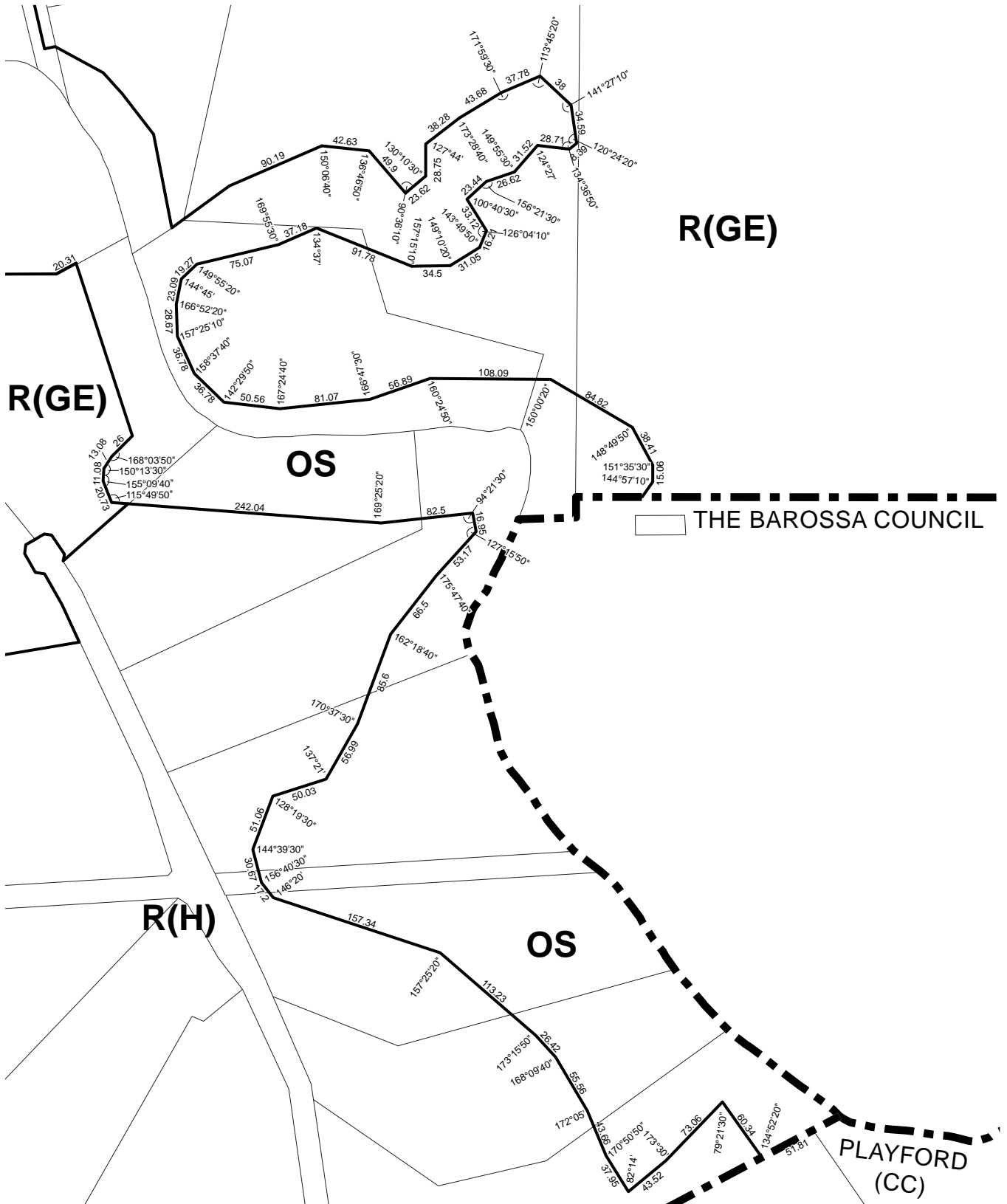
Special Uses Zone
 1 Caravan & Tourist Park Precinct

— Precinct Boundary
 - - - Development Plan Boundary



GAWLER (CT) PRECINCTS MAP Ga/19

Consolidated - 18 July 2019



OS Open Space
R(H) Residential (Hills)
R(GE) Residential (Gawler East)

— Zone Boundary
- - - Development Plan Boundary

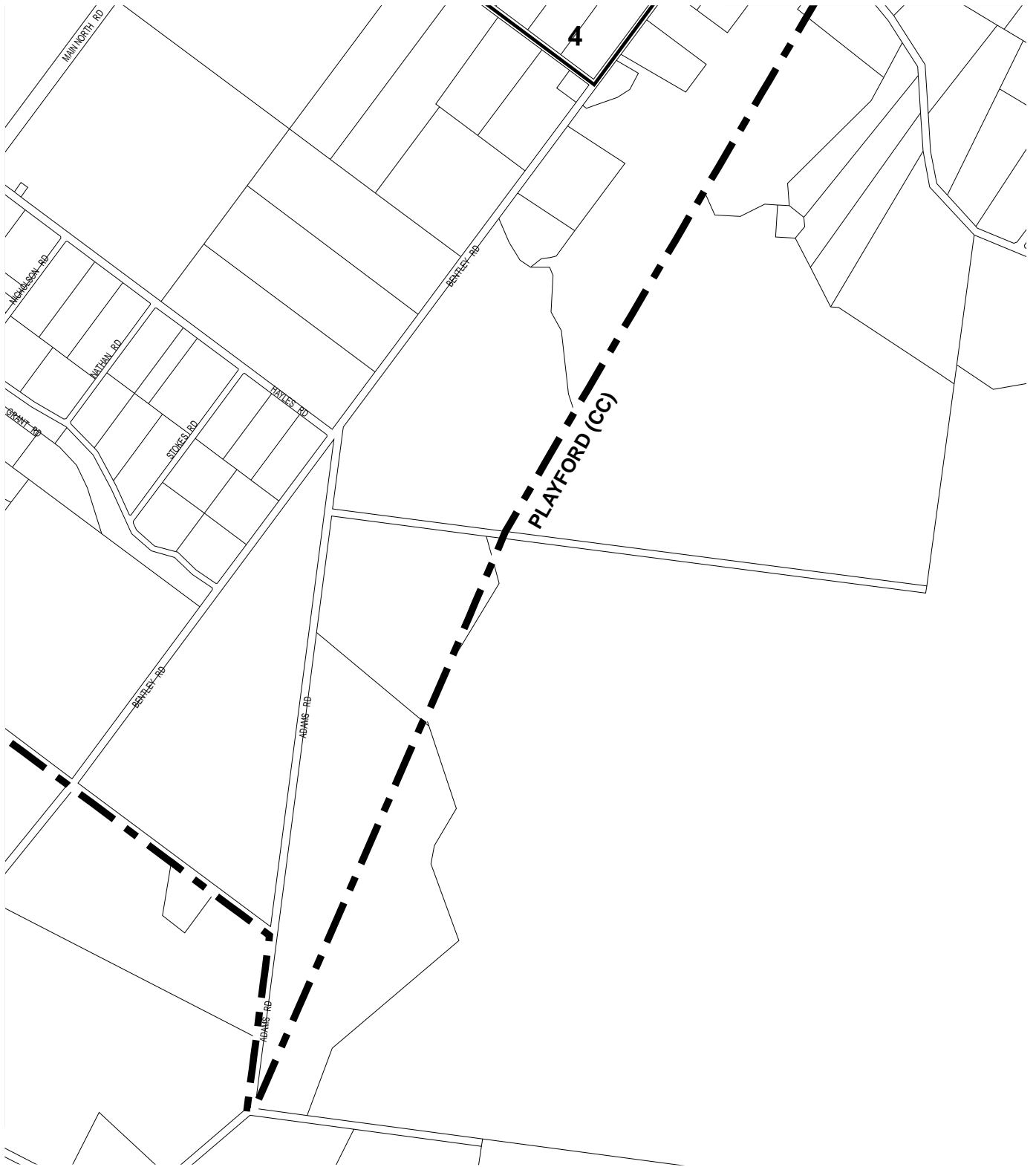
ENLARGEMENT A

Scale 1:5000



**GAWLER (CT)
ZONES
MAP Ga/20**

Consolidated - 18 July 2019



4 Evanston Gardens / Evanston South / Hillier Residential

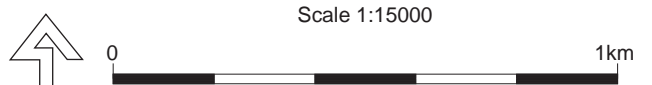


Scale 1:15000



**GAWLER (CT)
POLICY AREAS
MAP Ga/21**

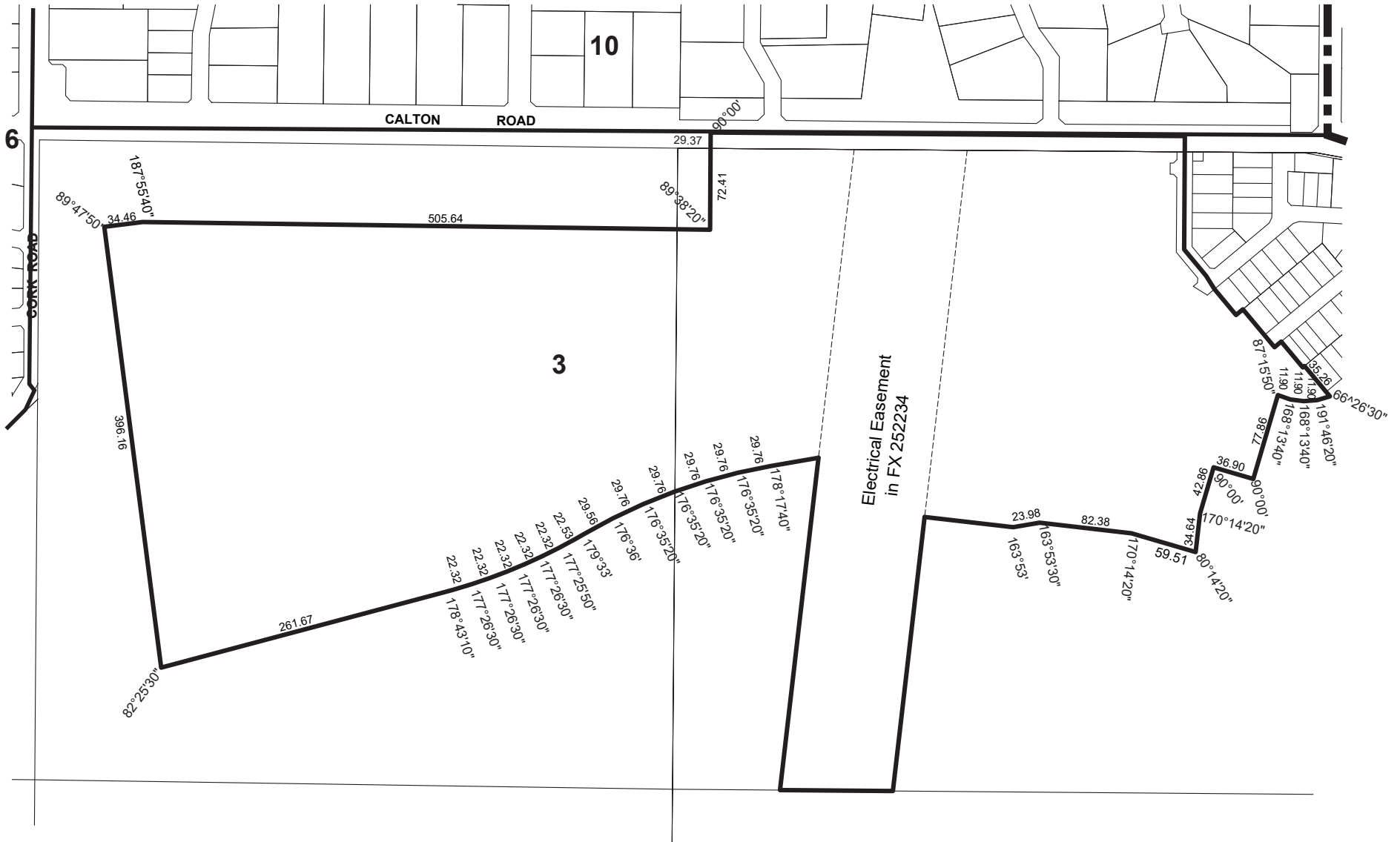
 Policy Area Boundary
 Development Plan Boundary



- 4
- Policy Area Boundary
- - - Development Plan Boundary

GAWLER (CT) POLICY AREAS MAP Ga/22

Consolidated - 18 July 2019



ENLARGEMENT A



- 3 Mixed Use Centre
- 6 Gawler East Residential
- 10 Wheatsheaf Residential
- Policy Area Boundary
- Development Plan Boundary

GAWLER (CT) POLICY AREAS ENLARGEMENT MAP Ga/23 Consolidated - 18 July 2019